



A G E N D A

PLANNING COMMISSION

Connie Coleman-Lacadie • Don Daniels
Nancy Hudson-Echols • Robert Estrada
James Guerrero • Paul Wagemann
Christopher Webber

Regular Meeting

Wednesday, May 2, 2018

City Hall Council Chambers

6000 Main Street SW, Lakewood, Washington

- 1. Call to Order**
- 2. Roll Call**
- 3. Approval of Minutes from April 18, 2018**
- 4. Agenda Update**
- 5. Public Comments**
(Each person will be allowed 3 minutes to speak, to a total of 15 minutes per topic. Groups with a designated speaker may have a total of 10 minutes to speak.)
- 6. Unfinished Business**
 - Staff introduction of Downtown Development Code, Comprehensive Plan Amendments
- 7. Public Hearings**
 - Downtown Subarea Plan, Planned Action Ordinance, Downtown Development Code and Comprehensive Plan Amendments
- 8. New Business**
 - Final Draft Review of 6 Year Transportation Improvement Program (2019-2024)
- 9. Report from Council Liaison**
 - Mr. Mike Brandstetter
- 10. Reports from Commission Members & Staff**
 - Written Communications
 - Future Agenda Topics
 - Area-Wide Planning / Land Use Updates
 - Other

Enclosures

1. Draft Meeting Minutes from April 18, 2018
2. Staff Report Downtown Subarea Plan and Associated Documents Update with Appendices A-I
3. Staff Report Final Draft Review of 6 Year Transportation Improvement Program (2019-2024)

Members Only

Please email kdevereaux@cityoflakewood.us or call Karen Devereaux at 253.983.7767 no later than Tuesday at noon, May 1, 2018 if you are unable to attend. Thank you.



**PLANNING COMMISSION
REGULAR MEETING MINUTES
April 18, 2018
City Hall Council Chambers
6000 Main Street SW
Lakewood, WA 98499**

Call to Order

Mr. Don Daniels, Chair, called the meeting to order at 6:30 p.m.

Roll Call

Planning Commission Members Present: Don Daniels, Chair; Robert Estrada, Vice - Chair; Connie Coleman-Lacadie, Nancy Hudson-Echols, James Guerrero, Christopher Webber; and Paul Wagemann

Planning Commission Members Excused: None

Planning Commission Members Absent: None

Staff Present: Tiffany Speir, Special Projects Planning Manager, Community Development; Courtney Brunell, Long Range Planning Manager; Community Development; Andrea Bell, Associate Planner, Community Development; and Karen Devereaux, Administrative Assistant

Council Liaison: Councilmember Mr. Michael Brandstetter

Approval of Minutes

The minutes of the meeting held on April 4, 2018 were approved by voice vote M/S/C Guerrero/Hudson-Echols. The motion to approve these minutes passed unanimously, 6-0. (Commissioner Mr. Paul Webber arrived at 6:39 p.m.)

Agenda Update

None

Public Comments

Mr. Glen Spieth, Lakewood resident, elaborated that it didn't make a lot of sense for Council to approve of the overlay project on 59th if the subarea plan intends to place a community park over the top of it. Mr. Spieth described 59th Avenue SW as currently being a straight shot into the Lakewood Towne Center and City Hall. Mr. Spieth requested the commissioners get ahead of the planning and leave the street intact and place the park elsewhere.

Public Hearings

None

Unfinished Business

None

New Business

Introduction to the Downtown Subarea Plan, Preferred Alternative Recommendation, Planned Action Ordinance, Comprehensive Plan Amendments and Form-Based Code

Ms. Tiffany Speir provided a detailed presentation explaining the four main parts of the downtown subarea plan that will be considered by Council. It was also explained that the DSAP geographic area encompasses 319 acres and includes 3 districts. Many key public investments and changes are being proposed. The draft downtown concept plan and future land use maps were discussed.

A summary of the draft DSAP Planned Action Environmental Impact Statement was provided as commissioners will be selecting one of the three alternatives to recommend for Council consideration. A draft SEPA Planned Action is being included to streamline future environmental review and permitting in the downtown area. If adopted, future projects in the Downtown will not require SEPA determinations at the time of permit application if they are consistent with the type of development, traffic assumptions, and mitigation measures studied in the PAEIS.

Ms. Speir requested commissioner's to continue to review of the packet contents as the next step is to hold a public hearing regarding the DSAP at their May 2nd meeting. A separate Developers Forum is being hosted by the City on April 26th. The input gathered at this event will be presented at a future date. The draft form-based code will be presented for discussion at the June 6th planning commission meeting.

Sign Code Amendments

Ms. Courtney Brunell, Planning Manager, and staff reviewed the survey completed by the planning commission to rate priorities and drafted a version of the sign code to launch discussion.

The commissioners were informed staff intends to implement a task force in early May. Commissioners suggested one of the target groups be International Business District property and business owners along South Tacoma Way, as well as utilizing the Chamber of Commerce to assist with outreach groups. Commissioners will participate in discussion again on June 6, where the task force will report back on input received prior to holding a public hearing.

The timeline is aggressive and is subject to change as follows:

May – June 2018 - Staff engages citizens in public outreach meetings

June 6, 2018 – Staff reports back results of public outreach efforts to-date

June 20, 2018 – Public Hearing (tentative)

July 18, 2018 – Decision on recommendation (tentative)

July 23, 2018 - City Council begins review (tentative)

Report from Council Liaison

Councilmember Mr. Mike Brandstetter updated the commissioners on the following Council actions:

Council received a briefing on the Department of Transportation receiving parts of the City roadways in the Tillicum area to complete infrastructure projects. Upon completion the ownership of those particular roadways will be returned to the City. One of the projects is installation of a traffic circle at Berkeley and Union Avenue; WSDOT has to own the property at the intersection before they can begin work on this site. The intersections and related parcels along Interstate 5 will be quit claimed for this purpose.

Council took action on the Woodbrook Hunt Club who recently proved that the two major structures, a clubhouse and a kennel, have historical significance and will be awarded historic designation which will provide them with grant monies to preserve the structures.

Council decided to accept the bid on the contract to repave 59th Avenue SW. This project has been on the 6 Year TIP for over two years and would repave in front of City Hall from the traffic circle to 100th St SW. The roadway currently has a paving index bus rating in the 70's and the slow moving, heavy traffic at this location is what typically quickly degrades the surface. If not done soon, the rating on the roadway will drop even further.

The City is gathering public input on exactly how to design Onyx Dr. in Oakbrook, from Oakbrook Park on 97th all the way down past the elementary school over to Phillips. This area receives large amounts of foot traffic and Council is concerned with the design standards of the whole project. The alternatives are being discussed over how much sidewalk, bike lanes and overall esthetics will be given to the streetscapes.

Council held a retreat to discuss goals and priorities for the next three years. A few of elements that surfaced as priorities were land use type items. These include dealing with the clear zone area via a land swap, working with the school district regarding a property in Woodbrook so allowing more land to be developed for industrial use with a focus on job enhancement, as well as progress toward relocating the WSDOT Maintenance yard from its Pacific Highway location, which in turn could be a catalyst for future private development in that area along the SR512 and I-5 orientation.

Reports from Commission Members and Staff

City Council Actions

No updates from staff at this time.

Written Communications

None

Future Agenda Topics

Ms. Tiffany Speir reminded commissioners of the tentative schedules as follows: May 2nd topics are a Public Hearing on the Downtown Subarea Plan and Final Draft Review of 6 Year TIP; May 16th will be review and deliberation on the public hearing

comments; and June 6th the commission will take action on the Downtown Subarea Plan; as well as hold a status discussion on the Sign Code Amendments.

Area-Wide Planning / Land Use Updates

None

Other

None

Next Meeting: Wednesday, May 2, 2018 at 6:30 p.m. in Council Chambers

Meeting Adjourned at 8:41 p.m.

Don Daniels, Chair
Planning Commission 05/02/2018

Karen Devereaux, Recording Secretary
Planning Commission 05/02/2018



TO: Planning Commission

FROM: Tiffany Speir, Planning Manager, Special Projects

DATE: May 2, 2018

SUBJECT: Proposed Downtown Subarea Plan (DSAP) and Associated Documents Update

I. Background: On April 18, the Planning Commission began review and consideration of the proposed Lakewood Downtown Subarea Plan (DSAP) and associated Planned Action Environmental Impact Statement (PAEIS.) These were two of the four pieces of the DSAP “packet” for the Commission’s consideration; the third and fourth pieces - the Downtown Development Code (DDC) and Comprehensive Plan Map and Text amendments - are discussed herein.

Attached to this Staff Report are the:

- Summary of the Spring 2018 Subarea Plan public survey responses (**Appendix A**);
- Summary of the March 21, 2018 public meeting (**Appendix B**);
- Summary of the public comments received electronically to date (**Appendix C**);
- Draft Downtown Development Code (DDC) (**Appendix D**);
- Draft Colonial District Design proposal (**Appendix E**); and
- Draft Comprehensive Plan Map and Text amendments to fully implement the Lakewood Downtown Subarea Plan (**Appendix F**).

Also attached to this Staff Report, for your review prior to your May 16 meeting, are the:

- Updates to the Downtown Subarea Plan (DSAP) made based on the April 18 study session discussion and direction provided by the Commission to staff (**Appendix G**);
- Updated Planned Action Ordinance (PAO) Discussion Guide (**Appendix H**); and
- Updated Planned Action Ordinance (PAO) including the PAEIS mitigation requirements and what a “Modified Alternative 1” could look like based on the April 18 study session discussion (**Appendix I**).

II. Downtown Development Code (DDC)

The area within the Downtown study area is currently regulated primarily by Lakewood Municipal Code Chapter 18A.30, Zoning Districts, and 18A.50, Development Standards. Section 18A.50.200 includes the community design standards applicable to commercial,

mixed-use, and multi-family development. The proposed new Downtown Development Code (DDC) will be housed in a new section of the Lakewood Municipal Code at Chapter 18A.35, and will regulate all development and land use in the Downtown Subarea. LMC Chapter 18A.35 will supersede the standards in Chapter 18A.50 for all development within Downtown subarea boundary unless incorporated by reference.

The proposed DDC is a “hybrid form-based code” (combining form-based code elements with traditional zoning.) Form-based codes address the “look” more than allowed uses of development: the relationship between building facades and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. The regulations and standards in form-based codes are presented in both words and clearly drawn diagrams and other visuals. They are keyed to a Regulating Plan that designates the appropriate form and scale (and therefore, character) of development, rather than only distinctions in land-use types.

By their nature, form-based codes are often very detailed in terms of streetscape design and development frontages. This makes them well suited to smaller targeted areas. Over time, various hybrid codes have been developed for unique local conditions that combine form-based code elements with traditional zoning.¹ This is the recommended approach here.

The draft DDC includes the following sections:

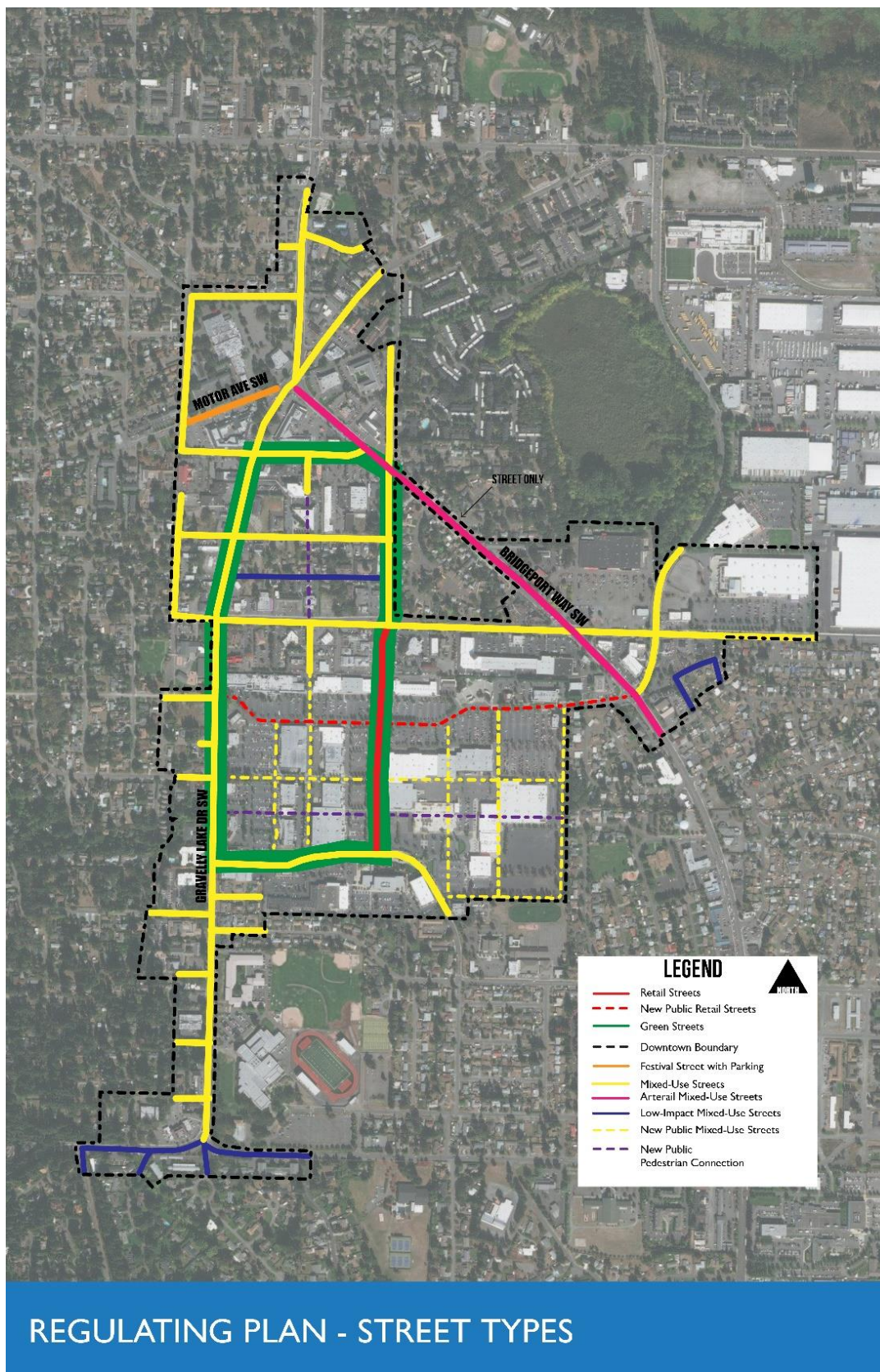
- 18A.35.100 Downtown District
- 18A.35.200 Land Use and Zoning
- 18A.35.300 Streets and Blocks
- 18A.35.400 Site Design, Buildings, and Frontage
- 18A.35.500 Landscaping, Open Space, and Green Infrastructure
- 18A.35.600 Parking
- 18A.35.700 Administration

The Downtown Regulating Plan translates the community vision into a map. The Regulating Plan designates the locations, sub-districts, and streets that are intended to embody specific physical characteristics. It specifies the location and applicability of specific design treatments and maps where they are required. The Regulating Plan works in tandem with the development standards, tables, and figures to define the shape, size, and location of streets, through connections, infill blocks, buildings, and landscaping.

Included below is the draft Downtown Subarea Regulating Plan graphic.

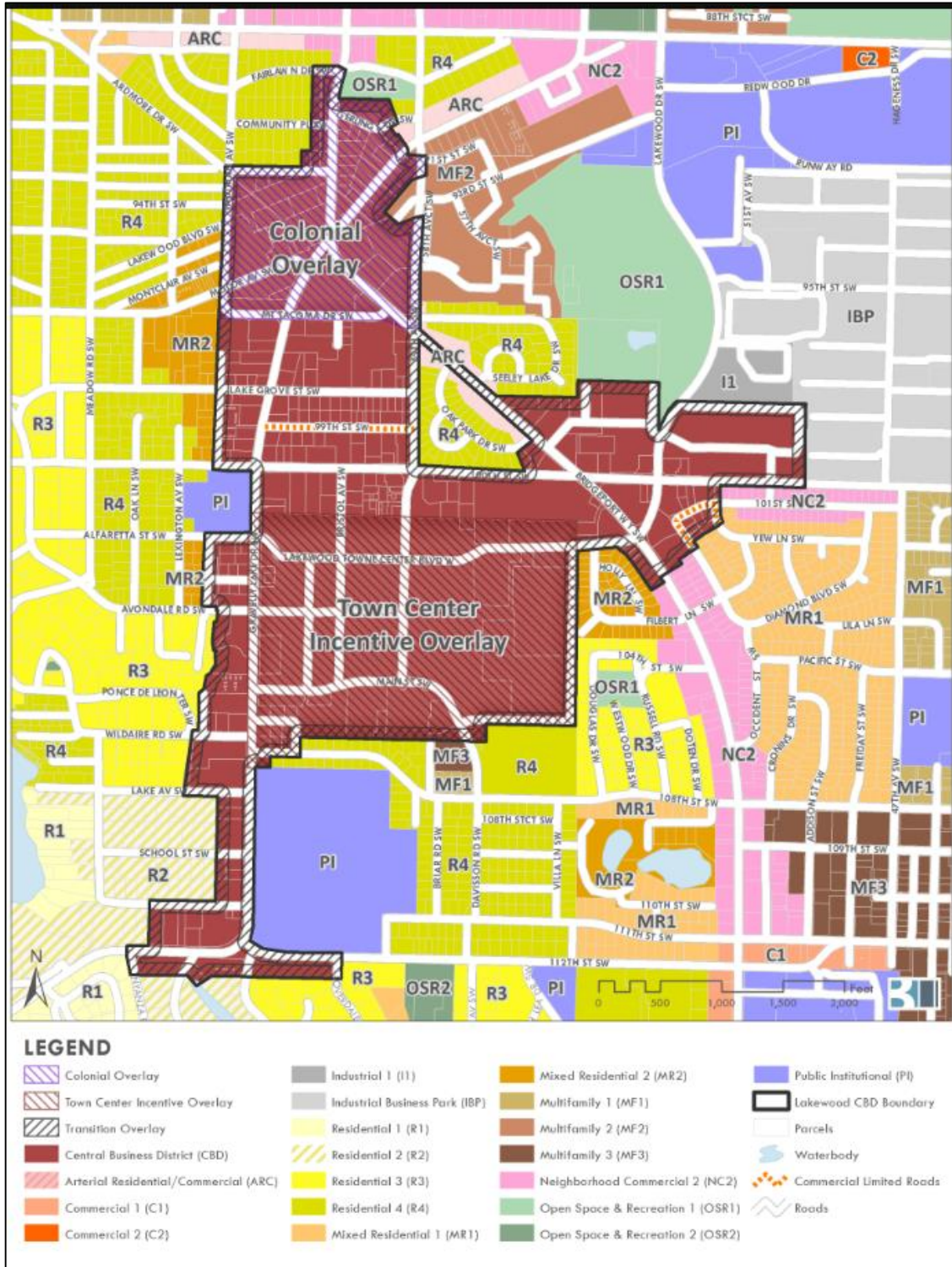
¹ Source: Form-Based Codes Institute, 2018; MRSC, 2012

18A.35.120 -1. Regulating Plan



Two zoning options are included in the draft DDC; the Planning Commission is requested to recommend one of the options for Council consideration.

Under Option 1, one “base zone” (Central Business District Zone) and several zoning overlays (Colonial, Town Center Incentive, and Transition overlays) would be applied within the Downtown District, as shown in the map below. Descriptions of the zone and overlays are at pages 6-7 of the DDC.



LEGEND

Colonial Overlay	Industrial 1 (I1)	Neighborhood Commercial 1 (NC1)	Residential 3 (R3)
Town Center Incentive Overlay	Industrial Business Park (IBP)	Neighborhood Commercial 2 (NC2)	Residential 4 (R4)
Transition Overlay	Mixed Residential 1 (MR1)	Open Space & Recreation 1 (OSR1)	Lakewood CBD Boundary
Arterial Residential/Commercial (ARC)	Mixed Residential 2 (MR2)	Open Space & Recreation 2 (OSR2)	Parcels
Commercial 1 (C1)	Multifamily 1 (MF1)	Public Institutional (PI)	Waterbody
Commercial 2 (C2)	Multifamily 2 (MF2)	Residential 1 (R1)	Commercial Limited Roads
Central Business District (CBD)	Multifamily 3 (MF3)	Residential 2 (R2)	Roads

The DSAP, PAO, DDC and Comprehensive Plan amendments were presented to a developers focus group on April 26, 2018 for feedback; any information gathered from that meeting will be presented to the Planning Commission on May 2 prior to the public hearing.

III. Downtown Development Code (DDC) Outline

Included below is a brief outline of what each section of the proposed DDC addresses.

18A.35.100 Downtown District

This section lays out foundational purposes and frameworks for the code.

18A.35.110 Vision and Guiding Principles

See Subarea Plan for Draft Vision and Guiding Principles

18A.35.120 The Regulating Plan

The Regulating Plan translates the community vision into a map. The Regulating Plan designates the locations, subdistricts and streets that are intended to embody specific physical characteristics. It specifies the location and applicability of specific design treatments and maps where they are required. The Regulating Plan works in tandem with the development standards, tables and figures to define the shape, size, and location of streets, through connections, infill blocks, buildings, and landscaping.

This regulating plan is similar to maps included in the Draft Subarea Plan and Draft Environmental Impact Statement except that it provides a street category for each street following discussions with an inter-departmental staff team.

18A.35.130 Definitions

18A.35.140 Relationship to Other Regulations

18A.35.200 Land Use and Zoning

Under either zoning map option, this section would also include a land use table (focusing on allowing flexibility for residential, retail, and office uses while specifying prohibited uses).

18A.35.300 Streets and Blocks

A. Blocks

B. Street Types

Graphics: Street Sections – included in Draft Plan and EIS

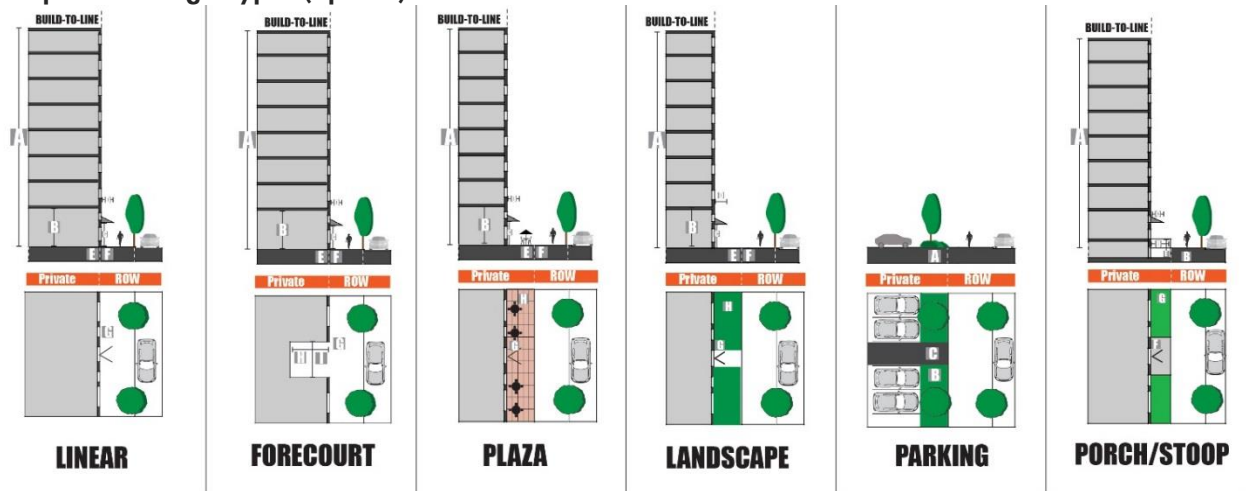
1. Gravelly Lake Drive
2. 59th Avenue
3. Lakewood Towne Center Boulevard
4. Motor Avenue
5. Mixed Use Streets (typical)

18A.35.400 Site Design, Buildings, and Frontage

A. Frontage Types

1. Linear/Retail: Meaning built to the street edge
2. Forecourt: Meaning some areas are at street edge and others are not (e.g. porch).
3. Plaza/Outdoor Dining/Seating
4. Landscape
5. Porch/Stoop/Terrae
6. Parking/Landscape
7. Civic Frontages (text)

Graphic: Frontage Types (up to 5)



LAKEWOOD
DOWNTOWN PLAN

FRONTAGE TYPES

Example Frontage Standards – vary by Frontage Type

Standard	CBD	Retail Street	Mixed-Use Street	Mixed-Use Arterial	Low-Impact Mixed-Use Streets	Festival Street
A Building Height						
B First Floor Minimum Height						
C Weather Protection Height						
D Weather Protection Minimum Depth and Linear Frontage						
E Building Maximum Setback from Planned Streets						
F Building Setback Minimum						
G Front Entrance						
H Window Transparency						

Note that heights reviewed in the Plan and EIS to date included 40 feet at transition areas and 90 feet elsewhere (per current CBD zone), with most likely height for mixed use at around 65 feet. Densities reviewed to date include 80-100 dwelling units per acre in place of 54 units per acre today. Since a form-based code addresses design and height, unlimited densities could be allowed and limited by these other parameters. That may be appropriate to considered in some areas for optimal implementation of the plan.

B. Building Types

Graphics: Building types

18A.35.500 Landscaping, Open Space, and Green Infrastructure

Addresses onsite landscaping, pedestrian, connections, and common open space (for multifamily uses).

18A.35.600 Parking

Includes reduced standards for selected residential and commercial uses.

18A.35.700 Administration

18A.35.710 Form-Based Code Review

Area-specific submittal requirements.

A. Pre-Application Conference

B. Review Thresholds

C. Concept Plan Review

D. Final Plan Review

E. Amendments to Approved Plans

18A.35.720 Master Planned Development – Town Center Incentive Overlay

Master plan standards. By going through a master plan process density incentives, additional uses are allowed.

18A.35.730 Appeals

18A.35.740 Update of Nonconforming Lots

This section will address proportional compliance with standards as development occurs – e.g. small changes to sites would comply with relevant standards (e.g. if changing windows meet transparency requirements or awning requirements, but not necessarily get kicked into full compliance unless making more substantial site changes or changes of use). With larger redevelopments or changes, full standards would apply.

IV. Lakewood Comprehensive Plan Amendments

To maintain consistency between the Comprehensive Plan and Downtown Plan, which will be considered a subarea plan and element of the Comprehensive Plan, some map, text and policy changes are proposed.

Currently, amendments to the following sections and maps are proposed in the attached document at Appendix E to reflect the adoption of the DSAP by the City:

- Updating Land Use Designations (2.3.6: Renaming “Central Business District” as “Downtown”)
- Updating Table 2.1 Comprehensive Plan Designation by Density and Housing Type
- Updating Section 2.4.1 Urban Center
- Updating Figure 2.1 Future Land Use Map to include the Downtown
- Replacing Figure 2.2 Lakewood Urban Center map with the Downtown map
- Updating Section 3.2.6 Housing Capacity
- Updating Table 3.2 City of Lakewood: Housing Unit Capacity
- Updating policies within Section 3.3.1 and 3.3.2
- Updating policies within Section 3.5
- Updating Table 4.2: Civic Boulevards.

- Updating Table 4.3: Key Pedestrian Routes.
- Updating Section 4.5.1
- Replacing old map of Central Business District Urban Design Framework with Figure 4.2 Downtown Plan Concept

Additional amendments to the Comprehensive Plan may become necessary as the Downtown Plan proceeds through the legislative process.

V. Upcoming 2018 Public Outreach and Legislative Meeting Schedule (*anticipated*):

- May 16, Planning Commission Hearings and Review/Deliberation on DSAP, PAO, Downtown Development Code (DDC), and Comprehensive Plan amendments;
- June 6, Planning Commission Action on DSAP, PAO, DDC, and Comprehensive Plan amendments;
- June 25, City Council Study Session on DSAP, PAO, DDC, and Comprehensive Plan amendments;
- July 2, City Council Public Hearing on DSAP, PAO, DDC, and Comprehensive Plan amendments; and
- July 16, City Council action on DSAP, PAO, DDC, and Comprehensive Plan amendments

VI. Attachments

- **Appendix A**, Summary of the Spring 2018 Subarea Plan public survey responses
- **Appendix B**, Summary of the March 21, 2018 public meeting
- **Appendix C**, Summary of the public comments received electronically to date
- **Appendix D**, Draft Downtown Development Code (DDC)
- **Appendix E**, Draft Colonial District Design proposal
- **Appendix F**, Draft Comprehensive Plan Map and Text amendments to fully implement the Lakewood Downtown Subarea Plan
- **Appendix G**, Updates to the Downtown Subarea Plan (DSAP) made based on the April 18 study session discussion and direction provided by the Commission to staff
- **Appendix H**, Updated Planned Action Ordinance (PAO) Discussion Guide
- **Appendix I**, Updated Planned Action Ordinance (PAO) including the PAEIS mitigation requirements and what a “Modified Alternative 1” could look like based on the April 18 study session discussion

Draft Downtown Plan Survey

Draft Results | April 25, 2018

Introduction

From March 21 to April 16, 2018, the City of Lakewood conducted a Draft Downtown Plan Survey. The link to the online survey was posted at the project website (<https://www.lakewooddowntownplan.org/>), and with the weekly City Manager’s report on March 23, 2018. The link was also provided through the City’s social media posts.

Approximately 191 persons viewed the survey; the maximum number responding to any question is 179. Interest in some of the questions varied with greater interest on the Vision and less on prioritization of improvements or on policies. Still for key questions over 100 interested persons commented on various issues, adding richness to the other outreach efforts of the City regarding the Draft Downtown Plan, such as the Draft Downtown Plan Open House and Workshop on March 21, 2018. Key areas of interest included housing and parking.

The survey results are summarized by major plan topics, with open-ended comments in the Appendix.

Introduction	1
Vision Statement	1
Downtown Concepts	5
How does the Downtown plan help small businesses and create jobs?	10
How does the Downtown Plan Offer Quality Housing Choices?.....	12
How does the Downtown Plan create parks and other fun places for the community?	14
How does the Downtown Plan promote transportation; including biking, walking, busing, and driving?	17
How does the Plan help the community and businesses work together?.....	20
Appendix.....	22

Vision Statement

Based on feedback in fall 2017, the Draft Downtown Plan includes a proposed vision that is a basis for policies and actions. The Downtown Vision Statement is compatible with the City’s Comprehensive Plan Vision that promotes a vibrant downtown.

Proposed Vision Statement

Our VISION FOR DOWNTOWN is that it is seen as the “heart” of Lakewood. Downtown is where people go to do fun things, see friends and neighbors, eat good food, and experience the cultural diversity of the City. Downtown brings a strong sense of pride for the community by celebrating all things Lakewood and bringing a strong sense of identity to the City and its people. Downtown is best experienced by walking or biking and is safe, inviting, and connected. The Downtown has a mix of retail, restaurant, employment, and housing options that are well-designed and support civic life and a strong economy.

Downtown is:

- A GREAT PLACE!
- The HEART of the COMMUNITY and CIVIC LIFE
- Designed for PEOPLE to WALK and BIKE SAFE and INVITING
- Where people of all ages go to do FUN things Rich with CULTURAL DIVERSITY
- SUSTAINABLE and connected to NATURE Part of a thriving LOCAL ECONOMY
- A source of PRIDE and IDENTITY for LAKEWOOD Where people LIVE, WORK, MEET, SHOP, and EAT

Q1. What do you like about the Vision Statement? (n = 179)

Focus Oriented Love Streets Statement Concise Idea
 Ideal Place Downtown Fast Food
 Safe and Inviting Sustainability
 Community Broad Lakewood Words
 Sounds Gathering Place Walk Business
 Thriving Local Economy Emphasizes Positive
 Green Goal Vibrant Cultural Inclusive

1. Lakewood	11.17%	20
2. Community	10.61%	19
3. Sounds	10.06%	18
4. Safe and Inviting	8.94%	16
5. Walk	8.38%	15
6. Downtown	6.15%	11
7. Thriving Local Economy	5.59%	10
8. Idea	5.03%	9
9. Positive	4.47%	8
10. Statement	4.47%	8
11. Goal	3.91%	7
12. Love	2.79%	5
13. Cultural	2.23%	4

14. Focus	2.23%	4
15. Inclusive	2.23%	4
16. Broad	1.68%	3
17. Sustainability	1.68%	3
18. Words	1.68%	3
19. Fast Food	1.12%	2
20. Gathering Place	1.12%	2
21. Ideal Place	1.12%	2
22. Business	1.12%	2
23. Concise	1.12%	2
24. Emphasizes	1.12%	2
25. Streets	1.12%	2
26. Green	1.12%	2
27. Oriented	1.12%	2
28. Vibrant	1.12%	2

- Commenters liked Vision Statement elements such as “safe and inviting” and “thriving local economy” as well as references to walkability, etc. Please see open-ended comments in the Appendix.

Q2. What would you change? (n = 162)

Caps Homeless Points Focus Plan Meet Great Place
 Fast Food Chains Sounds Realistic Center Shorten
 Parking safe Lakewood Idea
 Downtown Traffic Walking Grocery Vision
 Citizens Feels True Heart Important Dining

1. Lakewood	10%	17
2. Parking	9%	15
3. Downtown	7%	11
4. Center	6%	9
5. Walking	6%	9
6. Sounds	6%	9
7. Vision	4%	7
8. Great Place	4%	6
9. Feels	4%	6
10. Plan	4%	6
11. Heart	3%	5
12. Points	3%	5
13. Dining	3%	5
14. Caps	3%	5
15. Safe	2%	4
16. Shorten	2%	4
17. Idea	2%	4
18. Realistic	2%	4
19. Traffic	2%	3
20. Fast Food		
Chains	1%	2
21. Grocery	1%	2
22. Meet	1%	2
23. Citizens	1%	2
24. Focus	1%	2
25. True	1%	2
26. Homeless	1%	2
27. Important	1%	2

In terms of what respondents would change in the Vision (or in Downtown), comments addressed parking, making a center, and improving walking. Please see open-ended comments in the Appendix.

Q3. What's missing? (n = 148)

Fun Outdoor Downtown Covered Homeless Upscale
 Businesses Specifics Center Cultural
 Restaurants Far Think Public Art
 Lakewood Vision Park Library Walking
 Police Place Addressed Community Design Plan Pets
 Families Final Bullet

Lakewood	9%	14
Think	7%	11
Park	7%	10
Restaurants	6%	9
Walking	5%	8
Center	5%	8
Place	5%	7
Businesses	5%	7
Community	5%	7
Homeless	3%	5
Plan	3%	5
Downtown	3%	4
Families	2%	3
Fun	2%	3
Final Bullet	1%	2
Public Art	1%	2
Far	1%	2
Vision	1%	2
Cultural	1%	2
Library	1%	2
Specifics	1%	2
Police	1%	2
Upscale	1%	2
Addressed	1%	2
Covered	1%	2
Design	1%	2
Outdoor	1%	2
Pets	1%	2

In terms of what's missing in the Vision Statement (or in Downtown today), respondents indicated, parks, restaurants, and ease of walking. Please see open-ended comments in the Appendix.

Downtown Concepts

Based on insights from extensive public outreach in fall 2017 and a planning and design team based on best practices, a concept plan for Downtown has been developed. Following are highlights from the concept plan:

- **Green Street Loop:** To address the lack of park space, improve public streets, and improve circulation for pedestrians and bicyclists the green loop will include park like elements, green infrastructure, and support redevelopment in Downtown.
- **New Public Streets:** The Downtown will have new public streets to improve urban development, circulation, and an active public realm.
- **Central Park:** A new urban park of between two to four acres is proposed just north of City Hall to serve as the main gathering space for the community and to include a variety of features and programming.
- **Modified Gravelly Lake Drive:** As part of the Green Street Loop, a revised road design for Gravelly Lake Drive SW is proposed. The revised road design will allow for expanded sidewalks and a multi-use path on the east side of the street.
- **Motor Avenue Festival Street:** The City intends to move forward with creating a festival street along Motor Avenue consistent with the adopted concept plan. The plan includes a large central plaza, a pedestrian promenade, a farmer's market and event structure, street trees, landscaping, and public art opportunities.

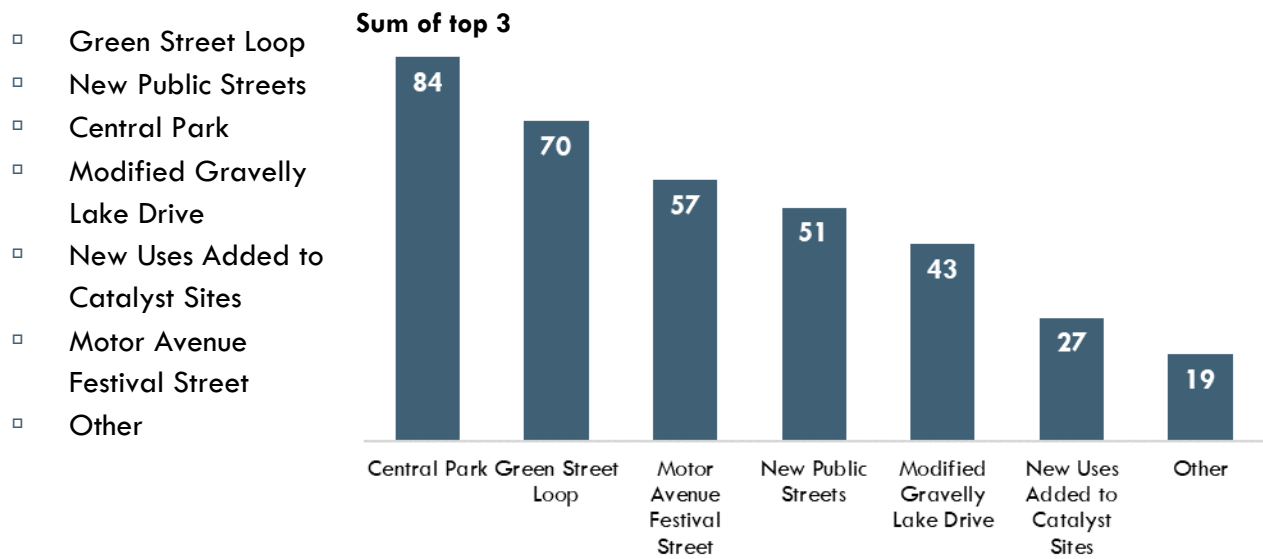
Q4. What do you like about the Downtown Plan Concepts? (n = 116)

Hard to Understand Connected Downtown Inviting Map
 Box Lake Town Center Street Motor Avenue
 Space City Hall Park Sounds Great Green
 Money Plan Improvement Lakewood Shopping Walk
 Pedestrian Needed Community

1. Park	40%	46	14. City Hall	2%	2
2. Space	15%	17	15. Motor Avenue	2%	2
3. Green	14%	16	16. Sounds Great	2%	2
4. Street	14%	16	17. Town Center	2%	2
5. Plan	9%	11	18. Money	2%	2
6. Lake	9%	10	19. Box	2%	2
7. Lakewood	8%	9	20. Improvement	2%	2
8. Map	7%	8	21. Inviting	2%	2
9. Walk	5%	6	22. Shopping	2%	2
10. Downtown	3%	4	23. Connected	2%	2
11. Needed	3%	4	24. Pedestrian	2%	2
12. Hard to Understand	3%	3			
13. Community	3%	3			

- Likes about the Downtown Concept focused on parks, green space, and streets. Please see open-ended comments in the Appendix.

Q5. What would be your top priority for the following investments? Rank in order of priority (1= top choice). (n = 124)



- Based on a sum of which features were ranked in the top three, priorities included the Central Park, Green Street Loop, and Motor Avenue Festival Street, all allowing for recreation and gathering spaces.

Q6. Tell us about your top 2 priorities... (n = 108)



1. Streets	20%	22
2. Central Park	17%	18
3. Needs	13%	14
4. Lakewood	12%	13
5. Think	10%	11
6. Sidewalks	9%	10
7. Gravelly Lake	8%	9
8. Business	8%	9
9. Priority	8%	9
10. Motor Ave	7%	8
11. Motor Avenue	6%	7
12. Green Space	5%	5
13. Gathering	5%	5
14. Love the	4%	4

Idea		
15. School	4%	4
16. Homeless Population	3%	3
17. Colonial Center	3%	3
18. Address	3%	3
19. Safety	3%	3
20. Law	2%	2
Enforcement		
21. Steilacoom Park	2%	2
22. Congested	2%	2
23. Walkers	2%	2
24. Foot	2%	2
25. Fun	2%	2
26. Paved	2%	2
27. Tie	2%	2

- While top ranked priorities were parks, many comments focused on streets, sidewalks, and related improvements.

Please also see open-ended comments in the Appendix.

Q7. Do you see some challenges with these Downtown Plan Concepts? (n = 110)

Hard Urban Safety Understand Taxes Course Challenges
Land Business Costs Traffic Answer
Downtown Road Construction Park Clear
Lakewood Retail Space Money Services
Homeless Question Mall Unless Concept Funding

1. Park	12.73%	14	15. Road Construction	2.73%	3
2. Downtown	10.91%	12	16. Answer	2.73%	3
3. Lakewood	9.09%	10	17. Clear	2.73%	3
4. Traffic	9.09%	10	18. Costs	2.73%	3
5. Money	7.27%	8	19. Retail Space	1.82%	2
6. Business	5.45%	6	20. Land	1.82%	2
7. Homeless	5.45%	6	21. Services	1.82%	2
8. Challenges	5.45%	6	22. Course	1.82%	2
9. Mall	4.55%	5	23. Question	1.82%	2
10. Taxes	4.55%	5	24. Understand	1.82%	2
11. Concept	3.64%	4	25. Unless	1.82%	2
12. Safety	3.64%	4	26. Urban	1.82%	2
13. Funding	3.64%	4			
14. Hard	3.64%	4			

- Respondents noted challenge with implementing parks, addressing traffic, finding funds, supporting businesses, addressing homelessness and other topics.

Please see open-ended comments in the Appendix.

Q8. What other ideas do you have to make Downtown great? (n = 105)

Community Hard Public Seeley Lake Housing
Local Businesses Downtown Options Look
Empty Buildings Stores Money Lakewood
Retail Park Traffic Shop Vacant Buildings
Homeless Safety Families Motor Ave Street Function
Love Quality Safe

1. Park	13.33%	14	16. Money	4.76%	5
2. Lakewood	13.33%	14	17. Traffic	3.81%	4
3. Shop	11.43%	12	18. Empty Buildings	2.86%	3
4. Stores	8.57%	9	19. Vacant Buildings	2.86%	3
5. Homeless	8.57%	9	20. Options	2.86%	3
6. Look	8.57%	9	21. Safety	2.86%	3
7. Families	7.62%	8	22. Local Businesses	1.90%	2
8. Downtown	7.62%	8	23. Motor Ave	1.90%	2
9. Street	6.67%	7	24. Seeley Lake	1.90%	2
10. Housing	6.67%	7	25. Function	1.90%	2
11. Love	5.71%	6	26. Hard	1.90%	2
12. Public	5.71%	6	27. Quality	1.90%	2
13. Safe	5.71%	6			
14. Community	5.71%	6			
15. Retail	4.76%	5			

- Respondents provided ideas addressing parks, shops and stores, homelessness, and attracting families.

Please see open-ended comments in the Appendix.

How does the Downtown plan help small businesses and create jobs?

Q9. Some draft policies - broad statements about what the Draft Downtown Plan wants to achieve - are listed below. Tell us what you think. (n = 123)

Policy	Sum of Strong Agree + Agree	Strongly Agree	Agree	Disagree	Strongly Disagree	Unsure/ No opinion
<ul style="list-style-type: none"> Policy: Develop downtown as a destination for retail, office, public services, cultural activities (art, culture, and entertainment), urban residential, and civic facilities of Lakewood. 	117	68	49	2	2	2
<ul style="list-style-type: none"> Policy: Ensure Downtown is home to a wide spectrum of business that reflect the area's most competitive and desired industries. 	107	44	63	7	1	8
<ul style="list-style-type: none"> Policy: Prioritize and market "catalyst sites" (sites that combine public improvements in infrastructure and amenities with private infill and redevelopment) identified through this Plan for mixed-use development. 	96	41	55	7	3	15

- All policies received some level of agreement – with a range of 96 to 117 out of 123 supporting all three policies.
- The policy with the greatest support was developing a downtown as a destination for many diverse jobs, cultural activities, entertainment, residential, and civic facilities.

Please see open-ended comments in the Appendix.

Q10. What are your ideas for strategies to attract businesses and keep businesses in Downtown Lakewood? (n = 102)

Some draft strategies the City could take to achieve the policies include: incentives to encourage new businesses to locate in Downtown, providing information and other resources to entrepreneurs and small businesses, encouraging businesses to work together to make downtown Lakewood clean and safe, investing in civic amenities and infrastructure, working with local financial institutions to help provide loans to qualified businesses, and others. What are your ideas for strategies to attract businesses and keep businesses in Downtown Lakewood?

Access Rid Plan Flow Homeless Chain Rent Quality
 Clean Gravelly Small Businesses
 Public Safety Lakewood Military Families Tax
 Increase Attract Housing Stores Survey
 New Business Easier Think Growing
 Towne Center Ruston Allow Fix

1. Tax	17%	17	15. Fix	3%	3
2. Lakewood	15%	15	16. Military	2%	2
3. Attract	13%	13	Families		
4. Small	10%	10	17. Public	2%	2
Businesses			Safety		
5. Stores	8%	8	18. Increase	2%	2
6. Clean	7%	7	19. Gravelly	2%	2
7. New	6%	6	20. Housing	2%	2
Business			21. Quality	2%	2
8. Rent	6%	6	22. Survey	2%	2
9. Think	6%	6	23. Chain	2%	2
10. Homeless	6%	6	24. Easier	2%	2
11. Towne	5%	5	25. Flow	2%	2
Center			26. Growing	2%	2
12. Plan	4%	4	27. Rid	2%	2
13. Allow	4%	4	28. Ruston	2%	2
14. Access	3%	3			

- Lowering taxes and providing incentives for new and small businesses were recommended.

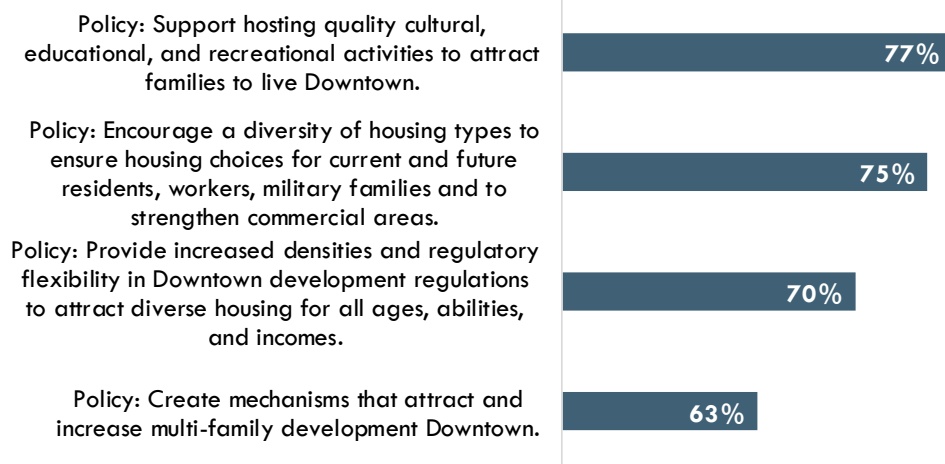
Please see open-ended comments in the Appendix.

How does the Downtown Plan Offer Quality Housing Choices?

Q11. The draft Plan proposes policies about achieving high quality housing in Downtown - tell us what you think. (n = 120)

Policy	Sum of Strong Agree + Agree	Strongly Agree	Agree	Disagree	Strongly Disagree	Unsure/No opinion
<ul style="list-style-type: none"> Policy: Support hosting quality cultural, educational, and recreational activities to attract families to live Downtown. 	92	53	39	21	6	1
<ul style="list-style-type: none"> Policy: Encourage a diversity of housing types to ensure housing choices for current and future residents, workers, military families and to strengthen commercial areas. 	90	52	38	16	7	7
<ul style="list-style-type: none"> Policy: Provide increased densities and regulatory flexibility in Downtown development regulations to attract diverse housing for all ages, abilities, and incomes. 	84	42	42	18	13	5
<ul style="list-style-type: none"> Policy: Create mechanisms that attract and increase multi-family development Downtown. 	76	37	39	23	13	7

Percent that strongly agree and agree



- Around three quarters of respondents indicated support for policies to **attract more residents to live Downtown** with options that include housing choices for current and new residents and to **encourage a diversity of housing types**.
- About two-thirds of respondents agreed with **providing increased densities, regulatory flexibility, and mechanisms to increase diverse housing and multi-family housing** Downtown.

Please see open-ended comments in the Appendix.

Q12. What are your ideas for strategies to include high quality housing in Downtown Lakewood? (n = 85)

Strategies to achieve the intent of the policies include: adopting new zoning and design standards, ensuring that landscaping and heights transition well from the center to abutting neighborhoods, encouraging housing and service providers to address the needs of the homeless, finding opportunities for transitional housing to address health and shelter needs of the homeless, and ensuring all developments are designed to help neighbors get to know each other and to have eyes on the street to create a sense of place and improve safety. What are your ideas for strategies to include high quality housing in Downtown Lakewood?

Town Trees Clean Kinda High Quality Housing Unsure Retail
Folks Attract Condos Apartments Existing
Downtown Parking Homeless Stop Family
Design Standards Transitional Housing Lake
Neighborhoods Incentives Ideas Point Drug Street

1. Homeless	21%	18	14. Town	5%	4
2. Downtown	14%	12	15. Parking	5%	4
3. Family	8%	7	16. Existing	4%	3
4. Apartments	8%	7	17. Stop	4%	3
5. Transitional Housing	7%	6	18. Condos	4%	3
6. Attract	7%	6	19. Design Standards	2%	2
7. Neighborhoods	7%	6	20. Folks	2%	2
8. Retail	7%	6	21. Lake	2%	2
9. Ideas	7%	6	22. Unsure	2%	2
10. High Quality Housing	5%	4	23. Incentives	2%	2
11. Drug	5%	4	24. Kinda	2%	2
12. Clean	5%	4	25. Point	2%	2
13. Street	5%	4	26. Trees	2%	2

- Homelessness is a focal point for addressing housing in Downtown. It is a high-priority topic for Downtown to be considered safe and attractive.

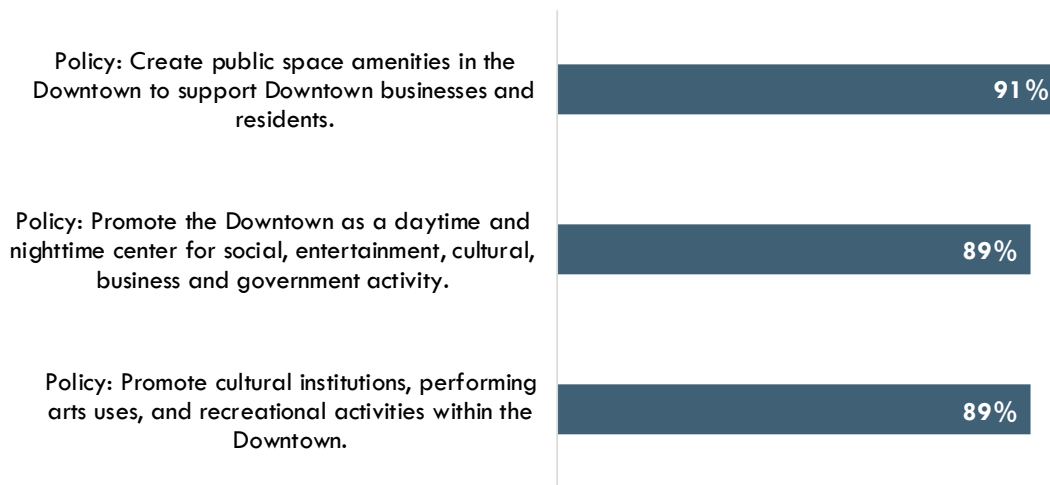
Please see open-ended comments in the Appendix.

How does the Downtown Plan create parks and other fun places for the community?

Q13. What do you think of these draft policies to help create parks and fun places? (n = 112)

Policy	Sum of Strong Agree + Agree	Strongly Agree	Agree	Disagree	Strongly Disagree	Unsure/No opinion
<ul style="list-style-type: none"> Policy: Create public space amenities in the Downtown to support Downtown businesses and residents. 	102	60	42	5	2	3
<ul style="list-style-type: none"> Policy: Promote the Downtown as a daytime and nighttime center for social, entertainment, cultural, business and government activity. 	100	66	34	5	3	3
<ul style="list-style-type: none"> Policy: Promote cultural institutions, performing arts uses, and recreational activities within the Downtown. 	100	54	46	5	4	3

Percent that strongly agree and agree



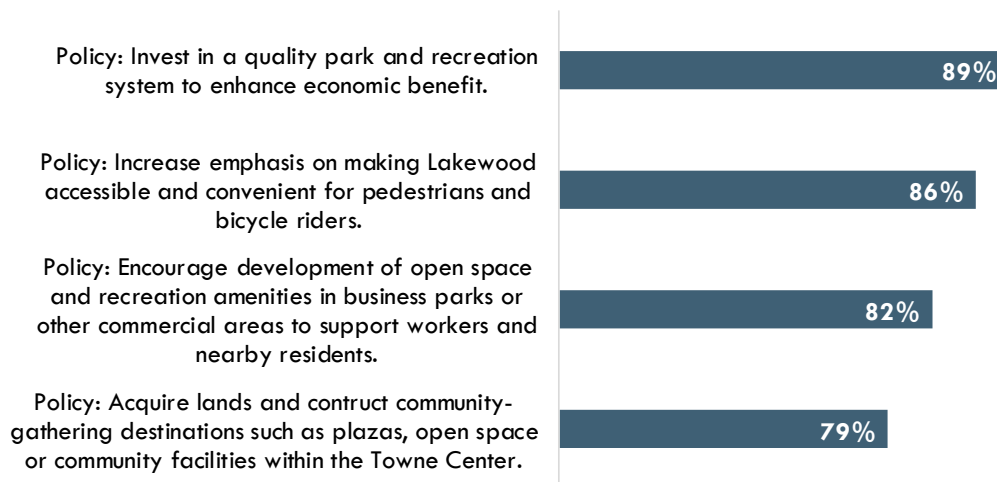
- A great majority of respondents support public space and activation in the Downtown area.

Please see open-ended comments in the Appendix.

Q14. What do you think of these draft policies to help create parks and other fun places?
(n = 114)

Policy	Sum of Strong Agree + Agree	Strongly Agree	Agree	Disagree	Strongly Disagree	Unsure/No opinion
■ Policy: Invest in a quality park and recreation system to enhance economic benefit.	101	57	44	7	3	2
■ Policy: Increase emphasis on making Lakewood accessible and convenient for pedestrians and bicycle riders.	98	62	36	5	6	3
■ Policy: Encourage development of open space and recreation amenities in business parks or other commercial areas to support workers and nearby residents.	94	48	46	8	4	6
■ Policy: Acquire lands and construct community-gathering destinations such as plazas, open space or community facilities within the Towne Center.	90	55	35	13	6	2

Percent that strongly agree and agree



- There is strong support for investing in the parks and recreation system in Lakewood. There is slightly less support for acquiring lands for constructing new open spaces, yet still nearly 80% of support.

Please see open-ended comments in the Appendix.

Q15. What are your ideas for strategies for parks and other fun places in Downtown? (n = 73)

Strategies to achieve the intent of the policies include: Programming and hosting events (e.g., farmers market, parades, holiday festivals or Oktoberfest) in Downtown public spaces, exploring grants and other funding opportunities to help create parks, allowing development to reduce their onsite open space obligations and support development of parks in downtown, designating a cultural district to celebrate art and attract funding, and creating streetscapes and trails that link the Downtown area to parks and recreational facilities outside of Downtown. What are your ideas for strategies for parks and other fun places in Downtown?

Families Homeless Kids Earlier Biking Existing Steilacoom Safety
 Places POOL Community Spray Park Ideas Access
 Lakewood Motor Ave Downtown Wi-Fi Events
 Basketball Farmers Market Concerts Business Grass Think
 Consider

1. Lakewood	16%	12	14. Families	4%	3
2. Ideas	12%	9	15. Access	4%	3
3. Downtown	11%	8	16. Spray Park	3%	2
4. Community	11%	8	17. Motor Ave	3%	2
5. Events	11%	8	18. POOL	3%	2
6. Places	10%	7	19. Wi-Fi	3%	2
7. Farmers Market	7%	5	20. Safety	3%	2
8. Steilacoom	7%	5	21. Basketball	3%	2
9. Business	5%	4	22. Existing	3%	2
10. Biking	5%	4	23. Concerts	3%	2
11. Think	5%	4	24. Earlier	3%	2
12. Kids	4%	3	25. Grass	3%	2
13. Consider	4%	3	26. Homeless	3%	2

- Placemaking ideas and strategies included using public places for events that are available to families, children, and accessible through transportation options that includes biking.

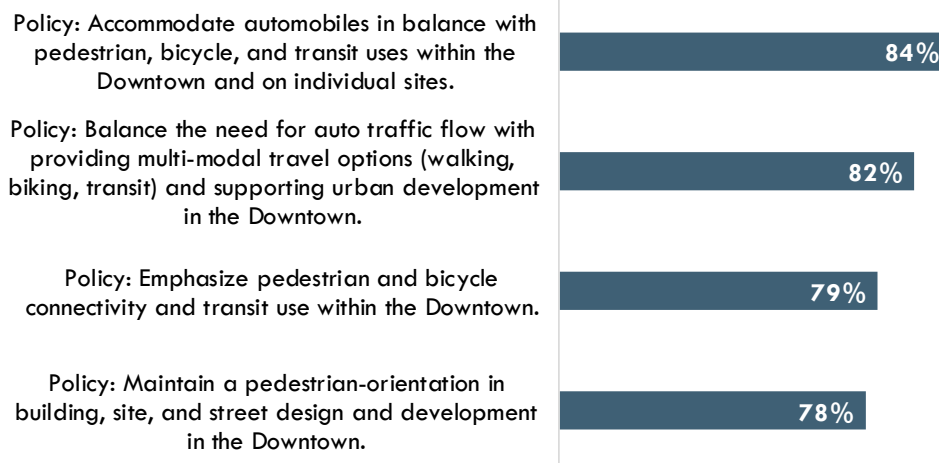
Please see open-ended comments in the Appendix.

How does the Downtown Plan promote transportation; including biking, walking, busing, and driving?

Q16. What do you think of these draft policies to help promote transportation? (n = 109)

Policy	Sum of Strong Agree + Agree	Strongly Agree	Agree	Disagree	Strongly Disagree	Unsure/No opinion
<ul style="list-style-type: none"> Policy: Accommodate automobiles in balance with pedestrian, bicycle, and transit uses within the Downtown and on individual sites. 	92	53	39	8	3	3
<ul style="list-style-type: none"> Policy: Balance the need for auto traffic flow with providing multi-modal travel options (walking, biking, transit) and supporting urban development in the Downtown. 	89	47	42	12	3	5
<ul style="list-style-type: none"> Policy: Emphasize pedestrian and bicycle connectivity and transit use within the Downtown. 	86	48	38	12	5	6
<ul style="list-style-type: none"> Policy: Maintain a pedestrian-orientation in building, site, and street design and development in the Downtown. 	85	54	31	8	5	10

Percent that strongly agree and agree



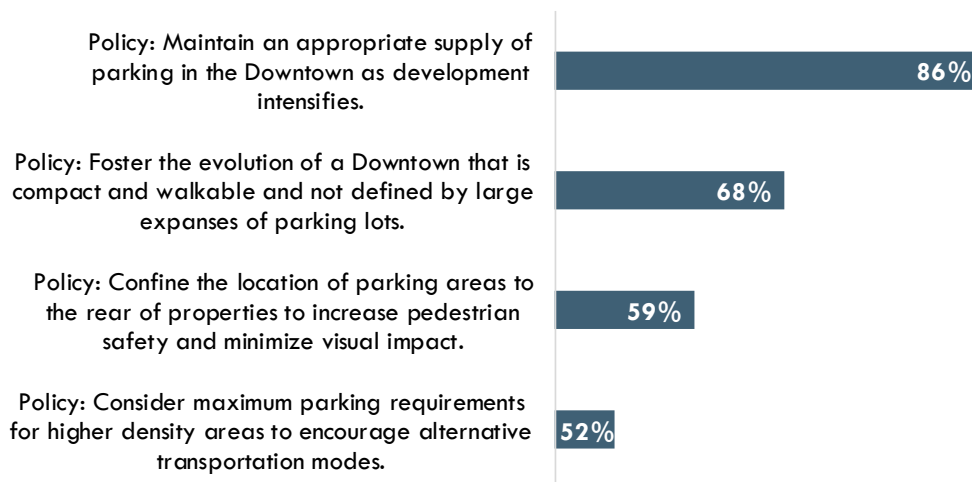
- Survey respondents agreed most with policies that accommodate transportation options in balance, including support for multiple options of travel modes.

Please see open-ended comments in the Appendix.

Q17. What do you think of these draft policies to manage parking? (n = 108)

Policy	Sum of Strong Agree + Agree	Strongly Agree	Agree	Disagree	Strongly Disagree	Unsure/No opinion
<ul style="list-style-type: none"> Policy: Maintain an appropriate supply of parking in the Downtown as development intensifies. 	93	59	34	7	3	3
<ul style="list-style-type: none"> Policy: Foster the evolution of a Downtown that is compact and walkable and not defined by large expanses of parking lots. 	73	46	27	17	7	10
<ul style="list-style-type: none"> Policy: Confine the location of parking areas to the rear of properties to increase pedestrian safety and minimize visual impact. 	64	36	28	21	13	8
<ul style="list-style-type: none"> Policy: Consider maximum parking requirements for higher density areas to encourage alternative transportation modes. 	56	25	31	23	18	10

Percent that strongly agree and agree



- Respondents agree most with policies to maintain an appropriate parking supply as Downtown develops. There is large agreement for policies to promote a compact and walkable Downtown, however there is less support for maximum parking requirements.

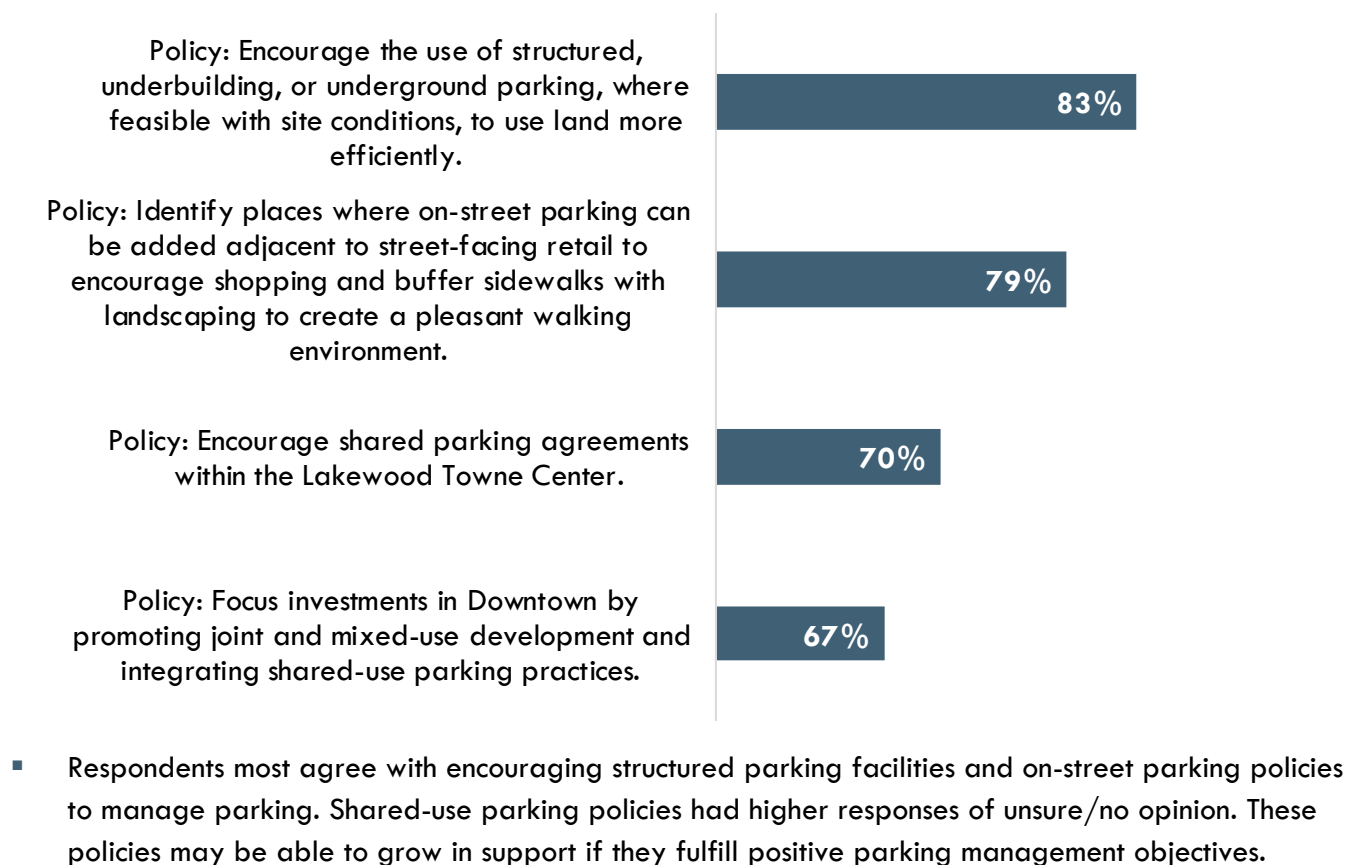
Given there is support for multiple modes of travel per Question 16, the balance of parking with other modes will need to be carefully considered.

Please see open-ended comments in the Appendix.

Q18. What do you think of these draft policies to manage parking? (n = 108)

Policy	Sum of Strong Agree + Agree	Strongly Agree	Agree	Disagree	Strongly Disagree	Unsure/No opinion
<ul style="list-style-type: none"> Policy: Encourage the use of structured, underbuilding, or underground parking, where feasible with site conditions, to use land more efficiently. 	90	55	35	11	5	2
<ul style="list-style-type: none"> Policy: Identify places where on-street parking can be added adjacent to street-facing retail to encourage shopping and buffer sidewalks with landscaping to create a pleasant walking environment. 	85	35	50	10	7	6
<ul style="list-style-type: none"> Policy: Encourage shared parking agreements within the Lakewood Towne Center. 	76	45	31	6	4	22
<ul style="list-style-type: none"> Policy: Focus investments in Downtown by promoting joint and mixed-use development and integrating shared-use parking practices. 	72	37	35	6	5	23

Percent that strongly agree and agree



Please see open-ended comments in the Appendix.

How does the Plan help the community and businesses work together?

Q19. What do you think of these draft policies to support partnerships? (n = 105)

Policy	Sum of Strong Agree + Agree	Strongly Agree	Agree	Disagree	Strongly Disagree	Unsure/No opinion
<ul style="list-style-type: none"> Policy: Focus on the revitalization of the Downtown through partnerships among the City, business and property owners, and the community; develop an organization whose primary function is to support implementation of this Plan. 	92	44	48	2	4	4
<ul style="list-style-type: none"> Policy: Support the formation of a Lakewood Towne Center association or similar organization to establish economic improvement strategies and to sponsor social and safety events. 	87	40	47	6	4	6
<ul style="list-style-type: none"> Policy: Support formation of business improvement organizations. 	87	32	55	5	4	7

Percent that strongly agree and agree



- Respondents agree with a partnership of organizations whose primary function is to support implementation of the Downtown Plan. There is slightly less support for policies to promote more general associations or organizations.

Please see open-ended comments in the Appendix.

Q20. Is there anything else you'd like to share with the City? (n = 54)

Survey Design Plan Civic Groups Schools Input Going
Homeless Lakewood City Hall Community Basically
Businesses Sidewalks Improvements Towne Center

1. Lakewood	35%	19
2. Going	19%	10
3. Community	15%	8
4. Schools	9%	5
5. Businesses	9%	5
6. Plan	7%	4
7. Improvements	7%	4
8. Survey	7%	4
9. Towne Center	6%	3
10. Homeless	6%	3
11. Input	6%	3
12. City Hall	4%	2
13. Civic Groups	4%	2
14. Basically	4%	2
15. Design	4%	2
16. Sidewalks	4%	2

- Comments varied and promoted local business formation, strong schools, community participation, addressing homelessness, and others.

Please see open-ended comments in the Appendix.

Appendix

Open Ended Question Responses

Q1. What do you like about the Vision Statement?.....	23
Q2. What would you change?.....	27
Q3. What's missing?	32
Q4. What do you like about the Downtown Plan Concepts?	36
Q6. Tell us about your top 2 priorities.....	38
Q7. Do you see some challenges with these Downtown Plan Concepts?	43
Q8. What other ideas do you have to make Downtown great?	46
Q9. Some draft policies - broad statements about what the Draft Downtown Plan wants to achieve - are listed below. Tell us what you think.	49
Q10. Some draft strategies the City could take to achieve the policies include: incentives to encourage new businesses to locate in Downtown, providing information and other resources to entrepreneurs and small businesses, encouraging businesses to work together to make downtown Lakewood clean and safe, investing in civic amenities and infrastructure, working with local financial institutions to help provide loans to qualified businesses, and others. What are your ideas for strategies to attract businesses and keep businesses in Downtown Lakewood?.....	50
Q11. The Draft Plan proposes policies about achieving high quality housing in Downtown - tell us what you think:.....	53
Q12. Strategies to achieve the intent of the policies include: adopting new zoning and design standards, ensuring that landscaping and heights transition well from the center to abutting neighborhoods, encouraging housing and service providers to address the needs of the homeless, finding opportunities for transitional housing to address health and shelter needs of the homeless, and ensuring all developments are designed to help neighbors get to know each other and to have eyes on the street to create a sense of place and improve safety. What are your ideas for strategies to include high quality housing in Downtown Lakewood?	56
Q13. What do you think of these draft policies to help create parks and fun places? (Part I)	59
Q14. What do you think of these draft policies to help create parks and other fun places? (Part II)	60
Q15. Strategies to achieve the intent of the policies include: Programming and hosting events (e.g., farmers market, parades, holiday festivals or Octoberfest) in Downtown public spaces, exploring grants and other funding opportunities to help create parks, allowing development to reduce their onsite open space obligations and support development of parks in downtown, designating a cultural district to celebrate art and attract funding, and creating streetscapes and trails that link the Downtown area to parks and recreational facilities outside of Downtown. What are your ideas for strategies for parks and other fun places in Downtown?	61
Q16. The amount and quality of public space are two defining features of Downtowns. Streets are	

the primary public spaces in Downtowns, in some cases accounting for almost half the land depending on the size and layout of the street grid. Lakewood currently lacks an urban street grid typical of a Downtown or the types of active public spaces that attract people to come Downtown. The proposed street concepts support improvements to existing streets, the expansion of the public street network, the green street loop, a better pedestrian experience and connectivity, and urban mixed-use infill development. What do you think of these draft policies to help promote transportation?63

Q17. As centers like Downtown become more successful, attract more jobs, residents, and customers, the management of parking becomes key. Downtowns may have more public parking, on-street parking, and shared parking options that tend to be located either behind or to the side of buildings or in parking structures. In addition, downtowns typically have a greater level of parking management such as time limits, parking pricing, permits, and other management strategies to ensure that parking is being used efficiently. What do you think of these draft policies to manage parking? (Part I).....65

Q18. What do you think of these draft policies to manage parking? (Part II).....67

Q19. What do you think of these draft policies to support partnerships?67

Q20. Is there anything else you'd like to share with the City?72

Note: Open-needed responses appear below by question, unedited for spelling or grammar.

Q1. What do you like about the Vision Statement?

1	Not a lot. There really isn't a "downtown" in Lakewood. Concerned that the proposed idea will just become a place where the homeless and druggies hang out and it become unsafe for anyone to use. It's not the same as places in Seattle, where many people travel on foot because of mass transportation. Not many people will walk to this area, but will drive and then the question of where they are going to park arises.
2	It's great, but I would add "accessible." It's not just the right thing to do, it's the law. Plus, diversity isn't just ethnic background.
3	It is in progress
4	I like everything about the vision statement...theory and practice...but, unless and until Lakewood recognizes that cohesive design is essential, it's just going to be a hodgepodge mess...
5	Agree
6	Safe and inviting
7	it's inclusive of a lot of things!
8	Not much.
9	Inclusive of all
10	Appreciate the focus on both cultural diversity and sustainability.
11	It hits all the appropriate hooah verbiage.
12	Nor much. Need indoor activity center, rains most of the time. How about a walking track & a running track.
13	The inclusion of all aspects a community should have in a central location, especially housing.
14	part of a thriving local economy
15	The aims sound good, not enough specifics to agree to it.
16	"Safe and Inviting". I would like to shop, eat, walk feeling safe doing the latter activities.
17	Safe, good restaurants. Not just fast food chains
18	Sounds pretty, but no substance
19	Leaves virtually nothing out and is making Lakewood seem to have it all
20	It has EVERY element one could possibly think of
21	I think they are the right priorities
22	Very little. It's not government's job.
23	Making Lakewood SAFE!
24	Sounds like an ambitious goal.
25	Part of a thriving local economy.
26	I SOUNDS LIKE A SAFE HAPPY PLACE. NOW WE NEED SAFE, HAPPY PEOPLE. MAYBE THIS WILL BRING THEM OUT.

27	Though cumbersome, the statement includes many valuable variables and allows an anchoring to decision making.
28	Fine
29	Ok as long as not attractive for homeless to gather
30	Good core
31	Great goals, it's optimistic
32	The vision wants to bring a sense of identity and community to Lakewood
33	It is inclusive and positive
34	Its fine.
35	sustainability and biking
36	I like it
37	I love it
38	I love it.
39	The capital words in the bullet list make it look like you're trying too hard. It should sell itself without so much manipulation. In the paragraph, it's not "warm" enough - the sentences sound strange starting with "Downtown", and it puts too much pressure on the literal word/place "Downtown" (that and one time it's referred to as "The Downtown" and other times as "Downtown").
40	Nothing. It isn't a statement, its a catch-all narrative in an attempt to appeal to everyone. Too long
41	Covers all the bases
42	Safe and inviting
43	Sounds like a pipe dream
44	Safe and Inviting. Designed for People to walk and Bike. Part of a thriving local economy.
45	nice ideals
46	To long
47	I like each of the highlighted points as focus areas.
48	I agree with the vision of what downtown should be
49	Providing a centrally located communities gathering place.
50	It's main emphasis is on giving Lakewood a place that is clearly the "living room" of the city. Right now we live in a house with a bedroom and a garage.
51	That it emphasizes ped/bike safety
52	A source of pride, safe and inviting, where people live, work, meet, shop, and eat
53	It's positive and good. It would be nice to develop a slogan for Lakewood. Not any of the phrases above.
54	active
55	Dreamy
56	Its good
57	the idea of it being designed for people to walk and bike, since there is a lot that's within walking distance for me but it's not currently pleasant to walk
58	it emphasizes a walkable/bikeable living space vs. automobile centric development
59	Streets being walkable and safe
60	I like the series of goals
61	Not bad.
62	It seems broad in scope.
63	Safe and inviting Part of a thriving local economy Where people go to do fun things
64	Part of Thriving local economy
65	It is a sweet thought but not very realistic.
66	Nothing
67	Good goals
68	I like that it's focused on bringing people together
69	Cultural Diversity and restaurants. We need another grocery store!!!!
70	Nothing
71	Safe and inviting Thriving local economy

72	where people live, work, meet, shop, and eat
73	appears comprehensive.....but it will depend on the draft plans uniting the ugly Villa Plaza area with the quintessential Lakewood Colonial Center.
74	I like the vision and it is something that Lakewood needs. This would be an ideal place for the farmers market, also.
75	The idea of having a downtown seems like a great idea. Walkable, a nice park, sounds great.
76	The intent of the idea
77	I like that it points out walking, since I don't drive and therefore walk everywhere in the neighborhood of the Towne Center.
78	I like it all!
79	I like how it mentions it being the "heart" of the community.
80	We need green spaces to set and people watch or just be out in the fresh air
81	Community coming together in a safe place.
82	Later
83	Good
84	Its pretty broad but sounds nice.
85	Designed for PEOPLE to WALK and BIKE
86	People oriented
87	good
88	Idealistic. The average person in the vicinity of town center is not a bike rider. Streets leading to the Center go through unsafe neighborhoods. Or else we are too old bike.
89	It embodies what I think of as a Downtown. It sounds like a fun place to be.
90	Use of warm words such as safe
91	it's ok
92	I like that it mentions the HEART of the community, and the statement "where people LIVE, WORK, MEET, SHOP AND EAT."
93	Sounds good
94	Community oriented.
95	Positive and inviting
96	Uh
97	safe, inviting, fun, thriving
98	I like the idea of a park. Where?
99	It's a good start on identifying Lakewood.
100	The first of things mentioned on the list is community. Creating an identity of Lakewood is vital in its growth. People want to be part of something bigger. I also like the mentioning of sustainability and nature. It shows lakewood has the right spectrum of thought on what the future is.
101	One cannot argue with the basic ideas here.
102	Sounds positive.
103	a great summary
104	Everything
105	The statement is ambitious and conveys a lot of energy. I'd want to visit there!
106	A GREAT PLACE!
	The HEART of the COMMUNITY and CIVIC LIFE
	Designed for PEOPLE to WALK
	SAFE and INVITING
	Where people of all ages go to do FUN things
	Part of a thriving LOCAL ECONOMY
	A source of PRIDE and IDENTITY for LAKEWOOD
	Where people LIVE, WORK, MEET, SHOP, and EAT
107	The mention of food as long as it's not more Red Robin, chick fil a, etc
108	very optimistic
109	Rich with Cultural Diversity, the heart of the community, Part of a thriving local economy.
110	Safe and inviting
111	My wife and I retired in 2004 and moved to Port St Lucie, Florida. The City Council there built a Civic Center

	exactly as you stated above. The developer declared bankruptcy. If you go to Google Earth and type in Port St Lucie Civic Center and go to street view you will notice the EMPTY parking lot and no one walking around. This is a city with temperatures in the 80s year around. This will not work here.
112	It demonstrates the many uses for a downtown area.
113	It paints a good picture of what would be an ideal place to live and utilize the city.
114	I like that it is trying to make a heart for downtown. I would love to see a heart everyone knows is the center of town.
115	I like it , it is very broad though
116	Well stated.
117	It is pretty inclusive as to the values and attributes for a vibrant downtown area.
118	Upbeat
119	It's refreshing to know that FINALLY Lakewood is willing to step up and bring a whole new and exciting look to the city! Bring back a restaurant(s) such as the former "Lakewood Terrace"; that was a dining experience!
120	Accurate
121	That is slated to be safe.
122	certainly complete
123	All of the above
124	seem to have touched on many of the ideas a resident would look for.
125	The lack of sidewalks as well as roadsides that are over run by vegetation, for example Meadow Road between Lakewood Blvd. and 95th, make walking a nightmare.
126	The focus on walkability, and on enhancing the community.
127	Not a lot.
128	it sounds nice, but fails to address current problems
129	I like that the city has ambitions to create a centralized business area that is a good revenue source and enjoyable to be at.
130	sounds great
131	The first four sentences are good because they don't entirely misrepresent the reality of how people use/interact with mall area.
132	The heart of the community and civic life.
133	Not much
134	Walkable, diverse, safe & inviting, sustainable and connected to nature
135	Add with w emphasis on LOCAL BUSINESS
136	sounds very update and culturally correct
137	multiple reasons for creating and using a downtown
138	Pride and Identity for Lakewood; where people live, work, meet, shop and eat.
139	It encompasses all that a downtown should offer.
140	It all looks good but I think we could do with less fast food dining in Lakewood in general.
141	SAFE and INVITING, Part of a thriving LOCAL ECONOMY, a source of PRIDE and IDENTITY for LAKEWOOD.
142	Very good
143	It is forward thinking in creating and maintaining economic growth.
144	It's a great idea, a great goal.
145	Concise and yet detailed.
146	It is well thought out and comprehensive and positive
147	The open concept, green areas and walk ability.
148	Makes it sound like a great place to be
149	I like that it is the heart of the community. That it is a source of pride Ann's that it is walkable and bike friendly. Also that it is mixed use.
150	The concept of a center that would be the 'beehive' of safe, vibrant, colorful and interesting activity.
151	This is exactly what I would love to see as a vision statement for our great city.
152	I like all elements. I really like the walking and biking.
153	It is positive and hopeful
154	A bit wordy —but the focus on community gathering space is key.
155	It is not very concise.
156	It's thorough, all the elements are good.
157	I like the goal of being Safe and Inviting and building the economy back up. I like the goal of walking and biking areas as well as being connected to Nature.
158	It offers things for everyone
159	I think washington in general, including Lakewood need to stop selling out to California development companies. They have destroyed our state since our current governor has taken his position. Washington used to be a beautiful place, now it sucks. I have lived here my whole life and now I want out. Lakewood sucks!

160	Progressive and nostalgic simultaneously
161	Nearly all vision statements seem to have a lot of buzz words but little of any substance. Right up there with "world peace", eradication of hunger, and a cure for the common cold. Unfortunately, you can't make a silk purse out of a sow's ear, and putting lipstick on a pig is an exercise in futility.
162	I like the feel of the statement and agree with what it says.
163	where people live work meet shop eat
164	Community, diversity, pride.
165	Very clear on what you want to do.
166	The goal for community.
167	Making a real community within a community
168	It's fun!
169	Appears it is already that way
170	Love it!
171	"Safe and inviting"
172	Certainly it's upbeat, but it sounds like an ad campaign for Disneyland.
173	A GREAT PLACE! The HEART of the COMMUNITY and CIVIC LIFE Designed for PEOPLE to WALK SAFE and INVITING Where people of all ages go to do FUN things Part of a thriving LOCAL ECONOMY A source of PRIDE and IDENTITY for LAKEWOOD Where people LIVE, WORK, MEET, SHOP, and EAT
174	I like how it all seems positive. And a clean beautiful place.
175	It sounds inviting and exciting
176	The description of Safe and Inviting are key. Designed to walk and bike are manifestation of Safe and Inviting. Cultural Diversity is a great statement that I would like to see brought out.
177	It makes Lakewood sound like community, not just a place to run errands
178	I like that it talks about cohesion and creating a centralized gathering place where people will come to experience a sense of community outside their individual neighborhoods.
179	Positive, family driven, like idea of having a true downtown

Q2. What would you change?

1	Not sure. I'm not a visionary but someone who sees what could be changed. But don't see how to improve or "tweak" this plan just yet.
2	Add accessible!
3	Just to continue to improve
4	Nothing
5	Focus on safety
6	Don't assume everyone walks and bikes. Access to close in parking is essential, so that downtown is accessible for all. Also need low income housing in the plan.
7	I would attempt to shorten it up, by making it more concise and powerful. Seems like it be 2-3 sentences instead of 4.
8	It should be more concise. There is no need to, basically, repeat the first paragraph with the bullet points.
9	Repetitive
10	The first sentence in the statement invalidates itself twice. The downtown either will be the heart of the city or it won't be. To make it seem like the heart sounds like it is not intended to be the heart of the city at all. Additionally, the quotation marks around the word heart work to invalidate the fact that it is the heart.
11	Get rid of the CAPS. Inside voice please.
12	The tone, it's too commercial.
13	I find the use of so many words in higher case offensive, like someone is shouting.

14	an inside place for seniors to walk, or sit and visit, nice restaurants
15	Really nothing.
16	No more discount stores or fast food chains
17	Get specific
18	Quit with the all caps. Inside voice please.
19	Shorten - hit main concepts not EVERY concept
	Add "play" to final bullet to attract families to a family area
20	I would shorten it so at least the major elements are somewhat memorable. I expected to see "kitchen sink" in there somewhere!
21	nothing
22	Ridiculous "pie in the sky" program designed by government to remake the world in its own image. Just because you call it town center doesn't make it so. Get your hands out of our pockets and provide what government should provide: police, parks, infrastructure support and stick to that and that alone.
	<ul style="list-style-type: none"> • A GREAT PLACE! Define "great" first. •The HEART of the COMMUNITY and CIVIC LIFE: the heart of the community is its people not some shopping center in the middle of town. •Designed for PEOPLE to WALK and BIKE: OK but must people are going to drive there. •SAFE and INVITING: parking lots are now not safe. Inviting primarily to go shopping. •Where people of all ages go to do FUN things: this what we have parks for. •Rich with CULTURAL DIVERSITY: stop with the politically correct BS. Neighborhoods like the international district are private and diverse. We don't need manufactured diversity....it already exists. •SUSTAINABLE and connected to NATURE: What? Buildings and parking lots? That's what we have parks for. •Part of a thriving LOCAL ECONOMY: That's private sector not government initiated. Want a thriving local economy? Get the hell out of our way with city regulations, restrictions and nonsense ordinances. •A source of PRIDE and IDENTITY for LAKEWOOD: Lakewood's identity doesn't depend upon some government ginned up "center". It already exists as a bedroom community. Stop trying to make it a cash cow by increasing the tax base for government spending and expansion. •Where people LIVE, WORK, MEET, SHOP, and EAT: over promise under deliver. Yes we live here regardless of what government does; we typically work elsewhere given that most of Lakewood is residential; we meet at church, social functions....we don't go to some "center" to meet.
23	More emphasis on getting rid of drugs and those who deal in drugs
24	I think your description is broad-based enough that it covers the basics.
25	Why are we building more apartments?
	Here is an idea, condemn the numerous rundown apartment complexes in Lakeview, Springbrook, Tillicum, Woodbrook area as a few examples. Who owns these slums? Why are they allowed to continue to exist? Havens for crime, over use of emergency service...etc, etc, all of which we long term property owners get the pleasure of paying for in ever increasing taxes for services we seldom if ever use. We're tired of it!
26	We can't have a safe happy place, if we have to dodge cars all the time. keep the traffic on the outside.
27	To long for a vision statement.
28	<p>The statement that the center seem the "heart" apologizes for itself twice. The downtown either is the heart of the city or it is not. To suggest it just seems so feels manipulative. I have no idea why you would place the word in quotes--</p> <p>Some writers put quotes around words they want to distance themselves from. Quotation marks used this way are commonly called scare quotes or shudder quotes. It's a way of implying that you're using a term in an unusual way or that you don't necessarily approve of it:</p> <p>This article was written by a "professional" writer. (Grammarly)</p> <p>Additionally, the sentence on being best experienced by walking or biking is awkward</p>
29	Where do u Park yr car
30	Nothing
31	Remove the great place comment-it's redundant, and the last bullet...superfluous.

32	I would love to have a Trader Joe's or a healthier grocery store
33	nothing
34	I still think we should have a Miss Lakewood pageant.
35	Nothing
36	Nothing. Sound lovely
37	-Either decide on "Downtown" or "The Downtown"
	-I'd remove the strong sense of pride and/or identity - in the way it's currently worded (and even the idea of it), it feels like it's creating pressure on me to enjoy/love/promote/FEEL pride in it. Almost like it's another task to add to my list. When nowadays people have so many things to do, Downtown should be a calming, fun, relaxing, obligation-free place to go.
38	Make it simple. Eliminate contradictions-None of the options "connect" to nature. Lakewood's current vision/goals parrot Joni Mitchell-"Pave paradise and put up a parking lot."
39	I wouldn't put words in all caps
40	I would address the homeless issue as not 1 thing is being done about it
41	Bring back the Lakewood Ice Arena.
42	reality to meet the ideals
43	Take half of it out
44	It is too long and complicated. Reduce it down to one sentence for maximum impact. Something like: "Downtown Lakewood will be a cultural hub of community and connection."
45	Nothing
46	I believe it is a well rounded statement.
47	The main town center, even with the central park alternative still assumes Lakewood Town Center will be largely big box retail with a sea of surface parking. The apartments east of city hall are great. I just don't think the likes of Burlington Coat Factory are going to be around for the life of this plan. If they are, great, but we should be thinking about more housing in here and less parking. Think about that part of Renton that used to be part of the Boeing factory.
48	It's a little long. Will there be a "tag line" or something?
49	nothing
50	Lakewood needs more businesses lining the streets with parking behind- like Red Robin. Think of classic downtowns with street parking, perhaps metered for revenue, with free offsite parking, lots of landscaping, flowers, trees, grass, decorative street lights and hanging flowers + street flower boxes. Think Michigan Avenue, Chicago. Think upscale.
51	University Village is a great example of what Lakewood should aspire to, copy it!
52	I hope you won't emphasize "fun" things or "identity" too much because other items in the statement are more important
53	Nothing, vision is still broad enough to incorporate a wide range of options while still achieving the goals
54	Don't know
55	I love the idea of it being connected to Nature, but I don't see that actually happening.
56	True reason behind it.
57	Nothing as of right now
58	A great place. I think that that statement is dependent upon the components of the city.
59	Not likely to be the heart of civic life. Civic life is and should be more dispersed.
60	Be more realistic. You would have to start over. Lakewood is too spread out with roads dissecting every strip mall. There isn't enough money for that so improve what we have.
61	Something's realistic.
62	Pick one statement
63	I would change the part about "best experienced by walking or biking" I have small children and trying to walk in from our house would be prohibitive. It is a necessity for me to be able to drive in with my kids until they are old enough to make the walk in a reasonable amount of time.
64	We need another grocery store! Safeway is entirely overwhelmed. Please do something with the empty colonial center. Please a new grocery store where the QVC was, preferably a Fred Meyer or even weird-o Sarrs! Or an ethnic Grocery!
65	Everything
66	nothing
67	make it safer and more restaurants
68	nothing at this point
69	Make sure there is ample parking for people who want to experience Downtown
70	Where's the connection to the local history? What was the land originally before it was the town center and even before it was the Villa Plaza? Perhaps your parks name and features should pay homage to that history? Is the historical society involved?

71	Nothing.
72	Nothing...
73	Nothing
74	Light up crosswalks.
75	Small parks
76	Nothing
77	Much. Later
78	Nada
79	Balance growth and opportunity, good stewardship of our natural environment and all citizens are heard
80	To Include parking and paths for disabled folks in wheelchairs or walkers.
81	Is housing realistic?
82	Forget biking. Clean up neighborhoods around center. They feel unsafe with furniture dumped by roads etc. kids walking around during school hours and they aren't going to the library! Which by the way has a parking lot that is not safe after dark
83	Too long.
84	Feels overly generic
85	can't think of any thing at this point in time
86	? nothing
87	Nothing, but it sounds like some high hopes.
88	Need to include parking. As someone who is handicap, parking is important.
89	Not sure
90	It all sounds good.
91	More police patrols on bicycles
92	A focus on the ability to invite new businesses and create competition and a better marketplace for consumers.
93	Nothing, assuming these are achievable
94	"eat good food" to possibly dining variety & "housing options", it sounds as if it is low income
95	Nothing
96	Get rid of the silly capitalizations, it looks like a brochure from someone selling "No Money Down" get-rich-quick schemes. And of course biking and walking are great, but please also allow for the fact that people may want to drive there.
97	Designed for PEOPLE to BIKE Nobody bikes, esp in the rain. It's a waste of valuable parking and city streets. Rich with CULTURAL DIVERSITY SUSTAINABLE and connected to NATURE
98	Not sure
99	Nothing.
100	Add reasons why it is all those things.
101	Do something to entice a "fine dining" facility so we wouldn't have to travel north for good dining.
102	nothing I like it.
103	I would also like it to say a place where people don't have to leave home to find the activities they love. Fine dining, Skating, Concerts, Crafts, Fairs, etc.
104	Needs water and park.
105	Nothing.
106	A bit too "green"
107	Better, and more of a selection of retail stores, such as the former Fredrick and Nelson or similar; also a Penney's store would be nice to bring back!
108	The first line "a great place" is nonspecific and doesn't sound professional
109	Unknown until I have more information. I just want to be sure that whatever happens won't hamper traffic.
110	nothing
111	Attract GOOD restaurants rather than fast food chains.
112	doesn't exemplify ease of getting around for seniors and disabled citizens.
113	More sidewalks and better vegetation removal on roadsides.
114	Nothings

115	This is not a vision statement-way too long. Almost a laundry list.
116	there is a huge lack of facts, fluff words sound nice, but do not create a plan
117	Although I like the ambition of the statement, I think it is unrealistic. I travel to all the major cities in the US. The place described in the statement does not and can not exist with the current design. There is no walking only area that would connect the businesses and access points. There is no "nature" as very little of the center is plantable or connected to a local travel belt. The travel through the area with cars everywhere is chaotic at best. Driving through the center is stressful because cars are turning in every direction possible. There is no flow.
118	need to know the associated costs first
119	<p>The sentence about "best experienced by biking and walking...." - you gotta take that out, it is ENTIRELY misrepresentative. Biking and walking around the mall - are you kidding me? There's no green dedicated bike lanes in the mall let alone on the surrounding streets like Gravelley Lake/59th/100th/108th/Bridgeport. There's 4 bike lockers at the transit center and 0 lockers in front of city hall. There's sidewalk but there's no distinct, landscaped walking path between Target and the transit center or the bus stops in front of the Keg and that's where the majority of peds are coming/going. The main intersection at Towne Center Blvd/59th isn't even striped or laddered. Walking through the mall feels like navigating through a sea of parking lots. And on top of that, where the mall area could be an attraction for the high school, library users, Cabrini kids - there's no after school activities for them except for the bookstore and eating. So, you gotta take out the sentence and the bullet point about being designed for walking and biking because that is totally false.</p> <p>Also, SUSTAINABLE and connected to NATURE?! Huh?! Is there a secret community garden in the mall that nobody knows about about? That's misrepresentative, take it out.</p> <p>The HEART of the community and CIVIC LIFE - and A source of PRIDE and IDENTITY for LAKEWOOD is also misrepresentative. Its ironic that the whole mission statement is trying to impress the term "Downtown" for what will always be the mall. In fact, there actually was more of a civic life and pride about the development when it actually was called The Lakewood Mall because there were live community auctions held in the food court, there used to be a annual spring/summer parade down Gravelley Lake and it used to line up and finish at the mall and it was a big festivity. Now, they moved Summerfest to Fort Steilacoom Park, there's no annual parade anymore - not near the mall at least. The farmers market feels fledgling. So I'm not really sure what is supposed to define Civic Life and Pride and Identity - I would take out one of those bullet points.</p> <p>Finally - WHAT is up with all the WEIRD awkward FEELING caps ALL over THE place?! I don't get what caps is trying to convey and how/why it is purposeful. And when you list bullet points - shouldn't there be something in the paragraph to preface why there is a bulleted list of items?</p> <p>Also, instead of saying "to be seen as the heart of downtown...." I would just say "to be the heart of downtown....."</p> <p>"To be seen" sounds utterly pretentious, like as if all the City cares about is its reputation" and there are already too many pretentious old money farts in Lakewood. Take it out, just be direct and honest.</p>
120	Not sure that the city center needs to be a recreational center.
121	It doesn't even say where the "downtown" is. It's a cut and paste piece of bland advertising
122	SAFE AT THE TOP
	Where people connect with LOCAL business
123	make it happen
124	Bike.
125	I would like to see a little upscale dining. No more fast food restaurants; i.e., hamburger fast food.
126	Encourage more sit down dining
127	I like bringing nature/park areas into the downtown area but am concerned the homeless will take it over.
128	Nothing
129	About the plan, nothing. I would caution that expectation about the plan's goals should be realistic.
130	Nothing, but presently it's not true
131	Nothing
132	Not so sure that the walking and biking is realistic in Lakewood
133	I would add more green areas to connect the different sections of the downtown core
134	I would change the list at the bottom to a single short sentence that encompasses the city's vision.
135	Empty buildings
136	Limit motor traffic. Make it more pedestrian-friendly, attractive, colorful for all age groups. (Look at some European and other examples.)
137	Nothing. These statements reflect how I feel about Lakewood.
138	nothing

139	It is not unique to Lakewood. I would add that it is a vision of the people by the people.
140	Tighten it.
141	I would shorten it drastically.
142	Nothing
143	My family has lived in Lakewood for over 30 years. We've seen lowlifes move in, crime going up, and cheap businesses crop up along Bridgeport and S. Tacoma Way. All these things need to go!
144	Nothing
145	Everything such as law enforcement, crappy strip mall stores that just has to be all outside, just like California. Remember we are Washington it rains alot. Another that can change is the school district. The school in Lakewood are horrible. Where does all the money go? Definitely not schools ,roads, or law enforcement..
146	No change to language itself but negative implications of the changes to the people and businesses in Lakewood leave me apprehensive.
147	The entire Lakewood Planning Commission.
148	Its WAY too long.
149	It seems more like a description than vision
150	Actually, nothing. I believe this to be a perfectly thought out plan.
151	Nothing
152	Better and or more security. That area is a high theft area.
153	N/A
154	Nothing
155	It doesn't seem to mention opportunities for businesses. Nor does it mention jobs development or the concern about taking care of its residents.
156	Rich with CULTURAL DIVERSITY SUSTAINABLE and connected to NATURE What does this even mean?
157	nothing jumps out to me.
158	Nothing
159	The fact that it is a great place should be last on the list. Everything stated above it should let you know it is a great place.
160	It kind of makes Lakewood sound like a housing development community rather than one that is diverse and different
161	I don't like the all caps in the bullet items. "A great place" doesn't tell me anything -- let's use examples of what "Great" means instead.
162	Adding something about increasing livability and affordability. (No one wants a great downtown if no one can afford it)

Q3. What's missing?

1	Security. Public transportation. Volume control of outdoor activities to be considerate of the surrounding neighborhoods.
2	Accessibility.
3	plan for youth
4	Acknowledgement of timeless, cohesive design...
5	More events
6	Parking and low income housing
7	I don't think anything is missing from what I can tell! you use the term "inviting" and I am wondering if welcoming might be a better term? warm, inviting, welcoming, hospitable, attractive, alluring, tempting, pleasing (synonyms for both words). I think we are going for welcoming as in hospitable vs tempting as in inviting.
8	Nothing
9	While I like the idea of accessibility by walking or biking, the statement feels to make no accommodation for people who are mobility impaired. An awful lot of our elderly folk and or disabled veterans do not seem to be included
10	A true "downtown"
11	Can't think of anything right now.
12	regular department stores--not discount stores! Places to sit, and inside walking. a NICE restaurant. Too many fast food restaurants and no quiet sit-down restaurants of a better quality.
13	Good restaurants. Clean places to gather..maybe outdoor cafes.
14	Concrete ideas

15	Your opinions are noted. But usually ignored.
16	Play in final bullet point
17	The word 'Play' could be added to the final bullet
18	nothing
19	What government should be doing: Police, parks and infrastructure.
20	More police to protect and in force the laws.
21	Link Rail?
22	Reality! City think tanks, again paid for with our tax dollars, shopping for every state and federal grant dollar possible, again, our tax dollars. To build something I'd be willing to bet, that most of Lakewood residences seldom if ever use.... Lets just keep building more sidewalks for the homeless and not focus on eliminating properties that have been eyesores and community problems for over 40 years. Clean up these old messes before you start creating an incremental mess with your city center pipe dream.
	<p>This town has always been partially filled with temporary residents, due to JBLM. It is a mixed blessing I get that. But trying to make Lakewood into the next Bellevue is not feasible due to the temporary residence's occupation. Some may retire here, but how many find true high end paying jobs here? Anyone making \$100k plus a year at Hopjacks?? We do not need another fast food restaurant, cheap retail outlet. We don't even have a true high quality dining establishment left in town.</p> <p>More access to buses, apartments, fast food restaurants, and cheap retail outlets are not going to fix what is now ailing Lakewood. They are part of what is causing it, we've attracted the problems. Perhaps by design, the City of Lakewood can grab more available grant money. The heck with what it really has caused, it's driving more jobs for city workers so we must be counting that as a win. Really??</p>
23	right now. most everything
24	There is no mention of accessibility for folks who are mobility challenged. I love the idea of walking and biking, but there are a lot of elderly and disabled people in Lakewood--many of whom are veterans. Failing to have intentional focus on their needs seems a problem
25	Don't know
26	The importance of community pride, downtown is not nature-centered, clean, or incredibly safe.
27	An upscale healthy grocery store.
28	We don't really have a "downtown" currently
29	The above
30	Everything is covered
31	I think you including everything
32	Above.
33	Commitment to sustain what little is left of Lakewood's origins-the old growth wooded areas, etc.
34	maybe the idea of attracting businesses or promoting Lakewood to new business opportunities could be added
35	Homelessness is not addressed.
36	A safe library.
37	a plan?
38	Nothing. It has too much.
39	Nothin
40	Nothing I can think of at this time.
41	I don't see a good low-stress bike network. Look at Clover Park HS, City hall/Central Park, Lakewood Transit Center (if that's the right place for it), the Villiage at Seely Lake and Clover Park Tech. Take those as nodes and connect them together with a low-stress bike network and create bik and ped permeable connections to the surrounding SF neighborhoods so you can access the low stress network backbone from the low-volume residential streets.
42	Transit
43	nothing
44	Public Art, murals, statues, all the above mentioned trees, green spaces, outside seating for restaurants 'al fresco' like Paris, even Point Ruston, if they can redevelop the former Asarco Plant, we can redevelop Lakewood! The University Village redevelopment in Seattle is also an excellent model.
45	A price tag
46	Family - no mention of families.
47	n/a
48	Typically a pedestrian/bike focused road network is going to be slightly less attractive for cars, less parking available. There is no mention of how transit might offset that reduced emphasis on automobiles.
49	Dont know
50	NA
51	heart
52	An IMAX!

53	Extensive range of health care services
54	Realism in the message. It is NOT safe and inviting. It is not one with nature. Fix the crime and potholes first. Look at what makes towns inviting? A main street that is walkable and with interesting things to see and do. Lakewood is NOT walkable.
55	Addressing the homeless problem
56	A new grocery store!!!
57	library and bigger retail competition
58	places to meet and a nice restaurant gathering place that is not a chain
59	see first statement regarding the "Villa" + Colonial Center
60	If Mary Dodsworth has endorsed this, I am sure that it will be the pride and joy of Lakewood. The concept is outstanding.
61	More connection to the arts. You have Lakewood Playhouse which is a fantastic almost hidden treasure in the corner, what more might be done to encourage more galleries, more live performance, more spoken word, maybe a concert venue in the park?
62	Can't think of anything.
63	Nothing
64	A great place to live?
65	Light up cross walks
66	People friendly sitting areas
67	A lot.
68	Specifics
69	See 2
70	See above
71	Traffic plan
72	People taking responsibility for their environment. You can't deal with schools but they are quite poor and they need more than the Center needs bikes and a park.
73	Ant thing identifying this as Lakewood specific vs "any town USA"
74	Can't think of anything
75	nothing that I note.
76	Where is it going to be? The town center or where gravelly lake and Bridgeport meet?
77	Don't forget about seniors being able to get around also.
78	Update....add "and backgrounds" to Where people of all ages and backgrounds go to do FUN things
79	Not sure
80	How do we ensure safety?
81	A big appliance center such as Best Buy
82	A reference to what are you going to improve about lakewood that already IS great. This will help create the identity.
83	Something about pets since we have an entire location for dogs to play in (Fort St.)
84	Nothing
85	Lack of any reference to our history or heritage as a community
86	Not sure
87	so far there has been no mentioning what to do to help people enjoy all this if they are not capable of walking longer distances. There are many older people in Lakewood who are either in wheel chairs, using walkers, walking with a cane or just having problems walking further. I think this needs to also be addressed and so far I don't see this.
88	Nothing I can think of.
89	What is described doesn't match the goal of fun
90	There should be a mention that downtowns are there to bring people of all walks together.
91	Fine dining establishments like the Lakewood Terrace used to be.
92	That we have everything right here in our backyard. You don't have to drive to Tacoma and Seattle for family fun and dining
93	Summer free fishing. Winter free ice skating
94	Might be nice to include something that encourages the "mix" of retail, restaurants, etc., to be more than national chains, but also vibrant "local" small businesses.
95	How will it bring people to Lakewood
96	See above answer.
97	I would eliminate the first line saying more like a thriving community that values its people, their cultures and protecting our environment
98	Nothing as far as I know.
99	the kitchen sink

100	GOOD restaurants
101	Easy walking to and from neighborhoods to "city center".
102	Nothing
103	Better yet, what can be deleted.
104	specifics on what changes will be made
105	This city has two major problems. Vacant buildings and homeless people. Until the the city publishes some form of plan to correct these issues the mission statement is not attainable. My kids don't feel safe, nor my wife, walking around the area. The homeless and squatters are taking over and the city hasn't done anything about it. People speed all over the side roads endangering pedestrians. I rarely see anyone getting a ticket.
106	nothing
107	Uh, I think it's missing a strong transition between the paragraph and the bullet points. Instead of saying "Downtown is" why dont you just say "The Downtown Vision is:" - that way it actually makes sense and reminds people that they're reading a vision statement not a factual actual current representation of the what's on the ground.
108	a place to gather to express community concerns (like the March For Our Lives this weekend).
109	Anything that would indicate just how diversified Lakewodd is. Oakbrook isn't anything like Lake City and what about Koreatown?
110	Keep signage unobtrusive
	Public art
111	Clean
112	might add in something about parking availability
113	QUALITY of shops and eating establishments. Let's try to get some "nicer" shops and restaurants.
114	Upscale shopping. It doesn't have to be a Neiman Marcus, but could be a Nordstrom Rack, Pottery Barn.
115	A green area where we can relax and enjoy some nature. Steilacoom Park is good but aimed for the more active.
116	Some thing to address the homeless population.
117	Nothing
118	The money to make a lot of the plan a reality.
119	Nothing
120	Nothing
121	Emphasis on need for retail and restaurants that will bring more people yo the city and make it an overall nicer community
122	Would like to see more definitive detail about how new businesses will be approached and secured.
123	Lots of homeless
124	I would add something about some of the local businesses it will support.
125	Parking for motor vehicles, transportation to and from the Center.
126	Nothing
127	nothing that I can see
128	See above
129	Nothing
130	A clear message.
131	Maybe something in regards to supporting local, smaller businesses.
132	We need to draw big businesses back into Lakewood, support a vibrant and strong LPD, and do more to engage the military in our community.
133	Good city officials that properly know how to run a city, without pocketing the money that should be used for schools, roads, and honest law enforcement. We don't need and more worthless stores. Stop wasting Lakewood money.
134	Concern for long-time residents and businesses
135	Detail.
136	???
137	What Goals/Vision/Values/whatever are the vision.... less description, bigger picture.
138	Maybe dog poop stations along the park area as people are sure to bring their pets and it will help to keep it clean.
139	Nothing
140	An actual park. I know there is a small one locally. It would be nice to have an updated one with a bathroom.
141	Get deeper with cultural diversity.
142	No ideas right now
143	Aren't Q. 2 and 3 the same thing?
144	I think you covered all areas.

145	Where will it be?
146	Define cultural diversity
147	Seems like a complete list.
148	history? Lakewood has had a rich history but it is all gone now, most of it has been torn down and thrown away. Is there a way to revitalize the feeling of the developing a place where families can come.

Q4. What do you like about the Downtown Plan Concepts?

1	Not much. Think there are more important things to spend the money on.
2	More parks, more green space.
3	Green
4	Locate farmers market to Motor Avenue
5	Needed more detail hard to understand
6	It's not on the west side of Lakewood
7	Right now it looks chopped up to me. I'm also not sure changing Gravelly Lake Drive is so necessary.
8	Improvement to Sealy Park
9	not sure- hard to picture
10	Based on the small cities that I have visited, Lakewood was old fashion. The plan addresses some of the lacking issues like the park, civic ct. and ease of getting around.
11	plenty of room for the Rotary's godforsaken pavilion.
12	Addition of park-like spaces Our core is just ugly rambling buildings
13	addition of green space near city hall and looks like one site might be for future library
14	the modified street changes
15	Nothing. A lot of tax payer money thrown at a nonexistent problem. Just how many citizens are clamoring for this or was it thought up in some bureaucrat's office as something for them to justify their existence?
16	Bringing the community together in a central area.
17	Again, who get the pleasure of paying for this dream?? You have spent thousands of dollars on this campaign....who is paying for it?? Lakewood residences, and as a very long time resident I have zero interest in paying for this dream and the ongoing maintenance and upkeep it will require.
18	not sure if I like any of it. the area for a park seems extremely small.
19	Not sure I understand what defines what, a key box would have been nice.
20	Park
21	A park is of the utmost importance, but with that, community interaction and open dialog with police and more available services for homeless and mentally ill. It's a beautiful plan, but if I don't feel safe it's moot.
22	Parks!!
23	Not sure yet if I like it
24	I love it. Little hard for me to imagine and understand how it would come together (the green street would go right through Lakewood town center)
25	N/A
26	to generic to comment
27	the Motor Ave Festival Street and Central park
28	It should improve Lakewood IF HOMELEANESS and crime are addressed
29	Pedestrian friendly Parks.
30	nice idea
31	A farmers market sounds great! The green street concept is interesting. The Central Park could be the focal point of the entire thing.
32	Central Park!
33	The street network is more connected and less confusing
34	Rehab of Seeley Lake, central park, clarified connections
35	All of it, but especially the Motor Ave Festival Street, it requires me to change my driving habits, but I would love to see this area redeveloped.
36	focused
37	Sidewalks, trees, green space.
38	I really like the green street loop because making it actually enjoyable to walk and bike through Lakewood would make that much more appealing for me

39	Providing multi-modal streets and mixed use developments
40	Green spaces
41	the parks
42	Doing something with vacant spaces.
43	Expanded sidewalks, park space.
44	Green street loop
45	I'm not sure, with out some landmarks for reference and a comparable road map to compare what it looks like now with the plans I can't tell if I like anything or not.
46	Nothing. I can't read the map, I have no idea what I'm looking at. How did you not label the towne center?
47	Green Street Loop Walking only
48	sidewalks and area to walk
49	the map is confusing. It would have been better to see an overlay or side by side maps of current roads/ named buildings to orient myself. Parking looks difficult, particularly for those requiring handicapped access. (there are not enough spaces now.)
	I was unable to attend the formal presentation on Wednesday.....will there be another time?
50	It will create a large inviting area for a huge diversity of activities. I can see a large 'downtown' area that is conducive to walking, riding and relaxing.
51	I'd have a better opinion if I knew what streets I was looking at. From what I can tell, it looks fine.
52	I like the idea of a park and green street loop.
53	I'm having trouble visualizing this part
54	Much
55	It's better than current
56	Map was hard to understand
57	Unsure at this time
58	It's a dream! Seeley lake park is not safe. Great if you can do it.
59	Increased safety for non motorized travel
60	I like the addition of park areas, and gathering spaces. I also like the festival street idea.
61	Expanding parks.
62	Adding parks and greenbelts coupled with public plaza
63	Not much
64	It sounds great. Financing all of this is a problem.
	It is impossible to really see what's what in the map provided.
65	Love the bicyclist and pedestrian green loop.
66	A focus on public parks
67	Love the motor av concept. I'm almost sad to see CVS move in where Kinkos was. I'd prefer that to all be local food trucks, local non-chain restaurants, park space and maybe reopen the old theater as a dinner movie theater or something along the lines of the Blue Mouse in Tacoma.
68	Everything
69	Love the idea of more green and parks.
70	I like that it's improvements all in the same area and connected.
71	New public streets
72	Very little! We need more park-like spaces in the outer areas where the population is concentrated.
73	Map is confusing Needs existing landmarks identified for reference. Good Start.
74	I would like to see some better spaces for bigger restaurants all we have is fast food crap!
75	I think it includes the appropriate components, including green and open space. I am concerned, however, that the Plan be REALISTIC and not "plan" for green space where there is already recent development--e.g., the new Hopjacks restaurant. It simply isn't going to be replaced by a "park" in the near future. Make the Plan visionary, but make it realistic and something that the City can actually accomplish in this lifetime.
76	It's a most interesting plan; lots of interest for all kinds of activities and shopping.
77	I like the idea of the farmer's market and festival areas but not sure using the area around motor ave for this
78	It sounds modern and inviting.
79	good concepts
80	The map doesn't tell me anything.
81	It gives Lakewood a personality. It isn't just a transient military town that's grown out of necessity.
82	not sure what i'm looking at. will need to drop by city hall to study the larger map
83	Likes:
	1) The idea of reclaiming Motor ave for festivals. The tree-lighting ceremonies used to be there and it was a lively

space.

2) The idea that Seely Lake could actually be a walk-friendly park space instead of a homicide swamp - IF the city is actually committed to cleaning it up

3) Like the idea of a small park space in the interior of the mall - but hopefully it will have some program features like a bocce ball area, or a demonstration garden, or some community garden spots so that people can interact with the space purposefully instead of a just a landscaped 2 acre patch of grass. It would be cool if there were a couple food truck parking space outlets with a generator box so at least a couple food trucks could set up.

84 Gravelly Lake Drive has been "improved" for the past year to the point that multiple businesses have been forced out and access to Columbia bank is almost impossible.

85 I think that Motor Drive is too far out of the city core to be effective as a "festival street".

86 Central Park

87 central park is a great idea as long as you can keep it nice and keep drug dealers out

Festival street seems like a good plan

88 green spaces

89 Park, civic gathering place, shopping.

90 See my lake improvement

91 The plans for Gravelly Lake Drive and Motor Ave,

92 The plan helps revive the area between 100 th and the colonial center which is good

93 The park and walkability of the plan.

94 No eager to undermine Ft Steilacoom Park

95 I like the central park idea, the bike friendly concept, redesign of Gravelly Lake Dr

96 Park concept

97 Walk ability...getting people out of cars is critical.

98 I like the addition of a park. Our city is lacking public parks that are close to residents. I also like adding sidewalks for gravelly lake.

99 A table model of the proposed 'Downtown Plan Concept' would make it easier for me to comprehend. Difficult for me to visualize what is proposed.

100 I love the idea of new parks in the area. As a mom of 2 kids parks are always a good choice. I just hope one will have a spray park features. I am tired of going to Tacoma for their spray parks.

101 more parks for to encourage outdoors, not needed to leave Lakewood for entertainment.

102 Integration of currently disjointed areas

103 It will make it harder to drive to the town center and shop. It's not walkable for many people.

104 Nothing

105 Not much. It's totally pie in the sky.

106 motor ave festival spot, new parks

107 parks, and walks

108 Motor Ave Festival Street and Central Park

109 Sidewalks and a downtown park.

110 Seely lake park restoration. Also the place for larger events outdoors

111 It looks like it will make the 'downtown' a little slower pace and not so car-centric like so many other cities.

112 I like that it will look more highend and hopefully bring in a new crowd of people for Lakewood

113 It seems like an upgrade for sure

114 Seems like a lot of empty space for homeless and rug addicts to hang out

115 The creation of more pedestrian-friendly access -- the idea that people can park in one place and then walk to their other destinations instead of having to drive everywhere. Love the central park idea and the Motor Avenue plans to create a public open space.

116 Like the idea of the green loop and clearly access to parks

Q6. Tell us about your top 2 priorities.

1 Don't like any of these choices.

2 More active usage of the Colonial Center. Enough parking.

3 Safety

4 They are the ideas that serve the varied Lakewood populations

5	I like the idea of the green loop, making walking, biking, etc easier. I didn't choose any other project because I don't feel I have enough information about them to make an informed decision. I am very hesitant about a central park (not opposed because it's a great idea!), with our current homeless population and drug epidemic. I do realize this hesitancy may not be based on facts, only a fear of what COULD happen.
6	Your priorities not mine.
7	Priority number one would be high quality high density housing located at both the town center and at the commuter transportation hubs. Number two priority would be free transportation via light rail or dedicated buses from the transportation hubs to the town center.
8	Green space is good. Maybe you should put the Rotary's godforsaken pavilion there
9	Emphasis needs to be on bringing businesses other than the same chain stores and restaurants every communication seems to have.
10	Both 1 and 2 would make it easier to "get around". There would be a connection between shops, park etc. It is now a congested mess!
11	Stay away from Ft. Steilacoom park
12	Addition of parklike areas to soften the area and attract walkers
13	Park in the downtown would greatly add to appeal, walkability. Turning Motor Ave area into desirable non-motorized area would do the same
14	Developing a gathering place and better farmer's market venue would be top priority.
	Improved streets would be good. Or spend the money filling in current potholes.
15	<p>1. Address run down properties in Lakewood, force them to uphold code requirements and if they are a burden on law enforcement and other city services, condemn them.</p> <p>2. Please understand that grants are not "free money" they come with strings attached relative to how they are spent. I submit to you that if we really thought before used all grant monies we'd see that the true cost in negative impact to our City far outweigh the benefits to use them.</p> <p>3. Understand and embrace that as a City tied to a huge military presence we will always have a high percentage of temporary residences in Lakewood.</p> <p>I'd really value away to get more owner occupied single family residences back into our existing neighborhoods. The 08' recession forced many properties to sell and become rentals. Absentee landlords of private residences are killing this city. Drive through our neighborhoods and see how rents homes are maintained and the filth and code violations they drive. You want to fix something start upholding city codes, put teeth in them and many of these absentee owner/rental properties will revert to owner occupied. This will only happen if city leadership deems this as a true priority. Invest an hour and drive through the cities neighborhoods and you will see what I mean.</p> <p>You can't perfume this pig folks!</p>
16	the map is not very clear to me.
17	Community pool like peoples pool in Tacoma. We used to have a couple of private pools in Oakbrook but country club pool is gone and OSTC will be gone in a couple of years too. All ages use pools. Maybe go together with school district since clover park pool is aged and needs replacement soon.
18	A central park, a water feature for our kiddos would be amazing. A hub for our community. And safety.
19	Traffic is congested and there are two schools there
20	Motor Avenue
21	I think they would bring people out more
22	Consolidate any new high density housing at this location. Stop plowing under lots throughout Lakewood for McMansions with zero yard space.
23	I see Motor street and Gravelly Lake dr. the most, those will have the greatest impact on my day to day life
24	Address homelessness and crime
25	Pedestrian friendly sidewalks and streets.
26	pedestrian safety and connectable paved sidewalks, paths, loops etc.

27	The Central Park and Green Street will make downtown feel like a destination (year-round) where you come and really spend time. It would be a first step in turning the culture of stop and go.
28	We need to make larger investments in Parks & Recreation in Lakewood
29	These creat the place and make the street network less confusing.
30	The area is *so* lacking in green space and, really, anywhere to spend time other than shopping. A central park and gathering area area will help community events. And the old mall area desperately needs infill. Residential standalone would be ok, despite that ppl may think mixed-use is better.
31	Motor Ave Festival St and Central Park allows for new events and happenings in our city and provides opportunities for new businesses.
32	central park motor avenue
33	A green walkable city. Adding sidewalks, trees and historical designation to "Veterans Memorial Parkway".
34	Because I live near Gravelly Lake Drive, the green street loop and improving that street particularly interest me.
35	A central park will provide a focus for the redevelopment activities to occur and build around. The new uses will be important to bring more residents into the downtown core and give it vibrancy.
36	Lakewood is ugly and concrete and has so many ugly buildings. Green spaces will really help it feel inviting and less nasty.
37	Parks are needed close by. Road improvement should be top priority.
38	Building an IMAX and opening a large children's play center along the lines of Charlie's safari in Lacey.
39	More business expansion other than burger/sandwich type restaurants
40	Gravelly Lake Drive needs to have an identity that is synonymous with Lakewood and with uniqueness both in commercial and residential uses.
	A new library, outdoor plaza, community center, and a four year university extension campus would be catalysts for expanded development.
41	New streets, green street loop. I have lived here for 15 years and now have 4 teenagers who enjoy the walkability from our house. We love at Avondale and Lexington. My priorities are anything that makes the area more pedestrian friendly and safe.
42	Gravelly lake desperately needs good sidewalks.
43	A new grocery store Something being done with the empty QVC/QFC
44	Major projects with traffic tie ups, but so much better when finished.
45	modified Gravelly lake, I think sidewalks should be added on Nyanza as many people live in that area and could walk to town. (no sidewalk now)
46	green spaces throughout, not just central park. I cannot judge beyond that without more information. what are the traffic considerations?
47	Motor Ave Festival Street is needed first so as to relocate the Farmers Market and create the hub on which we can rebuild. That area is now an eyesore and needs to be in rehab. The infrastructure of new streets is #2 so traffic can readily move into the new areas to be created.
48	Central park, and more sidewalks (like through the entire Towne Center, not just up to a certain point).
49	A park would be cool spot to gather at the heart of the town.new public streets are vital for ease of access and traffic flow.
50	They both emphasize gathering
51	Sidewalks
52	Later
53	They're not my priorities
54	Sidewalk on gravelly lake would make it safer to get to central activities
55	Don't know enough about the options. Can't figure out plan knowing streets that I know
56	I like the idea (although I know it goes against common practice) of keeping pedal bikes off the road- they should not be competing against motorized vehicles- but also not running down the walkers
57	i love the idea of adding a park area, it would give the whole area a more natural feel.

	i think having a designated area for festivals or events is a great idea.
58	Park space for everyone. Motor Ave for fun events is good.
59	None
60	I like to get to places on foot or bicycle for exercise.
61	Central Park to replace unused former dept. store properties; Gravelly Lk. Dr. could use some planting in median (think Univ. Place!).
62	Motor Av and QFC property development. Maybe tie in the library? Also, a way to encourage a family environment and discourage transient congregation. The two sites I mentioned are basically unusable for most families in their current condition. I'd like my kids to be able to enjoy the city center without having to park their bikes next to a row of shopping carts loaded with someone's belongings. If we could centralize social services and mental health services somewhere away from these proposed areas, that would be my number one priority. As an example, we don't put the methadone clinic next to Fort Steilacoom Park or an elementary school; we put it up on Front Street. Common sense.
63	It would be wonderful to create some sort of civil presence on Motor Avenue to help bring the community together. The green loop sounds wonderful as long as it does not mean you are reducing the number of lanes. Please don't be dumb and reduce the number of lanes.
64	I think a central park and new uses to existing space is important to the community.
65	Central Park with recreational facilities like a public pool, or activity centers besides skate boarding.
66	More later
67	We need to have family event spaces that are not outdoors with our weather.. I pavillion or civic center that can have programs going all the time for all ages. Like the Kroc center in Oregon. That is what Lakewood needs. A hub that the whole community can use for swimming, classes, computers, art, concerts, banquets, etc.
68	Motor Avenue is something that will actually happen; that is why it is a priority--we can focus on it and get it done NOW rather than in a decade or more. Same with a Central Park and associated walking amenities within the Town Center. We realistically can do that now (assuming cooperation with the RPAI ownership), provided it works around existing and recent development and perhaps plans for upcoming development. It should encourage and compliment development and not thwart it.
69	The streets need improvement too many potholes also a nice park would be nice but would need security
70	#1 - Lakewood streets are horrific; so many holes and troublesome for cars and pedestrians. #2 would benefit residents and visitors both.
71	I think they are self explanatory. alz
72	Without more info this is not a question that can be answered. You've given labels but not much about function or how it will improve Lakewood.
73	Cars will remain the primary mode of transportation and streets are what will get us there. Streets should represent all that is good in a community. Streets are often the only thing people see and should be inviting and welcoming to bring them back.
74	priority is to promote an attractive business/retail shopping environment. attractive to the eye
75	Fix Motor Ave if that's what it take to fill all the vacancies at the Colonial Center, the city needs the business tax revenue. Then work on fixing Gravelley Lake so there's a key corridor that connects Motor ave to the Mall
76	Gravelly Lake Drive MUST remain a main arterial and open to traffic going toward University Place and Tacoma. Development in the area of Colonial Center and the area where QFC used to be should be encouraged for private businesses,
77	The downtown area needs more green space and street tree to "soften" the industrial look of the area. Adding new elements to the area would be nice.
78	Keeping it GREEN not a paved jungle
79	central park would be great to have a nice park area near town center, like the idea for a designated for farmers market the rest of the plans may be good but very difficult to understand what you are going to do with gravelly lake dr
80	Better areas to walk and addressing the homeless population
81	Motor Ave and the Central Park would be great

82	Attractiveness sells, and that is what I would like to see.
83	Doing something to make productive use of all the vacant buildings in Lakewood— there is so much empty and run down retail space. Also making walking similar to Chambers Creek in UP people go to Up for that amenity
84	Make the area more bike friendly. This should apply to all of Lakewood. Just like you are doing a good job of adding sidewalks, add bike lanes or paths.
85	Green str loop and central park
86	Motor ave festival street and the green street loop
87	The festival area is to packed in at the current location, an outdoor market requires open space and air to relax, browse and mingle. Relocation will make it more visible and accessible to Bridgeport a major thorough way. New public streets need to occur to define the roads access for the other changes to follow.
88	More parks to bring more families and draw kids outdoors. Also sidewalks so that the community can access them safely.
89	More sidewalks, especially along routes people like to use now, e.g., along Gravelly Lk. Unique and attractive buildings (not the 'cookie-cutter' type). New, larger library in Lakewood. Would like more info on proposed housing in the area.
90	Other = Spray park.
	#2 - I love the idea of a festival street. It would be a great area to house our farmer's market.
91	Other. The homeless crisis in Lakewood. It does not make Lakewood a desirable place. People urinate in the streets, in people's yards. What is being done to assist the homeless and assist tax payers from having their property encroached upon?
92	More pervious surfaces and soft scaping- if the city is going to be 'green' we must think about the environment
93	I think we should promote/attract 'hip' businesses, not just more family-friendly stuff and chain restaurants. If that's all we aim for, that's all we'll ever be.
94	Park
95	Schools and honest law enforcement
96	I love the idea of the old theater area getting use again. It was once such a great place to go with the theater and dinner or dessert , or special occasions at the terrace - it holds a lot of memories for old timers :)
97	Think it will give sense of community.
98	No comment
99	I would like to see Motor Avenue developed but I'm concerned about the mention of building an event venue. There is a nice peaceful neighborhood behind, and I'm worried about the noise.
100	This woume bring our community together and give more fun things for families to do.
101	A crucial issue to making the area "safe and inviting" as well as attracting new business, and more revenue generating foot traffic is to address the explosion of the homeless population. There are far too many areas now where it is simply frightening to be, especially alone. this is going to be a major factor in the success or failure of this project. If people and business do not feel safe, then they WILL NOT COME, and all this money and effort will be wasted.
102	Motor Ave; I'm looking forward to all the ways this can be used.
	Central Park; I was never keen on our City Hall being located in a mall. This will help break up the lot.
103	I drive Gravelly Lake everyday to the freeway. With the line of cars and crumbling road it would be nice to have it updated with more lighting.
104	Motor Avenue Festival Street could be a great central point for visitors and events for the city.
	Modified Gravelly Lake Drive would give great access to the central points.
105	Improved streets in the area. Road conditions are awful
106	We need more spaces for people to gather so safe areas where are are not allowed are important
107	I love the idea of a central park -- a place for people to come outside in the nice weather and have lunch, walk around, spend time outdoors while in the center of a commercial retail area. I think creating a Motor Avenue street festival atmosphere is another great draw to bring people into the city center and get people to experience the community beyond just running errands.

Q7. Do you see some challenges with these Downtown Plan Concepts?

1	Yes. Lots of complaints of the cost versus putting that money towards improving road conditions, helping Tillich and low income families, low income housing projects.
2	Is it going to mess up traffic?? It is awful already.
3	Yes
4	Yes, safety being the main one.
5	just the before mentioned Central Park concerns, although love all concepts!
6	please do not attract homeless
7	Need more details to answer this.
8	The biggest challenge I see is the lack of accessibility to primary transportation corridors. The transportation is happened out along Interstate 5 to include bus, train, and automobile. And then there is a mile or so of genuinely ugly sprawl before one gets to be downtown area
9	I worry about adding so many streets to the area.
10	Not at this time. I am a hands-on person and do not have the education needed to answer this question. I am an RN!
11	Yes, traffic changes would be problematic Where is the new housing in the core?
12	Yes, the impact of traffic changes would be major both on GLD and on internal mall streets
13	of course, you are never going to make everyone happy.
14	Not a priority
15	no
16	I see a big challenge with getting there. From I-5 there is not a particularly efficient or attractive way to get to the town center. I have had enough bad experiences on my bike in Lakewood that, while I might bring the bike to the center to ride around, I cannot think of a route where I would feel safe getting there. With my best understanding of the plan, it still feels like I am going to the mall: the center feels more like going to Southcenter, South Hill, or Tacoma Mall than like a focus area for the city such as has been developed/seems to be developing for UP.
17	Please do not ever decide to do road construction on two separate intersecting streets on the same day. Can't be an hour late to work every time you guys do that.
18	I wish there would be a designated sidewalk around the perimeter if the Lakewood Towne Center to encourage foot and or bicycle activity. When I walk behind Target, motorists will honk at me if I walk on the street (perimeter). There may be no other motorist around! Walkers do not feel welcome. It happens frequently!
19	Yes. It is very hard for me to picture. I am concerned with the old qfc building and what is going there. Along with the intersection on Lakewood drive and 74 (I love casa,Mia and Tacoma boys bit everything else isn't doing well) really miss the cheese shop and seafood
20	Do not eliminate traffic lanes on Gravelly Lk Blvd. Major development needs to address impact costs. Developers need to carry their share-if unwilling to pay for their impact, too bad. Lakewood's existing residents should not carry the burden of new traffic impacts and increased school capacity
21	driving routes
22	Yes property taxes will rise and the only onea using it will be the homeless unless that gets addressed
23	Taxes
24	lots of traffic breaks in the so called "safe" paths in much the same way that this entire city does not connect well for pedestrians or even school children for that matter.
25	Yes. The Green Street is a huge loop, which does not address how un-pedestrian friendly downtown already is.
26	Money
27	The green loop does not go anywhere and does not help connect the town center to anything. Also, the motor ave festival street is too far away from anything else. A festival street should complement and not compete with the central park. Sitting on its own and off to the side, I think it will be under used.
28	Inertia. Perceived or actual expense to existing property and business owners. Concern about less-visible commercial enterprises.
29	Sure the inconvenience of construction and money possibly affecting our taxes.
30	acceptance & utilization
31	In an attempt to save money they will short change the plan
32	n/a
33	The south and east extensions of the identified Downtown may be difficult to fully embrace all the goals of the plan. Understand the desire for more connectivity with the new streets, but will that auto focus construction detract from the walking and biking goals of the plan?
34	Im sure it will take forever
35	Yes

36	Yes.
37	Funding concerns, maintenance, safety.
38	Still to much focus on cars and resultant space set aside for parking.
39	Homelessness. The crime/meth addict theft is increasing. In the last 3 years there has been an increase in prowler/mail theft activity in my neighborhood
40	Unfortunately without a comparison map so I can see what it looks like now and what the plans would do I can't attempt to foresee challenges.
41	You may want to start with maps that are more clear
42	always
43	traffic and parking?
44	flow, as presented seems awkward.
	will there be play spaces for children?
	How will the homeless situation be mitigated?
	How will this grand plan be paid for?
	where will the new library be located?
45	Without being directly involved the answer is no. It all looks good on paper.
46	Not really, but I'm not an urban planner.
47	I'm wondering about the timeline and funding. This appears to be a large project with a lot of investment. Times change quickly.
48	Traffic is already a nightmare, construction will make it much worse
49	Yes
50	Landlords
51	Money
52	Access and parking in the Motor Ave area. Is community gathering park replacing our community activities at Ft Steilacoom. Can this whole concept support the increase in traffic
53	Safety doesn't seem to be addressed.
54	Time and money and inconvenience that the work to accomplish all this will cause
55	I'm trying to understand what exactly the green loop would be?
56	Alot happening in a relatively small space. Do not attempt to change all at once. Too hard to navigate to conduct daily business.
57	Yes
58	Will it have good thoroughfares for people who are going from the Gravelly Lake area up to the Bridgeport area or will traffic be congested by the new Downtown?
	It cannot be financed by raised property taxes again!!!
59	Not yet
60	Parking?...or more public transport into area??
61	Encouraging local businesses to move in, not just chain restaurants. Can we give them some incentives? We would love to have at least one restaurant in town good enough to require a dinner reservation. We have enough drive thrus. Also, getting some mixed use housing into the new downtown that is high enough in quality to get some decent tenants. We're a nice place to live. Not everything not on a lake or in Oakbrook has to be priced for section 8.
62	No
63	I admit to being freaked out that my home on School Street is adjacent to or possibly within a dotted line that has some shapes in it that I don't recognize. I had no idea I lived in downtown! So there may be some educational opportunities as I think most of us think of downtown as the colonial center and maybe the mall.
64	Construction pisses people off.
65	I don't see anything proposed to help the homeless or drug addicted residents that may end up gathering in the central park. This may be in another proposal or current program I am not aware of.
66	Will the roads be maintained? It is already a struggle to give current roads upkeep.
67	Sure, but nothing good is easy,
68	Yes, Motor ave is such a pain now with the waiting to get on 100th and the lights. It is not a friendly area to walk or do much of anything!
69	Yes, existing infrastructure and existing development. It makes no sense to be "pie in the sky" unless the Plan is realistic and doable.
70	There always will be some challenges; this has to be a well thought out plan and it indicates a great deal of thought was put into it.
71	The motor Avenue area not enough area to develop and people drive through to get to the downtown area

72	If Lakewood doesn't solve the homeless problem. None of this will end up very good.
73	Yes. This is as clear as mud.
74	Funding and deadlines are always challenges.
75	costs
76	The physical concept seems fine, but for anyone that's grown up or lived here for the last 30 years - its really hard to call that area Downtown over "the mall." Dude, its the Lakewood Mall. Its the Mall over Downtown because when the City became incorporated and created its own identity as its own place/City - the MALL was a big friggin deal. Any gen-Xer, late gen-Xer, gen-Yer that graduated from Lakes or CP and still living in Lakewood likely got their first job in life working at the Mall, so its hard to grasp the idea of Downtown overnight. Also, I can see how the Seeley Lake area is a desirable inclusion in the Concept Plan - but its is clear that there are no physical plans or anything envisioned to transition movement between the park and the downtown. If vision is for people to walk/bike/drive from an idyllic dog walk around the lake to a cup of coffee at the mall - then there needs to be some distinguishable paths or pedestrian improvements through the shitty old K-mart area.
77	Clover Park High School and the doctor and dentists offices need to be involved in this plan.
78	Yes, I have real problems with the Motor Avenue concept. I think it is a great concept, but in the wrong place. It will take the focus away from the city core.
79	Keeping some local flavor in the businesses. Not just Chain stores and restaurants
80	no clue what this green loop is about how big of an area are you talking about a pathway? might be a good idea but can't be sure from info given don't take away needed parking when yu actually host events here
81	Steilacoom park is putting in huge new venue to host events --- why duplicate in Downtown?
82	Safety and cleanliness. Who will pay for police safety and keeping the areas clean?
83	Funding
84	Obtaining the land for the park, and obtaining critical mass for shopping and dining near motor Ave
85	It is always a question of financing and meeting most expectations.
86	Looks very very expensive. Also there are many issues that Lakewood needs to address and this could detract from all the other needs the city has
87	Looks like you are taking over some retail space in the Lakewood Mall, which is fine. Are you moving (or de-emphasizing) the retail core area, maybe to 100th and Bridgeport?
88	No
89	Yes...how will it be paid for. Secondly...the downtown is very spread out for a city of Lakewood's size...can it be shrunk?
90	Yes, until cities learn how to install zipper streets for ease of access, updates etc. Road construction, road construction, road construction who wants to go anywhere?!?!
91	costs. But that's normal. Pushback from anti bike people.
92	Of course. Financing, community involvement and support, ability to modify the Concept as needed, etc.
93	My biggest concern is with the road construction that would be associated with the green loop.
94	time, money
95	Urban design is challenging in a built it and they will come approach. It appears that this is the strategy for attracting businesses or services that will attract people.
96	Yes, many. Why develop something pedestrian-centered when so few people can walk there? Folks aren't going to walk downtown from across the Interstate or lake Steilacoom. We have many seniors and folks who commute to other places. Making it harder to get to the shops via car is a very bad idea.
97	Yeah, it's not important
98	Yes. Land acquisition.
99	Change is always hard.
100	There are always challenges to any plan but Lakewood always works through it effectively
101	No
102	The traffic while its being built.
103	Again, finding a viable solution to the homeless situation
104	It looks like some retail space will be taken over for public use. Good for residents, but will it be bad for revenue?
105	Maybe some push back because people think taxes are going to go up. But I think just pushing forward will be what's best. We need to go for more high end revamp to get the image of Lakehood out
106	wonder how this would affect the LPD coverage of city as a lot of concentration may have to move to central downtown.
107	Not really
108	Too many places for unsafe people to be. making these areas inappropriate for families
109	My concern is having the level of retail and services that will be a draw to bring people in. A hope would be that

with these added improvements new retailers would move into the area to create an additional driver of people to the commercial center.

- 110 MONEY. How is this getting paid for? Time this could take a decade and if there is money today if there is a downturn the money could dry up. The mess it will create, again this will take time will all this construction drive people away and business loose business. Assume the new central park will require demo of old GI Joes?

Q8. What other ideas do you have to make Downtown great?

- 1 Change the restrictive laws that keep driving stores out of Lakewood. More places to shop are more important than making things look pretty.
- 2 Roundabouts where they can replace stop signs or lights. Do something to make Seeley Lake (soccer fields, dog park, etc) a destination and drive out the sketchy elements. Also, this would alleviate the demand on Ft Steilacoom. Public transportation into green areas for those with challenges. Make sure there is ample parking. Lakewood Town Center has a nice amount of parking now.
- 3 Deal with the homeless issue.

Become more business friendly to bring more commercial services.
- 4 Less traffic
- 5 A tangible effort to promote and use independent small businesses by the city is needed. Our chain restaurants, stores, and service oriented businesses choke out local businesses and limit entrepreneurs to higher income people.
- 6 You have a great place like the Y really close to the Seely Lake restoration. It makes me think, could there be elements of some calisthenics that could be incorporated along the green loop? Or fun facts or great places to visit that are a part of our great city that would promote a healthy lifestyle? Family Resources? Food banks, etc. that could help people meet their needs?
- 7 More things to do. More retail and restaurants. Activities.
- 8 High density housing with sufficient elevation to give views above the tree line and therefore the Cascades – to include Mount Rainier – the Olympics, and our lakes
- 9 Clean it up. get rid of the empty buildings that attract problems.
- 10 Only make it known that it is a safe environment for car and body. Have lots of lights and maybe patrols in plain sight.
- 11 Knock down the trashy empty buildings and give the homeless a ticket to Seattle
- 12 Housing and a water element
- 13 Add a water element since we are Lakewood and I've experienced the appeal of the little stream at the Children's Museum of Olympia -- kids and parents cluster there on warm days
- 14 Let the private sector decide where it wants to put its money in development and let government butt out and get out of the way.
- 15 Over by the transit area, adding secure lockers for the homeless so they could possibly go to work and not have to worry about their possessions. It would be close to transportation and they wouldn't have to use shopping carts.
- 16 Fix the neighborhoods downtown will take care of itself in time.
- 17 I can't see where this map looks like a down town

that have ever been too. Guess I am thinking about a happy town like in Pollyanna
- 18 I would love to see the Library moved into the heart of the area.
- 19 Looks pretty busy
- 20 Think Puyallup's main park and stage and play area off Meridian. Or Olympia's farmers market. Bustling, safe, kid friendly, CLEAN.
- 21 I wish we could carry out a theme. The colonial Center already had a very clean look. I wish that look had been carried out to the Lakewood Towne Center when we first started to reconstruct it.

I also wonder why we don't uncover the Ponce de Leon Creek. We could put pretty landscaping along the banks and a lovely bridge.
- 22 Better shops and some anchor stores to draw people in. Chick-fil-a will be great for 100th. Would love to see something like Nordstrom rack go in the old qfc or even a trader joes
- 23 This is Lakewood, not Seattle, or Boston. We live here because it is a open, residential community, not a concrete canyon maize
- 24 Street name signs at every corner that are larger and more visible
- 25 Lakewood Ice Arena !!!
- 26 a paved area that does not allow cars in that parents can take their children to learn to ride their bikes and enjoy their scooters, hot cycles, trikes or whatever and mingle with other parents while enjoying lunch or a coffee without worrying about all the threats that come along with occupying a place such as the new Waughop Lake loop that's

littered with unleashed dogs and grown adults trying to get some exercise; seriously, it looks like a playground there on the weekends but what can you do in a city that has such few sidewalks and the ones that exist don't even connect.

- 27 Connect to schools. CompSD is right there. Coordinate art for display, activities and events.
- 28 Walking, urban plazas. If you build it set aside or make sure you have the money to maintain it long term. Otherwise shinny & bright for a few years, then the slow decline.
- 29 anywhere that commercial is allowed, high densit residentail and office should also be allowed. That way there is a clear productive use of the land no matter what direction the retail sector goes. The US is over build on retail, we need to make sure any retail property in the city has an exit strategy if and when it is needed. Housing is the main shortage in Puget Sound right now.
- 30 residential, clear and interesting and safe ped connections.
- 31 I would love to see a higher end restaurant or bar in our city so I don't always have to go to Tacoma for a night out.
- 32 Just look at University Village and copy that concept.
- 33 I don't know how feasible this is but it seems like some of the poor quality retail buildings in the Colonial Square area just need to be demolished and replaced with better mixed use buildings, and without bigger improvements like that I'm not sure how much things like the green street loop will be utilized
- 34 The Festival Street location needs to be where major community events will happen. I'm not sure the location shown is the best for the plan. Between City Hall and the Central Park might be a better location, but defer to people who know City events better.
- 35 Kid water park surfaces like in downtown Olympia or down by Titlow in Tacoma. They make spaces so inviting for families.
- 36 Better roads. spend our money wisely!!!!!!!!!!
- 37 Open an IMAX and a low cost family fun attraction.
- 38 Water features- spray park in central park.
- 39 We need another grocery store! Safeway is entirely overwhelmed. Please do something with the empty colonial center. Please a new grocery store where the QVC was, preferably a Fred Meyer or even weird-o Sarrs! Or an ethnic Grocery!
- 40 Bringing major companies and businesses to our area.
- Lakewood trolley car system to cut down on traffic in the area. Safer for predestrians and enjoyment of walkers.
- 41 More sidewalks from Nyanza park area! More small restaurants and shops!
- 42 We need a restaurant the is more upscale than HopJacks!!! A grocery alternative would be wonderful (Trader Joes/Whole Foods) I drive to H&L and those 2 for my shopping. Safeway is a disaster.
- We need to encourage young families to become a part of Lakewood once again. Having live here over 50 years, the changes in demographics from my view are substantial.
- 43 Safe sidewalks, especially along 100th. Better traffic light timing and function.
- 44 Walker-friendly and more greenery are good.
- 45 Nice affordable housing for a diverse population of people.
- 46 Perhaps some housing for all ages
- 47 Many
- 48 Food trucks with nice, clean public bathrooms, covered eating area (a building would be nice) with outdoor seating and stage for music
- 49 Need places for teens to gather, summer swimming pool etc. Also we have a growing homeless problem to resolve.
- 50 Fill vacant buildings before we move on
- 51 It is all down market. Hard to buy women's clothes here or shoes that are good ones. A few on sale at marshals.
- 52 More nice low income housing
- 53 i love the ideas of having the designated gathering spots for people.
- 54 Need a vibrant community center to include meals for seniors
- 55 None
- 56 Have more bicycle lanes or jogging lanes built for safety and exercisers.
- 57 A theater building/auditorium for public performances etc.
- 58 Sidewalks/bike lanes all over the city so that families who live more than a few blocks from downtown can ride there safely and enjoy what we're planning on building.
- 59 more eco-friendly
- 60 Adding more recreation options. Such as somewhere similar to Dave and Busters/ESPN zone, etc., or an arcade or bowling alley.
- 61 Public art that incorporates historical aspects of the city
- 62 Mixed use buildings, restaurants, stores and bars on the first floor, street level and apts on top.
- 63 A legal cannibas shop in the Town Center would have been great but I saw that was voted down. Somehow filling

	up the large empty retail locations in the Town Center seems important.
64	Community gardens options.
65	Open an egress street out of St. Frances Cabrini church to allow traffic to exit into the street on the south of the Town Center, towards Barnes and Noble.
66	Several-- later. Need time to think and consider overall space
67	We need a MAJOR activity center. People can't afford the YMCA and Fitness clubs with families. Please make things that will be more accessible to families and people with disabilities that are indoors spaces
68	I think connectivity is important, but I also think we should focus on a bit more intense development and sense of place in both the Colonial Center (Motor Ave) and Town Center areas....the Central Park, where it "fits," along with housing and smaller scale retail. Big Box Stores are simply not going to give that sense of space and place around a swath of lawn. Think more like Kent Station or a mini version of University Village. Take a look at the fantastic plaza feeling created by Point Ruston. We have to be able to do SOMETHING on a reasonable scale to start.
69	Clean up garbage and establish a homeless center
70	Solve the homeless problem!
71	Safe walking to and from downtown.
72	Forget the cut through to Active Park. It's not needed with Central Park. It will also give opportunity to crime in the surrounding neighborhood.
73	make is attractive enough to keep shoppers here and not to the malls, downtown Tacoma ect.
74	Build or design a space and a PROGRAM where junior high and high school kids can interact - like basketball courts where Sheriff vs. Lakes/CP kids can play. Or even just public basketball courts near City Hall where kids can hang out and not feel harassed by the cops. Or a culinary garden where Cabrini or CP home-ec kids can take guided walking field trips to. Moving the Library to the Mall area would be a good idea. Moving a Lakewood Voc Tech or Pierce College program (like a student dental clinic or student-run bakery/cafe) to the Mall would be cool.
75	Find some way to help the owner of Lakewood Towne Center to fill or redevelop the empty buildings. No, I don't mean use public funds but perhaps, reviewing permitting or other "bumps in the road".
76	Walkway to Seeley lake area
77	get rid of the beggars, drug dealers etc need more of a police presence, hard to shop there because of people wandering the parking lots asking for money,
78	Fill up some of the large parking areas with either more shop space or at minimum, make them more pleasing to the eye.
79	Continue to attract business near motor Ave and continue to clean up 99th and Lakegrove
80	None
81	A legitimate farmers market — current one is very sad; more restaurants and shops to generate revenue and interest in the city
82	Trees lining street and sidewalks
83	Already answered this in a previous question
84	Adding more bike racks. Enticing local businesses and entrepreneurs.
85	Higher quality restaurants vs profusion of fast foods places.
86	Let me say it again - spray park!
87	until all the vacant buildings in Lakewood are cleaned up, repurposed or torn down, Lakewood will not look great. Until the homeless crisis is taken care of Lakewood will not be a draw. Please do something about the old buildings (QFC and many others) and the homeless crisis first. That is where the money should be spent first. Address the elephant in the room.
88	Form that follows function - if what is developed reflects the lifestyles of a wide range of people it will work.
89	I think we should have some trendy shops and eateries, maybe some local/handmade things that aren't food. It would be nice if we could have cannabis stores, too.
90	Somewhere for the homeless to go.
91	Better law enforcement
92	Get Lakewood Town Center revitalized. There is a lot of unused parking space that could be used for activities that draw in shoppers, etc.
93	Id like to see a splash area in the new park . I think now that we've been to Point Ruston and see the rejuvenation a park and shop area can bring - it would be wonderful to have something like that here. Understandably it wont have the water view - but possible the mountain view or at least with the grass and trees - bring beauty to all the concrete that is there now.
94	I would love to see the Lakewood Theater get used again.
95	None
96	Building walkable streets around or close to downtown is a must.
97	All year round family activities
98	Look at the Western State campus to create a working shelter as well as resource center for the homeless

	population. Change the laws to make sure these folks get proper assistance as opposed to setting them into a cycle of jail, street, jail, etc.
99	More entertainment options to include family choices. And you know what I've always loved about the big cities is buskers. Jugglers. Troubadours. An avenue of lights (solar powered, of course). I'd love to see Lkw recreate a small version of the World's Fair.
100	Regulating what goes into the space. No pot shops, no discount stores A high end grocery store like a barber greens or Met would be great. All we have is Safeway and Albertsons. :/
101	Invite Lakewood community out to the Downtown area as the connection to the local area. Perceived ownership is a good way of keeping it safe.
102	Improving the safety feel. That's going to be tough to overcome
103	more non chain restaurants, more stores
104	During the dry/warmer months putting on events like the city's Farmers Market -- but not just during the work day. It would be nice to also see a weekend market on a Saturday or Sunday. Many other communities do this and it would be great to see that as a weekend draw to come into the commercial area. Also the creation of a downtown business district association that could help support and put on events like concerts in the new central park during the summer, or other type events to bring the community together.
105	Getting rid of all the vacant buildings down Gravelly Lake. Making bringing in new businesses a priority. Increasing safety and decrease blight.

Q9. Some draft policies - broad statements about what the Draft Downtown Plan wants to achieve - are listed below. Tell us what you think.

1	Diversity in business is good, but we don't need warehouses downtown!
2	Affordable housing is also a highly sought after necessity.
3	It would be nice to encourage upscale restaurants, shops, and services to come to our area.
4	I don't see housing mentioned anywhere
5	Not government's call on development. That's what happens when private investors make the decision to open businesses based upon market data. Best thing government can do is get out of the way. How about government reducing its regulation from the size of a NY phone book to a single loose leaf folder? How about government cutting back on taxes that it seems to search every nook and cranny for?
6	Seems senior housing in the town center area would be important.
7	Why not put a nice park on the corner of Bridgeport and Gravelly, instead of yet, another drug store. Probably because the drug store will bring in more \$\$\$
8	the public SHOULD NOT carry the cost of development. Impact fees need to address the impact to the surrounding community and be paid for by the developer.
9	Policy 1. No urban residences in the space. What does this contribute, other than adding congestion? Policy 3. Layman's terms, please!
10	Need to more emphasis on residential. A good downtown needs foot traffic.
11	Have to have the catalyst sites, good to work from the central park outwards if possible.
12	We need an IMAX
13	New grocery store!!
14	Affordable housing in the downtown area to draw people back to Lakewood.
15	Combining residences with retail and shopping needs is a great idea.
16	Not clear what the catalyst would mean
17	Sidewalks
18	None
19	This is the only place lower income can afford to buy their own homes
20	With NO pot shops (marijuana) in our city!!!
21	we need more small/mom & pop shops
22	Seems pretty common sense.
23	I still hate to leave Lakewood to have a good dining experience in upscale restaurants

24	Lots. Later
25	Lakewood has made it very hard for businesses to come here. The zoning and rules are not at all competitive. I had a business here and like many I know they all left for tacoma. You need to be more open and also offer larger brand restaurants and stores a way of wanting to come here. Offer some incentives
26	Need an indoor community park with a climbing wall skate park and places where people can go when the weather is bad.
27	Don't forget to make all improvements easily accessible to senior and/or disabled citizens!
28	I don't think people need to have their jobs in Lakewood
29	I used to believe in "catalyst" development but it seems like it always fails.
30	Keep it Local we have plenty of Formula retail locations already
31	need to bring in some big stores or outlets to this area, nordstrom outlet, lulu lemon outlet something upscale otherwise everyone just goes north also need more diversity restuarants only fast food is available no nice dining at all
32	Lakewood really needs a high quality restaurant, NOT more fast food restaurants.
33	Make sure to support local small businesses currently in the CBD now
34	More emphasis on cultural activities and residential housing.
35	Would greatly depend on what the proposed use was.
36	Work on a better and safer city before upgrading it. Drugs swarm Lakewood, but the city doesn't seem to care.
37	Yes, it would be nice to have Pot Stores in Lakewood, and get the revenue created by them.
38	Covered earlier.
39	No more large industrial buildings

Q10. Some draft strategies the City could take to achieve the policies include: incentives to encourage new businesses to locate in Downtown, providing information and other resources to entrepreneurs and small businesses, encouraging businesses to work together to make downtown Lakewood clean and safe, investing in civic amenities and infrastructure, working with local financial institutions to help provide loans to qualified businesses, and others. What are your ideas for strategies to attract businesses and keep businesses in Downtown Lakewood?

1	Have heard from business owners (who have left Lakewood) that the laws for small businesses are extremely restrictive compared to other towns in the area. Might want to seriously reexamine them and see why there aren't that many new mom& pop stores coming to town.
2	Places for employees to walk, space to have offices, data to back up the diversity of skills in the community (do you know where people work now??) Small business is good, but big business tends to pay more livable wages.
3	Attract business of prestige and that won't attract a rough crowd
4	Support them! When police action is needed to protect small businesses and their customers, provide it. We have a great, but overloaded public safety force. Use local businesses as much as possible!
5	Car prowls and thievery seem to have an affect on businesses. i don't have any great ideas to be attractive other than keeping things clean and fresh, which we seem to do a pretty good job of!
6	High quality high density housing with effective transportation from housing areas to transportation
7	see prior comment: clean it up
8	Need to assure potential businesses that the community will support them. I've heard many have been reluctant over the years because they think military families make the majority of their purchases on base.
9	Provide a safe environment for shoppers and make the area attractive for shoppers. Restaurants with outside seating in the summer and nice music in some areas. Give small business low rent for a time and do your best to get people into the area, so they can spend their money.
10	clean it up
11	People are the key to successful businesses, ie shoppers and folks who live nearby or in the town center
12	Low regulation, low taxes, flexible building standards and zoning.....otherwise get out of the way.
13	Ruston Point seems to attract residents and businesses to give a sense of community. Maybe look at some of their strategies.
14	Sorry I thought this survey was for a pleasant place, not for more businesses.
15	Visibility and access
16	Need a Nordstrom rack in town center
17	Not sure how it's done, but watched Portland, Oregon do it in several neighborhoods
18	We need to find anchors to attract like development
19	We need a women's depth. Store, Kohl's

- 20 You need to provide a space people want to go first. Lakewood needs to spend money in order to make it more desirable. These new concepts will help a lot (ex: downtown tscoma and downtown university place) they did some major revamping and the look amazing and people want to go there
- 21 Again, don't charge me fix traffic/education capacity impacts brought on by a developer's lined pockets.
- 22 Control rent for businesses, too many lose their space because the rents are not affordable
- 23 Address homelessness and make the city safer for those who own businesses and other properties
- 24 Lower Taxes
- 25 I feel like I have to get in my car and drive from point A to point B even though they are in complete walking distance...
- 26 Don't put residences in downtown. It complicates things. Look at data for crime rates.

Advertise to JBLM!!! Most military families rarely leave the installation for retail. What would attract them? Make them part of the community? What retail needs are not being met on post?

Get a good anchor- like Kohl's.

- 27 Streamline planning/ permit process
- 28 I think planned action SEPA can support development, and making sure public infrastructure is complete and attractive can really help.
- 29 Lower tax and licensing for downtown businesses initially.
- 30 city does its part with continued recognition of unique Lakewood community culture
- 31 Better schools- now say that fifteen times!
- 32 encourage developers to redevelop larger pieces of land that currently have outdated and low quality retail/office building, rather than trying to bring new small businesses into those low quality buildings
- 33 More people living and working in Downtown. Density is a key, along with pedestrian/bike friendly access to businesses within mixed use buildings.
- 34 Get rid of the homeless in the area. fix the streets.
- 35 Open an IMAX. Later restaurant closing times. Take a page out of pt. Ruston and Renton landing.
- 36 Improved schools.
- 37 Allow buildings up to six stories
- 38 Some incentive for commercial building owners to modify renovate or replace existing storefronts/commercial space in downtown. 100th near Gravelly and Gravelly between 100th and Bridgeport have been dead for years.
- 39 Add a new grocery store so that someone opening a new business can grab lunch without having to wait for 20 minutes in line in the only store in town which is extremely busy ALL THE TIME!

A new grocery store is also a new business and we don't have enough!!

- 40 Make it appealing and with good incentives for working with elders and military.
- 41 Put in a trader Joe's, also a nice restaurant - not a chain where people can get a good meal and mingle, maybe a town square area as well.
- 42 Make sure businesses provide goods/services that will meet the needs of our citizens. Has our population been surveyed?

Keep in mind the historic values/properties Lakewood has to offer.

Don't forget our growing senior population.

- 43 Create areas that have central shopping with perimeter traffic flow. Make it safe for the pedestrian
- 44 The physical layout of businesses in Towne Center is confusing to a newcomer, like there were just scattered around. Any way to centralize them, maybe have a focal point in the center?
- 45 Several. Later
- 46 Work with landowners to develop incentives to attract restaurants such as no rent (just operating expenses, utilities, property taxes, etc.) for a period (until profitable), property tax relief for a period, B&O tax relief until profitable, etc.
- 47 Bring businesses that the community needs and the customers will come. Take a survey of your citizens.
- 48 Control over saturation of the area and all improvements must be able to handle the increase in traffic volume
- 49 Clean attractive low crime safe - how to get this don't know

50	I think once the public sees that Lakewood is making these changes, it will invite/attract businesses.
51	I don't have any.
52	Plenty of police protection, no vagrancy, no smoking and a clean concept for all people.
53	Wheel and deal. Offer them massive initial tax breaks and take advantage of us being the southern end of the Sound Transit line. Anyone wanting to commute south for work from Seattle or Kent would have one easy mass transit commute. Can we cut a deal with a prospective companies employees to make that initially super cheap or free?
	Consider opening up the currently underground Ponce De Leon Creek that runs under the Towne Center and make it a centerpiece for the new downtown. How great would that be to have cafes and bike paths overlooking running water under the shade of oak trees??
54	Many businesses have left Lakewood: WHY? No tax incentives or too high taxes and/or zoning problems and/or other restrictions?? Examine city's structure re business.
55	lower taxes
56	Stress that we have lower taxes than Tacoma, are business friendly, and don't offer the hellish commute to Seattle
57	I think the above covers what I think is important.
58	Support of local businesses, free advertisement for mom&pop shops, discounted rent for local shops over chains
59	Allow some tax incentives to business that might develop here, like 10 exemption from business taxes, and maybe reduced permit fees.
60	Lots. Later
61	All for it! Go after some of the bigger restaurants. Everything in lakewood is the same fast food and burger places. We leave to go eat out almost all of the time. Plus new businesses have no parking
62	Why don't we become a hub for the growing craft brewing and distilling. What about indoor farming.
63	I think some tax incentives for development for the small scale business owners would be great, as it would encourage those owners to risk capital to help create a sense of space and place. Plus small boutiques and restaurants add that sense of interest and community that large box or chain retail and restaurants simply cannot provide.
64	Keep taxes and rent costs down for businesses too be able to afford establishing new businesses in Lakewood.
65	Work on the roads clean up garbage and work on the homeless problem and more police for security and to cut down on crime
66	For one thing, make it easier to set up a business in Lakewood. The rules, regulations, and hoops one has to jump through at this point are ridiculous. Years ago when I opened my small, 750 square-foot business, I had to have 7 or 8 different inspections for my tiny office, which I was remodeling. Simplify the process and businesses will come.
67	At one time, prestigious names such as Weyerhaeuser and Clapping and famous names such as Linda Evans and Yannie were associated with Lakewood. This distinguished Lakewood from other nearby communities. My friends were in awe when I told them I saw Linda Evans jogging along Gravelly Lake or Yannie's tour bus leaving Madeira. This gave the appearance that Lakewood was THE place to live. It would be great to regain those glory days.
68	Either tear down or fix up the mess where QFC used to be. It's an eyesore and a blight on the city.
69	tax incentives for new business.
70	Attract hipster businesses and artisans by renovating the dilapidating housing
71	Open roads and parking that allow residents to access these businesses.
72	People need a reason to shop in stores instead of on-line. Entertainment, events, festivals, farmer's markets, etc. all bring people to the city center to participate and to shop.
73	Keep rents reasonable and promote small startups
74	bring in better stores so someone would want to shop here maybe TraderJoes would be a good addition, the nicer stores might bring nicer people less crime
75	Help make sure the homeless and crime is controlled, work to get multiple new restaurants to open near motor Ave close to the same time so it becomes a destination
76	Market to other communities to get shoppers here, especially if we were ever able to attract higher end stores and restaurants.
77	Lakewood is looked at as a poor. low-class area with a population of service members and low income. Until you upgrade the offerings of places to live and the bldgs. in and around the Towne Centre are upgraded this will continue to be the thought from surrounding communities. I like what has been done so far and I hope to see continued improvement.
78	Good streets, attractiveness and public safety.
79	Provide incentives so businesses will take a risk on Lakewood — many businesses who come here fail
80	Provide tax breaks to businesses. Infrastructure needs to be modern, people friendly, traffic needs to flow freely, unlike now there are too many stop lights.
81	Keep rental and tax rates competitive
82	develop a plan and get residents that can afford to do the purchases/proposed activities to keep a business there

	while providing affordable activities for all residents
83	Lower taxes, accessible locations with streetside fronts.
84	If you make the area attractive, safe, conducive to congregation, the businesses will come.
85	No opinion
86	make it easier. Ahabada bing (might be spelled wrong) had to leave because it could not jump through Lakewood's hoops, or at least that is the perception.
87	This sounds like gentrification - does this plan consider who is living here?
88	Well, we always shop in Tacoma because the cannabis store is there and it's right by the Winco, so we end up grocery shopping there instead of Lakewood. We usually see something else that interests us while we're out, too. If people weren't having to regularly go shop in other towns, maybe they would spend more money close to home?
89	Clean up the homeless
90	Take out walmart and Amazon and smaller businesses will survive.
91	Get rid of Washington State social services.
92	People have always been afraid of Lakewood because the entrances in have never had any Curb appeal. Now that we have the new lakewood signs it helps. Tree lines and lit street with sidewalks will be a big benefit to giving our city a better feel. If we have the heart of the city with what has been proposed can only help to give us a better reputation.
93	I think safety is a top priority.
94	Tax breaks, Incentives, Programs that support the local small business.
95	Again, make the area feel safe and inviting by removing homeless congregations, and policing areas of crime. Seely lake apartments for example are a hotbed of criminal activity, as are some areas inside the Towne Center. More security is needed, along with cameras to monitor more effectively.
96	Convert abandoned structures like CVS into non-profit clearinghouses. Identify a region for office building construction. Allow office-based business into the Industrial Park.
97	As long as they don't have a large footprint, the amenities and infrastructures should attract small businesses.
98	More name brand stores and a touch local.
	To much turn over with small businesses.
99	Overcoming the reputation of being full of crime. That will help
100	Business will come with customers. Dont be like other places and give huge tax breaks and then run out of money. Have to invest in government. Decrease time for planning permits (need more planners), increase safety (more community resource officers), increase livability (more code officers), increase outreach to business (improve economic development office staff)
101	Address the homeless population
102	Finding the right partners who are willing to take a risk and make an investment -- using the "if you build it, they will come" mentality to see new and innovative ideas brought to Lakewood.

Q11. The Draft Plan proposes policies about achieving high quality housing in Downtown - tell us what you think:

1	Biggest need is for apartments for low income families with rent control. That will lessen the homeless problems in the future. Too many people are being driven out of current apartments with rate increases and can't find new places to rent.
2	Please make sure there is parking for housing. Don't make the mistake Seattle or the Proctor district has- please require sufficient parking for the residents.
3	High amenities mean high rents. That's not being realistic about the diversity of housing needs in Lakewood. Celebrate that diversity with affordable, safe housing .
4	Just keep it all "downtown" and quit prostituting to the builders to put 10 houses per acre.
5	Go for it!
6	Jamming apartments into a commercial area might work for Seattle but will ruin the residential neighborhood feel of Lakewood and generate huge amounts of traffic in its core. Government likes these developments which then depend upon government services.....it's called dependency.
7	This all sounds wonderful, the greatest concern when developing this is traffic flow and parking. I would think each multifamily development should have sufficient parking in their own footprint (i.e. underground parking).
8	people don't live downtown, the go to shop, have an ice cream cone, and visit friends. houses and apartments will only make it more crowded
9	I profoundly prefer good quality density to sprawl. Point Ruston and Foss Waterway seem to indicate the efficacy of the strategy
10	Gentrification. ...it eventually backfires and no one ends up being able to afford living there
11	lots of people mean a lot more cars and traffic thru the downtown area, do we have a robust enough public

	transport system to handle more people? will new housing have attached parking? Will we have enough security/police presence to maintain safety levels?
12	Lakewood Ice Arena
13	This is terrible. Do NOT increase density. We are dense. What I love about Lakewood will be crushed if it takes this on. We are at capacity. Be patient. The efforts to regulate rental housing conditions will pay off. There are several developments coming up.
	Please do not do this.
14	Regulatory flexibility should be in use and density, but should not compromise on the look and feel of the public realm.
15	If you do the first three, and create some public space, the last one will happen
16	There still needs to be much more help for our homeless problem.
17	Have a Lakewood Marathon. High density mix of retail/ residential.
18	This is the true key to the plan, get dense, get away from the automobile centric development. Live and play where you work.
19	A bunch of downtown gentrified apartments will be an eyesore
20	Multi family housing units need to be limited. Lakewood does not need more large apartment complexes.
21	Housing should be limited to a few specific types with architectural guidelines that will create an identifiable character and not lead to a haphazard pattern of development.
22	The housing offered should be proportional to the need. Has Lakewood done a study like Tacoma? https://www.tacomahousing.net/sites/default/files/Estimated%20Housing%20Need%20By%20Income%20Groups%202010-2030%20Final%202016-11-30.pdf
23	All these new families need a new grocery store!!! Or Two!
24	Neighborhoods that are complete with residence, shopping and entertainment capabilities.
25	All of these strong agreements are based on the assumption that existing land will be put to use. Down town has a sprawl to it, arrange that better rather than destroy interesting mid century homes.
26	Family friendly city
27	Re think
28	I don't think this area can handle the increase in multi family dwellings
29	See prior note regarding single family homes
30	I can't imagine diverse housing options in downtown area. I can see multi family or quality apartments.
31	No more multi-family apartments; focus on medium density owner occupied residential units to include townhomes and condos. No more traditional apartment complexes -- we have more than enough of those in our city.
32	Our housing is adequate as is. The housing we do have needs to be upgraded to respectable standards. Lakewood has a poor department enforcing the law on upkeep to housing and apartment living. Garbage, old vehicles sitting, tall weeds, etc. Needs to be enforced before we think of new housing, etc.
33	We already have plenty of affordable housing and apartments and too many trailer parks and motels turned expended stay. It's ok to focus on mid to upper range housing in the downtown area. I'd make an exception for added SHAG or senior housing. maybe patriots landing would want to partner with us for a small satellite to there well run facility in Dupont?
34	There is a giant, unused parking lot on the east side of the mall...how about developing it??
35	If this is all code words for "bring in more low-income rental housing" I don't support that, but it's crazy that we are so near a train to Seattle and there are so few condos. Multifamily housing is great if it includes condos and all income ranges; if this is an effort to get more low-income housing, the last census data I saw showed that we have a higher percentage of this already compared to Tacoma.
36	The area in thr heart of downtown, would be best to not have a ton of housing because then it can be centralized location for everything else. I would prefer not having new residences with in a quartet mile of town center. That still would promote actvuty
37	Improve near downtown housing centered on elementary schools, Establish a light bus rapid flow to serve these areas
38	You will make a mess of downtown if you bring in too much housing. Too congested and transits already panhandle. Close proximity is a good place but not in the Towne center Area.
39	Yesss - Asian night market like they do Orange County/Southern California.
40	We have quite a bit of multi family but GOOD, unique multi family (such as condos, which foster more home ownership than simply apartments) would be preferable. We want to increase ownership and investment rather than transitory dwellers.
41	Retirees and Senior Citizens would welcome housing for all ages, abilities and income. Lakewood needs to step up for providing availability to all.

42	I think working on the crime and homelessness would help more we already have lots of housing options maybe improvement for the poor areas
43	The eyesore mentioned above could be developed as multi-use similar to UP has where Whole Foods is, apartments, businesses and shops.
44	Get Rid of the RIP It has killed more good rental housing than it has the Trashy appartments
45	we don't need housing in that area already have homes with all price ranges nearby, if you bring in apartment buildings the nice houses go away and again no one will go this area
46	JBLM has incorporated housing within walking distance of "center" of base. Cars not needed. Shopping and entertainment within walking distance
47	I would love to see an indoor recreational space brought in. With Washington's weather it makes sense to either offer something that can go from outdoor to indoor space (retractable roof/walls) or simply indoor. Look at STAR Center but with more options to open it up on great weather days.
48	And make sure rental properties are well maintained as we increase singl family dwellings.
49	Not so sure about housing downtown...
50	Need a plan to support the homeless.
51	Agree with all of the above.
52	We do not need more housing. Our schools are struggling as is. Address this first before we attract more children. Get schools right should be the first concern.
53	Density is great but the infrastructure has to be there- how will our roads support high density multi family home when we have stoplights every 800 feet?
54	Build housing like ruston way. Build up. Have business on bottom and living on the top
55	These are not important right now. Start charging a out of state move in tax to those who move here from other states, which will slower population growth and the money can be used toward development.
56	Oh yes, let's create a slum downtown.
57	Higher end communities are needed on that side of Lakewood. Less low income, section 8, and sliding scale. We need a senior living facility.
58	Although I support the military, I do not believe that they are the kind of folks that should be attracted to the area. From personal experience many do not have a connection to the local community due to their temporary stationing, and therefore they are much less inclined to feel ownership or responsibility for the improvement of the area. Most are just passing through and treat their homes and neighborhoods with little care or pride.
59	Residential areas need to be separate from commercial so that commercial can have maximum flexibility.
60	Please come when it is safe and affordable. Tacoma is putting in this tiny apartments for large prices-DO NOT BE TACOMA. It is unclear what is meant by diverse? Diverse different sizes or mixing low income with high income- unsure if that really works
61	The only concern with adding housing is there is a desire to keep it from getting run-down or over time becoming an eye sore. If homes/multi-family is going to be developed downtown I'd like to see it done in a way where it will remain well maintained and high performing. If we're trying to draw people into the downtown, then we want our housing to reflect a high-quality of life and high standards.

Q12. Strategies to achieve the intent of the policies include: adopting new zoning and design standards, ensuring that landscaping and heights transition well from the center to abutting neighborhoods, encouraging housing and service providers to address the needs of the homeless, finding opportunities for transitional housing to address health and shelter needs of the homeless, and ensuring all developments are designed to help neighbors get to know each other and to have eyes on the street to create a sense of place and improve safety. What are your ideas for strategies to include high quality housing in Downtown Lakewood?

1	High quality housing sounds like big \$\$\$\$. Those with the least income would benefit from living within walking distance of their basic needs. Please consider the least of these. Those with \$\$ can afford a car and gas. Low income, this is a luxury for them these days.
2	Tear down Village at Seeley Lake.
3	Single family homes
4	Yes, be realistic! 12 says it all!
5	I dont want to live near a homeless shelter or I would move to Seattle
6	First, you would have to explain to folks what being neighborly means...
7	I sympathize with the homeless but don't want Lakewood turned into what Seattle has become with tent cities popping up all over.
8	This has to be done with care. I am a nice person but would be uncomfortable shopping next to a homeless shelter.
9	Have to address the problem of homelessness and substandard housing, while attracting higher income. Guess that means mixed housing stock with strong partnership agreements, landlord tenant cooperative compacts and provision of services to those in need
10	It doesn't belong there! Perhaps in areas like Tillicum when redevelopment occurs there given the location along American Lake
11	Sammamish Plateau might give you some ideas how to capture high quality interest. I do think stricter regulations for homeless and transitional housing is necessary.
12	no ideas, either have a town, or a housing village.
13	Housing complexes high enough to give views above the trees and the sprawl
14	Place housing capabilities on top or aside existing commercial facilities. NO MULTIFAMILY DWELLINGS
15	This is difficult as you want it to remain safe and not a hang out of crime and drug use. Possibly putting apartments above shipping or outlet center. Similar to idea at point ruston
16	Vagrants are not royalty, they are criminals. Stop facilitating their behavior by giving them anything. You need shelter, work for it.
17	we need great street lighting
18	I think that an apartment building that offers affordable studio and 1 bedroom apartments for low income or minimum wage workers should be built to inspire everyone to live a productive life and know that they can earn a sense of independence even if they are only qualified for the most menial of jobs.
19	Don't have it.
20	Unsure
21	You mention buffers to surrounding residential. That's fine as long as they don't wall off the downtown from the neighborhoods. Place a strong emphasis on making the boundary between the surrounding neighborhoods permeable to pedestrians. Don't really care about permeability for cars.
22	The transition zone is really important where bigger buildings abut sfds, or where traffic is all sharing the smaller-capacity roads. Do design standards so that building entries are obvious and residents have to encounter each other. Require common open space provision and make private open space optional.
23	Get our homeless and panhandling issues under control.
24	parking
25	Make it kinda upscale
26	redevelop some of the low quality, outdated retail spaces in the Colonial Center area into mixed use housing and retail spaces
27	Getting neighbors to join in volunteer efforts that improve their specific neighborhoods will do wonders for getting them to talk and socialize with each other. Doing improvements, such as rain gardens, that improve their storm drainage but also add natural, green environment to otherwise concrete landscape, as a neighborhood group can galvanize neighborhoods.
28	Mixed neighborhoods, with single family, duplexes, triplexes, and a limited number of apartments.
29	The downtown is not the place to encourage density of social service providers, transitional housing, shelters and other uses that will negate incentives for businesses to locate in the downtown.

30	Im not sure I have an opinion
31	Housing above retail and entertainment businesses. Condominiums.
32	Nothing to add
33	Sidewalks so the elderly can walk to town and stay in their houses
34	Don't do it
35	More high quality restaurants, entertainment, family events, year-round and multi-day (at least one evening and one weekend day farmers' market with seating area, stage, entertainment, no porta-potties, outdoor hand washing stations
36	The dupont city and rustin point are great examples
37	I don;t agree with adding multi unit dwellings in this area. I think we can add homeless shelters but not multi unit. Takes away from the charm of Lakewood
38	can't think of anything
39	?
40	Just as long these places don't look tacky. Maybe bottom floor to include retail and underground parking.
41	See above -- medium density, owner occupied residential units. Transitional housing should not be in downtown, there are areas better suited for that like other communities given we have more than our fair share.
42	No homelessness! People who are homeless can work for the city of Lakewood to clean up our streets and highways. In return, portable no drug housing. Police find drugs, they are out! Illegal aliens, out. Turn them over to ICE. Homelessness = drugs 85%. Check the stats in Seattle. I can show you homeless in Seattle that is almost all drug infested. Are you willing to see for yourself?
43	We don't need to add homeless housing. We're a bedroom community. Add better transportation for homeless, more referral services, educational options at our two local colleges. That's all a great way to show compassion, but please get the panhandling and homeless moved along out of our neighborhoods if they are choosing to not take advantage of the opportunities afforded them. Let's be a Kirkland or a Gig Harbor, not an Olympia or Seattle.
44	All of the above!
45	I'm a little unclear how you expect to attract jobs and also want to attract the homeless. There's too much loaded into this one statement to comment on it intelligently but having condos that would attract people desirable to attractive businesses would be good.
46	I really haven't thought about it.
47	Limit housing downtown to high rent units. Increase pockets of affordable high density housing served by effective rapid transit (light rapid minibus). Better security.
48	Perhaps city subsidized housing for lower income folks.
49	I feel these are good goals but again not right downtown, but in close proximity
50	Build some higher end apartments near the Sounder station, that way at least you can take the train to Seattle or south to transfer and go on to Portland, and build some cool housing projects near South Korean Way where there's some cool businesses to walk to
51	We need to work (perhaps as part of our legislative agenda) to tweak the condo laws to reduce statutory liability on such development in order to encourage more of it, thereby increasing opportunities for home ownership and longer term investment in the community (along with other housing choices, single family, cottage style, etc.).
52	This has to be addressed. It is imperative that services be provided for the homeless.
53	I think helping the poorer neighborhoods improve would be better
54	If you want to attract high-quality people, unfortunately the truth is, you have to have high-end housing. Low-end housing will not attract the kind of people that will keep Lakewood vibrant and attractive.
55	I don't have any additional ideas. Just please don't turn Downtown into a mini Seattle with homeless camps and human excrement on the sidewalks.
56	not to destroy the attractive areas that we have such as the Gravelly lk dr areas.
57	Any apartment type units should include include both guaranteed low income housing, senior housing, (Like SHAG) and increasing the police budget to guarantee the safety of both old and transition population.
58	Some of the most visible areas of town (Bridgeport Ave, etc.) need some serious sprucing up if you want to bring high-quality development. Need street trees, landscaping, give grants to force property owners to paint and clean up their property - then penalize if they don't do it.
59	Get Rid of RIP and enforce existing building codes to get owners to clean up propertied and maintain a good looking and functional rental
60	absolutely no drug injection sites in Lakewood ever! instead provide money for St Clare to help with the drug addicted people just giving them a homeis not the solution they need access to drug rehab programs
61	Would not put homeless housing in downtown area --
62	Continue working with developers to modernize the housing market from old single family homes to more dense multi family
63	Safety first. Walk ability and discourage this as a location for homeless and loiterers.

64	Condos built on some of the empty acreage. I have also always thought housing above retail is a great idea.
65	Tax incentives to build.
66	This above statement seems to conflict with the question. Not sure mixed use — housing with retail — will work in Lakewood
67	I don't think housing in the downtown core will be successful. It works in other communities where there is an attraction (other than downtown) like water or a view. This downtown core has neither.
68	Make high quality housing truly high quality, kinda like Tacoma has done along the water front. Especially at first. Don't make it a bunch of low income housing that won't attract high wage earners. Low income housing can come later, once the downtown core is established as a nice place to live.
69	Increased density and mixed use buildings downtown will, as you say, create a centralized hub of our community. I think shelters and food donation sites will also help our community support one another and provide volunteer opportunities for us. High and low income housing will create a well mixed community that will thrive downtown.
70	Establish standards & enforce them to insure housing and associated areas are clean, safe, attractive,& neat so that blight does not set in and residents are proud of the areas they live in & pleasing to others.
71	unsure
72	Not a big supporter- would rather see other options like redevelopment of neighborhoods and integration into existing sites that can handle traffic
73	Wider streets, parking garages, mixed use property with shopping on the ground level and housing above. Sidewalk cafes and coffee shops
74	Build up not out.
75	There is no such thing as affordable high quality housing, so stop conning people.
76	Don't do it.
77	I like the idea of housing with businesses on lower level.
78	allow large scale condos and housing - new - modern - hip
79	No apartment complexes.
80	Town parades are a great way to get people together. All throughout the year.
	Yes we need a homeless shelter, food bank, maybe even a place for just homeless to pick up things like hygiene products and provide showers.
81	There is tons of room in and around Western State hospital. Make use of it. Many of the homeless migrate from there when released anyway.
82	I have never heard of this being done elsewhere. How will kids get through retail to school, where can people walk their dog, wash their car, get a tan...
83	If you want families living in downtown you can't have a homeless shelter in downtown. Families with small kids do not feel safe. Invest a little more money in LASA to develop transitional housing. Invest in some worker retraining. If you want a livable downtown then make decent size apartments for families that are reasonable with parking garages. Creating a sense of place needs intervention by community resource officers--block watches, meetings, etc,. But you can't just reassign staffing from other parts of the city. Yes you want strong downtown but you have to keep up the rest of the city or no one will move here. All the mismatched buildings are not attractive either. Reface so everything matches. Look into moving the bus stop (can it go into the Hobby Lobby parking lot?). This place draws people and brings people into downtown that are not there for good. (Talk to the Lakewood Playhouse about the problems-they had to put up cameras for worker safety because of the bus stop).
84	Making sure that families are moving in and not a bunch of drug dealers and people living off the system. It's important to break free of the reputation Lakewood has
85	I'd be concerned about offering transitional housing and housing for homeless in the downtown. I don't believe this reflects the vision we have for downtown. I believe it is important and needs to be addressed, but it shouldn't be the cornerstone of the downtown residential development. Design standards should be more strict to regulate building design so there is better cohesion in appearance.

Q13. What do you think of these draft policies to help create parks and fun places? (Part I)

1	Restaurants that are not chains! People who attend culture events don't want Applebee's.
2	Don't center your park area at LTC; think about adding it to the Motor Inn area.
3	I don't see Lakewood with a night life. Lots of problems go with this idea
4	I think these significant cultural hubs of Seattle and Tacoma leave the market for the arts pretty well saturated – in fact, arts community well vibrant struggles with sustainability. I think liquid would be well served to see itself as a bedroom community to the larger economic engines in the region and align itself for very high quality lifestyle opportunities for a bedroom community with high quality access to the larger economic and cultural centers
5	keep it "downtown"
6	Always thought City Hall should have included a coffee shop and covered seating on the backdeck. Now be sure some of these amenities are located in a family oriented area between city hall and theaters
7	You don't need all this stuff in downtown. We already have great parks and a cultural/performance at Clover Votek. Stop trying to reinvent the wheel.
8	Including a walking path with a few workout stations (stretching stations, pullups, etc.) might be worthwhile.
9	we already have theaters, we have two high schools for games, etc. plus several smaller parks for little league, etc.
10	All good
11	Do not plan for anything that does not have existing space to accommodate it.
12	Nothing will work unless the homeless/criminals/mentally ill/drug addicted vagrants are addressed
13	I think these will follow population. Also, if the orgs that are already there (or nearby) are promoted then others will come.
14	Absolutely, need the day and night activities to make this a vibrant core. As the area becomes more dense, the public spaces become even more important.
15	Spaces and events have to be safe and well maintained.
16	Sounds great! Just add a new grocery store!
17	don't forget what is already planned for the venue at Ft. Steilacoom Park. Duplication might inversely affect both areas.
18	Agree totally.
19	Maybe locate things like the spray park somewhere else? The red gates are bad enough, we don't want Disneyland
20	Short sighted
21	We have plenty of parks around to spread out various outside activities. Put money into these instead of creating more to spread the money out thinly
22	Yes, yes, yes!
23	Lets not go overboard with tax payer money. This is the bottom line of what you are talking about.
24	Think about a policy to increase living outside of downtown wit ease of access to downtown. see comments above/
25	Yes - but if you're gonna promote the theater the City needs to look for grant to renovate the building and give it some windows. Hold outdoor improv festivals
26	Definitely a spray park how about an indoor one for all year round fun
27	Yes, Yes, Yes. We will need to do needed public investment as a catalyst to making these things happen.
28	We definitely need places to attract younger individuals and have safe environments for them to participate in activities.
29	Improve the roads would help
30	My concern about all this is, where is the money coming from? If any of this increases property taxes or other taxes, then I'm very concerned. The taxes are already very high in our area. Any additional taxes will not attract businesses, but drive them away; it may also drive away residents.
31	Improved street lighting would help.
32	all improvements should be done with 'attracting new business' as the goal
33	Right now the city center is a "drive-through". There is nothing to encourage people to come and spend a couple of hours. Need a city park with amenities, parking, a tree-lined walking trail, etc.
34	We just build a new public area for the arts in Ft Steilacoom Park. How will that fit in?
35	food truck space is a great idea, also open the market up to more times the few of your residents who work can't ever go to the market
36	Increasing house in density needs increased public spaces and places to park for the local business
37	You seem to focus heavily on attracting businesses. Focus on attracting residents by creating decent housing and recreation/community space, and the businesses will come. I think it is easier for residents to attract businesses than it is for businesses to attract residents.
38	MORE PARKS!!!

39	Agree with all of the above.
40	Great in principle but concern about traffic and infrastructure and cost- will this involve incentives?
41	Once again a waste of city spending
42	YES - the down town Olympia area has much of what your aiming for... would LOVE to see that here in Lakewood.
43	Must be "safe and inviting" though. Many places are definitely not that way now in the day, and even more so in the evening. At this point I can not think of any area in Lakewood I would feel safe walking around in at night.
44	Covered earlier.
45	Great idea but how will the city keep them safe at night?
46	Building things like a spray park, or even water fountains that people can play in/splash in during hot summer days, is a great way to draw people to a central location and is something that is lacking in Lakewood.

Q14. What do you think of these draft policies to help create parks and other fun places? (Part II)

1	So many parks are being taken over by less desirable elements. What would you do to ensure that something new wouldn't turn into a new hangout for them?
2	Also accessible for those who are elderly or have limited walking capacity or get cold in the rain!! Walking is great from June to October. Less so the rest of the year.
3	How about paving Ft Steilacoom park roads
4	I love the idea of accessibility for pedestrians and bicycle riders, but I would like to also see a focus on accessibility for people with disabilities
5	Lakewood is too sprawling to ever be a true walking town.
6	Just how much is this "development" going to cost the rest of us? Sounds like this is a jobs programs for those in Lakewood's community development department!
7	we already have Ft. Steilacoom Park for gathering, games, etc. as well a two AmericanLake parks.
8	POOL
9	Quality park definition? Retaining natural beauty or Lakewood Council's Pave-It vision?
10	Of course the vagrant issues need addressed first
11	All development should be required to incorporate open space and connections for bikes and peds.
12	I think the Towne Center as a whole will have to be made more walkable for things like public plazas to actually be used
13	Open space in business parks will only support those businesses directly related to them, generally. Better to have public open space and parks that are readily understood to be for the general public.
14	More of easy access and out of your car area where walking is a big plus.
15	I envision a core community that will attract businesses as well as new residents and visitors.
16	Short sighted
17	The phrase "acquire lands" scares me from who and where
18	Your last policy was the best. When you say invest, what is the taxpayer going to pay?
19	Man it would be nice to have a place to have a cup of coffee or lunch with friends on the water, considering how much water front we have in this city. Currently, this is a privilege only afforded those who live in the country club.
20	It's great to add amenities for pedestrians and bike riders, but without residents you won't have pedestrians. And no one wants to come to a place with bad traffic. So please don't cut down on the number of traffic lanes.
21	Build some basketball courts and add an edible garden
22	The accessible issue is really one of zone and location (and perhaps the "connection" streets and paths that can be created). We are a bit stuck with what we have (e.g., Gravelly Lake Drive), but we certainly could see a "Trolley" or similar running between the Colonial Center District and the Town Center District that might encourage folks to stay out of their cars as they go "back and forth" to enjoy amenities.
23	Already have bike areas
24	Bike Lanes in the road are a big help separating bikes, cars and walkers
25	don't be Seattle where the person with a car can't even afford to come into the city much less park, making a bike path throughout the area is fine but don't devote the whole area to cyclists the real cyclists have many areas to go and this area is not long enough to attract them. If the green loop is a walking/cycling path that is fine again hard to figure out what you are doing with that space. We need to have areas for people to walk for sure in our area more recreational areas make healthier citizens
26	Bringing people to the central core from other parts of the city requires parking area tha are low cost or free ans good sidewalks
27	Pedestrians fine...Lakewood is a bad place to ride a bike! Hate the Share roads! Not safe.
28	MORE PARKS THAT ARE ACCESSIBLE!!!
29	Connecting trails for pedestrians and bicycle use through the area.
30	and clean up, repurpose and tear down unused buildings. If they have been vacant for a period of time they need

to go.

- 31 Great in principle but potentially problematic within current infrastructure- how can you engage homeless into planning? They will be most impacted- what do they think?
- 32 Lakewood is just not the town to dump money into making pedestrian-friendly. Everyone works somewhere else and we are really spread out.
- 33 But don't make it totally difficult for cars.
- 34 Great plan.
- 35 What would the city of the future look like. Get there.

Q15. Strategies to achieve the intent of the policies include: Programming and hosting events (e.g., farmers market, parades, holiday festivals or Oktoberfest) in Downtown public spaces, exploring grants and other funding opportunities to help create parks, allowing development to reduce their onsite open space obligations and support development of parks in downtown, designating a cultural district to celebrate art and attract funding, and creating streetscapes and trails that link the Downtown area to parks and recreational facilities outside of Downtown. What are your ideas for strategies for parks and other fun places in Downtown?

- 1 If it means more noise, then I'm against. We need more quiet places.
- 2 How about more public access to the lakes? Kayaking, etc. Skate and bike parks for younger people.
- 3 See above. Consider our weather when centering everything g outside
- 4 I think Fort Steilacoom Park is an amazing asset for Lakewood. I would like to see energy in expanding quality access to American Lake. Lake Steilacoom is more immediate to the downtown center, but may lack the capacity to sustain highly increased use that might come with a destination public park
- 5 Put your godforsaken pavilion and spray park there
- 6 Use the existing park system
- 7 Connect a bike/walking path (not on the side of the road) to other trails, possibly to Fort Steilacoom Park, etc.
- 8 one small pleasant park, with grass and benches, would do in my opinion.
- 9 POOL
- 10 Create a perimeter walking/bicycle path around the Lakewood Towne Center.

Uncovering the Ponce de Leon Creek and placing benches, grassy slopes etc. along its shores.

Providing bicycles for rent, scooters etc for encouragement for people to experience the pathways
- 11 Splash park, outdoor movie theater, pool and sports area for families, disk golf, outdoor gym (equipment is fixed I have seen in other cities) adult swings family park area.
- 12 n/a
- 13 Create a calendar of annual events that can grow in popularity and size over time, things people can learn to count on and look forward to attending
- 14 Buy my property
- 15 Lakewood Ice Arena and water park.
- 16 That would be great but why aren't there linking sidewalks for kids to walk or ride to schools safely?
- 17 Ideas for fun places is the easy part. Finding additional resources is the real challenge. Council needs to be focusing on this
- 18 none really
- 19 safety and control of illegal substance use
- 20 Last City Marathon.
- 21 Prioritize walkability and trails connecting existing parks. I think something like a cultural district won't make much of an impact if Lakewood remains an unpleasant place to walk
- 22 By all means, include public art. That has been very successful in downtown Puyallup. A multi-purpose meeting building located adjacent to a festival street and central park would likely be a great asset as well.
- 23 Open an IMAX.
- 24 Keep them well lighted. They also must be safe, not hang outs for drug users and homeless.
- 25 Build a community center with flexibility to serve all ages and types of events. Ensure Wi-Fi hot spot(s) throughout the downtown area.
- 26 New grocery store!
- 27 I like the ease of access and trail links to designated area.
- 28 Same thing

29	Not too much park, maybe locate it in Ft. Steilacoom Park where there's more land
30	Several
31	See prior comments
32	There is a big empty space in the middle of the lakewood mall - what is it going to be used for? More concrete and grey buildings
33	i like all of the ideas listed above, farmers market, holiday festival, Octoberfest, creating parks.
34	Walking and biking trails would be good.
35	Steilacoom Park is fine with me. Farmers market in Lakewood is good. You have already in place all strategies.
36	We need a large auditorium!! Get the third-rate business out of the old Safeway bldg. and transform it into an auditorium!
37	The cultural district can also celebrate the international nature of the community and its rich history and home to a number of interesting historic figures.
38	Agree.
39	They don't have to be big spaces. A random swing set here or bench on some grass here for there. Also community gardens
40	Consider pop density. If high density popdown town urban life is squeezed out.
41	develop parks in population centers out from Downtown because when there is an event in downtown it is hard to get to the sites due to bad traffic flow.
42	Covered and Indoor facility is greatly needed. Our weather is not good for planning all outdoor activities. INVEST in BUILDING a large indoor event center than can be open and host activities for all
43	Do stuff like Asian Night Market! http://www.ocnightmarket.com/ Collaborate with the CP School District and do some new things that get students out and into the town center. Host a high school job fair, hold celebration events for high school division championship wins, host 5ks between CP/Cabrini/Town Center area, host 5ks from Seely Lake to the Mall, Find ways to show add community character to the downtown by hanging pole banners with local school mascots/colors, host a City bbq in the parking lot of the vacant QFC, host cultural festivals or work with the Rotary/Kiwanis/Booster Clubs to host outdoor fundraisers like a crab feed or pig roast.
44	Yes, all of those things. Intensify uses and festivals in the downtown areas to incentivize people traffic (and investment/spending) which may well encourage small business owners to take a risk and open shop.
45	Improve safety
46	Like the idea of farmers market.
47	I like everything except the trails to outside parks and fun places. I am concerned about opportunistic crimes and encampments.
48	Playgrounds for kids that are easily accessable. The skateboard park is great but only for a small segment of community. Swings, slides, tennis courts, handball wall basketball hoops etc. would go a long way towards improving outdoor activity.
49	only as it relates to attracting new business. you need the business base to afford the improvements
50	I love farmers markets. I have been to the Christmas tree lighting with my Boy Scout troop for the last three years. This is a good idea if it's near to Towne Center.
51	Need to ensure that all city parks are non-smoking and have available Wi-Fi. Both of those will encourage young people to linger longer in the parks.
52	Keep them Green, reduce paving. Water feature attracts families. Stroller friendly
53	think that is a great idea! Do this one first! If you can bring people into the area for something nice and fun they are likely to buy gas here, eat at a restuarant etc.
54	The city should consider some small parks in the north area, possibly buying a few old homes and making a small kids park as long as the homeless can be controlled, also kids play areas in the motor Ave and city park
55	Veterans Day and Memorial Day Celebrations. Fourth of July fireworks and celebration. Concerts and/or movies in the park.
56	See earlier comments
57	I think we are good with what we have.

58	Like all ideas mentioned above
59	This will need fully supported through city administration. To be successful, this needs to be fully staffed to promote and create these ideas. There are unlimited ideas, food trucks, outdoor ice rink, art festivals, taste of lakewood...
60	I really like the idea of creating streetscapes and trails to connect downtown to other parks in Lakewood (such as Ft. Steilacoom).
61	We need kid parks, play fields, trees. Someplace that the nyanza park community can get to by walking or biking. We also need enforcement so that vagrant people don't scare families away.
62	Farmers market, etc. should be located in areas that do not foster traffic congestion, etc. The current site for the farmers market is not well located and unsafe for pedestrians crossing a busy road. I feel a more outlying site would be larger and more conducive to attracting people.
63	unsure
64	Are we becoming Tacoma? Please let form follow function - it sounds like Lakewood will become a destination. Work with businesses to provide community events and gathering spaces- outdoor/ sidewalk cafes, pop up activities, civic discussion gatherings, community improvement projects, neighbor to neighbor assistance, etc.
65	There need to be more public parks where you can go fishing. We're called Lakewood but there are almost no good public fishing spots.
66	There are several parks in Lakewood. Stop destroying more places for more people.
67	Would LOVE to see farmers market and more public spaces
68	Battle of the food trucks, Local art by artistes placed around the city. Like statues. Spray park with play ground. We NEED more trash cans with recycle options on the street
69	Stronger social media presence would help. There are many sites, groups, and pages which attract and include Lakewood residents.
70	Covered earlier. But I'd love to see a busker location a la Pike Place Market. Maybe a Small Business Expo to spotlight area mom&pop offerings. Or a Resource Fair that would tell residents services that are available they may not have known about.
71	I saw review what other cities have done. Leavenworth draws in thousands in December and all they do is turn on some lights. Many towns have first Friday walk. 1st Friday of every month all stores get together and other vendors are brought in with food. Bring in experts -ex. gardening, plants- for Saturday events. You need quality stores. All the empty buildings are an eye sore. Have concerts in the Motor Ave section (bonus- parking is across the street).
72	A large playground for children. Basketball courts.
73	These all sound like great ideas. Having a park downtown where community events can be scheduled and hosted is a big draw and would bring people downtown.

Q16. The amount and quality of public space are two defining features of Downtowns. Streets are the primary public spaces in Downtowns, in some cases accounting for almost half the land depending on the size and layout of the street grid. Lakewood currently lacks an urban street grid typical of a Downtown or the types of active public spaces that attract people to come Downtown. The proposed street concepts support improvements to existing streets, the expansion of the public street network, the green street loop, a better pedestrian experience and connectivity, and urban mixed-use infill development. What do you think of these draft policies to help promote transportation?

1	You can have parking and pedestrian access. Look at downtown Olympia. They charge a remedial amount for street parking. They also have the FREE Dash. It's fantastic.
2	need sidewalks for pedestrians
3	Teach pedestrians to take responsibility for themselves and look both ways
4	Provide a streetcar or inter-connector transit for locals to connect easily with downtown
5	Forget about walking; people drive and the millions spent on sidewalks which are seldom used has been a colossal waste of money given the condition of our road network which carries 99.9% of our transportation needs. You are creating a problem by upping density in the city's core which then leads to more government intervention in the form on now needed public transportation.....to the exclusion of autos.....as the solution to the problem you created.
6	a real need to control the cars.
7	Bus transit centers need bathrooms.

8	Do not impact current traffic flows through Lakewood. There is sufficient room for sidewalks. Bike lanes are not an effective loss of vehicle capacity and are not safe-this is from personal, decades long experience.
9	A safe way for residents to get to downtown in the first place needs to be established.
10	There is traffic congestion
	Also, remember the weather.
11	I don't think the plans as shown really show how you are going to meet these goals. This is the most incomplete part of the plan.
12	I agree with improving the downtown area, but do not forget the other dangerous areas in Lakewood, I live on Interlaaken and frequently almost get hit walking my dog, getting my mail, or just trying to pull out of my driveway.
13	It is a very hard thing to transition from auto-centric development to multi-modal focused development, but it will be even harder to get there if the focus of cars is allowed to continue to dominate. This issue will also be very difficult from a public opinion standpoint, since most of our citizens still have an auto-centric mindset.
14	Policies should discourage automobile use through the central portion of the downtown by creating parking on periphery and have pedestrian only new streets in the core.
15	Definitely geared toward biking, walking and trolley type services.
16	to balance transit, it would be wise to have shuttle type vehicles for transportation within the district that connect with regular suburban transit. It's difficult to use transit in Lakewood.
17	Keep traffic on the perimeter to ensure safe walking and biking.
18	I'm handicapped, access for bikes and pedestrians doesn't address my issues
19	Sidewalks
20	Needs total Rethink
21	I don't understand the "Green Loop"
22	I'm all for walkers, joggers and bike riders.
23	Sorry, it all sounds like code for 'cut down on the number of traffic lanes' obviously walking and bike space is great.
24	There should be foot paths/side walks. However, there is not a need for excessive concrete.
25	We can do this, but we must not favor one concept too much at the expense of others.
26	"Downtown" is pretty spread out and difficult for seniors to get from one area to another. I don't know what the solution is but know that when "construction zones" are finished, it should open up more sidewalks to traverse to different areas. Maybe a city "trolley" to circulate the downtown area during daytime, free of course.
27	Please don't ignore the connectivity TO/FROM the Downtown - that's the whole point. It's one thing to stripe the Lakewood Mall center full of green bike lanes but if there's nothing but sharrows surrounding the mall that its kinda a waste of money. 59th St would be perfect for green lanes because the traffic volumes are lower, same with 100th, similar with Gravelly Lake between Motor Ave and the Library. A shared use path on one side along Lakewood Dr/Bridgeport (between the Community Center/YMCA to the Mall) would be a dramatic but well-worth it vision and investment.
28	Pedestrian orientation in building, site and street design and development is essential. Otherwise, we remain a "drive to the box" Town Center. Create that "Stickiness" necessary for folks to want to hang out after the movies (e.g., the Central Park), take in a restaurant, or sit outside in a summery, visually distinct area where there are opportunities to enjoy public art, music, water features (for families with kids, etc.).
29	Most people use cars due to the weather
30	I am handicapped so pedestrian and bike traffic is not important.
31	Remember, Lakewood also has an aging community that needs safer ways to get around. Sometimes driving is the only reality for us.
32	Homeless issue can cause families Not to walk. We need to address this as part of the Safe downtown development plan
33	again we all still drive cars if I am going grocery shopping I am not going to be on my bike so if you make it so we can't park Ill just drive to other neighborhoods to do my shopping
34	Don't impact traffic flow in the process
35	The community needs to evaluate it's bicycling and transit policies. Share-Roads not safe! If we want to encourage bicycling we should have bike lanes. Transit bus routes should be reviewed as well.
36	Nice ideas here; implementation could be tough.
37	The stop lights in the proposed downtown core are inefficient and seem to have been designed with no thought of traffic flow. In addition, there are too many. To slow speeds on Gravelly, add curb appeal, medians, then patrol it...once a person gets a ticket, behaviors will begin to change.
38	make downtown walkable and safe for bikes and provide parking around the cbd that doesn't detract from the use and access for all. Grid streets would be great. Keep an eye on how public transportation accesses and will access downtown in the future.
39	More 'no vehicular traffic' area.

40	Hope it works
41	How many people live close enough to downtown to walk there? Not many. How many want to bring groceries home on a bike? Even less.
42	Who wants to live in a over developed crap hole?
43	Make sure enough parking provided.
44	I would welcome a region that was car-free and locate businesses and retail that would benefit from that arrangement.
45	People love their cars, keep transit buses off already small streets. Need parking garage. Keep parking free! Move bus stop across 100th (it is still close enough).
46	it'd be great to make the Lakewood Towne Center more like University Village or Uptown Gig Harbor, where people park and walk between stores/businesses instead of having to get in their cars to drive one minute to the next location. Right now the streets through Towne Center feel more like they are used by through traffic and not always people getting to a destination in the center.

Q17. As centers like Downtown become more successful, attract more jobs, residents, and customers, the management of parking becomes key. Downtowns may have more public parking, on-street parking, and shared parking options that tend to be located either behind or to the side of buildings or in parking structures. In addition, downtowns typically have a greater level of parking management such as time limits, parking pricing, permits, and other management strategies to ensure that parking is being used efficiently. What do you think of these draft policies to manage parking? (Part I)

1	People should not have to pay for street parking, as it is now. If need be, multi-story parking garages should be built. Do not allow parking for events to overflow to residential areas so that homeowner won't be forced out of their own parking spaces.
2	It rains 7 months a year, and we have an aging population. People don't give up there cars when limited- they just find alternative, obnoxious places to park elsewhere.
3	This discounts our considerable disabled population
4	how far do I have to carry groceries
5	You cannot force people to give up their cars. They will go elsewhere
6	We've already seen the "solution" to the problem you created with central core development. Typically instead of minimum parking in development there has become a push for maximum parking which by design limits its availability to autos and forces people to use public transport. The problem is that people won't give up their cars, will go somewhere that is car friendly regarding parking with the result that your vaunted "downtown" strangles itself as people go elsewhere less intensively developed. Unintended consequences
7	Again, require businesses to provide underground parking. Charging for parking is not going to be a welcome addition. Check out Mill Creek north of Bothell. They have an interesting design.
8	parking in the rear of properties would not be good for those with handicap issues.
9	Limiting parking will not encourage alt transportation, it will force people elsewhere. You've heard of Seattle? Multilevel parking has it's own issues, but is a consideration
10	Meter the closest spots as a revenue source. Check out the map of Wiregrass mall in Wesley Chapel, FL. It works beautifully.
11	Parking should be market driven and should not be provided free on public rights of way.
12	Do not restrict parking to the point where it becomes a hassle. Walking is more challenging for the elderly which we have many and for people who have temporary or permanent disabilities. Also for people running quick errands, they will avoid a hassle.
13	Very hard to densify to levels needed if parking lots continue to consume large chunks of land. May want to consider structured parking only vs. surface lots. Lakewood has a difficult starting point because it is prototypical auto-centric development in its current state. Going to a ped-bike focused community will be hard. Look for examples like Redmond, and even Portland, on how to get more ped-bike focused and successful downtowns.
14	Encourage parking space tucked under new multistory buildings.
15	As a resident who lives a block away from the border of your expanded gravelly Lake retail residential plan, I would like to make sure nobody uses my street for parking
16	all very good ideas.
17	Do NOT turn into Seattle with diminishing parking options. With poor transit service, automobiles are generally the only option. Keep parking accessible for the elderly and moms/dads with young children in tow.
18	Multi level parking would be a must.
19	See last answer. I'm handicapped. Haven't seen this access mentioned once.
20	Too early to consider

21	As someone who can not walk very far, handicap parking is a must. Lakewood has an aging population you can not forget.
22	Get the cars out of Lakewood. We need more walking!!
23	So I work next to Mercer Avenue in Seattle where it can take a half hour or more to go a mile, because people are supposed to use alternative transportation .. but they don't. It's a hallucination to think you'll cut down on the number of cars by wishing it so through pretty designs.
24	Underground parking sructures or parking on businesses roofs. Since we are planning now, implementing this would save on time and space in the future. Plus having parking on roofs, isn't done much in the area so it would make us more unique than the near by cities.
	Please keep street parking free, having to pay for parking will detract people from utilizing the plans you are making
25	Too much thought today. More later
26	A "downtown trolley" type of transportation might help this issue.
27	I like what they did at the University Place town center with the built parking garage. Please, no more wide expansive surface lots at the Mall. Build parking up if you have to.
28	We really need to incentivize the creation/building of structured parking behind the old Gottchalk building, which would provide more and better parking for the entertainment "zone" (theater, Barnes and Noble, and possible the new Central Park area), and would help eliminate the need for the sprawling asphalt. Again, think Kent Station and/or University Village.
29	People won't come if parking is difficult
30	Do not make it more difficult to travel by car! Do not take away parking spaces or begin charging for parking spaces. I intentionally avoid Seattle, like the plague because of the ridiculous lack of parking and expensive parking if you can even find any.
31	Use of Rear parking is good but we will need Booth. Some businesses do not have Rear access and will never have it.. Example. the strip area where Habitat and Moon Rise are now
32	Again not going to carry cartons of milk to my car, people already steal the grocery carts they certainly will if you take away their parking. Maybe a small multilevel parking area to handle big events is a good idea but not for the general person again you need to monitor these parking areas for safety
33	We are not seattle or Tacoma, if you make it hard to park people won't come....
34	Proctor District development has increased retail footprint without adding much parking. Not conducive to attracting patrons.
35	Making has to work for this driving to Lakewood not the other way around. If parking is a nightmare people won't come to the venue or businesses
36	These are all excellent plans. Someone must have read a planning book.
37	As noted earlier, I would like to see areas in the proposed Concept solely for pedestrians and bicycle use.
38	Huge challenge- I imagine this will involve fees to park and parking garages
39	Depends on the layout and traffic. We shouldn't have the busy streets backed up because everyone is trying to turn into the same alley to park.
40	Whatever you do make it look pretty. Flowers, attractive lighting etc
41	Tacoma has a huge parking structure located near the Dome, Amtrak, bus station and light rail into town. Might be time for Lkw to consider trolleys, pedicabs, courtesy shuttles, etc.
42	Confining parking sounds like a recipe to invite criminal activity.
43	People love their cars. If you are attracting families-they often come with a lot of stuff and they wont come if they cant drive in. Having sidewalks are key. Do not limit time (unless it became a problem) telling people they can only stay 2 hours turns off shoppers who want to shop, eat and see a movie. One problem cities face is employee parking-employees take up parking slots (just look at Safeway-the employees take up a bunch of slots in front). Have employees park next to street.Build a parking garage. DO NOT CHARGE. Charging means you have to hire enforcement officers and legal has to add staff it is not worth it. You will gain fans if you give people enough parking slots for free with no time restrictions. If this became a problem in future you can always slowly tighten restrictions. If you do not let people drive/park to downtown how do they get there? Public Transportation? Family with a stroller/diaper bag and 3 kids do not want to take a bus. And the public transportation system does not have the greatest infrastructure in Lakewood. It is not just being on a bus in downtown it is taking a bus from your house where you might have to walk a few blocks (with no sidewalks) to get to the bus sop and the bus only comes 1x an hour. Sorry we love our cars and just because the City says it wants us to walk doesn't mean we will. You can always direct traffic to other locations (but where are those) during really busy events.

Q18. What do you think of these draft policies to manage parking? (Part II)

1	I would also favor high density parking area a bit off site with free or heavily subsidized shuttle or link to be downtown area
2	Define "shared parking agreements"
3	Every one of these options is going to cost you for parking that is now free. I avoid areas in Tacoma and Seattle (I won't even go there) because of these types of "non-solutions"
4	No parking in the downtown area should be restricted unless it is under buildings. It should all be open to public use.
5	Of course the homeless will overrun any plan you great unless dealt with
6	For the first policy- how does adding street parking help with beautification?
7	any on-street parking should be timed. these could be accomplished with road diets
8	I do not want residents taking up all of the parking in the shopping area.
9	Multi-modal streets should use more space for sidewalks (10-12 feet), bike lanes (2 ea. 5 foot lanes) than for car parking or through traffic (maybe 2 12' lanes, with left turn pockets as needed)
10	Prefer to not have parking unless for handicap. Just use parking garages and behind retail parking for the healthy population.
11	All good ideas.
12	See part 1
13	Or could the large unused area in the middle of the mall be a parking garage
14	Market not sufficient to support either a private or certainly a public structured parking garage or even underground parking, perhaps in 20 years, maybe longer
15	Looking to the future, your parking sites could be futile if we are to experience high gas prices like our Governor wants us to have. Electric cars will have to have plug ins costing the taxpayer even more money. Unless you have a pot of gold somewhere and the taxpayer don't have to pay, your ideas are nice but not practical in the common sense.
16	Later
17	Work on security people don't go to the town center due to the crime problem
18	Not having shared parking causes a big waste in space
19	Currently expensive parking options do not make economic sense for the local business so having public parking will be the best way to attract and retain business
20	How do you have underground parking in an area tha was once a wetland and still has water collecting sites when there is heavy rain?
21	No further comment.
22	Most people coming here will be driving
23	Trees are nice but there is a limit on how many

Q19. What do you think of these draft policies to support partnerships?

1	Not a lot. There really isn't a "downtown" in Lakewood. Concerned that the proposed idea will just become a place where the homeless and druggies hang out and it become unsafe for anyone to use. It's not the same as places in Seattle, where many people travel on foot because of mass transportation. Not many people will walk to this area, but will drive and then the question of where they are going to park arises.
2	It's great, but I would add "accessible." It's not just the right thing to do, it's the law. Plus, diversity isn't just ethnic background.
3	It is in progress
4	I like everything about the vision statement...theory and practice...but, unless and until Lakewood recognizes that cohesive design is essential, it's just going to be a hodgepodge mess...
5	Agree
6	Safe and inviting
7	it's inclusive of a lot of things!
8	Not much.
9	Inclusive of all
10	Appreciate the focus on both cultural diversity and sustainability.
11	It hits all the appropriate hooah verbiage.
12	Nor much. Need indoor activity center, rains most of the time. How about a walking track & a running track.
13	The inclusion of all aspects a community should have in a central location, especially housing.
14	part of a thriving local economy
15	The aims sound good, not enough specifics to agree to it.
16	"Safe and Inviting". I would like to shop, eat,walk feeling safe doing the latter activities.
17	Safe, good restaurants..not just fast food chains

18	Sounds pretty, but no substance
19	Leaves virtually nothing out and is making Lakewood seem to have it all
20	It has EVERY element one could possibly think of
21	I think they are the right priorities
22	Very little. It's not government's job.
23	Making Lakewood SAFE!
24	Sounds like an ambitious goal.
25	Part of a thriving local economy.
26	I SOUNDS LIKE A SAFE HAPPY PLACE. NOW WE NEED SAFE, HAPPY PEOPLE. MAYBE THIS WILL BRING THEM OUT.
27	Though cumbersome, the statement includes many valuable variables and allows an anchoring to decision making.
28	Fine
29	Ok as long as not attractive for homeless to gather
30	Good core
31	Great goals, it's optimistic
32	The vision wants to bring a sense of identity and community to Lakewood
33	It is inclusive and positive
34	Its fine.
35	sustainability and biking
36	I like it
37	I love it
38	I love it.
39	The capital words in the bullet list make it look like you're trying too hard. It should sell itself without so much manipulation.
	In the paragraph, it's not "warm" enough - the sentences sound strange starting with "Downtown", and it puts too much pressure on the literal word/place "Downtown" (that and one time it's referred to as "The Downtown" and other times as "Downtown").
40	Nothing. It isn't a statement, its a catch-all narrative in an attempt to appeal to everyone. Too long
41	Covers all the bases
42	Safe and inviting
43	Sounds like a pipe dream
44	Safe and Inviting. Designed for People to walk and Bike. Part of a thriving local economy.
45	nice ideals
46	To long
47	I like each of the highlighted points as focus areas.
48	I agree with the vision of what downtown should be
49	Providing a centrally located communities gathering place.
50	It's main emphasis is on giving Lakewood a place that is clearly the "living room" of the city. Right now we live in a house with a bedroom and a garage.
51	That it emphasizes ped/bike safety
52	A source of pride, safe and inviting, where people live, work, meet, shop, and eat
53	It's positive and good. It would be nice to develop a slogan for Lakewood. Not any of the phrases above.
54	active
55	Dreamy
56	Its good
57	the idea of it being designed for people to walk and bike, since there is a lot that's within walking distance for me but it's not currently pleasant to walk
58	it emphasizes a walkable/bikeable living space vs. automobile centric development
59	Streets being walkable and safe
60	I like the series of goals
61	Not bad.
62	It seems broad in scope.
63	Safe and inviting
	Part of a thriving local economy
	Where people go to do fun things

64	Part of Thriving local economy
65	It is a sweet thought but not very realistic.
66	Nothing
67	Good goals
68	I like that it's focused on bringing people together
69	Cultural Diversity and restaurants.
	We need another grocery store!!!!
70	Nothing
71	Safe and inviting
	Thriving local economy
72	where people live, work, meet, shop, and eat
73	appears comprehensive.....but it will depend on the draft plans uniting the ugly Villa Plaza area with the quintessential Lakewood Colonial Center.
74	I like the vision and it is something that Lakewood needs. This would be an ideal place for the farmers market , also.
75	The idea of having a downtown seems like a great idea. Walkable,a nice park, sounds great.
76	The intent of the idea
77	I like that it points out walking, since I don't drive and therefore walk everywhere in the neighborhood of the Towne Center.
78	I like it all!
79	I like how it mentions it being the "heart" of the community.
80	We need green spaces to set and people watch or just be out in the fresh air
81	Community coming together in a safe place.
82	Later
83	Good
84	Its pretty broad but sounds nice.
85	Designed for PEOPLE to WALK and BIKE
86	People oriented
87	good
88	Idealistic. The average person in the vicinity of town center is not a bike rider. Streets leading to the Center go through unsafe neighborhoods. Or else we are too old bike.
89	It embodies what I think of as a Downtown. It sounds like a fun place to be.
90	Use of warm words such as safe
91	it's ok
92	I like that it mentions the HEART of the community, and the statement "where people LIVE, WORK, MEET, SHOP AND EAT."
93	Sounds good
94	Community oriented.
95	Positive and inviting
96	Uh
97	safe, inviting, fun, thriving
98	I like the idea of a park. Where?
99	It's a good start on identifying Lakewood.
100	The first of things mentioned on the list is community. Creating an identity of Lakewood is vital in its growth. People want to be part of something bigger. I also like the mentioning of sustainability and nature. It shows lakewood has the right spectrum of thought on what the future is.
101	One cannot argue with the basic ideas here.
102	Sounds positive.
103	a great summary
104	Everything
105	The statement is ambitious and conveys a lot of energy. I'd want to visit there!
106	A GREAT PLACE!
	The HEART of the COMMUNITY and CIVIC LIFE
	Designed for PEOPLE to WALK
	SAFE and INVITING

Where people of all ages go to do FUN things

Part of a thriving LOCAL ECONOMY

A source of PRIDE and IDENTITY for LAKEWOOD

Where people LIVE, WORK, MEET, SHOP, and EAT

107	The mention of food as long as it's not more Red Robin, chick fil a, etc
108	very optimistic
109	Rich with Cultural Diversity, the heart of the community, Part of a thriving local economy.
110	Safe and inviting
111	My wife and I retired in 2004 and moved to Port St Lucie, Florida. The City Council there built a Civic Center exactly as you stated above. The developer declared bankruptcy. If you go to Google Earth and type in Port St Lucie Civic Center and go to street view you will notice the EMPTY parking lot and no one walking around. This is a city with temperatures in the 80s year around. This will not work here.
112	It demonstrates the many uses for a downtown area.
113	It paints a good picture of what would be an ideal place to live and utilize the city.
114	I like that it is trying to make a heart for downtown. I would love to see a heart everyone knows is the center of town.
115	I like it , it is very broad though
116	Well stated.
117	It is pretty inclusive as to the values and attributes for a vibrant downtown area.
118	Upbeat
119	It's refreshing to know that FINALLY Lakewood is willing to step up and bring a whole new and exciting look to the city! Bring back a restaurant(s) such as the former "Lakewood Terrace"; that was a dining experience!
120	Accurate
121	That is slated to be safe.
122	certainly complete
123	All of the above
124	seem to have touched on many of the ideas a resident would look for.
125	The lack of sidewalks as well as roadsides that are over run by vegetation, for example Meadow Road between Lakewood Blvd. and 95th, make walking a nightmare.
126	The focus on walkability, and on enhancing the community.
127	Not a lot.
128	it sounds nice, but fails to address current problems
129	I like that the city has ambitions to create a centralized business area that is a good revenue source and enjoyable to be at.
130	sounds great
131	The first four sentences are good because they don't entirely misrepresent the reality of how people use/interact with mall area.
132	The heart of the community and civic life.
133	Not much
134	Walkable, diverse, safe & inviting, sustainable and connected to nature
135	Add with w emphasis on LOCAL BUSINESS
136	sounds very update and culturally correct
137	multiple reasons for creating and using a downtown
138	Pride and Identity for Lakewood; where people live, work, meet, shop and eat.
139	It encompasses all that a downtown should offer.
140	It all looks good but I think we could do with less fast food dining in Lakewood in general.
141	SAFE and INVITING, Part of a thriving LOCAL ECONOMY, a source of PRIDE and IDENTITY for LAKEWOOD.
142	Very good
143	It is forward thinking in creating and maintaining economic growth.
144	It's a great idea, a great goal.
145	Concise and yet detailed.
146	It is well thought out and comprehensive and positive
147	The open concept, green areas and walk ability.
148	Makes it sound like a great place to be
149	I like that it is the heart of the community. That it is a source of pride Ann's that it is walkable and bike friendly. Also that it is mixed use.

150	The concept of a center that would be the 'beehive' of safe, vibrant, colorful and interesting activity.
151	This is exactly what I would love to see as a vision statement for our great city.
152	I like all elements. I really like the walking and biking.
153	It is positive and hopeful
154	A bit wordy —but the focus on community gathering space is key.
155	It is not very concise.
156	It's thorough, all the elements are good.
157	I like the goal of being Safe and Inviting and building the economy back up. I like the goal of walking and biking areas as well as being connected to Nature.
158	It offers things for everyone
159	I think Washington in general, including Lakewood need to stop selling out to California development companies. They have destroyed our state since our current governor has taken his position. Washington used to be a beautiful place, now it sucks. I have lived here my whole life and now I want out. Lakewood sucks!
160	Progressive and nostalgic simultaneously
161	Nearly all vision statements seem to have a lot of buzz words but little of any substance. Right up there with "world peace", eradication of hunger, and a cure for the common cold. Unfortunately, you can't make a silk purse out of a sow's ear, and putting lipstick on a pig is an exercise in futility.
162	I like the feel of the statement and agree with what it says.
163	where people live work meet shop eat
164	Community, diversity, pride.
165	Very clear on what you want to do.
166	The goal for community.
167	Making a real community within a community
168	It's fun!
169	Appears it is already that way
170	Love it!
171	"Safe and inviting"
172	Certainly it's upbeat, but it sounds like an ad campaign for Disneyland.
173	<p>A GREAT PLACE!</p> <p>The HEART of the COMMUNITY and CIVIC LIFE</p> <p>Designed for PEOPLE to WALK</p> <p>SAFE and INVITING</p> <p>Where people of all ages go to do FUN things</p> <p>Part of a thriving LOCAL ECONOMY</p> <p>A source of PRIDE and IDENTITY for LAKEWOOD</p> <p>Where people LIVE, WORK, MEET, SHOP, and EAT</p>
174	I like how it all seems positive. And a clean beautiful place.
175	It sounds inviting and exciting
176	<p>The description of Safe and Inviting are key.</p> <p>Designed to walk and bike are manifestation of Safe and Inviting. Cultural Diversity is a great statement that I would like to see brought out.</p>
177	It makes Lakewood sound like community, not just a place to run errands
178	I like that it talks about cohesion and creating a centralized gathering place where people will come to experience a sense of community outside their individual neighborhoods.
179	Positive, family driven, like idea of having a true downtown

Q20. Is there anything else you'd like to share with the City?

1	Please publicize activity around the project on a public Facebook page, so that many, many people will see it as it's happening. And listen to their input, please.
2	Go to small towns that already have an active downtown and see how they do it.
3	The new "Lakewood" signs on Bridgeport and South Tacoma Way are a disappointment: boring, drab, and nothing indicating international" or "regional", since they do not even incorporate glacial till stones that are the foundation of this area. (If it is a question of supply and cost, residents are surely glad to donate a pile!)
	Please adapt the Lakewood Town Center archway design. It is light-hearted, with an international flair. This combo pole motif of vertical, horizontal, and diagonal units can be customized for archways, signs, banners, lights, planters; colors chosen for specific location and events: Blue American Lake, green Steilacoom, lime Festival Street, yellow parking. This motif is a fun marketing gem, already recognizable as "Lakewood". Use it!
4	Don't forget your small businesses. Add legislation to force absentee business property owners to maintain useful space for rental businesses to prosper.
5	it would be wise for Lakewood to see itself as a bedroom community and as such to become the best sort of bedroom community. The key elements to such a strategy would be the development of high density housing, the type of businesses that cater to the resident, and very efficient transportation from population density center to transportation hubs for the larger economic region
6	I think the tendency for the "city" to ignore the tax payers is the biggest problem. We said no to the \$7 mil city hall, no to the new cop shop, no to the pavilion, no to the RIP. I guess I should say no to Lakewood.
7	Thanks~!
8	Stick to doing what government is supposed to do and get out of private business. If you're bureaucrats are so inclined to "invest" with taxpayer money in development I suggest they get out of their protected environment and jump in the deep end of the pool, invest their personal fortunes and see how long they can float.
9	allow the marijuana businesses in Lakewood
10	not now
11	I would be willing to contribute to a go fund me type startup for community pool
12	I was in City Hall a couple of weeks ago, the flags on display are in very sorry shape---please get them cleaned or replaced
13	Homeless homeless. homeless
14	Good Luck
15	Thank you for taking these issues into consideration.
16	Please, please, please do not add high-density housing. Think of the impact on community, schools, and what adding more rental housing does for property values.
17	I often go to Lakewood Towne Center over the Tacoma Mall because of the ease of parking and the quick in and out, do not limit parking too much.
18	Better schools, honor our veterans, more sidewalks to provide a walkable city. Go upscale!
19	It is an ambitious plan, but it will have it's challenges. I believe the hardest part will be convincing people that to be truly successful the focus must be on density, multi-modal development and greatly reducing the emphasis on cars. If that focus is compromised to any significant degree, the plan will have difficulty. I would also say, don't forget vertical in the plan. Conditions are available in Lakewood to take buildings to greater than 5 story height without much additional foundation costs. 10 story and even taller should be considered.
20	A vibrant city is only part of the community. Strong schools also impact a vibrant city. Clover Park School district lacks when compared to Steilacoom and University Place Schools. Community input has failed to change the direction of the CPSD. I think that more pressure should be brought to bear on the school district to involve the community and involve the schools in community events.
21	This is a very long survey. Mostly people aren't going to have the time or patience to complete it. You might consider integrating some sort of survey feature in the Lakewood app and send out micro/pulse surveys
22	Im sure you got my point. I look forward to seeing what happens. I love living in Lakewood!!
23	I always have something to say, but I've spent nearly an hour going through this survey. I'll be watching.
24	Let's make Lakewood vibrant again.
25	Sidewalks were all getting older, we shouldn't be walking in the streets
26	Not at this time
27	If Amtrak is going thru zlakewood, it needs to have regularly scheduled stops on all of its trains (Coast Statliughy, Cascades, etc.) at Lakewood Station, Tillicum and DuPont
28	Very concerned about increased traffic in the area. It is already a high volume area and I don't think this area can handle the increase. Also, what is the plan to pay for all of the improvements?
29	Make the process transparent.
30	Yes, your grandeur thoughts of making Lakewood a grand city are very nice but not to the taxpayer. In this whole questionnaire, not one mention of who is going to pay for this. Shame on you.

31	Please empower the local civic groups and churches to help those in need, as you already have with the two local food banks. I want my tax dollars spent making my town more usable for my family and to make my property values go up. There are many examples in the region of what happens when towns pour large amounts of money into homeless services. I would challenge the city to find even one example of a city that did that in our state that had a positive outcome for the taxpayers.
32	Make sure you have people involved with aesthetic taste! Some of the current city design is blah or of little value.
33	No
34	Open up another egress street from the west-side city other than Steilacoom Blvd. particularly when there is construction going on. This is a traffic gridlock right now.
35	Good work. This is a critical project, but make the Plan both visionary AND achievable (even in small bites over time). We have to show progress and soon.
36	I'm so looking forward to seeing many improvements to the City. I feel that over the years the city of Lakewood has stagnated and there is so much potential for making it a vital city again.
37	Support law enforcement and lessen crime start addressing the homeless problem fix the roads help improve the poorer neighborhoods
38	My parents moved to Lakewood from North Carolina because of a job at Camp Murray in August 1957. Other than a brief stint as a military wife, I've lived here ever since and experienced Lakewood transitioning from a place of character and prestige to unsightly neighborhoods/homes and apathy. I hope implementing this plan changes this. Also, I've read anything about a timeline. When will ground be broken. Plus we need another large grocery store in Downtown besides Safeway.
39	Keep the taxes down. Soon I'll be priced out of my home that my grandfather had built. It's less than 1,000 sq. ft. but the taxes are keeping me from basically maintaining my home such as painting the exterior.
40	The survey is good but it is too long. Where the early questions are thoughtfully answered, the later ones were quickly checked just to get the survey done. Fewer questions would have provided better quality answers throughout.
41	Local Businesses are Lakewood's future. We are Not University place, we are Lakewood. Develop Locally owned businesses that then are vested in our community, our schools and our neighborhoods.
42	make the towne center a safe place to shop, dine and enjoy is your top priority, if not no one will comeback.
43	Thank you for the chance to comment.
44	Not at this time
45	Build up not out. Make downtown a place Lakewood is proud of.
46	No
47	I believe this initial survey should have been simpler; it should have been constructed so as to appeal to more people in our community. Personally, I do not know enough about the proposal, even with what was provided, to adequately provide constructive criticism. Basically, I like the idea of needed change and improvement in the downtown area, but this is nothing new.
48	Give the citizens of Lakewood a job- more than input - organize citizen brigades to build and educate through civic groups, churches, non-profits. Our buyin will come from our participation and ownership of the development
49	I like Lakewood but wish it had more of the hip vibe that Seattle and Tacoma do. Our more conservative policies keeps the younger crowd from wanting to be here, and that is what generates a lot of retail buzz whether you like the hipsters or not.
50	Yeah, Lakewood sucks so clean up before you build up.
51	More opportunities to show off school outreach, job fairs. I'd love to see Lkw take the lead in some critical way.
52	Thanks for working to improve our community.
53	Lakewood has some cool history that has been lost. Reviewing history and bringing back some of the feeling might build community.
54	No

Draft Downtown Plan Open House and Workshop

Meeting Date: March 21, 2018 | Summary Date: April 25, 2018

On March 21, 2018, the Planning Commission hosted an Open House and Workshop to introduce the Draft Downtown Plan and Environmental Impact Statement (EIS), and to encourage participants to review the documents and provide written comments or participate in an online survey through April 16, 2018.

The meeting was advertised on the project website (<https://www.lakewooddowntownplan.org/>) and through email to an interested parties list. It was also referenced on the weekly City Manager's report and posted on social media.

About 40 persons participated in the Open House and Workshop.

Informational boards at an Open House provided an opportunity to review key concepts and write on "sticky notes". A real-time online poll was conducted around the Draft Vision Statement, likes about the Downtown Concepts, and priorities for investments.

Some themes included:

- Improving walkability
- Prioritizing a Modified Gravelly Lake Drive
- Promoting activities Downtown and incorporating the Motor Avenue festival street
- Looking to other successful example downtowns, e.g. Puyallup
- Protecting Colonial District Character and improving design overall

Appendices are included and provide more detailed results, comments, and workshop materials as follows:

Appendix A: Real-time Poll – Slido Results	2
Appendix B: Board Notes	7
Appendix C: Meeting Materials.....	10

Appendix A: Real-time Poll – Slido Results



Lakewood Downtown Plan Open House

Interaction report

20 March 2018 - 23 March 2018, Seattle, United States.



Attendees asked 5 questions
with a total number of 0 like

60% of questions were asked anonymously

60%

What were
the most popular questions?



Anonymous

0



0



Plans for preservation of the historic Colonial Center buildings, notably the Lakewood Theater and preservation of the oak trees around that space.



Pat

0



0



Originally in a program with the city was new it was suggested to have a building allowing a business on the main floor and above that would be a condo home.



Pat

0



0



Lakewood colonial center is the most attractive part of Lakewood but does not belong to the city. How can we keep it a vibrant part of our city



Anonymous

0



0



Is there an option to open the coveted portion of Ponce Deleon Creek?



Anonymous

0



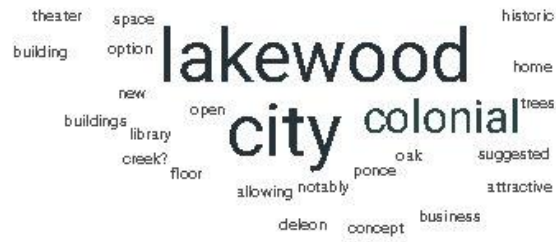
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Would you explain the concept for the area south of the library

What were
the main topics?





Who were
the most influential participants?

P	Pat	2		0	
	Anonymous users	3		0	

Participants sent
59 votes in 3 polls

< 1 / 3 >

What do you like best about the Downtown Vision Statement? (Choose up to 2) 19 23

Downtown is Safe and Inviting 32 %

More Things to Do 26 %

Supports a Thriving Local Economy 16 %

Better Design for Walking and Biking 21 %

Support Cultural Diversity 5 %

Nothing 0 %

Made with ❤️ by

slido

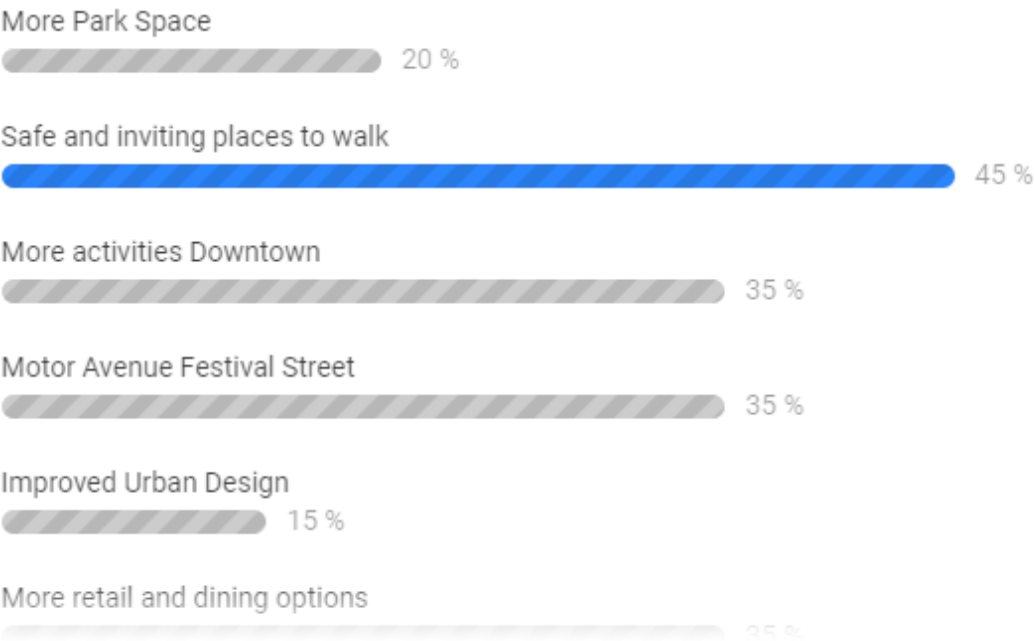
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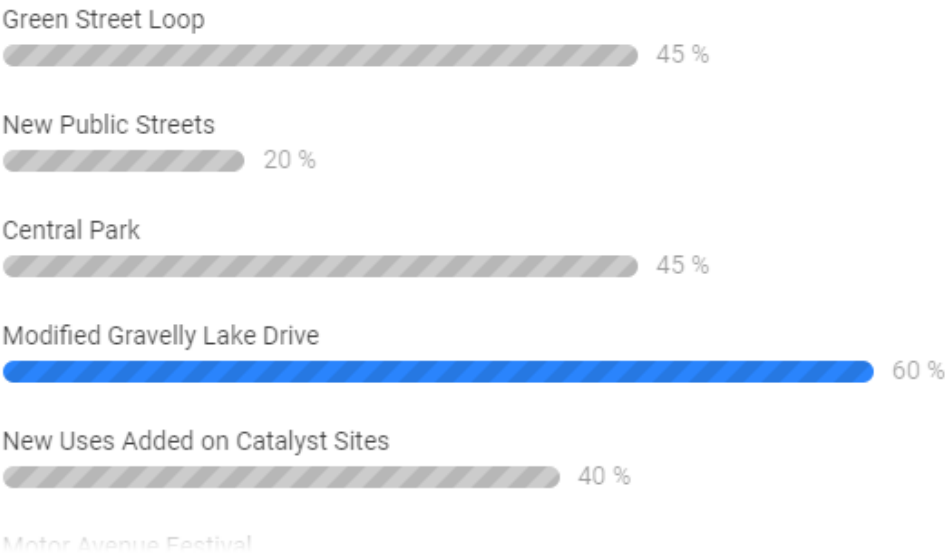
What do you like about the Downtown Plan Concepts? Select up to 2)

20 



What would be your top priority for the following investments? (Choose up to 3)

20 



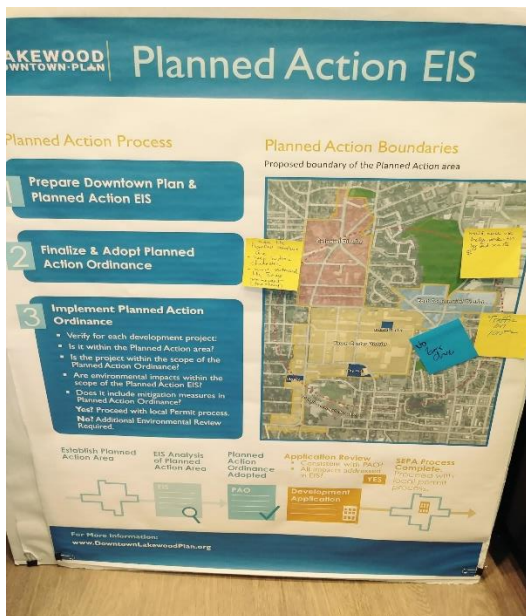
Appendix B: Board Notes

PROJECT OVERVIEW & SCHEDULE

Other ideas from the community

- Transportation connectivity

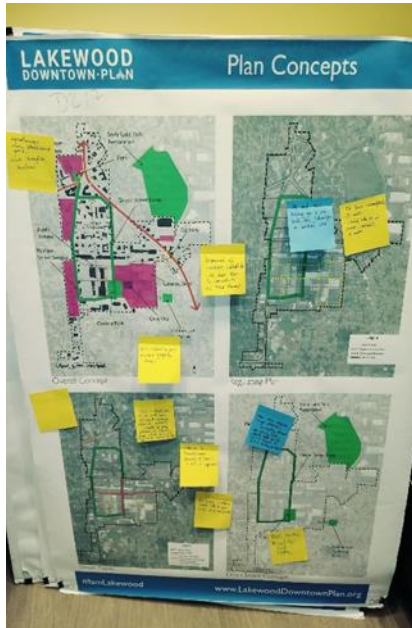
PLANNED ACTION EIS



Planned Action Boundaries

- More like:
 - Puyallup downtown core
 - Keep historic character
 - More restaurants like Terrace restaurant (fine dining)
- Would never use Seely Park – too big and remote
- No zoning changes
- Traffic on 100th

PLAN CONCEPTS



Overall Concept

- Speed bumps near skateboard park
- Cut through on Fairlawn
- Will development increase property taxes?
- Support of increased walkability. Are there plans to connect to the train station?

Regulating Plan

- City and PCLS looking for new life for library in central core
- It's been uncomfortable to walk
- Would like it to be more walkable and safe

Street Types

- PCLS – what can we do with our current library when we move?
- Could be of interest to Clover Park School District
- City and PCLS working on site
- Attention to transportation planning & transit in EIS is important
- No more “Mattress Ranch” like buildings with cows on front

Open Space Concept

- How was hookah lounge allowed?
- Currently allowed by zoning code. Health department would like to close and we are ready for that.
- Park services road in & out (word?)

Appendix C: Meeting Materials

Draft EIS

What is an environmental impact statement?

Purpose of EIS

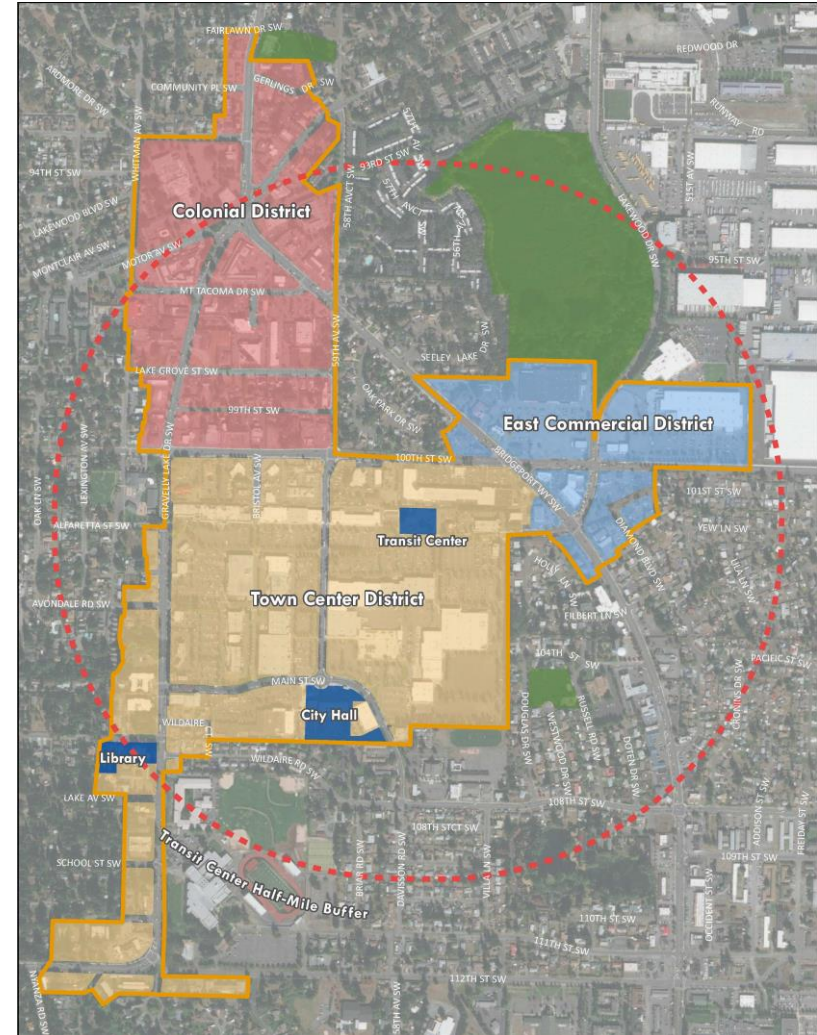
- Consider environmental information in decision-making process
- Solicit feedback from public and government agencies
- Help shape proposals by considering alternatives & mitigation

What is an EIS?

- An informational document developed under State Environmental Policy Act
- An EIS describes:
 - proposed actions
 - alternatives
 - existing conditions
 - impacts that may occur if proposals are implemented
 - mitigation measures to reduce or eliminate adverse impacts
 - potential significant, unavoidable, and adverse impacts.

Draft EIS Environmental Topics

- Natural Environment
- Population, Employment & Housing
- Transportation
- Land Use and Plans & Policies
- Public Services
- Utilities



Alternatives for EIS

The Environmental Impact Statement (EIS) studies three alternatives.

No Action

- Future Baseline under Current Plans

Action Alternative 1

Action Alternative 2

- Final EIS can test a Preferred Alternative in the range



An alternative describes a different means of achieving a proposal. Proposal is to develop plan, zoning, and code changes that help City achieve vision

Comparison of Alternatives

FEATURE	NO ACTION	ACTION ALTERNATIVE 1	ACTION ALTERNATIVE 2
Height	+	+	+
Jobs	+	++	+++
Housing	+	++	+++
Civic Parks	+	++	+++
Transportation Connectivity	+	++	+++
Pedestrian/Bicycle	+	++	+++
Parks / Community Gathering	+	++	+++
Ecosystem – e.g. creek daylighting, menu of stormwater requirements	+	++	++

Legend: + Current Plan – Least Incentives or Investment | ++ Phased/Targeted Investment |

+++ Greatest Investment & Transformation

Land Use

Range of Growth

Alternative	Population	Housing	Jobs
Base Year Units	909	419	5,248
Net Growth			
No Action	990	456	1,667
Action Alternative 1	3,426	1,579	4,147
Action Alternative 2	4,898	2,257	7,369
Total Units 2035			
No Action	1,899	875	6,915
Action Alternative 1	4,336	1,998	9,395
Action Alternative 2	5,807	2,676	12,617

Range of Density

- No Action: 54 du/ac
- Alternative 1: 80 du/ac
- Alternative 2: 100 du/ac

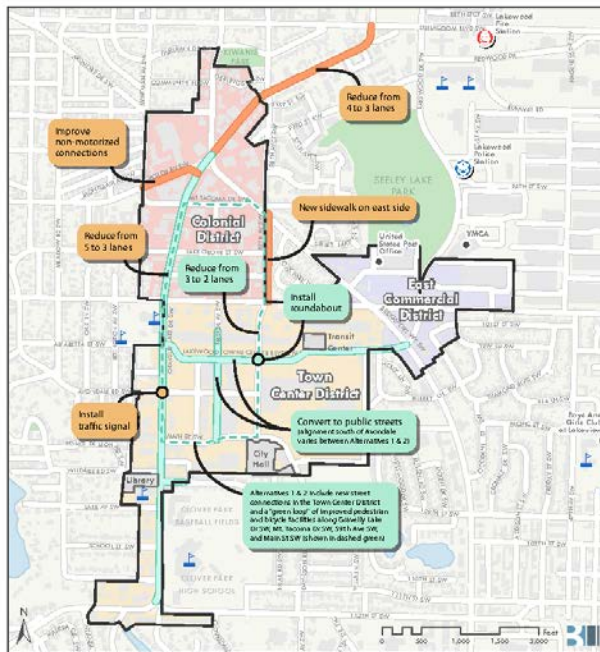
Commercial Square Footage

- Up to 3M sf
- Alternative 1: 50%
- Alternative 2: 95%

Transportation

Six Year TIP + Test Additional Network and Multimodal Improvements

Review Options, Mitigation and Tradeoffs



LEGEND

- All Alternatives
- Action Alternative
- Lakewood CBD Boundary
- Public Facilities
- Parks
- Schools
- Waterbody
- Roads

Improvements	Reduced Vehicle Delay or Improved Auto Mobility	Multi-modal Focus	Traffic Distribution	Recreation or Amenity Value	Independent Implementation
1. Gravelly Lake Drive SW Revised Street Section	No	Yes	No	Yes	No, 3 lanes – requires public streets (#2). Yes, 4 or 5 lanes
2. Conversion of Lakewood Towne Center Blvd and Bristol Ave as public streets. Addition of new street connections to support walkability.	Yes	Yes	Yes	Yes	Yes
3. Lakewood Towne Center Blvd at 59th Ave SW, consider roundabout.	Yes	No	Yes	Yes	Yes
4. Reduce 59th Avenue SW to two lanes, allowing for bicycle facilities	No	Yes	No	Yes	Yes
5. Potential Additional Transportation Mitigation in Table 4.	Yes	No	No	No	No, 108th St SW/ Bridgeport Way SW and 112th St SW/Gravelly Lake Dr SW required with 3-lane Gravelly Lake Drive SW section Yes all others

Parks, Stormwater & Natural Environment

Environment & Stormwater

- Implement City Manuals
- Encourage stormwater facilities that serve multiple features (e.g. recreation)
- Promote water quality measures to protect surface and groundwater

Parks - Central 2-4 Acres and Linear

DRAFT DATE HERE

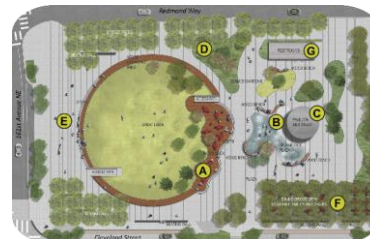
Central Park Case Studies



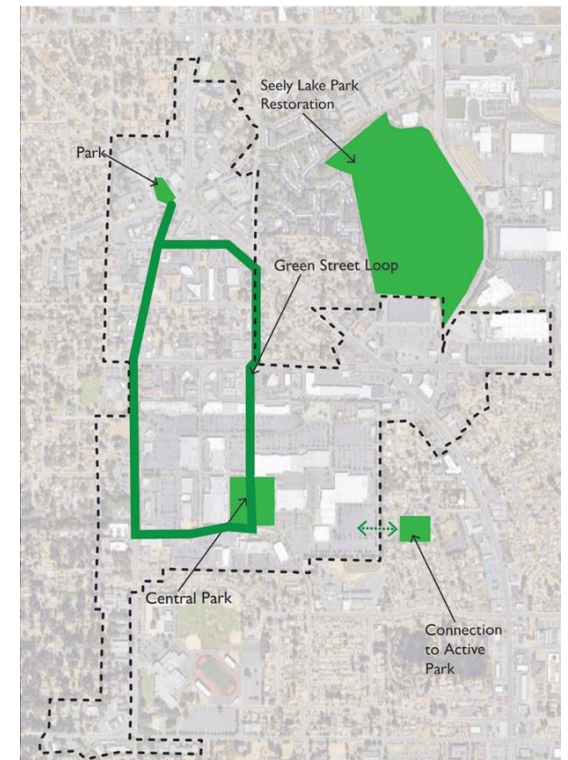
Downtown Puyallup - Pioneer Park - 2 acres



Downtown Burien - 1 acre



Downtown Redmond - 2 Acres, Under Construction



What is a planned action?

- Defines range of future development – type & amount
- Analyzes potential impacts in an associated EIS
- Environmental “homework” completed in advance
- Adopts an ordinance with thresholds & mitigation
- Developers & public have clear rules
- Future development would not need additional SEPA review when consistent with Ordinance
- Proposals still go through permit review
- Can help facilitate private and public investment in the study area

Planned Action Process



Public Engagement in EIS & Planned Action

- Scoping:
 - 21-day comment period in December
- Draft EIS
 - 30-day Draft EIS comment period

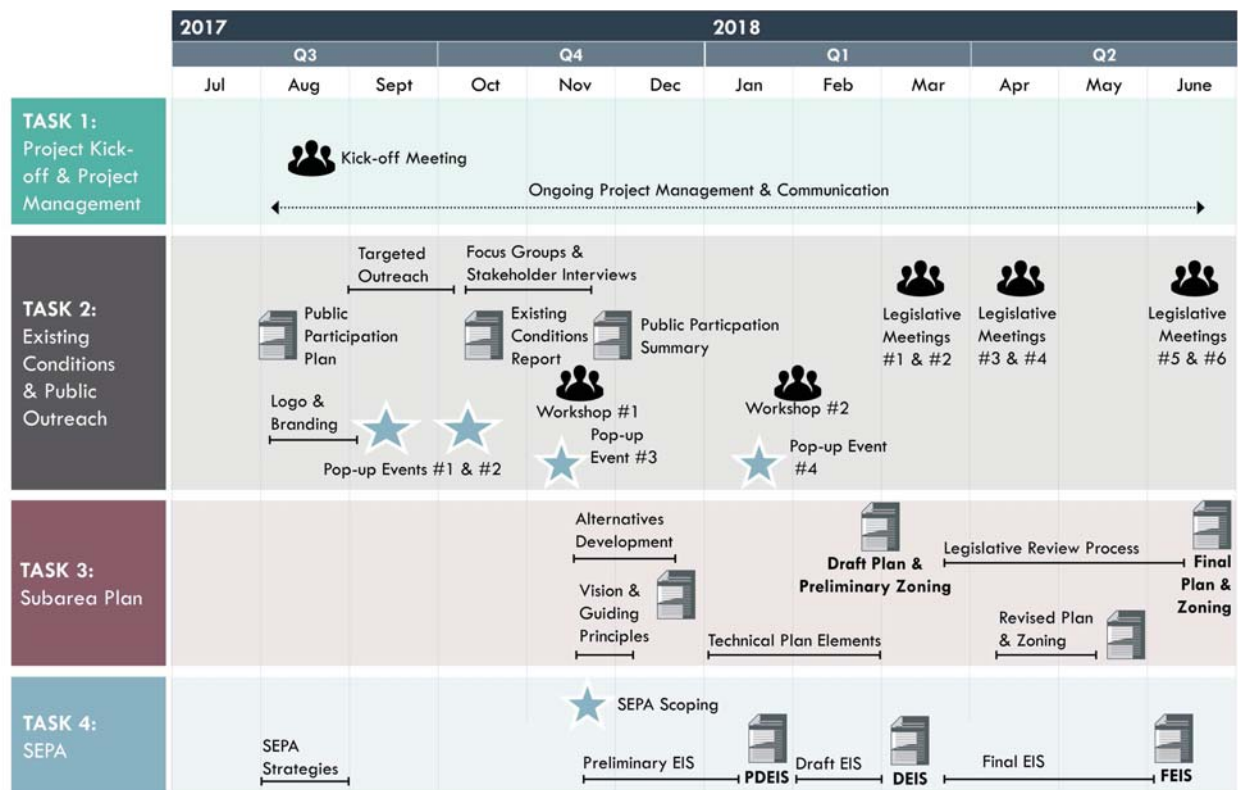
Advertised on project website, published notice, sent to mailing list

PROJECT OVERVIEW

The Lakewood Downtown Plan will build upon past City planning efforts and community input to solidify and strengthen the vision for Downtown, to engage the entire community in the planning process, and develop a realistic and actionable strategy to implement the plan. Actions will include short and long-term strategies to address:

- Housing
- Parks and Open Spaces
- Economic Development including Local Businesses and Services
- Urban Design
- Streets and Public Spaces
- Public Art
- Arts and Cultural Space
- Public/Private Partnership Opportunities
- Other Ideas from the Community

PROJECT SCHEDULE



FROM THE 2014 VISION PLAN:

Our **VISION** for Lakewood is a thriving, urban, South Puget Sound City, possessing the core values of family, community, education, and economic prosperity. We will advance these values by recognizing our past, taking action in the present, and pursuing a dynamic future.

The City Council's VISION for Lakewood at its 30 Year Anniversary is a community:

- Inspired by its own sense of history and progress.
- Known for its safe and attractive neighborhoods, vibrant downtown, active ▫ arts and cultural communities.
- Sustained by robust economic growth and job creation.
- Recognized for the excellence of its public and private schools, and its community and technical colleges.
- Characterized by the beauty of its lakes, parks and natural environment.
- Acknowledged for excellence in the delivery of municipal services.
- Leveraging and embracing of our diversity.



Our **VISION FOR DOWNTOWN** is that it is seen as the “heart” of Lakewood. Downtown is where people go to do fun things, see friends and neighbors, eat good food, and experience the cultural diversity of the City. Downtown brings a strong sense of pride for the community by celebrating all things Lakewood and bringing a strong sense of identity to the City and its people. Downtown is best experienced by walking or biking and is safe, inviting, and connected. The Downtown has a mix of retail, restaurant, employment and housing options that are well-designed and support civic life and a strong economy.

DOWNTOWN IS:

A GREAT PLACE!

The **HEART** of the **COMMUNITY** and **CIVIC LIFE**

Designed for **PEOPLE** to **WALK** and **BIKE**

SAFE and **INVITING**

Where people of all ages go to do **FUN** things

Rich with **CULTURAL DIVERSITY**

SUSTAINABLE and connected to **NATURE**

Part of a thriving **LOCAL ECONOMY**

A source of **PRIDE** and **IDENTITY** for **LAKEWOOD**

Where people **LIVE, WORK, MEET, SHOP,** and **EAT**

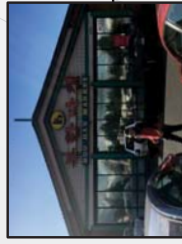
Between September 2017 and November 2017, we hosted a series of public outreach and engagement efforts to encourage local residents to participate in conversations about their community's future. Activities and events were designed to promote meaningful dialogue within the city's diverse community of businesses and residents to ensure the Downtown Plan reflects local desires and needs, and lays a foundation to create **the district's best future**.



FARMER'S MARKET
Booth | Sept. 5
24 Participants



TRUCK & TRACTOR
Booth | Oct. 14
31 Participants



BOO HAN
Intercept | Oct. 14
27 Participants



JBLM
Intercept | Nov. 5
49 Participants



5TH GRADE
Focus Groups | Nov. 13, 14 & 15
97 Participants



CHARETTE KICKOFF
Meeting | Nov. 15
24 Participants



DEVELOPER FORUM
Focus Group | Nov. 15
11 Participants



EL MERCADO
Intercept | Nov. 5
7 Participants



CHARETTE OPEN HOUSE
Meeting | Nov. 18
7 Participants



VISION SURVEY
Online | Nov. 10 – Dec. 1
338 Participants

YOUTH COUNCIL
Focus Group | Oct. 16
22 Participants



KWA
Focus Group | Nov. 1
8 Participants

What three words best describe Downtown Lakewood today?



Dream big! Imagine you can change or improve any aspect of Downtown Lakewood. What would you want to see and where?



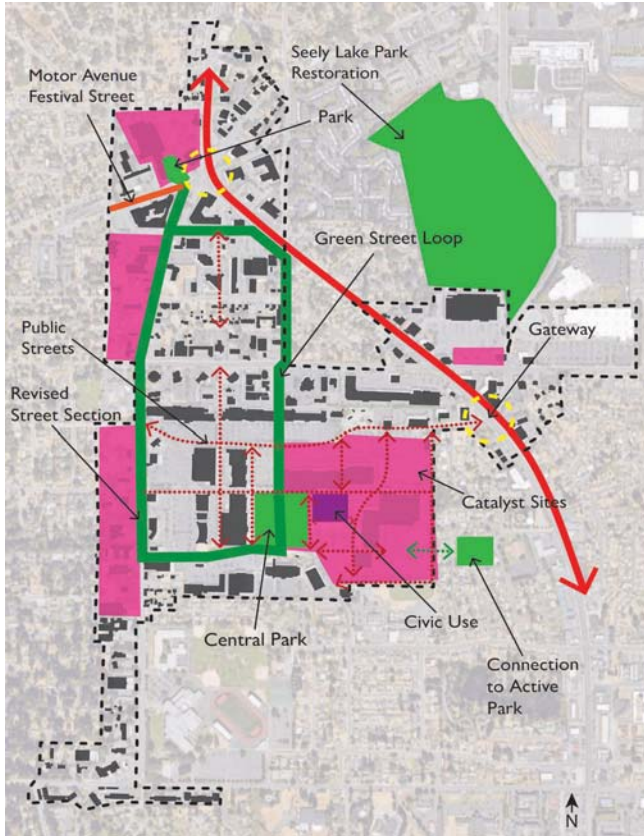
KEY THEMES

More **entertainment** venues and restaurants.
More **retail choices**, both mom and pop and brand stores.

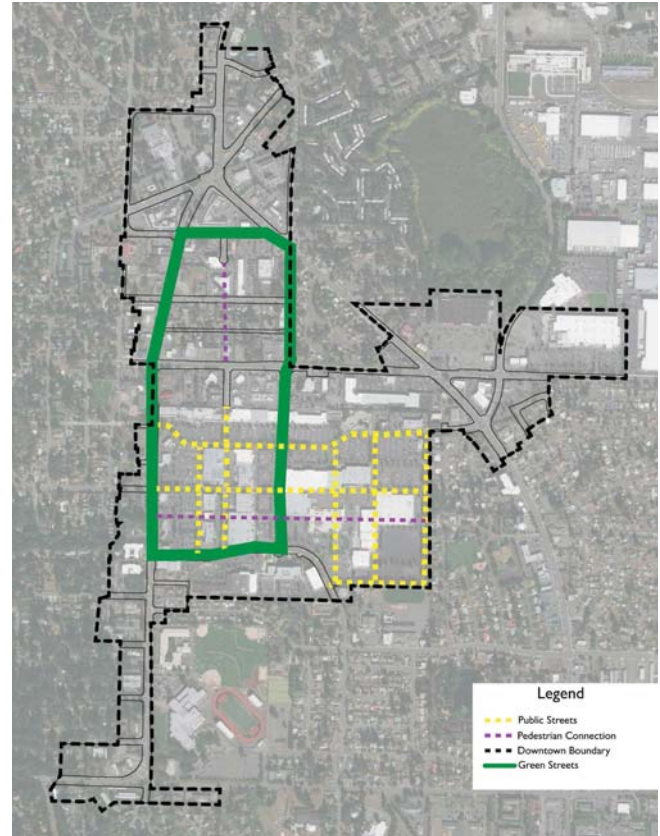
Well-designed housing for seniors & disabled and mixed use with housing and commercial together, within walking distance of work, shopping and buses.

Pedestrian friendly street design, well-maintained and safe roads.

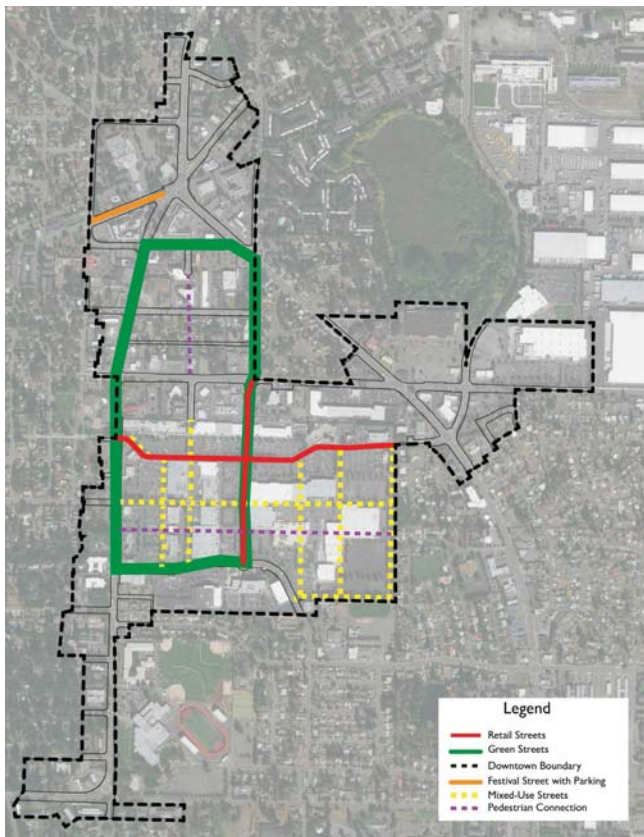
Family activities and gathering spaces, including Outdoor recreation (e.g. spraypark, climbing walls, skating rink, other) and indoor cultural facilities (e.g. expanded library, children's museum, etc.).



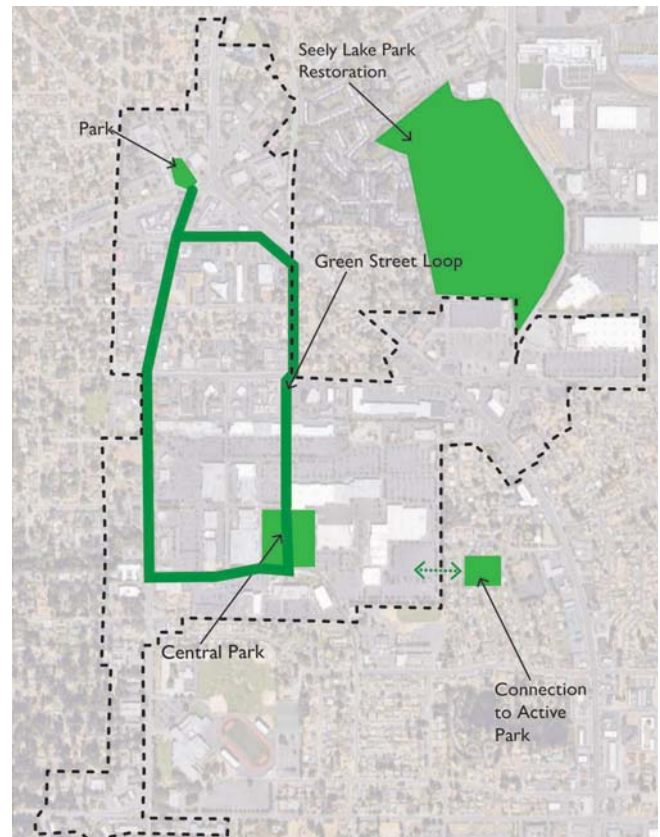
Overall Concept



Regulating Plan



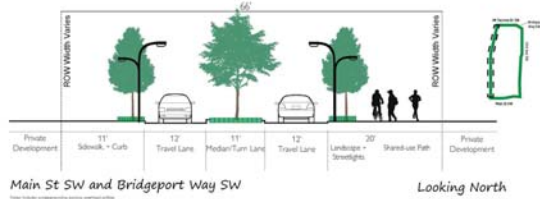
Street Types



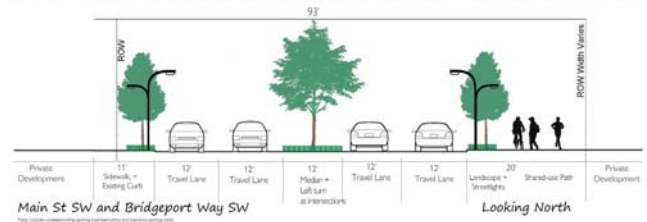
Open Space Concept

Several options are tested in the EIS that can meet objectives for mobility and the green loop.

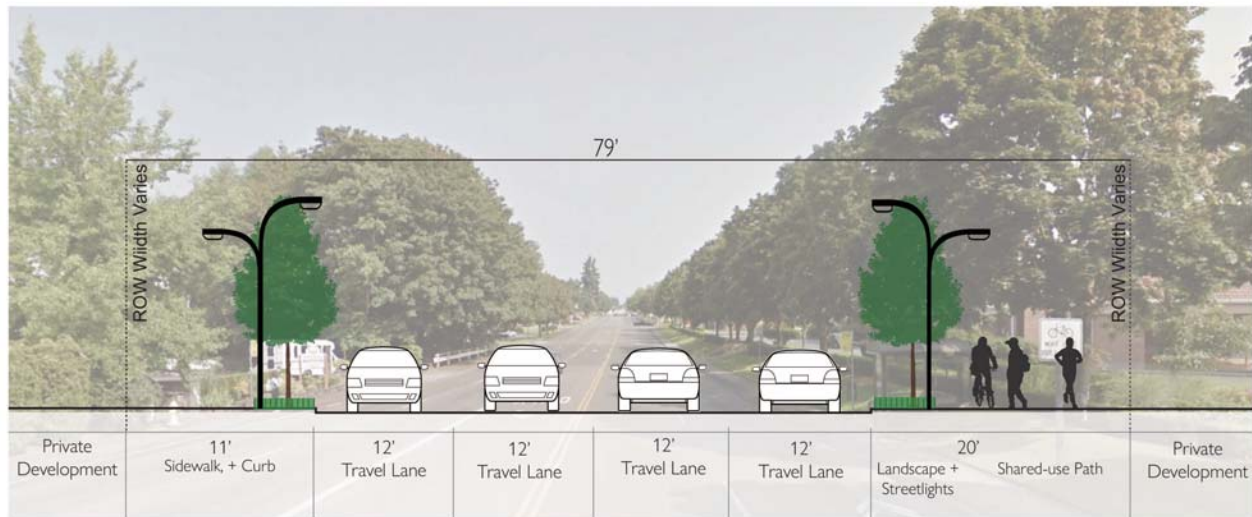
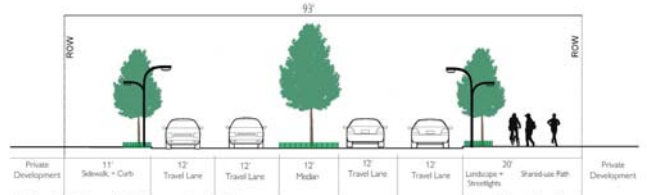
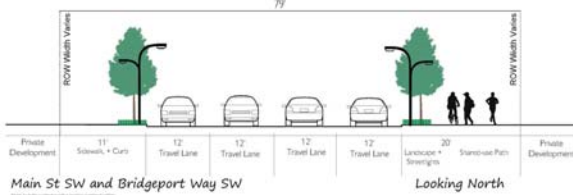
Gravelly Lake Drive Green Street - 3 Lane Option



Gravelly Lake Drive Green Street - 4 Lane + Median Option



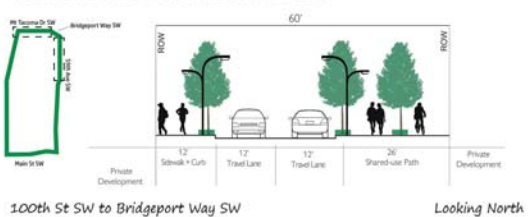
Gravelly Lake Drive Green Street - 4 Lane Option



59th Avenue SW (Existing ROW)



59th Avenue SW Green Street



59th Avenue SW Retail + Park Street





Towne Center Reimagined



Incremental Build-out



Existing



Alternatives for EIS

The Environmental Impact Statement (EIS) studies three alternatives.



- Final EIS can test a Preferred Alternative in the range



An alternative describes a different means of achieving a proposal. Proposal is to develop plan, zoning, and code changes that help City achieve vision

Potential Features of Alternatives

FEATURE	NO ACTION	ACTION ALTERNATIVE 1	ACTION ALTERNATIVE 2
Height	+	++	+++
Jobs	+	++	+++
Housing	+	++	+++
Civic Parks	+	++	+++
Transportation Connectivity	+	++	+++
Pedestrian/Bicycle	+	++	+++
Parks / Community Gathering	+	++	+++
Ecosystem – e.g. creek daylighting, menu of stormwater requirements	+	++	+++

Legend: + Current Plan – Least Incentives or Investment | ++ Phased/Targeted Investment |
+++ Greatest Investment & Transformation

Planned Action Process

1 Prepare Downtown Plan & Planned Action EIS

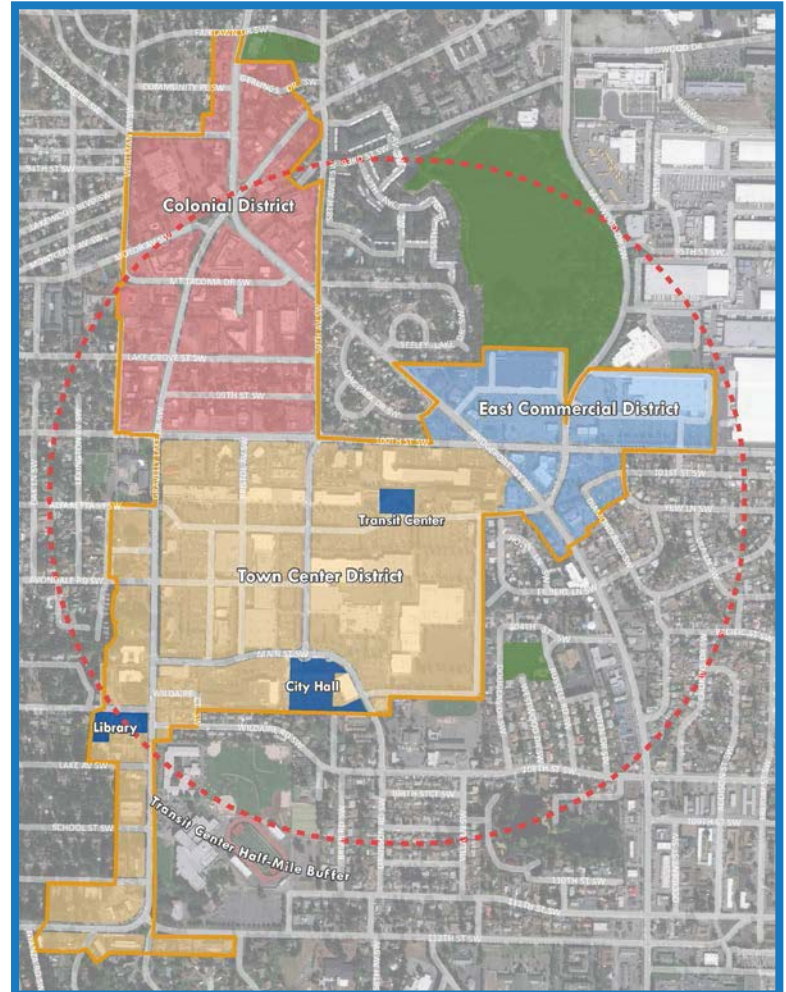
2 Finalize & Adopt Planned Action Ordinance

3 Implement Planned Action Ordinance

- Verify for each development project:
 - Is it within the Planned Action area?
 - Is the project within the scope of the Planned Action Ordinance?
 - Are environmental impacts within the scope of the Planned Action EIS?
 - Does it include mitigation measures in Planned Action Ordinance?
- Yes?** Proceed with local Permit process.
No? Additional Environmental Review Required.

Planned Action Boundaries

Proposed boundary of the Planned Action area



Establish Planned Action Area

EIS Analysis of Planned Action Area

Planned Action Ordinance Adopted

Application Review

- Consistent with PAO?
- All impacts addressed in EIS?

SEPA Process Complete. Proceed with local permit process.

YES

Development Application



Draft Downtown Plan and EIS Comments and Web Comments

April 25, 2018

Attached are comments that were provided through the project website fall 2017 to spring 2018, not otherwise included in prior outreach summaries. Additionally, the attachment includes formal comments (approximately 10 submissions) returned to the City during the March 16 to April 21, 2018 Draft Downtown Plan and Draft Environmental Impact Statement comment period. Each comment submitted during the comment period will be included in the Final Environmental Impact Statement. Comments will be numbered and provided a response. Some of the comments suggest plan or policy adjustments that the Planning Commission may consider in their recommendations for the Downtown Plan.



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Downtown Plan

1 message

Squarespace <no-reply@squarespace.info>

Tue, Oct 17, 2017 at 2:33 PM

Reply-To: ldean@klp.com

To: comments@lakewooddowntownplan.org

Name: Lea Dean

Email Address: ldean@klp.com

Subject: Downtown Plan

Message: Yay!!! So excited about what may come to Lakewood. I just moved here in 2015 because my son in law is stationed at JBLM. We purchased a house here in the Oakbrook neighborhood and we love the community. We love exploring the area but we don't go the Lakewood Center often. Our family mainly goes there for the movie theater or to hop on the bus to the airport. We normally head out to the Tacoma neighborhoods to hang out, shop and eat. We always thought the Lakewood Center and surrounding area had a lot of potential so we can't wait to see the city plan for the area. Updating the buildings at the Lakewood Center would be nice and hopefully attract better shopping/ eating options. The discount stores seem to have the bulk of the shopping center; the dollar store, ross, marshalls etc. They are fun to shop at but they seem like the center piece to that shopping area so it's not on the list for a fun day out. Also, we have toddlers so we don't find that area walkable. For example, Ruston point is pretty small but the frontage is very walkable from the movie theater to the restaurants. You don't have to get in your car and drive to the next parking area. I hope that makes sense?

We have a lot of family visit throughout the year that we normally take to Tacoma. It would be nice to show off Lakewood.

My wish list for the area is that we can spend a full afternoon downtown and be entertained:

A wine bar would be awesome, an updated bowling alley or any fun indoor activity like rock climbing, walkable restaurants from our movie theater, an updated library, a larger park area for the farmers market and community events, something visually fun too like trees with lights, sculptures by local artists. I'd also like to see more locally owned restaurants not so many chain restaurants. There are a lot of artists in this area, it would be nice if we had a little art block/walk for these talented people where it was affordable for them to sell their art. Pop-up store opportunities for each season.

Good luck you guys! Lea Dean and Family.

(Sent via [Lakewood Downtown Plan](#))



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - notification of Charrette

1 message

Squarespace <no-reply@squarespace.info>

Tue, Nov 7, 2017 at 3:45 PM

Reply-To: kcoman008@yahoo.com

To: comments@lakewooddowntownplan.org

Name: Kristine Coman

Email Address: kcoman008@yahoo.com

Subject: notification of Charrette

Message: I was thankful to receive an invitation to the Nov Charrette event in Lakewood through my UWT email. I am a bit surprised that I did not receive something in the mail because I live in Lakewood. I hope my neighbors are aware of this event. Please make sure you send out post card notices as soon as possible to residents living in Lakewood.

(Sent via [Lakewood Downtown Plan](#))



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Make Lakewood great!

1 message

Squarespace <no-reply@squarespace.info>

Fri, Nov 10, 2017 at 5:30 PM

Reply-To: alohaxamylee@gmail.com

To: comments@lakewooddowntownplan.org

Name: Amy Schultz

Email Address: alohaxamylee@gmail.com

Subject: Make Lakewood great!

Message: I took a survey recently pertaining to the development of Lakewood. I would like to add a space that can house indoor, year-around community events such as the Farmers Market, Local Artisans and Locally made products. Also, an indoor rec center for kids, year around pool, hot tub, sauna, indoor tennis courts, climbing area, classes etc. Keeping the community fit and healthy year around and inspiring more community involvement. Know your neighbors better and take back the neighborhoods. Lakewood has such a terrible reputation, and I can see why after living here for 5 months. I never feel safe even at my own home.

(Sent via [Lakewood Downtown Plan](#))



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Options 1 and 2

1 message

Squarespace <no-reply@squarespace.info>

Thu, Mar 8, 2018 at 10:41 AM

Reply-To: detalbo@gmail.com

To: comments@lakewooddowntownplan.org

Name: Ellen T.

Email Address: detalbo@gmail.com

Subject: Options 1 and 2

Message: Hello Lakewood Planners and your Consultants,
Could you please post on the project website the graphics and/or a summary of what the different development options are? The only place you can find it is to watch the entire youtube presentation and that is not practical for people's time.
Thank you greatly!

Also, I grew up in Lakewood and the Mall and Colonial Center have been in need of fresh planning for YEAAARRS!! So this Downtown Plan is long overdue and I'm happy to see it move forward. Please don't overdevelop or mess with the character of the Colonial Center. It's history isn't glamorous but it is important to the regional history of Pierce County/South Sound. In other words, I think it would be good for the City to develop architectural design guidelines for the Colonial Center - nothing terribly stringent, but at least a tool to ensure that things like signage/awnings/parking/lighting fixtures, etc fit in with the brick buildings, rooflines, and bellfree towers that are unique to the buildings. Also, why/how the heck did the City approve the massive freeway-like signal mast at the Gravelly Lake/Bridgeport intersection?!! The poles and signal masts are wayyyyyyy too massive and out of place for that type of intersection - it looks like the type of infrastructure you'd use for a freeway SPUI interchange, not an arterial intersection. With it installed now, it makes it hard to incorporate neighborhood scale design to the area. If now is the appropriate time to redesign that intersection - please CONSIDER design elements to improve the visual impact of the signal infrastructure at that intersection. Can a "welcome to Lakewood" sign/lettering be added to help smooth the visual transition at least?? Or some kind of holiday/festivity signage or banners would help.

(Sent via [Lakewood Downtown Plan](#))



Pre-comment period

Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Pink area across Gravelly LK DR?

2 messages

Squarespace <no-reply@squarespace.info>

Thu, Jan 4, 2018 at 8:26 AM

Reply-To: rgirwin_01@msn.com

To: comments@lakewooddowntownplan.org

Name: Ron Irwin

Email Address: rgirwin_01@msn.com

Subject: Pink area across Gravelly LK DR?

Message: Can you tell me what is the purpose of the area in pink boarded by a black dotted line and the area where the dotted line itself is with no pink across Gravelly Lk Dr where the word Road Diet is. This is in the Overall Concept Plan.

Thanks

(Sent via Lakewood Downtown Plan)

Jeff Arango <comments@lakewooddowntownplan.org>

Sun, Mar 25, 2018 at 11:47 AM

To: rgirwin_01@msn.com

Thanks for your question. The pink areas are catalyst sites, which are areas that are more likely to redevelop in the future. The black dotted line is the study area for the Downtown plan. If you have any further questions please let us know.

The Downtown Lakewood Team.

[Quoted text hidden]



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Development of Lakewood Down town

1 message

Squarespace <no-reply@squarespace.info>

Thu, Jan 25, 2018 at 7:50 PM

Reply-To: mosarram@gmail.com

To: comments@lakewooddowntownplan.org

Name: Mahmood Sarram

Email Address: mosarram@gmail.com

Subject: Development of Lakewood Down town

Message: Each city must have a focal point around which people gather to see each other and talk and connect with each other and visit shops that are located around this focal point. This focal point could be a fountain, a statue or an "sound and light exhibitions. The entire colonial square and land around it could serve this purpose

(Sent via [Lakewood Downtown Plan](#))



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - our house on the border**EIS/Plan Comment**2 messages

Squarespace <no-reply@squarespace.info>
Reply-To: old1931sarge@gmail.com
To: comments@lakewooddowntownplan.org

Fri, Mar 16, 2018 at 5:22 PM

Name: harry black

Email Address: old1931sarge@gmail.com

Subject: our house on the border

Message: Our home sits what it looks like the line of improvements, we live on [6405 wildaire rd sw](#) and it looks like were in the zone, can you confirm? I'm 87 and don't want to move.

(Sent via [Lakewood Downtown Plan](#))

Jeff Arango <comments@lakewooddowntownplan.org>
To: old1931sarge@gmail.com

Sun, Mar 25, 2018 at 11:41 AM

Thanks for submitting your question. It appears your property is on the border, but outside the study area boundary for the Downtown plan. If you have any further questions please let us know.

[Quoted text hidden]



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - City Center1 message

Squarespace <no-reply@squarespace.info>

Tue, Mar 20, 2018 at 10:38 AM

Reply-To: zjay41@comcast.net

To: comments@lakewooddowntownplan.org

Name: Jay Roney

Email Address: zjay41@comcast.net

Subject: City Center

Message: How about instead of spending money on areas that are already okay and do something about the other neighborhoods. Seems this city has forgotten about us that don't live on the main streets of Lakewood. I pay property taxes like everyone else but nothing has been done on our street in 20 years. They used to clean the street and put down new gravel once in awhile. But nothing has been done in many many years. Maybe think of us now and then

(Sent via [Lakewood Downtown Plan](#))



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Sidewalks

1 message

Squarespace <no-reply@squarespace.info>

Tue, Mar 20, 2018 at 6:02 PM

Reply-To: Heresmyaddy@gmail.com

To: comments@lakewooddowntownplan.org

Name: Elizabeth Jackson

Email Address: Heresmyaddy@gmail.com

Subject: Sidewalks

Message: Love some of the ideas. Unfortunately I'm unable to attend the meeting tomorrow.

But while all of this is being done for downtown, what about the entry way to downtown?

A welcome to Lakewood was put up up just off the Gravelly Lake exit, and aside from Bridgeport, this is a main entryway to Lakewood.

Why are there no sidewalks along Nyanza Rd?

It's awful to watch women with children in strollers trying to navigate that terrain, and have to choose to go into the street.

I have mobility issues, and can't walk along the road without hurting myself ... It's happened once, so now I take my car rather than try to walk.

Make the downtown lovely, but please think about those of us who'd love to be able to enjoy a walk to it.

(Sent via [Lakewood Downtown Plan](#))



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Lakes

1 message

Squarespace <no-reply@squarespace.info>

Sat, Mar 24, 2018 at 8:02 AM

Reply-To: aikendavid74@yahoo.com

To: comments@lakewooddowntownplan.org

Name: David Aiken

Email Address: aikendavid74@yahoo.com

Subject: Lakes

Message: This does not have to do with the downtown but I think the community should improve lake access and fishing opportunities. The law requires lake access but in most instances people either don't know about it or those live nag near it are very much against it. Apparently, the city has not had the political will to do anything about it. People like to fish off the Interlaken bridge but there is no where to park. There is a boat ramp on lake steilacoom but no where to park your trailer. Bottom line is that 99 percent do not get to use the resources as intended. Something should be done to make fishing and water access easier and in turn improve our community.

(Sent via [Lakewood Downtown Plan](#))



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Lakewood Downtown Plan

1 message

Squarespace <no-reply@squarespace.info>

Mon, Mar 26, 2018 at 4:01 PM

Reply-To: julianfwheeler@aol.com

To: comments@lakewooddowntownplan.org

Name: Julian Wheeler

Email Address: julianfwheeler@aol.com

Subject: Lakewood Downtown Plan

Message: Thank you for your presentation last week. I provide here additional comments to follow up on the ones I left at the Truck and Tractor event.

Upon further reflection, I would say that a tourist draw could be a reflecting pool using the aggregate square footage currently available, and possibly shaped like the Reflecting Pool in the Mall in Washington, DC. At this draft point of consideration, I would suggest that it could be laid out along the corridor currently identified with 59th Ave SW, and for such a distance that would help realize scenic values when people view the Lakewood City Municipal building at any distance.

Another amenity worthy of consideration is the placement of a facility on the grounds of Lakewood Towne Center that would be used as a sweat lodge and multi-tribal museum, particularly by officially recognized Native American Tribes, and help local residents learn more about the Native American history of this locality. Support for this endeavor may come in various forms as relationships can be more affirmatively established between the City and local tribes, including the Puyallup or Steilacoom Tribes, and who may find this opportunity worthy of funding. A museum as such could draw tourists from in and around our city, including students and teachers who may need to patronize surrounding establishments during lunch, etc. It may also host other functions, as El Centro de la Raza does in its local Beacon Hill community near a VA hospital. A sweat lodge, while hosting a nominally religious activity, may be allowed by local code and such treaties that have been in place with local tribes for over a century.

If consideration is still in its draft phase, I would also like to recommend the construction and availability of a meeting facility geared towards non-profits. An example of this is the 2100 Building in south Seattle, WA (at [2100 - 24th Ave S, http://2100building.com/](https://2100building.com/)). Like the above two suggestions, it could grow into a draw for professionals who will gain the appropriate impression of the City of Lakewood as a community committed to a fairly-paced progress.

Thank you for your time ... again. -j.


(Sent via [Lakewood Downtown Plan](#))

From: Dane.Jaster [mailto:Dane.Jaster@target.com]
Sent: Wednesday, April 04, 2018 2:49 PM
To: Tiffany Speir
Subject: Lakewood Downtown Plan - notice

Ms. Speir,

Hello. My name is Dane Jaster and I manager the existing store portfolio for Target Corp in Washington state. I received a notice in the mail regarding an upcoming community meeting. Unfortunately I am unable to attend. I took a cursory look at the document provided on the project website, "Lakewood Downtown Plan" with a draft date of March 2018, and have a few comments. Target's preference is for the "Lakewood Towne Center Incremental Build-Out" concept shown in Figures 13-16, as this would have less impacts to the Target store. I couldn't help but notice the conceptual plan, labeled as "Lakewood Towne Center Reimagined" (Figures 17-20), could reallocate the retail node section of Lakewood Towne Center with a mixed use development, effectively removing the Target store located at 5618 Lakewood Town Center Blvd SW.

Please keep me informed with further developments and potential impacts to the Target store. My contact information is below.

Dane Jaster | Real Estate |  Target | 1000 Nicollet Mall-TPN-12H
Minneapolis, MN 55403 | Dane.jaster@target.com | Direct 612-761-6422

From: Tiffany Speir
Sent: Wednesday, April 04, 2018 3:04 PM
To: 'Dane.Jaster'
Subject: RE: Lakewood Downtown Plan - notice

Hello:

Thank you for your comments.

You can track the latest updates and schedule of public meetings upcoming for the Downtown Lakewood Subarea Plan here: <https://www.lakewooddowntownplan.org/projectupdates/>

As of now and subject to change, the Planning Commission will be holding meetings on April 9, April 18, May 2, May 16 and June 6; the City Council is currently scheduled to hold meetings on the proposal on April 9, June 25, July 2 and July 16.

There will be two meetings held on April 26 at 12:00 and 1:30 – one to discuss the Planned Action for the subarea and one focused for downtown developers – that you may wish to attend (or have a local representative attend.) Both will be good opportunities to learn more details about how Target may be affected by any of the alternatives. I include more information about those meetings below.

If you would like to have a separate discussion regarding the proposal, please feel free to let me know as well.

Thank you,

Tiffany Speir
Planning Manager, Special Projects



6000 Main St SW, Lakewood, WA 98499
253.983.7702 | c 253.204.9643
tspeir@cityoflakewood.us

Check out the latest about the [Lakewood Downtown Plan](#)
Take the [survey](#)!

Notice of Community Meeting

LAKEWOOD DOWNTOWN PLANNED ACTION

Pursuant to RCW 43.21C.440 (3)(b) the City of Lakewood will host a community meeting on the draft Downtown Planned Action Ordinance.

The community meeting will be an open house format, held on April 26, 2018 at noon at City Hall, 6000 Main St SW, Lakewood, WA 98499. City staff and the project consultants will be available during the community meeting to provide general information about planned actions and the draft ordinance.

For more information, please visit the project website, <https://www.lakewooddowntownplan.org/>, or you may contact Tiffany Speir, Planning Manager, Special Projects, at tspeir@cityoflakewood.us or 253.983.7702.

DATE/TIME: **April 26, 2018 at 12 noon.**

LOCATION: City of Lakewood, City Hall, 6000 Main St SW, Lakewood, WA 98499



Developer Focus Group

Thursday April 26 at 1:30 pm
Lakewood City Hall, 6000 Main St SW

You are invited!

Join us and your peers on **Thursday, April 26 at 1:30 pm** at Lakewood City Hall to learn about and provide your perspective on the proposed Lakewood Downtown Subarea Plan.

The draft Subarea Plan, associated Draft Planned Action Environmental Impact Statement, and background information are available at www.lakewooddowntownplan.org.

You can also view a brief three-dimensional video explaining the potential build-out alternatives for the Towne Center area, one of the Downtown's catalyst sites, that grew out of the 2017 Downtown design charrette and outreach results [here](https://vimeo.com/247520352):
<https://vimeo.com/247520352>

For more information, please contact Tiffany Speir, Planning Manager, Special projects, at 253.983.7702 or tspeir@cityoflakewood.us See you April 26!

From: Dane.Jaster [<mailto:Dane.Jaster@target.com>]

Sent: Wednesday, April 04, 2018 2:49 PM


To: Tiffany Speir

Subject: Lakewood Downtown Plan - notice

Ms. Speir,

Hello. My name is Dane Jaster and I manager the existing store portfolio for Target Corp in Washington state. I received a notice in the mail regarding an upcoming community meeting. Unfortunately I am unable to attend. I took a cursory look at the document provided on the project website, "Lakewood Downtown Plan" with a draft date of March 2018, and have a few comments. Target's preference is for the "Lakewood Towne Center Incremental Build-Out" concept shown in Figures 13-16, as this would have less impacts to the Target store. I couldn't help but notice the conceptual plan, labeled as "Lakewood Towne Center Reimagined" (Figures 17-20), could reallocate the retail node section of Lakewood Towne Center with a mixed use development, effectively removing the Target store located at 5618 Lakewood Town Center Blvd SW.

Please keep me informed with further developments and potential impacts to the Target store. My contact information is below.

Dane Jaster | Real Estate |  Target | 1000 Nicollet Mall-TPN-12H
Minneapolis, MN 55403 | Dane.jaster@target.com | Direct 612-761-6422



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Citizen Comment

1 message

Squarespace <no-reply@squarespace.info>
Reply-To: scullyta@comcast.net
To: comments@lakewooddowntownplan.org

Thu, Apr 12, 2018 at 6:36 PM

Name: Tim Scully

Email Address: scullyta@comcast.net

Subject: Citizen Comment

Message: Appreciate the opportunity to share my opinion on the future of Lakewood in the Onyx and Downtown surveys
(Sent via [Lakewood Downtown Plan](#))



Jeff Arango <comments@lakewooddowntownplan.org>

Form Submission - New Form - Adapting the T own Center sinage motif

1 message

Squarespace <no-reply@squarespace.info>

Sun, Apr 15, 2018 at 4:13 PM

Reply-To: sz4design@comcast.net

To: comments@lakewooddowntownplan.org

Name: Susan Wollwage

Email Address: sz4design@comcast.net

Subject: Adapting the Town Center sinage motif

Message: I just sent a statement, perhaps in the wrong cubby.

(Sent via [Lakewood Downtown Plan](#))

From: Amy Pow [mailto:APow@tpchd.org]

Sent: Monday, April 16, 2018 2:39 PM

To: Tiffany Speir

Cc: Miae Aramori; Marianne Seifert; Rob Olsen; Brad Harp; Jessica Gehle; Anthony Chen

Subject: Lakewood Downtown Plan EIS

Tacoma-Pierce County Health Department is pleased to see so many wonderful policies in your well-written draft plan and mitigation measures in the DEIS. They provide a foundation for a healthy downtown, comprising healthy and socially-connected people, a vibrant local economy, and a safe and clean environment. The adoption of this plan is only the beginning of building a healthy downtown in Lakewood. We encourage the City to invest in implementing the plan, and empower local communities and businesses to actualize the ambitious vision for this great place to be.

Urban Design, Streetscape and Public Spaces

We are particularly excited to see Lakewood's intent to introduce form-based code, or its hybrid, to create inviting public spaces and functional plinths, or [the city at the eye level](#), to promote physical activity, pedestrian-oriented businesses and social gathering, thus building a sense of place. Form-based code is instrumental to foster community health and well-being.

Equitable placemaking comes only with meaningful community engagement when form-based code elements are being discussed and developed. A vibrant downtown should be a central gathering place for all walks of life. The Health Department encourages the City to engage diverse communities and local residents and businesses in conversations around design compatibility and aspirations sensitive to their social, cultural, and lifestyle needs. The Health Department is available to partner with you during the public process when putting the form-based codes together.

Activate empty and underutilized spaces for creative placemaking, community gardens or other social and recreation events to keep downtown safe and vibrant at all times.

Housing and Job Balance

Many people have limited choices regarding where they work and live. We share your vision that the downtown is a place where people can live and work; besides meet, shop and eat. To achieve this vision, we strongly recommend a wide range of quality housing affordable to anyone who works in downtown. We support live-work arrangements to enable those working in a mixed-use building can afford to live in the same building or within walking distance. This would keep commuting and demand for parking minimal, while making downtown vibrant 24/7.

To encourage live and work in the same locality, the plan needs to ensure (1) sufficient numbers of dwelling units available to support the number of workers in the area, and (2) the dwelling units created are affordable to local workers with corresponding wage levels. Consider these two strategies to match and balance housing with jobs:

- Ensure a balanced job to housing ratio—It appears from DEIS Exh 1.7-3 that there are more jobs created than housing units can support. Can this Plan target for a healthier job to housing ratio with no net export of workers, taking into account the trends of diminishing average household size over time?
- Ensure the dwelling units created is affordable to workers working in the downtown. Track price points of housing units and job wages. We encourage a broad spectrum of businesses,

particularly professional and personal services, to make downtown a mixed-income place to live, work and play.

We encourage the Plan to address the lack of medical offices in the downtown area. Co-locating professional and personal services, particularly medical clinics, near transit facilities is particularly meaningful for those who rely on alternative transportation. Well-served by local and regional transits, Downtown is an ideal place to locate medical and other professional offices.

Linear Parks, Urban Parks and Open Space

As downtown houses more people and jobs over time, the need for urban parks will become more imminent. We encourage the City to review the park LOS adopted by the Legacy Plan to support downtown living. The 0.75-mile service radius may be too challenging for seniors and children. Many cities are trending towards a 10-minute (half-mile max.) walking distance as their LOS. Besides, some kind of per capita LOS may be appropriate to ensure sufficient amount of open space to support high-density built environments.

To waive fee-in-lieu towards the central park development (Section 1.7.5 of DETS), some form of development agreement is required of the developer to maintain and dedicate the open space for public use at all reasonable hours.

To make the green street loop a linear park serving downtown area, it's desirable to develop both public and private open spaces or plazas along the green street loop. A system of smaller urban parks connected by a green street loop helps promote physical activity and social gathering, thus improving physical, social and mental health.

Extend the green street loop to connect with Seely Lake Park and Active Park by indicating probable on-street trail extensions (subject to further detailed design) on the Plan and associated maps.

Transportation and Parking

To achieve an active downtown, develop mode split targets to achieve over time; and monitor its progress. Successful TDM measures are concurrently needed to support mode split targets at any phase of development.

Require parking utilization studies when (re)developments occur. Also require shared parking agreements with other property owners before approving development proposals.

Improve connectivity between local transit (Lakewood's Transit Center) and regional transit (Lakewood Sounder Station) to facilitate regional mobility. Provide amenities near Lakewood Station.

While the DEIS concludes there's no indication that collision rates would be increased significantly in both alternatives, we encourage the City to identify strategies to bring the baseline collision rate to zero over time.

Identify safe-route-to-school strategies for students originating from the downtown area.

Local Business Displacement

The plan correctly pointed out the potential negative impacts of gentrification on local businesses. As potential mitigation measures, we recommend:

- Provide incentives for local businesses to stay.
- Form a local business association to create sense of pride and ownership.
- Empower local businesses with resources to implement community projects and events that foster pedestrian-oriented commercial environment.
- Use form-based codes to encourage small scale businesses by limiting the GFA of the ground level units. Besides providing affordable space for mom and pop shops to stay, this would create functional plinths and foster social interaction.

Environment and Utilities

As potential mitigation measures, we recommend:

- The green roof tops for high rises.
- Previous natural play areas serving as private open space for apartment buildings.
- Rain gardens and community gardens connected by the green street loop to enhance the linear park experience.

The Health Dept continues to partner with Lakewood, addressing threats to drinking water resources, limiting toxic exposure, and improving economic vitality through cleanup of contaminated sites.

Thank you for giving the Health Department the opportunity to provide input.

Sincerely,
Amy

Amy Pow, MCIP

Principal Planner

Environmental Health

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Shaping Sustainable and Smart Built Environments to Promote Healthy Communities

Program Website: <https://www.tpchd.org/healthy-places/planning-for-healthy-communities>

Healthy Community Planning Toolbox: <https://www.tpchd.org/healthy-places/planning-for-healthy-communities/toolbox>

From: Jay Peterson [mailto:jpeterson@piercetransit.org]
Sent: Monday, April 16, 2018 9:05 AM
To: Tiffany Speir
Subject: Lakewood Downtown Plan EIS

Hi Tiffany –

Thank you for giving Pierce Transit the opportunity to review and submit comments on the City of Lakewood's draft Downtown Plan. While we have reviewed the entire plan, Pierce Transit's comments focus mostly on the Transportation section of the plan and how it relates to our transit service. In general, our agency is supportive of this plan and the potential for opportunity that it will create in transforming the downtown area of Lakewood. The draft plan provides a good overview of the future vision of Downtown Lakewood and we hope to remain an active partner with the City of Lakewood in the realization of this plan. If you need any follow-up, please contact Jason Kennedy from our Planning Department who took the lead in drafting our response jkennedy@piercetransit.org 253.581.8135.

Beyond the specific comments provided below, one general comment that is a common theme throughout our response is that the City of Lakewood should keep transit-friendly design in mind and that our agency continues to be included in the planning process to ensure our bus operations are not negatively impacted. Additionally, if it has not already incorporated into its planning process, Pierce Transit suggests the City of Lakewood use the National Association of City Transportation Officials (NACTO) *Transit Street Design Guide* to help guide the redesign of the streets of Downtown Lakewood. The *Transit Street Design Guide* is a professionally developed and recognized reference meant to help cities plan and design their street network with transit elements in mind.

Pierce Transit's specific comments are as follows:

Page 5: Pierce Transit agrees with the report's statements about improving pedestrian and other non-motorized connections at the Towne Center, including the Lakewood Transit Center (LTC). In addition to the eight Pierce Transit routes, LTC is also served by the Sound Transit Route 574 to Sea-Tac Airport and the GO7 Route to JBLM.

Page 6: Has the City of Lakewood considered including an officially designated Park and Ride lot adjacent to the LTC in their plan?

Pierce Transit supports additional/improved signing to increase awareness of, and ease wayfinding to, the LTC.

Page 10: Pierce Transit requests consideration for transit-friendly elements of any improvements made to the Green Loop and Revised Gravelly Lake Drive.

Page 33: Pierce Transit requests transit-friendly elements to any improvements to public streets, especially in and around the LTC. Access improvements for buses as well as pedestrians and others connecting to the services at the LTC should be included in the street improvement plans.

Page 35: Pierce Transit requests consideration for bus operations and access to the LTC when designing the “Festival Street with Parking” along Lakewood Towne Center Blvd SW. Pierce Transit generally supports the idea of improving Lakewood Towne Center Blvd SW, however issues such as how curb-side parking near intersections and LTC access points can impact safety and operations of buses needs to be taken into consideration.

Page 36: Pierce Transit requests that the City of Lakewood take into consideration our Route 214, which operates on Gravelly Lake Drive between 100th and 112th, when redesigning the road and potentially reducing the number of lanes. Currently PT has several in-lane bus stops that occur in the right-hand curb lane and traffic can pass a stopped bus in the left-hand lane. Please be cognizant of the impacts a redesign of the travel lanes will have on operations of our buses and traffic in general. Pierce Transit does prefer in-lane stops and therefore either design would allow for this to occur, however the three-lane design as depicted in Figure 31 does not appear it would allow other vehicles to pass a bus servicing a bus stop.

Page 47: Similar to the request regarding the Route 214 Pierce Transit requests the City of Lakewood take into consideration our Route 48, which operates on Gravelly Lake Drive between Steilacoom Drive and 59th, when redesigning the road and potentially reducing the number of lanes.

Do the traffic prediction models your consultant ran indicate the proposed signalization of Gravelly Lake Drive / 59th is still necessary if the number of lanes on Gravelly Lake Drive is reduced?

Pierce Transit prefers standard intersection designs over roundabouts, however if a roundabout is the preferred design for Lakewood Town Center Blvd / 59th Ave SW please consider a geometric design that will allow safe operation of 40' buses.

Pierce Transit suggests the City of Lakewood consider concrete pads at bus stops to increase the longevity of the road surface at locations listed for pavement restoration.

In Table 3, does the Vehicular Mode Trip Ends include bus transit trips? Does the Non-Vehicular Mode Split not include transit? If not, would it be possible to indicate the estimates for transit mode share?

Page 48: Pierce Transit supports the improvement of traffic control at intersections that our routes turn at. The agency would welcome a signal at the intersection of Gravelly Lake Dr SW and 59th Ave SW with a protected left turn phase to improve safety and operations of our Route 48.

Page 50: Pierce Transit supports Transportation Demand Management (TDM) programs and policies and would welcome involvement with employers in the Downtown Lakewood plan area.

Page 51: Pierce Transit supports policies geared to emphasizing transit use within the Central Business District. Pierce Transit also supports the strategies listed that involve the promotion of multimodal travel, improved transit facilities, and increasing connectivity to transit facilities for pedestrians and other non-motorized transportation users.

Overall, the draft Lakewood Downtown plan presents a positive vision for improvements in that area. Thank you again for allowing Pierce Transit the opportunity to review and comment and we look forward to working with the City of Lakewood as this plan progresses in the future.

Regards,

Jay Peterson, Transit Development Manager | T: 253.984.8203 | C: 253.377.8510
3701 96th St. SW, Lakewood, WA 98499 | piercetransit.org



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Downtown Development Code

Draft April 25, 2018 | BERK and Framework

Overview

The area within the Downtown study area is currently regulated primarily by Sections 18A.30 Zoning Districts and 18A.50 Development Standards of the Lakewood Municipal Code. Section 18A.50.200 includes the community design standards applicable to commercial, mixed-use, and multi-family development.

The new hybrid form-based code will become a new section of the Municipal Code 18A.35 Downtown Districts and will regulate all development and land use in Downtown. The Downtown code section would supersede the standards in Section 18A.50 for all development within Downtown boundary unless incorporated by reference.

The draft code includes the following sections:

18A.35.100 Downtown District

18A.35.200 Land Use and Zoning

18A.35.300 Streets and Blocks

18A.35.400 Site Design, Buildings, and Frontage

18A.35.500 Landscaping, Open Space, and Green Infrastructure

18A.35.600 Parking

18A.35.700 Administration

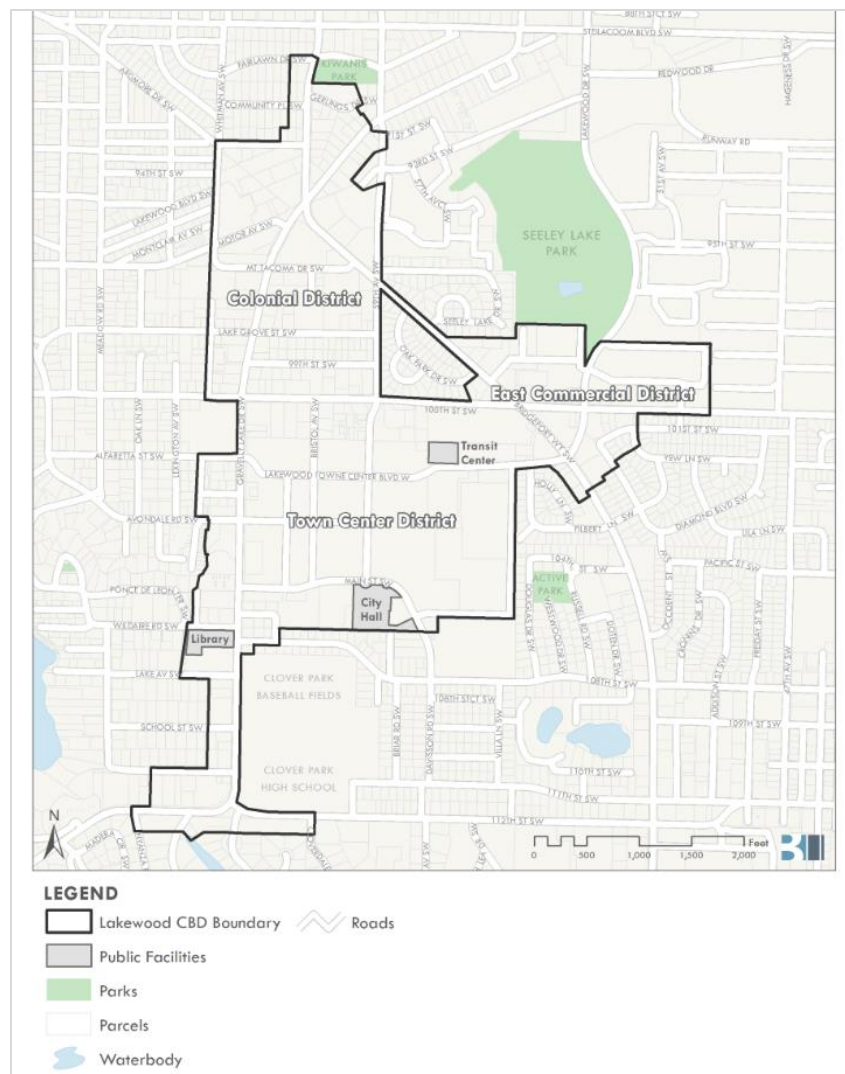
18A.35 Downtown Development Code

18A.35.100 Downtown District

The purpose of the Downtown District is to implement the direction and policies of the Lakewood Comprehensive Plan, the Lakewood Community Vision, and the Downtown Plan. The Downtown District will be redeveloped into an area of rich civic amenities, walkable streets, and a mix of uses including housing, entertainment, restaurants, and retail that builds upon the cultural and economic assets of the city.

The Downtown District is defined as the area shown in the map below.

18A.35.100-1. Downtown District



18A.35.110 Vision and Guiding Principles

Downtown is the “heart” of Lakewood where people go to do fun things, see friends and neighbors, eat good food, and experience the cultural diversity of the City. Downtown carries a strong sense of pride for the community by celebrating all things Lakewood. Downtown is best experienced by walking or biking and is safe, inviting, and connected. It has a mix of retail, restaurant, employment, and housing options that are well-designed and support civic life and a strong economy.

The following principles are intended to guide the development of Downtown according to the community's vision for the district. All land use decisions made for Downtown shall demonstrate how they are consistent with and implement these guiding principles. Downtown is:

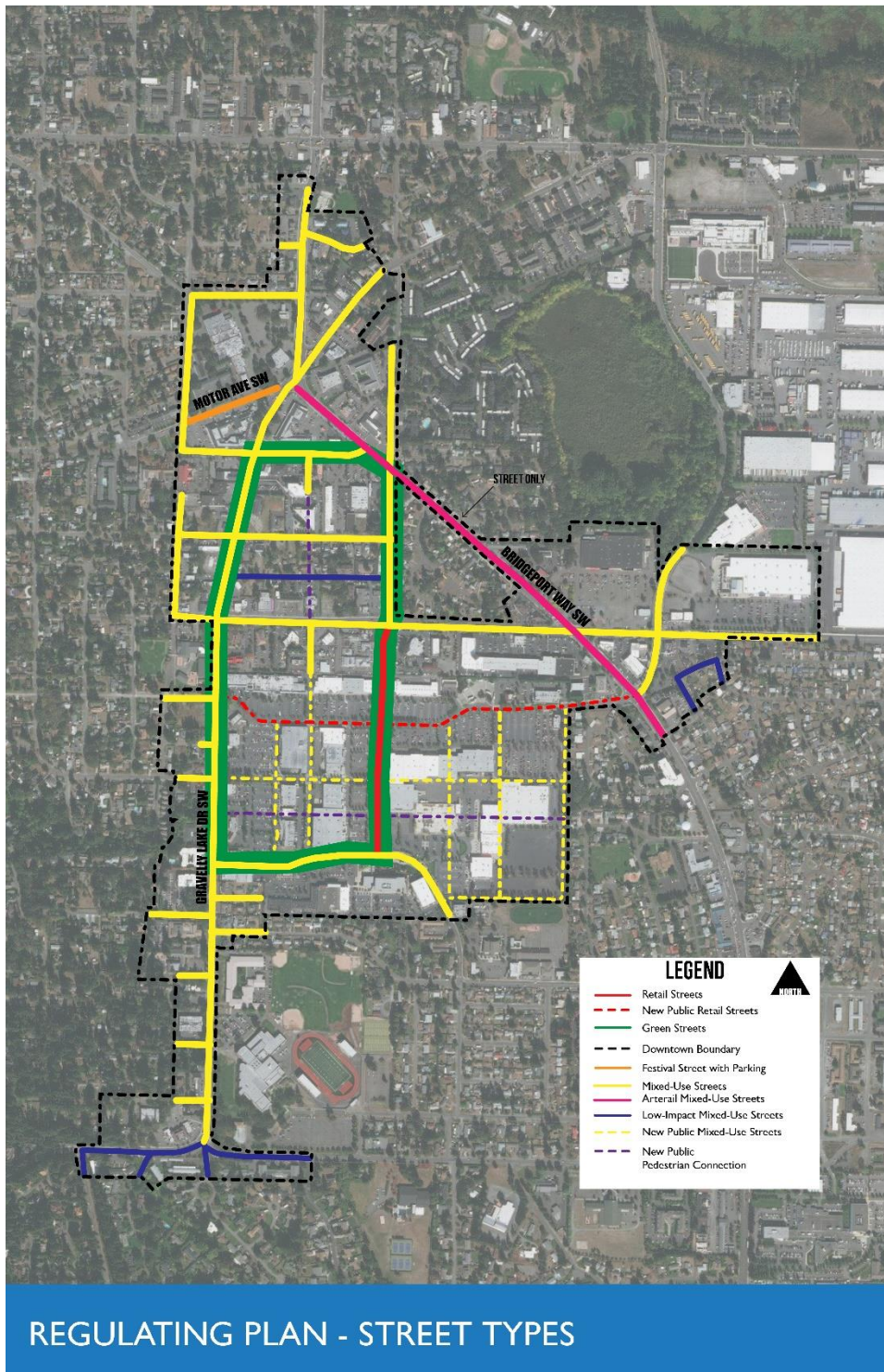
- A vibrant mixed- use community. It's an inviting place where people live, work, shop, and recreate.

- A multi-modal environment. It's designed for people to walk and bike safely and freely throughout the district.
- A signature part of Lakewood's identity. Downtown is a community gathering place that celebrates Lakewood's rich heritage, cultural communities, and civic pride.
- Environmentally sustainable. Greenery, open space, and landscaping connect this urban environment to nature and mimic natural systems where possible.
- A thriving business community. Increased day-time and night-time populations support local businesses and create a lively place to shop, eat, or own a business.

18A.35.120 THE REGULATING PLAN MAP

The Regulating Plan translates the community vision into a map. The Regulating Plan designates the locations, subdistricts, and streets that are intended to embody specific physical characteristics. It specifies the location and applicability of specific design treatments and maps where they are required. The Regulating Plan works in tandem with the development standards, tables, and figures to define the shape, size, and location of streets, through connections, infill blocks, buildings, and landscaping.

18A.35.120 -1. Regulating Plan



18A.35.130 DEFINITIONS

Pending.

18A.35.140 RELATIONSHIP TO OTHER REGULATIONS

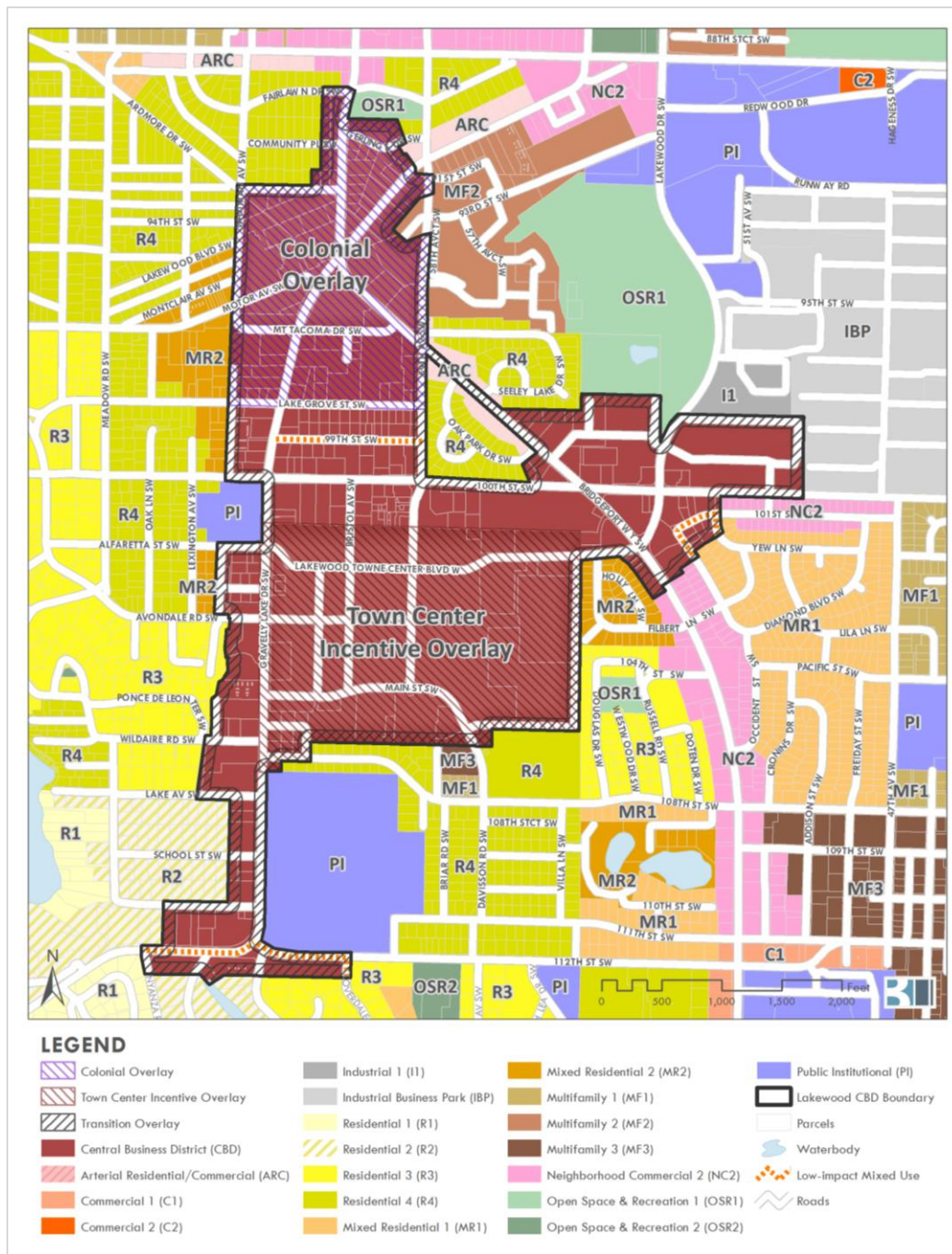
In the case of a conflict between the regulations in 18A.35 Downtown Districts and the rest of the Lakewood Municipal Code, the regulations in 18A.35 Downtown Districts shall control.

18A.35.200 Land Use and Zoning

One base zone and several zoning overlays are applied within the Downtown District, as shown in the map below.

- A. The Central Business District (CBD) zoning district is the primary retail, office, social, urban residential, and government center of the city. The complementary and interactive mixture of uses and urban design provides for a regional intensity and viability with a local character. The regional focus and vitality of the district is evident in the urban density, intensity, and composition of the uses in the district. Local character is reflected in the district's design, people orientation, and connectivity between uses, structures, and public spaces that foster a sense of community.
- B. Colonial Overlay (C-O) district is a special design district in the CBD zone that preserves the unique colonial style aesthetic within that area.
- C. Town Center Incentive Overlay (TCI-O) district allows for the holistic development of the Lakewood Towne Center in alignment with the vision and policies of the Downtown Plan. This area is available for Master Planning accordance with the provisions in LMC 18A.35.720.
- D. Low-Impact Mixed-Use Roads (LIMU-0) district supports the transformation of the Downtown District according to the Downtown Plan and the fulfillment of the purpose of the CBD zone, but allows for existing single-family residential development to remain in place.
- E. Transition Overlay – The Transition Overlay (TO) is any property or portion of a property in the Downtown District that is within 100 ft. of an abutting single-family residential zone or mixed residential zone (also called the district receiving the transition). Properties within the Downtown District that are separated from a single-family residential or mixed residential zone by a city-owned right of way of at least 60 ft. in width do not have to provide a transition area.

18A.35.200-1. Zoning Map



18A.35.220 USES

A. All Residential Uses, Civic Uses, and Commercial Uses and their related Accessory Uses as defined in LMC Chapter 18A.20 are permitted within the Downtown District, except in the Low-Impact Mixed-Use Roads district. Permitted uses are subject to the approval of all required development permits. Provided that the following uses are prohibited:

1. Prohibited Residential Uses:

- a. Single-Family Residential.
- b. Group Homes, Type 3, 4, 5

2. Prohibited Civic Uses:

- a. Military Installations
- b. Postal Services, Level 3
- c. Public Maintenance, Level 2/3
- d. Public Safety Services, Level 3/4
- e. Transportation, Level 4/5

3. Prohibited Commercial Uses:

- a. Bulk Fuel Dealers
- b. Funeral Services, Level 2/3
- c. Lodging, Level 2
- d. Modular and Manufacturing Home Sales
- e. Motor Vehicle Sales and Rental
- f. Motor Vehicles Service and Repair, Level 3/4/5
- g. Pet Sales and Service, Level 3/4
- h. Rental and Repair, Level 4
- i. Sales of Secondhand Property, Level 2/3
- j. Sexually Oriented Businesses
- k. Storage

4. Allowed Residential Uses, Civic Uses, and Commercial Uses subject to approval of a conditional use permit and all applicable development permits:

- a. Education Facilities
- b. Outdoor Recreation, Level 4
- c. Amusement and Recreation, Level 4
- d. Drive-Through Facilities

- e. Essential Public Facilities not otherwise permitted or prohibited
- B. All Utilities Uses, Industrial Uses, and Agricultural Uses are prohibited within the Downtown District with the exception of the following permitted uses, which are subject to the approval of all required development permits:
 - 1. Permitted Utilities Uses:
 - a. Communication Facilities
 - b. Electrical Facilities
 - c. Sewage Collection Facilities
 - d. Stormwater Facilities
 - e. Water Supply Facilities
 - 2. Permitted Industrial Uses allowed in the Downtown, except not allowed on Retail Streets, Festival Streets, Or Low-Impact Mixed Use Streets. Where permitted, such uses shall be secondary to the primary use of the sites for commercial, retail, or residential uses:
 - a. Limited Manufacturing/Assembly, Level 1
 - b. Flex Space, Level 1/2¹
 - c. Printing and Publishing
 - d. Research, Development, and Laboratories, Level 1
 - e. Industrial Accessory Uses, if accessory to one of the permitted Industrial Uses listed above.
- C. Low-Impact Mixed-Use Roads district: Permitted uses include professional office uses, personal services, private training school, community and cultural services, multiple-family residential uses, and community and cultural services in areas not suitable for general commercial development or adjacent to residential development. The Director may permit other uses consistent with the uses allowed in the NC2 zoning district where the footprint of development and customer visits are compatible with adjacent single-family residential development within or outside the Downtown district.

18A.35.230 District-wide Development Standards

Unless otherwise specifically modified by an adopted [development](#) agreement or Master Plan, in addition to the regulations and requirements contained in other sections of this title, the following property [development](#) standards apply to all land and [buildings](#) in the Downtown District:

- A. *Density.* Maximum density 80 units per acre except that density may be increased up to 100 units per acre for affordable housing according to the provisions of LMC 18A.50.740 Inclusionary Density Bonus-

¹ Related Code Update: Consider adding retail into the flex space definitions in 18.20.700.E This may allow artisanal industrial production, such as furniture making, breweries, coffee roasters with a retail store in the front.

Housing Incentives Program. No density limit may be allowed in the Town Center Incentives Overlay if a Master Plan is approved per LMC 18A.35.720, provided that that total number of dwellings is consistent with the Planned Action Ordinance (XXX) and allowable height and bulk per 18A.35 design parameters.

B. *Lot Size*. There is no minimum established [lot](#) size for the Downtown District. Proposed [uses](#) and the applicable design standards in this Chapter shall be used to establish the minimum [lot](#) size for a project.

C. *Lot Coverage*. There is no maximum lot coverage standard for the Downtown District. However, lot coverage may be reduced on individual properties due to stormwater or landscaping requirements.

D. *Setbacks*. The minimum distance [setbacks](#) for the Downtown District shall be determined by frontage type in LMC 18A.35.400, except where increased [setbacks](#) due to [building](#)/fire code requirements apply.

E. *Building Height*. The maximum [building height](#), not including any applicable height bonus, for the Downtown District shall be determined by frontage type in LMC 18A.35.400, except where the Transition Area Standards under LMC 18A.35.250, require less height.

F. *Large-Scale Commercial Facilities*. [Large-Scale Commercial Facilities](#) shall meet the additional requirements specified in LMC [18A.50.241\(L\)](#).

G. *Design*. Design features shall be required as set forth in LMC 18A.35.300 Streets and Blocks and 18A.35.400 Site Design, Buildings, and Frontage and the community design standards in 18A.50.231.

H. *Tree Preservation*. [Significant tree](#) identification and preservation and/or replacement shall be required as set forth in LMC [18A.50.300](#), [Tree](#) Preservation.

I. *Landscaping*. [Landscaping](#) shall be provided as set forth in LMC 18A.35.500 and [18A.50.400²](#), [Landscaping](#).

J. *Parking*. Parking shall conform to the requirements of LMC 18A.35.600 and [18A.50.500](#), Parking.

K. *Signs*. Signage shall conform to the requirements of LMC [18A.50.600](#), Sign Regulations.

L. *Tax Incentive*. The Downtown District is considered a residential target area for purposes of LMC 3.64³, Tax Incentive Urban Use Center Development.

18.35A.240 Colonial District Standards

American Colonial Revival Design is a part of Lakewood's history. The Lakewood Theater, constructed in 1937, is the most prominent example of Colonial Architecture in the District. Newer buildings in the district continue to exhibit Colonial Architectural elements and the community desires to reinforce the character of the district through adoption of design standards, but also provide flexibility to support other goals for activating public spaces in Downtown including along Motor Avenue adjacent to the Lakewood Theater.

² Minor consistency edits in this section would be needed but vast majority still applies

³ Amend map to include new Downtown boundary.

- A. Purpose and Intent. To maintain and enhance the colonial character and design elements within the district and require new development and modifications to be compatible with the scale, materials, and architectural elements of American Colonial Revival architecture. Sites and buildings should be designed to be recognizable as modern structures and not to appear as historic structures. Creativity is encouraged to design sites and buildings that represent modern interpretations of Colonial Architecture through building scale, materials, symmetry, window patterns, entry design, and other elements. Development shall be consistent with the Secretary of the Interiors Standards for the Rehabilitation of Historic Structures for new additions, exterior alterations, and related new construction (Standards 9 and 10).⁴ Additionally, the Colonial District Design Standards are intended to achieve the following:
1. To improve the image and character of the District and the Downtown.
 2. To connect to Lakewood's History and identity as it relates to colonial architecture and the district's role as a community gathering place
 3. To support the community's vision for a vibrant Downtown for all and the implementation of the Downtown Subarea Plan
 4. To create a great experience on Downtown Streets and in public spaces that is unique to Lakewood
- B. Relationship to Other Standards. Development in the Colonial District shall be consistent with the frontage standards in Section 18A.35.400.A and all other standards in Section 18A.35 unless waived by the City based on site specific conditions and to further purpose and intent of the Colonial District design standards.
- C. Review Levels. The following types of review are required for development and modifications in the Colonial District.
1. Facade Improvements and Modifications. Modifications to the facade of existing structures are required to comply with the standards of the Colonial District only when determined by the City to be practical and consistent with the scope of the planned improvements. The addition of

⁴ These are the two standards that would apply: 9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.

10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

detailed Colonial Architectural elements to non-colonial style buildings is not required. Examples of modifications that require compliance with the Colonial District standards may include:

- i. Replacement of facade materials
 - ii. Replacement of windows
 - iii. Modification of building entrances
 - iv. Roof replacement
2. Additions. Building additions shall comply with the requirements of the Colonial District Design Standards where practical and consistent with the design of the existing building and other buildings on the site. Additions and modifications that are valued at more than 50% of the value of existing improvements on site shall comply with the Colonial District Design Standards for new buildings and redevelopment.
 3. New Buildings and Redevelopment. New buildings and redevelopment shall comply with all Colonial District Design Standards.
- D. Design Standards. Buildings and structures in the Colonial District shall comply with the following design standards:
1. Roofs. Building shall use the following roof lines:
 - i. Gambrel
 - ii. Gable
 - iii. Side-Gable
 - iv. Hip Roof
 2. Symmetrical Window Fenestration. Window patterns shall be designed to be symmetrical and consistent with American Colonial Revival architecture for all stories above the first floor. First floor facades shall meet the frontage and transparency requirements in Section 18A.35.400.A
 3. Facade Materials. The following facade materials shall be used unless an alternative is Approved by the City upon determination it is consistent with the purpose and intent of the Colonial District Design Standards
 - i. Brick
 - ii. Clapboards
 - iii. Wood
 4. Prominent Front Entry Design. The entry shall be designed to be prominent and, where practical, centered along the primary street frontage.
 5. Design Elements. Designs should include some of the following architectural elements:
 - a. Columns
 - b. Chimney

- c. Cupola
- d. Arched Windows
- e. Gablet Dormers
- f. Pediment
- g. Shutters
- h. Portico
- i. Other elements, as approved by the City, that are consistent with American Colonial Revival Architecture.

18.35A.250 Transition Area Standards

The transition area provides a buffer between higher intensity uses in the Downtown District and lower intensity uses in the residential zones that surround Downtown. When development is planned adjacent to residential uses it shall incorporate the following elements into its site and building design to soften its impact and result in a compatible transition.

A. Building Height. Building Height in the transition area is limited to 10 feet higher than the maximum height of the district receiving the transition.

B. Building Setbacks. Structures within the transition area must be setback 30 feet from the property line of the district receiving the transition. At least 20 feet of the setback shall be planted as a landscaped buffer consistent with the landscape standards in LMC 18A.50.400.

C. Parking and Loading. Surface parking lots and loading zones shall be located away from adjacent residential properties when feasible. Surface parking lots and loading zones that are visible from the ground level views of the abutting residential district shall be screened through the use of berms, hedges, walls, or combinations thereof.

D. Refuse Containers. Refuse and recycling containers shall be located on the side of the building facing away from the abutting residential district, but may not be located in a front setback. All refuse and recycling containers shall be contained within structures enclosed on all four sides and utilize lids made of molded plastic or other sound buffering materials.

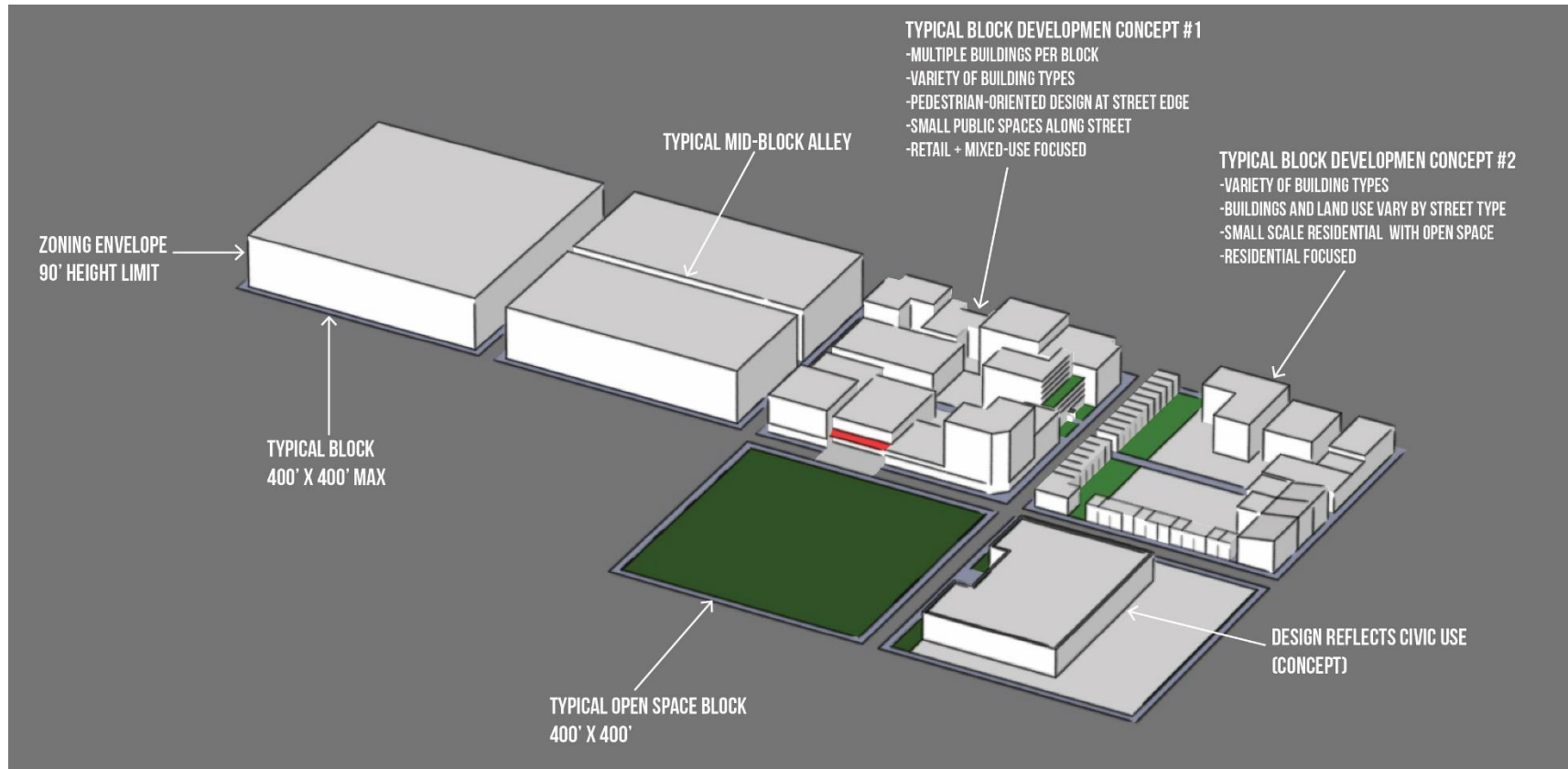
E. Mechanical Equipment. All mechanical equipment which is located on the roof shall be incorporated into the roof form and not appear as a separate penthouse or box. Mechanical equipment shall be fully screened and accommodated within the maximum height limit.

18A.35.300 Streets and Blocks

A. Street Grid and Blocks.

1. *Street Grid.* New and redevelopment must demonstrate the plan supports and accommodates the expansion of the public street grid to improve circulation for vehicles, pedestrians, and bicyclists. A circulation plan must be submitted for review by the City as part of any development permit in Downtown unless waived by the City upon finding the project will not impact circulation or the enhancement of the public street grid.
2. *Block Size.* The maximum block size is 400' and the maximum distance permitted between public streets. New public street alignments shall be consistent with the regulating plan map. The City may approve modifications to the street alignments and waive the 400' maximum block size to take advantage of existing travel corridors, the location of utilities, and required improvements.
3. *Private Streets.* Private streets shall only be permitted when the City has determined there is no public benefit for circulation in the Downtown. All private streets must be constructed to public standards.
4. *Mid-block Connections.* A minimum 20' wide mid-block connection shall be provided at the midpoint along each block face or every 200'. The mid-block connections shall be designed to accommodate service needs and for pedestrian use and be free from permanent obstructions.
5. *Street Sections.* The typical street sections provided below are the minimum requirements for the design of public streets. The City may approve modifications to the typical street section based on localized conditions and adjacent land uses. Modifications may include adding or removing on-street parking, wider sidewalks, loading zones, bicycle facilities, and transit accommodations.
6. *Block Development.* The minimum number of buildings per 400' of block frontage is 4 or one building per 100' to create variety in the streetscape experience and support human-scale design. A single-building may meet this requirement through building design and architecture that visually appears as multiple buildings. The City may approve modifications to this requirement based on site specific conditions including parcel ownership and configuration.
7. *Street Grid and Block Diagram.* The diagram in Figure 2 highlights the major features of the form-based code for the CBD including block size, building height, mid-block connections, and typical development within the block and grid structure. Development in the CBD should be generally consistent with the major features highlighted in this diagram.

18A.35-300-1. Street Grid and Block Diagram



B. Street Types

1. *Retail Streets.* Street level retail is required along the frontage of designated retail streets including 59th Street SW and Lakewood Towne Center Boulevard SW. Pedestrian-oriented design standards for retail streets address site and building design, building frontages, window transparency, weather protection, sidewalk widths, street entrances, access, and on-street parking. Building frontage types are more limited on retail streets.

18A.35-300-2. 59th Street SW Retail Street Concept



2. *Mixed-Use Streets.* Mixed-use streets support a variety of activities and functions both in the public right-of-way and development along the street edges. Street level retail is permitted, but not required and a wider range of building frontage types are permitted including street-level residential and office uses. Mixed-use streets also require pedestrian oriented design and requirements may vary based on the location with the Downtown.
 3. *Arterial Mixed-Use Streets.* Arterial mixed-use streets maintain the existing vehicle capacity and allow a range of land use and building frontage types and including surface parking lots.
 4. *Low-Impact Mixed-Use Streets.* Low-impact mixed-use streets allow for less intense commercial uses while maintaining the existing residential character for site and building design. Lower-density multi-family residential uses are permitted. Street design shall be consistent with City standards for residential streets including sidewalks.
 5. *Green Loop.* Streets designated as part of the Green Loop include design features for pedestrian and bicycle use and vary by street. Pedestrian and bicycle facilities include a shared-use path, sharrows, sidewalks, and other park like amenities that may include seating, bicycle parking, a bicycle repair station, signage, and landscaping.
 6. *Festival Street.* Festival streets are designed to support a variety of activities and events in addition to typical street elements such as travel lanes, parking, and sidewalks. Festival streets are developed with high-quality materials and are intended to be significant community gathering places within the CBD.
- C. *Street Standards and Frontage Types.* The following street standards are the minimum required and modifications to the standards may be approved by the City upon finding that the modification is consistent with the Vision and Guiding Principles in 18A.35.110 and the Downtown Plan, supports pedestrian-oriented design, and balances the needs for traffic flow to minimize congestion.

18A.35-300-3. Street Standards and Frontage Types

Street Type	Sidewalk Width	Linear	Forecourt	Plaza	Landscape	Porch/Stoop/Terrace	Parking
Retail Street	14' Minimum	P	P	P/R ¹	X	X	X
Mixed-Use Street	10' Minimum	P	P	P	P	P	X
Low-Impact Mixed-Use Streets	As determined by Public Works	P	P	P	P	P	X
Arterial Mixed-Use Street	As determined by Public Works	P	P	P	P	P	P
Festival Street	10' Minimum	P	P	R ²	X	X	X

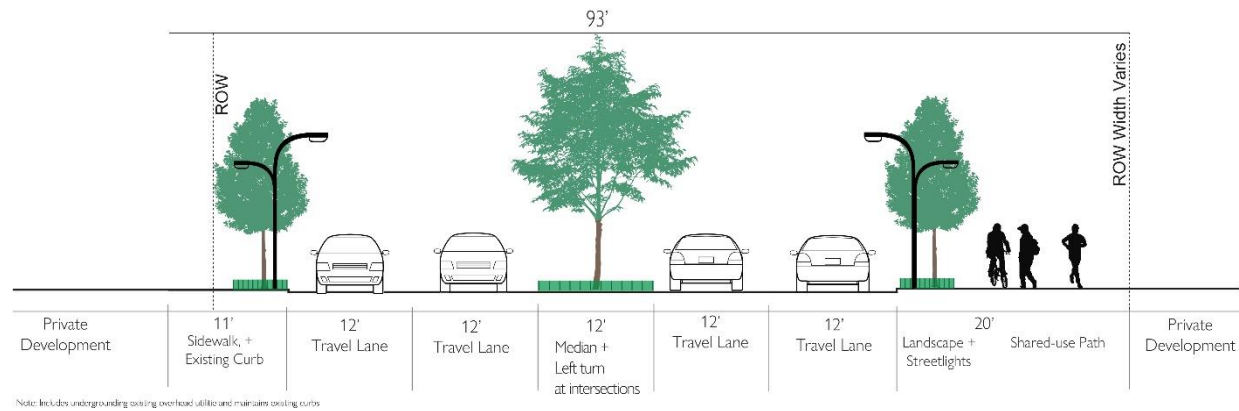
P=permitted, X=Prohibited, Required

¹Required when on a corner lot.

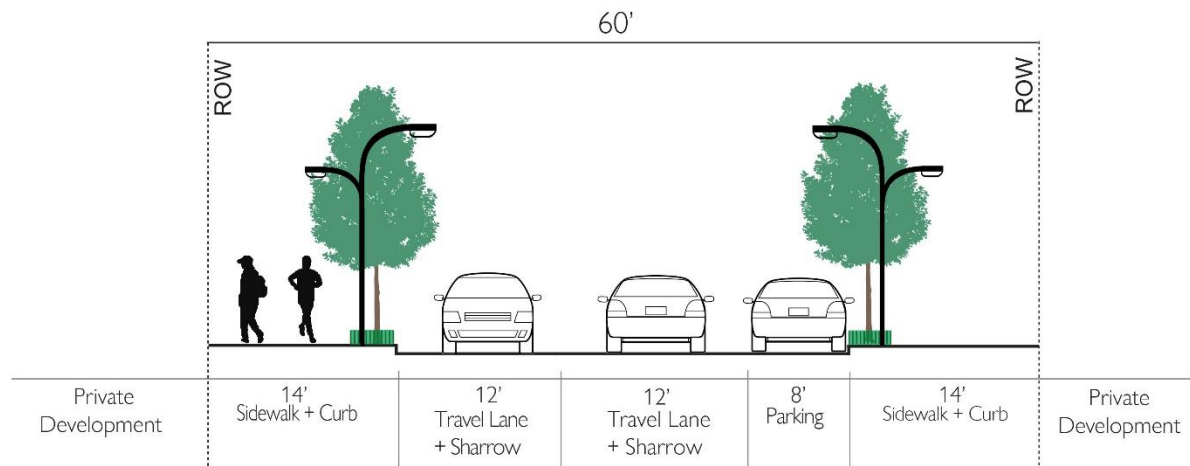
²Required pursuant to Motor Avenue Design Plan.

- D. Street Sections. The following street sections show the basic elements of the streetscape for each of the streets highlighted in the CBD. The City may approve modifications to the typical cross sections based on site-specific conditions including adjacent land uses, traffic management, parking needs, and right-of-way constraints.

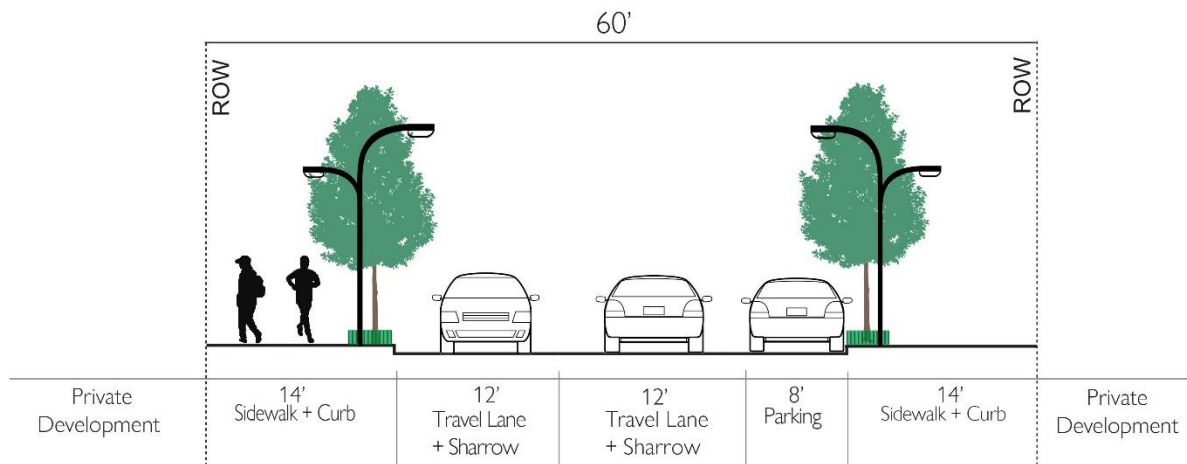
18A.35-300-4. Gravelly Lake Drive (Between Main Street SW and Bridgeport Way SW Looking North)



18A.35-300-5. 59th Avenue SW (Between Main St SW and 100th St SW Looking North)

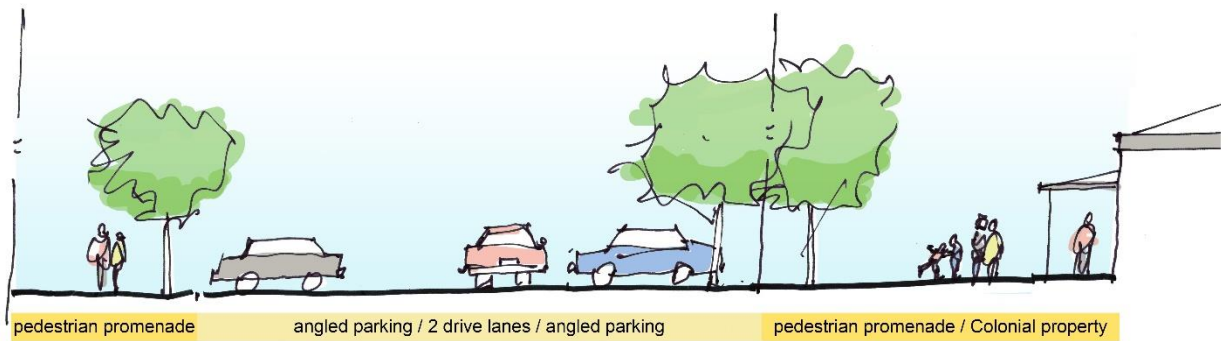


18A.35-300-6. Lakewood Towne Center Boulevard SW Looking North

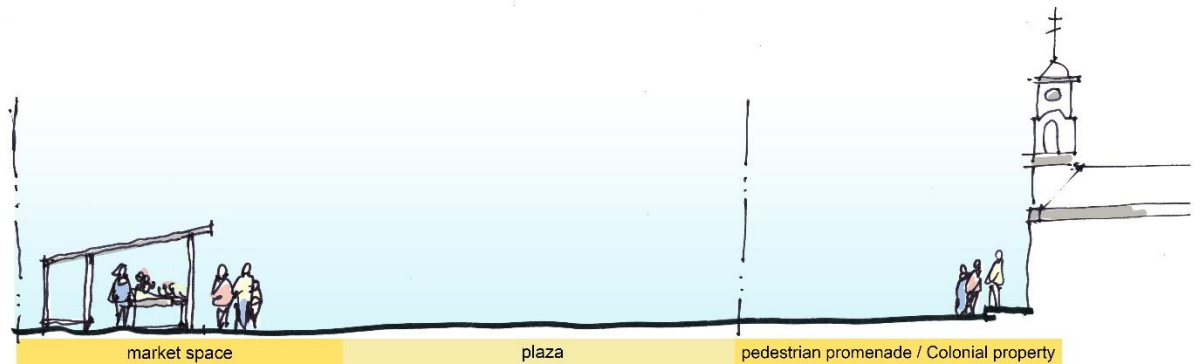


1. Motor Avenue Festival Street. Improvements to Motor Avenue should be consistent with the Motor Avenue Urban Design Plan Preferred Alternative as adopted by the City Council or as modified by the City.

18A.35-300-7. Motor Avenue Festival Street Section Looking Northeast (Preferred Alternative Selected by City Council)



18A.35-300-8. Motor Avenue Festival Street Plaza Section (Typical)

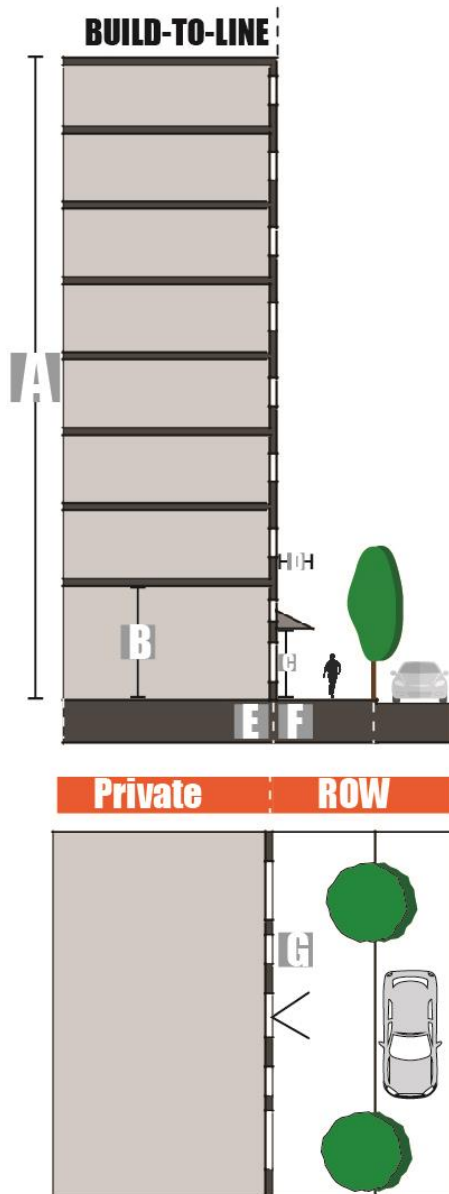


18A.35.400 Site Design, Buildings, and Frontage

- A. Frontage Types. The building frontage types below address the required standards for the relationship of buildings to the edge of the street and other site plan and design requirements. The permitted frontage types vary by street type as shown in Section 18A.35.300.C. The building setback may be modified as approved by the City when necessary to expand the width of the right-of-way to accommodate the desired street design and cross section.

1. Linear. The linear building frontage has zero setback from the street edge and is the primary frontage type on retail streets and is also appropriate for land uses such as townhouses and row houses.

18A.35-400-1. Linear Building Frontage

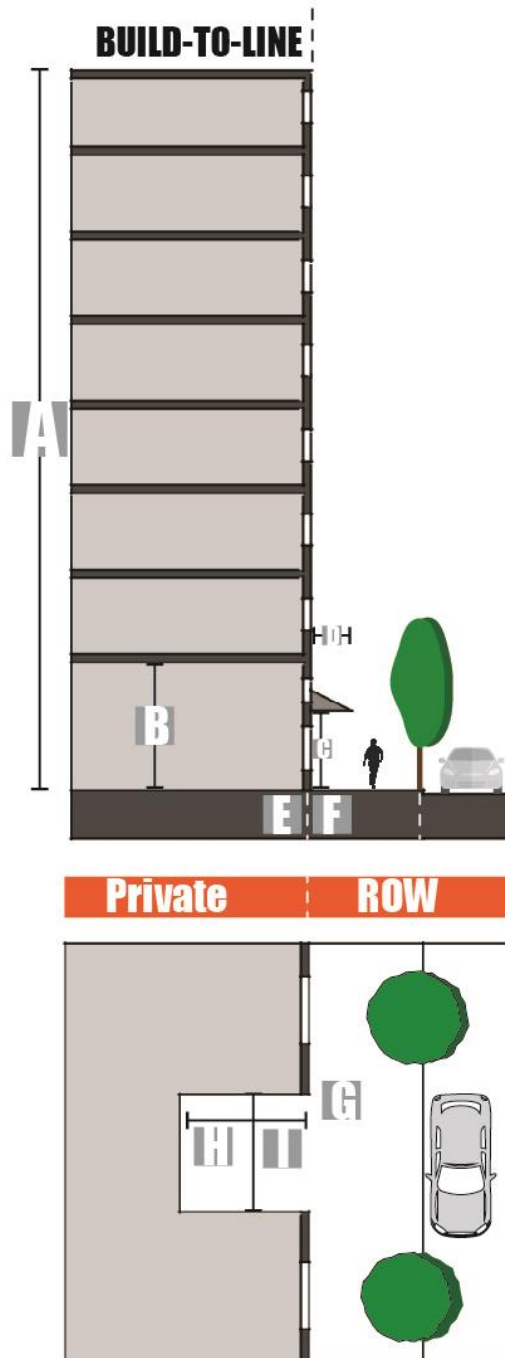


18A.35-400-2. Linear Frontage Standards

	Standard	CBD	Retail Street	Mixed-Use Street	Mixed-Use Arterial	Low-Impact Mixed-Use Streets	Festival Street
A	Building Height	90'	90'	90'	90'	35'	90'
B	First Floor Minimum Height	16'	16'	16'	16'	None	16'
C	Weather Protection Height	10'	10'	10'	10'	None	10'
D	Weather Protection Minimum Depth and Linear Frontage	10' Depth 60% of frontage must have weather protection	10' Depth Weather projection required for minimum of 60% of frontage	10' Depth Weather projection required for minimum of 60% of frontage	10' Depth Weather projection required for minimum of 60% of frontage	None	10' Depth 60% of frontage must have weather protection
E	Building Maximum Setback from Planned Right of Way	0'	0'	0'	0'	0'	0'
F	Building Setback Minimum	0'	0'	0'	0'	0'	0'
G	Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required
H	Window Transparency	A minimum 80% transparency required at street level between 30" and 8'	A minimum 80% transparency required at street level between 30" and 8'	A minimum 60% transparency required at street level between 30" and 8' for non-residential uses.	None.	None.	A minimum 80% transparency required at street level between 30" and 8'

2. Forecourt. The forecourt building frontage type that has an open area at the entrance along the street edge. This building type is applicable to a wide range of land use types and mixed-use development.

18A.35-400-3. Forecourt Frontage Type

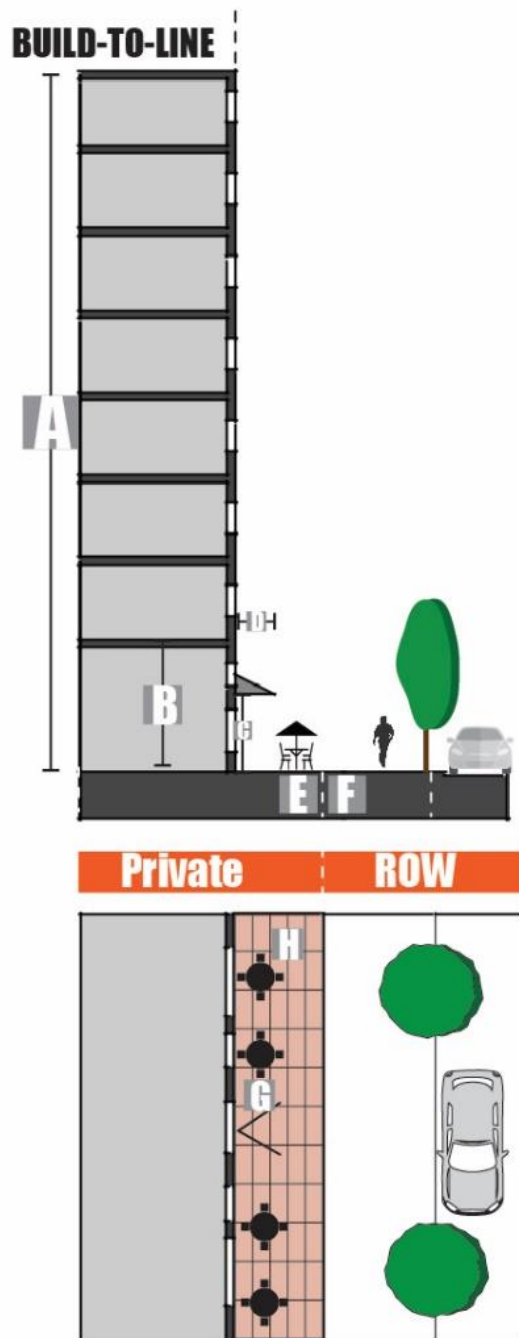


18A.35-400-4. Forecourt Frontage Standards

Standard		CBD	Retail Street	Mixed-Use Street	Mixed-Use Arterial	Low-Impact Mixed-Use Streets	Festival Street
A	Building Height	90'	90'	90'	90'	35'	90'
B	First Floor Minimum Height	16'	16'	16'	16'	None	16'
C	Weather Protection Height	10'	10'	10'	10'	None	10'
D	Weather Protection Minimum Depth and Linear Frontage	5' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	5' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	15' min depth 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	5' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	None	5' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.
E	Building Maximum Setback from Planned Right of Way	0'	0'	0'	0'	0'	0'
F	Building Setback Minimum	0'	0'	0'	0'	0'	0'
G	Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required
H	Forecourt Depth	10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum
I	Forecourt Width	10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum
J	Window Transparency	A minimum 80% transparency required at street level between 30" and 8'	A minimum 80% transparency required at street level between 30" and 8'	A minimum 60% transparency required at street level between 30" and 8' for non-residential uses	None	None	A minimum of 80% transparency required at the street level between 30" and 8'

3. Plaza. The plaza frontage type includes a pedestrian-oriented plaza between the building and the street edge. The plaza frontage type is applicable to retail and dining uses and can support activities such as outdoor dining, public art displays, seating, entertainment, and events. The plaza must be designed to support human activity and support a safe and inviting streetscape environment.

18A.35-400-5. Plaza Frontage Type

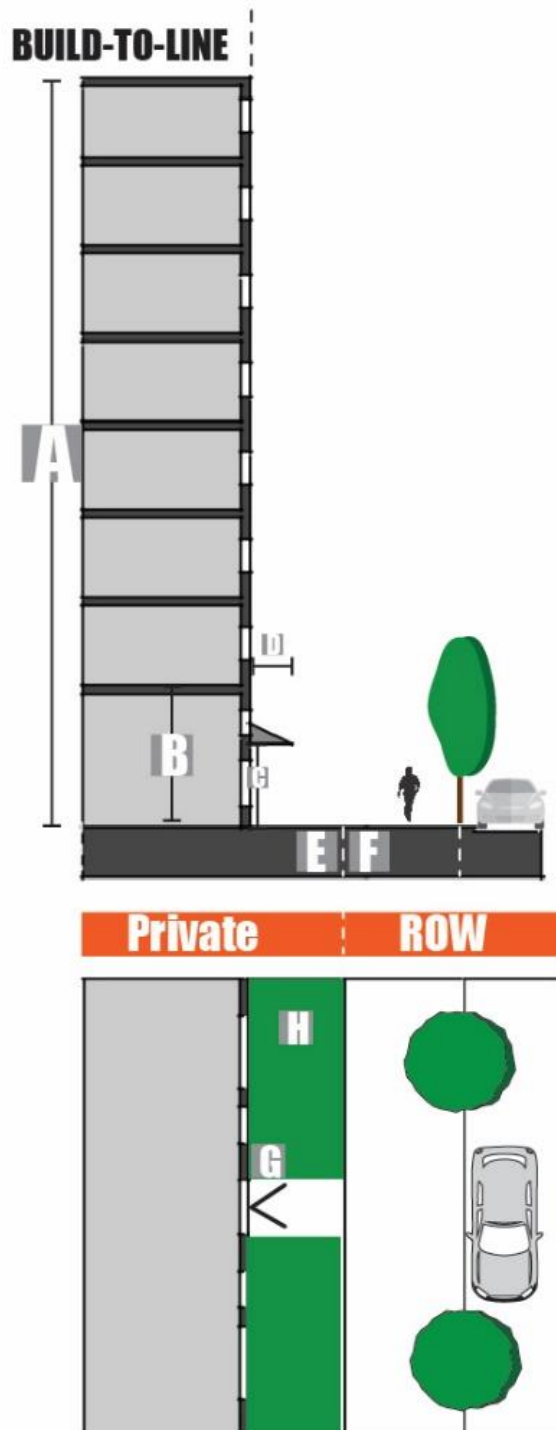


18A.35-400-6. Plaza Frontage Type Standards

	Standard	CBD	Retail Street	Mixed-Use Street	Mixed-Use Arterial	Low-Impact Mixed-Use Streets	Festival Street
A	Building Height	90'	90'	90'	90'	35'	90'
B	First Floor Minimum Height	16'	16'	16'	16'	None	16'
C	Weather Protection Height	10'	10'	10'	10'	None	10'
D	Weather Protection Minimum Depth and Linear Frontage	5' min depth; 60% of frontage along sidewalk must have weather protection.	5' min depth; 60% of frontage along sidewalk must have weather protection.	5' min depth; 60% of frontage along sidewalk must have weather protection.	5' min depth; 60% of frontage along sidewalk must have weather protection.	None	5' min depth; 60% of frontage along sidewalk must have weather protection.
E	Building Maximum Setback from Planned Right of Way	0'	0'	0'	0'	0'	0'
F	Building Setback Minimum	0'	0'	0'	0'	0'	0'
G	Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required
H	Plaza Design	Plaza area must be designed for human activity and include seating, landscaping, and other amenities	Plaza area must be designed for human activity and include seating and other amenities.	Plaza area must be designed for human activity and include seating and other amenities.	Plaza area must be designed for human activity and include seating and other amenities.	n/a	Plaza area must be designed for human activity and include seating, landscaping, and other amenities

4. Landscape. The landscape frontage has landscaping between the building and street edge. The landscape frontage is not permitted on retail streets and is appropriate for office and residential uses particularly when on the ground floor.

18A.35-400-7. Landscape Frontage

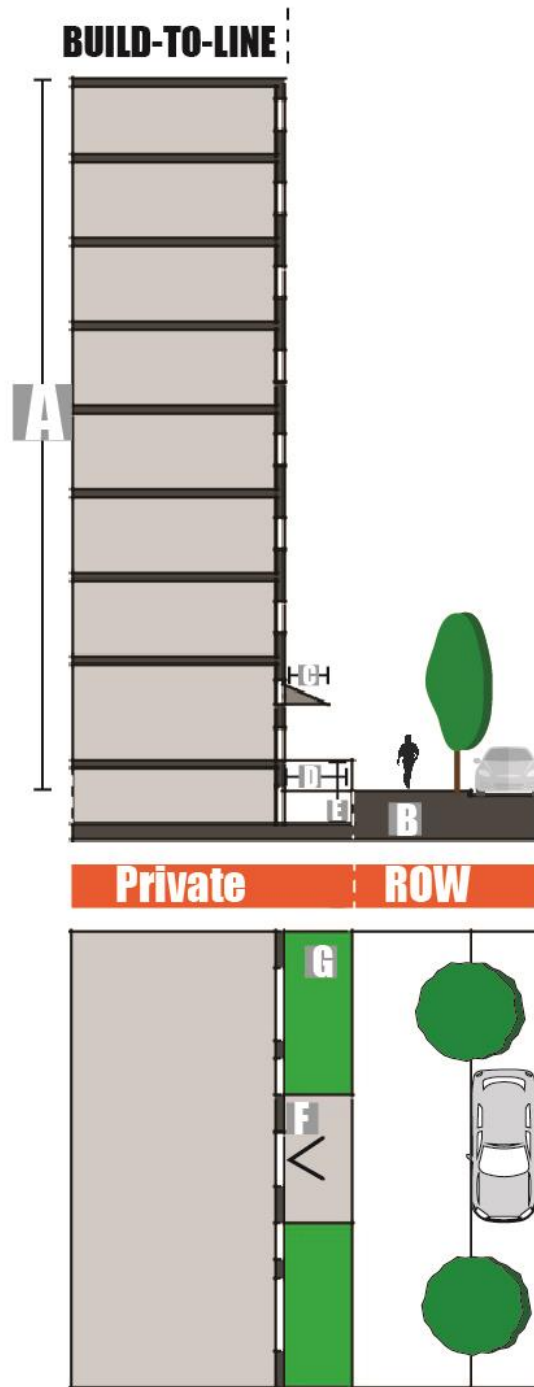


18A.35-400-8. Landscape Frontage Standards

	Standard	CBD	Retail Street	Mixed-Use Street	Mixed-Use Arterial	Low-Impact Mixed-Use Streets	Festival Street
A	Building Height	90'	n/a	90'	90'	35'	n/a
B	First Floor Minimum Height	16'	n/a	16'	16'	None	n/a
C	Weather Protection Height	10'	n/a	10'	10'	None	n/a
D	Weather Protection Minimum Depth and Linear Frontage	5' minimum depth; required over primary entrance	n/a	5' minimum depth; required over primary entrance	5' minimum depth; required over primary entrance	5' minimum depth; required over primary entrance	n/a
E	Building Maximum Setback from Planned Right of Way	20'	n/a	20'	20'	20'	n/a
F	Building Setback Minimum	10'	n/a	10'	10'	10'	n/a
G	Front Entrance	An entrance must be located along the primary street frontage	n/a	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage	n/a
H	Landscape Requirements	The landscape area shall comply with the City's landscaping requirements.	n/a	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	n/a

5. Porch/Stoop/Terrace. This frontage type has the building setback from the street edge to accommodate a porch, stoop, or terrace to serve as the primary access to the building. This frontage type is applicable primarily to residential and non-retail commercial uses and is not permitted on retail streets.

18A.35-400-9. Porch/Stoop/Terrace Frontage Type

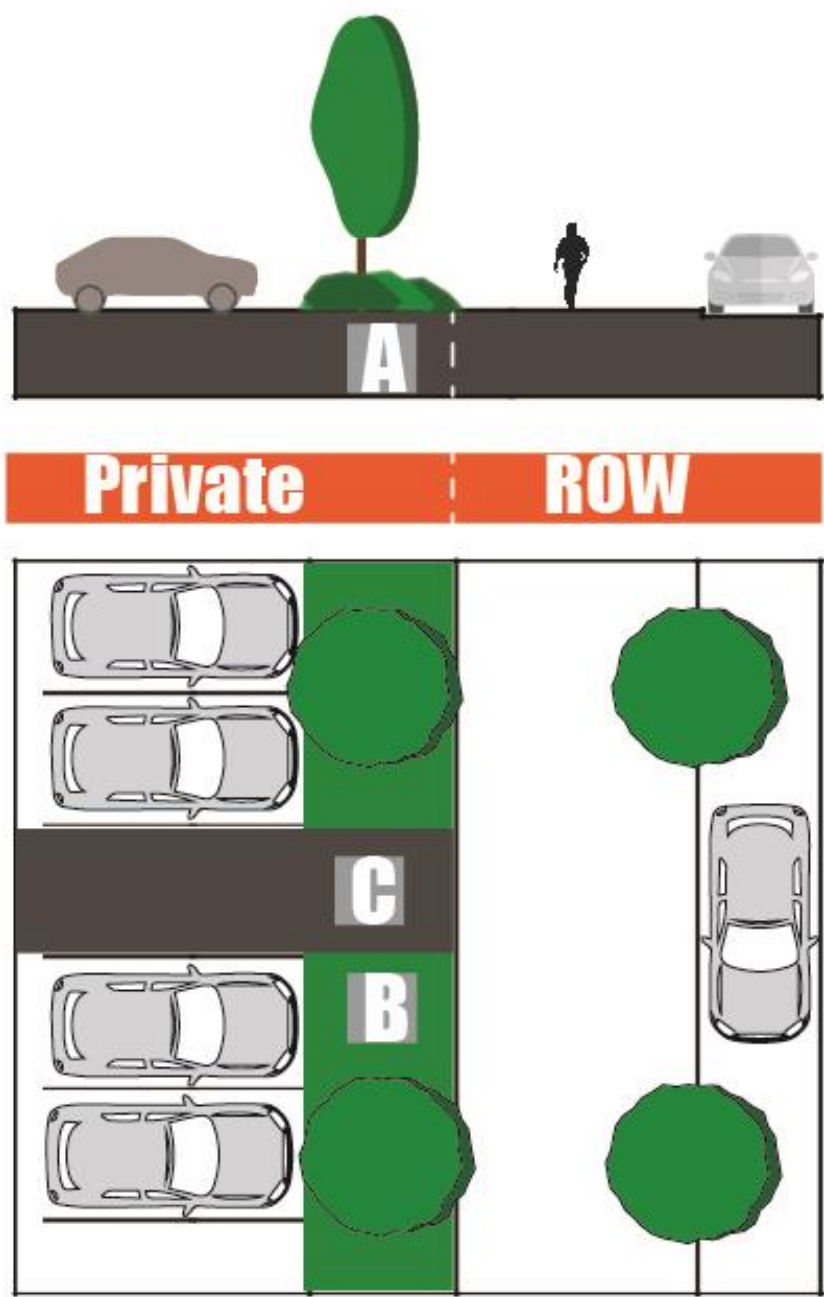


18A.35-400-10. Porch/Stoop/Stoop Frontage Standards

	Standard	CBD	Retail Street	Mixed-Use Street	Mixed-Use Arterial	Low-Impact Mixed-Use Streets	Festival Street
A	Building Height	90'	n/a	90'	90'	35'	n/a
B	First Floor Minimum Height	16'	n/a	16'	16'	None	n/a
C	Weather Protection Height	10'	n/a	10'	10'	None	n/a
D	Threshold Depth	4' minimum	n/a	4' minimum	4' minimum	None	n/a
E	Threshold Height	Below-grade maximum 4'; Above-grade maximum 5'	n/a	Below-grade maximum 4'; Above-grade maximum 5'	Below-grade maximum 4'; Above-grade maximum 5'	Below-grade maximum 4'; Above-grade maximum 5'	n/a
F	Front Entrance	An entrance must be located along the primary street frontage;	n/a	An entrance must be located along the primary street frontage;	An entrance must be located along the primary street frontage;	An entrance must be located along the primary street frontage;	n/a
G	Landscape Requirements	The landscape area shall comply with the City's landscaping requirements.	n/a	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	n/a

6. **Parking.** The parking frontage type is only permitted on Arterial Mixed-Use streets and is the only frontage type where parking is allowed between the building and the street edge. Landscape and trees are required in the landscaped area between the street edge and the parking area.

18A.35-400-11. Parking Frontage Type



18A.35-400-12. Parking Frontage Standards

	Standard	CBD	Retail Street	Mixed-Use Street	Mixed-Use Arterial	Low-Impact Mixed-Use Streets	Festival Street
A	Landscape Buffer Width	Minimum 10'; Maximum 20'	n/a	n/a	Minimum 10'; Maximum 20'	n/a	n/a
B	Landscape Requirements	The landscape area shall comply with the City's landscaping requirements. A minimum of one tree must be planted every 30'.	n/a	n/a	The landscape area shall comply with the City's landscaping requirements.	n/a	n/a
C	Pedestrian Walkway	Required from the street to primary building entrance(s)	n/a	n/a	Minimum width 8'	n/a	n/a

B. Civic Uses. Sites and buildings where the primary use is civic should be designed to reflect a public use and therefore may deviate from the frontage standards. Civic buildings and sites should be recognizable and accessible as public buildings in the Downtown and to reflect civic pride in the community. The following standards address the design of civic uses:

1. Civic uses are permitted on the ground level on all street types except the low-impact mixed-use streets and may be designed in accordance with any of the frontage types except Parking.
2. Civic uses are not subject to the maximum setback requirements.
3. Civic uses may provide the main entry on the side of the building when necessary to the function of the site including safety and security. A pedestrian connection shall be provided from the sidewalk to the entry.
4. The design sites and buildings for civic uses should incorporate public open space when feasible and practical and provide for a variety of activities such as public art, seating, play equipment, games, and events.

18A.35.500 Landscaping, Open Space, and Green Infrastructure

18A.35.510 BASIC STANDARDS

Landscaping shall be provided consistent with 18A.50.400 Landscaping except as supplemented or superseded by this Chapter 18A.35.

18A.35.520 SPECIAL LANDSCAPE TREATMENTS

The following special landscape treatments are required or permitted along street types.

18A.35.520 -1. Special Landscape Treatments

Street Type	Foundations	Plazas	Courtyards	Pedestrian Walkways
Retail Street	R	R	P	R
Mixed-Use Street	R	P	R	R
Low-Impact Mixed-Use Streets	P	P	R	P
Arterial Mixed-Use Street	P	P	P	R
Festival Street	R	R	P	R

P=permitted, R=Required

- A. Foundations: Buildings shall be accented by foundation plantings. Foundation plantings may frame doors, anchor corners, or screen undesirable areas with. Foundation plantings shall allow for 5' of unobstructed movement on the sidewalk. Examples of foundation plantings include raised planter boxes, containers, and similar.
- B. Plazas and Courtyards: Plazas are required for commercial and mixed-use developments fronting retail streets and festival streets. Courtyards are required for multifamily and mixed-use developments along mixed-use streets and low-impact mixed use streets.
- C. Plazas and courtyards shall be visible and accessible to the public.
- D. Minimum dimensions of plazas and courtyards are 20'. Courtyards may qualify as common open space per 18A.35.520 Common and Open Space Standards.
- E. Plazas or courtyards shall include one or more element from each category below.
 - 1. Special Paving: Scored concrete, stained/colored concrete, concrete pavers, paving inlays, mosaics, or other special paving material.

2. Landscaping: Trees, shrubs, trellises, flowers, or container plants. Landscaping shall be consistent with LMC 18A.35.500 and 18A.50.400, Landscaping.
3. Seating Area: Benches or low seating walls. Walls or benches shall be a min. of 4' long. Seating shall be provided at a ratio of at least one seat per 60 square feet of plaza area or open space.

F. Pedestrian Walkways: Pedestrian walkways are required as follows:

1. Street to Building: Required between a public right-of-way and building entrances where the building is set back from the street, sidewalk, or parking area.
2. Parking: Required between parking lots and building entrances.
3. Mid-block connections: One pedestrian walkway shall be provided between the street and the rear property line, spaced an average of every 200' or less of street frontage.

G. Dimensional Standards:

1. Pedestrian Walkway Width

- a. 6' minimum exclusive of landscaping or site furnishings, except that mid-block crossings shall have minimum walkway widths of 12'.
- b. Lines of Sight: Walkways shall be located and designed with clear sight lines for safety.
- c. Definition: Pedestrian walkways shall be defined through paving, landscaping, furnishings, and lighting.

2. Design:

- a. Pedestrian walkways shall be defined using a combination of one or more of the following techniques as follows:
- b. Special Paving: Scored concrete, stained/colored concrete, concrete pavers, paving inlays, mosaics, or other special paving material. Not all portions of the pedestrian walkway are required to be paved; however, a min. of 4' in width of the pedestrian walkway shall provide an all-weather walking surface.
- c. Architectural Features: Trellises, railing, low seat walls, weather protection, bollards, or other architectural features. Chain link fences are not allowed.
- d. Landscaped Edges: A continuous, landscaped area consistent with Type II Streetscapes where the pedestrian walkway is from the street to building or mid-block connection, otherwise Type III, Open Space is required, per LMC 18A.50.400, Landscaping. If the walkway abuts a driveway or driving aisle on both sides, the landscaped edge shall apply to both sides.
- e. Lighting: Pedestrian walkways shall include lighting such as pedestrian lights, bollards, and accent lighting to assist pedestrian navigation and promote a safe and comfortable walking space.

18A.35.530 COMMON AND OPEN SPACE STANDARDS

The purpose of this section is to provide residents with access to useable privately maintained and owned open space to create opportunities for active living and respite for onsite residents. This section provides a fee in lieu of providing onsite open space recognizing different site configurations, resident needs and desires, and opportunities to provide and enhance publicly owned and maintained system parks. Publicly owned urban parks include nodal and linear parks consistent with the Downtown Plan and the City's Parks, Recreation, and Open Space Plan. Transferring the fee in lieu to a nearby nodal or linear park reinforces accessibility to larger public open spaces within walking distance of current and future residential and mixed-use development in Downtown Lakewood.

A. Common Open Space – Private:

1. Each mixed-use or residential development shall provide a common open space per dwelling unit of 100 square feet per dwelling unit.
2. For the purposes of this section, common open space means an open air area intended for use by all residents, guests, employees or patrons of a site and may include lawns, gardens, squares, plazas, courtyards, terraces, barbecue and picnic areas, games court or multi-use recreational areas, and other types of built space.
3. Design Standards:
 - i. Required setback areas shall not count toward the open space requirement unless they are part of an open space that meets the other requirements of common open space.
 - ii. Space shall have a minimum dimension of 20 feet in any direction to provide functional leisure or recreational activity. This dimension can be adjusted by the Community Development Director based on site conditions such as topography or irregular lot geometry.
 - ii. Space shall feature paths or walkable areas, landscaping, seating, lighting, play structures, sports courts, or other pedestrian amenities to make the area more functional and enjoyable for a range of users.
4. Common space shall be separated from ground level windows, streets, service areas and parking lots with landscaping, low-level decorative fencing (no chain link), or other treatments as approved by the Community Development Director that enhance safety and privacy for both the common open space and dwelling units.
5. The space shall be oriented to receive sunlight and preferably face south, if possible. Open space may also face east or west, but not north, unless the Community Development Director determines that site conditions such as topography or irregular lot geometry warrant waiving this requirement. The common open space shall be designed to provide landscaping that defines the open space but permit surveillance from units and roads.
6. The space must be accessible from the dwelling units. The space must be oriented to encourage activity from local residents.
7. No more than thirty (30) percent of the area may be covered by a structure.

8. The common open space may include multi-use stormwater detention facilities, if the Community Development Director determines that the facilities are designed to function as common open space by providing an enhanced nature or visually aesthetic design.
9. The common space shall be designed to ensure that the open space network addresses CPTED⁵ principles such as security and surveillance from residential units. Common recreational spaces shall be located and arranged to allow windows to overlook them.
10. The common space shall provide adequate lighting in the open space network, but place and shield lighting so that it does not glare into housing units.

B. Private Open Space:

1. Each dwelling unit shall have a private open space, at a minimum of 48 square feet with a minimum width or depth of 6 feet.
2. For the purposes of this section, private open space includes individual decks, balconies, or patios.

C. Fee in Lieu for Portion of Open Space:

1. Within the Downtown Planned Action Area, a development may pay a fee in lieu of providing common open space or a portion of the private open space to support park land purchase and improvements within the Downtown Planned Action Area for urban nodal or urban linear parks identified in the Downtown Plan.
2. The fee in lieu is allowed for each 100 square feet of common open space not provided. No less than 50 square feet of common open space per unit shall be provided on-site.
3. The fee in lieu is allowed for balconies or patios not provided on street-front façade. No less than 50 percent of the units shall include private open spaces.
4. The fee shall be equal to the average fair market value of the land otherwise required to be provided in on-site common or private open space.
5. Fee payment and use:
 - a. The payment shall be held in a reserve account by the City and may only be expended to fund a capital improvement for parks and recreation facility identified in the Downtown Plan;
 - b. The payment shall be expended in all cases within five years of collection; and
 - c. Any payment not so expended shall be refunded with interest to be calculated from the original date the deposit was received by the City and at the same rate applied to tax refunds pursuant to RCW 84.69.100; however, if the payment is not expended within five years due to delay attributable to the developer, the payment shall be refunded without interest.

⁵ Crime Prevention Through Environmental Design.

18A.35. 540 GREEN INFRASTRUCTURE

- A. Development shall implement stormwater standards that incorporates low impact development (LID) principles and standards consistent with City standards in LMC Chapter 12A.11.
- B. Where onsite filtration is feasible, it shall be provided.
- C. Permeable surfaces shall be incorporated into plazas, courtyards, and pedestrian walkways, unless demonstrated infeasible to the satisfaction of the City Engineer, and shall meet City standards per LMC Chapter 12A.11.
- D. Open ponds shall be designed with both natural landscape and urban access and edge treatments consistent with LMC 18A.50.420.
- E. Native and/or drought tolerant landscaping shall be incorporated into required landscape plans.

18A.35.600 Parking

- A. **Off-street Parking Requirements.** The following off-street parking requirements supersede the requirements in 18A.50.560. Uses not listed below must comply with the requirements in 18A.50.560.

18A.35-600-1. Off-street Parking Requirements.

Land Use	Parking Requirement
Residential	1 per dwelling unit
Retail, Services, Restaurants	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum
Office	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum
Street level retail 3,000 sq ft or less per business	None where there is available public parking within 500'

- B. **Parking Reductions.** The amount of required parking may be reduced or eliminated based on a site-specific parking study that demonstrate one or more of the following:
 - 1. *Shared Parking at Mixed-use Sites and Buildings.* A shared use parking analysis for mixed-use buildings and sites that demonstrate that the anticipated peak parking demand will be less than the sum of the off-street parking requirements for specific land uses.
 - 2. *Public Parking Availability.* The availability of public parking to accommodate the parking demand generated by the site or building. The City may approve a reduction in the amount of required parking by up to 50% for any parking stalls that will be open and available to the public.
 - 3. *Lower Parking Demand.* Demonstrating that anticipated parking demand will be less than the minimum parking required based on collecting local parking data for similar land uses on a typical day for a minimum of 8 hours.
- C. **Parking Location and Design.** Parking shall be located behind the building or in a structure except in locations where the parking frontage type is permitted.
- D. **Shared Parking.** Shared parking is encouraged to support a walkable and pedestrian-oriented CBD where people can park once and visit multiple destinations. Off-site shared parking may be authorized per the standards in 18A.50.550.

- E. **Public Parking.** Public parking is permitted as a principal or accessory use in the Downtown District subject to the frontage and design standards.
- F. **Dimensional Standards.** Parking stall and circulation design shall meet the standards of 18A.50.

18A.35.700 Administration

18A.35.710 FORM-BASED CODE REVIEW

- A. **Purpose.** The purpose of Form-Based Code Review is to ensure that all development in the Downtown District implements the Downtown Plan.
- B. **Applicability.** All development requiring a land use or building permit in the Downtown District shall be subject to Form-Based Code Review.
- C. **Review Process.** Form-Based Code Review is a Process I application type under 18A.02.540. Review shall be conducted concurrently with any related planning review process or building permit, including consideration of the proposal as a Planned Action (Ordinance XXX), and is subject to LMC 18A.02.530⁶ Permit Procedures.
- D. **Preapplication Conference.** A preapplication conference is required for all development in the Downtown District. The preapplication conference shall be conducted in accordance with LMC 18A.02.622. The site plan, landscaping plan, and building design may be conceptual in form for the preapplication conference.
- E. **Submittal Requirements.** Applications for Form-Based Code Review shall be filed with the Community Development Department. All projects submitted for Form-Based Code Review shall be subject to the submittal requirements in LMC 18A.02.630 Project Permit Applications. Additional information or materials may be requested during the review process if the Community Development director determines they are necessary for proper review of the application. Request for additional materials shall not affect the Determination of Completeness under LMC 18A.02.635, but is subject to LMC 18A.02.735 Time Limitations.
- F. **Review Decision.** The Community Development Director shall provide the applicant with a written decision either approving, denying, or approving the application with modifications and/or conditions of approval. In such case that the application is processed in conjunction with a Process III or higher permit application under the provisions of LMC 18A.02.630 Permit Procedures, the Community Development Director shall provide a written recommendation to either approve, deny, or approve with modifications and/or conditions of approval to the appropriate decision-making body for the higher process permit, as listed in LMC 18A.02.502 Table 3.
- G. **Review Standards.** A decision on a Form-Based Code Review Application shall be based on the standard criteria for Process I Permits in LMC 18A.02.540 and the following:
 - 1. Consistency with the vision and policies of the Lakewood Comprehensive Plan.
 - 2. Consistency with the guiding principles of the Downtown Plan in LMC 18A.35.110.

⁶ Note for additional code change: Add permit type to 18A.02.502 as a Process 1 permit.

3. Compliance with the standards in LMC Chapter 18.35.
 4. Compliance with the Downtown Planned Acton Ordinance (XXX).
- H. Amendments to Approved Applications. Applicants wishing to amend approved applications may submit the amendment to the Community Development Director for review. The application shall meet the submittal requirements of 18A.35.610C unless waived by the Community Development Director. A preapplication conference is not required, but is encouraged for amendment applications that incorporate major changes.

18A.35.720 MASTER PLANNED DEVELOPMENT – TOWN CENTER INCENTIVE OVERLAY

- A. Purpose. The purpose of a Master Planned Development in the Downtown District is to provide the developer and the City the opportunity to implement the Downtown Plan in way that could not be achieved through strict application of the standards in this chapter. It also allows for the development of larger, more complex, and multi-phased projects to develop with certainty.
- B. Applicability. Development within the Town Center Incentive Overlay may apply for a Master Plan for the development of ten or more acres.
- C. Allowed Modification. Adoption of a Master Planned Development allows for the modification of Downtown District development standards within the Town Center Incentive overlay district including modification of standards for height, site design, building design, landscaping, parking, and signage. In addition, density may be calculated by evaluating the number of residential units over the entire acreage subject to the Master Plan instead of the acreage of a particular site. This allows housing units to be clustered at higher intensities, if desired.
- D. Review Process. A Master Planned Development is a Process III application under LMC 18A.02.550. Notice of application shall be provided pursuant to LMC [18A.02.670](#).⁷
- E. Submittal Requirements. Applications for a Master Planned Development shall be filed with the Community Development Department on forms provided by the Department along with established fees. An applicant for a Master Planned Development shall submit the following items to the City, unless the director finds in writing that one or more submittals are not required due to unique circumstances related to a specific development proposal:
1. A detailed narrative that includes:
 - a. A description detailing how the proposed [development](#) will be superior to or more innovative than conventional [development](#) methods as allowed under the City's land use regulations and how the approval criteria set forth in LMC [18A.35.720F](#) have been satisfied;
 - b. A description of how the proposed [Master](#) Planned Development will benefit the public in a manner greater than that achieved if the project was to be developed under the adopted standards of the CBD zone;

⁷ Note for additional code update: update the applicability for Process III permits in 18A.02.502 Table 3 to include Master Planned development.

- c. A table illustrating the density and lot coverage of the overall development, with the proportion of the site devoted to public and open space clearly indicated;
 - d. A description of the land use planned for the site, including types and numbers for commercial, civic, industrial, and residential land uses;
 - e. A description of the dwelling units proposed and the overall density and intensity, including the types and numbers of affordable housing units;
 - e. A description of the proposed park, open space and recreation areas including any proposed improvements, including specific details regarding the ownership and maintenance of such areas;
 - f. Detailed information regarding all proposed landscaping that is not included on an associated landscaping plan;
 - g. A description of the specific City standards as set forth in the underlying zoning district that the applicant is proposing for modification;
 - h. A description of how the Master Plan meets guiding principles of the Downtown Plan in LMC 18A.35.110.
2. A site plan with the heading “Master Planned Development Site Plan” that includes information including, street frontage types, building footprints, proposed landscaping, open space and parks and/or recreational areas including trails, public spaces, pedestrian walkways, parking locations, and proposed setbacks;
 3. Conceptual elevation drawings illustrating facade and building design elements, including height, overall bulk/mass and density and proposed residential design features that will provide for a superior development;
 4. A conceptual landscape plan/map showing the proposed location and types of vegetation and landscaping. The landscape plan may also be incorporated into the Master Planned Development site plan and narrative;
 5. A phasing plan, if the development will occur in distinct phases with a written schedule detailing the timing of improvements;
 6. A draft development agreement, if proposed by the applicant, or as required by the City; and
 7. All of the submittal requirements in LMC 18A.02.630 Project Permit Applications.

E. Additional Information. Additional information or materials may be requested during the review process if the Community Development director determines they are necessary for proper review of the application.

F. Required Findings: A Master Planned Development shall only be granted after the Hearing Examiner has reviewed the proposed use and has made written findings that all of the standards and criteria set forth below have been met or can be met subject to conditions of approval:

1. The Master Planned Development is consistent with the comprehensive plan; and

2. The Master Planned Development is consistent with the guiding principles of the Downtown Plan in LMC 18A.35.110.

3. The Master Planned Development by the use of permitted flexibility and variation in design, is a development practice that results in better urban design features than found in traditional development. Net benefit to the City may be demonstrated by one or more of the following:

- a. Placement, type or reduced bulk of structures, or
- b. Interconnected usable open space, or
- c. Recreation facilities, or
- d. Other public facilities, or
- e. Conservation of natural features, or
- f. Conservation of critical areas and critical area buffers beyond, or
- g. Aesthetic features and harmonious design, or
- h. Energy efficient site design or building features, or
- i. Use of low impact development techniques;

4. The Master Planned Development results in no greater burden on present and projected public utilities and services than would result from traditional development and will be served by adequate public or private facilities including streets, fire protection, and utilities; and

5. Open space within the Master Planned Development is an integrated part of the project rather than an isolated element of the project; and

6. The design is compatible with and responds to the existing or intended character, appearance, quality of development and physical characteristics of the subject property and immediate vicinity; and

7. Roads and streets, whether public or private, within and contiguous to the site comply with guidelines for construction of streets and the street frontage standards; and

8. Each phase of the proposed development, as it is planned to be completed, contains the parking spaces, open space, recreation space, landscaping and utility area necessary for creating and sustaining a desirable and stable environment.

9. The Master Plan development is consistent with the Planned Action Ordinance (XXX).

G. Action of the Hearing Examiner. In addition to demonstrating compliance with the criteria as determined by the Hearing Examiner, the applicant shall accept those conditions that the Hearing Examiner finds are appropriate to obtain compliance with the criteria.

18A.35.730 UPDATE OF NONCONFORMING LOTS⁸

A. Purpose. The establishment of design standards to implement the Downtown Plan resulted in the creation of nonconforming lots. This section defines how nonconforming lots are to be updated to meet the Downtown District design standards for street frontage, site design, and landscaping when incremental changes occur.

B. Applicability. This section applies to all nonconforming lots in the Downtown District, with the exception of lots subject to a development agreement or Master Planned Development. It supplements the standards and requirements for nonconformities in LMC 18A.02.800 through 18A.02.870.

C. Full Compliance. Within any three (3) year period, the expansion of any structure or complex of structures on a lot, which constitutes fifty percent (50%) or more of the existing floor area or building footprint, whichever is less, shall activate the requirement to bring the lot into full compliance with the Downtown District standards for street type, site design, and landscaping.

D. Proportional Compliance. Remodels, alterations, or other improvements to the existing structure activate the requirement to make improvements to the nonconforming lot to reduce the extent of the nonconformity. The degree to which the standards are applied shall be evaluated on a project specific basis and related to the improvement proposed. For example, if new windows are proposed to be installed, the project should address standards related to window transparency and weather protection (if located on a designated street frontage). The Community Development Director shall determine the type, location, and phasing sequence of proposed proportional compliance.

⁸ Notes for Additional Code Updates: Amend the definition in 18A.90:

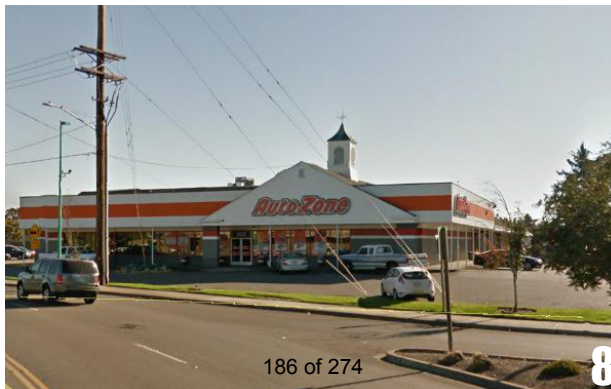
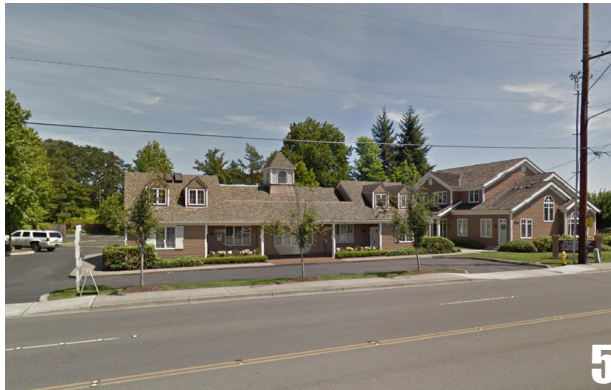
NONCONFORMING LOT. A lot which does not conform to the design or density requirements of the zoning district in which it is located. A non-conforming lot is a lot that was legal when it was created but no longer meets the current area, width, or depth dimensional requirements and/or does not meet the landscaping, site planning, or site design requirements for the zoning district in which the property is located. Nonconforming lots may be occupied by any permitted use in the district, provided that all other development regulations in effect at the time of development must be met.

Update 18A.02.850A to apply to lots in addition to uses and developments. Suggest renaming developments to structures for precision.

Update 18A.02.855 to change the title to Nonconformities instead of Nonconforming uses. Add a provision to subsection A that specifies the lot must be brought into conformity if damaged or destroyed greater than 50% of the value. Add a provision to subsection B that exempts development in the Downtown District from this standard

COLONIAL DISTRICT DESIGN

EXISTING





PRECEDENTS



ELEMENTS

* MATERIALS

- * BRICK
- * CLAPBOARDS
- * WOOD
- * GLASS

* COLOR

- * RED
- * WHITE
- * YELLOW
- * BLUE/GRAY
- * BLACK

* ARCHITECTURAL (COLONIAL REVIVAL)

- * CUPOLA
- * GABLET (DORMERS)
- * COLUMNS
- * PORTICO
- * PEDIMENTS
- * SHUTTERS
- * CHIMNEYS
- * PROMINENT ENTRY DESIGN
- * CORNICE WITH DETAILING
- * ORNATE DETAILING
- * ARCHES
- * ROOF FORMS
 - * SIDE GABLE
 - * GAMBREL
 - * HIPPED
- * WINDOWS
 - * SYMMETRICAL FENESTRATION
 - * MULTI-PANE
 - * ARCHED
 - * SMALLER UPPER STORY WINDOWS

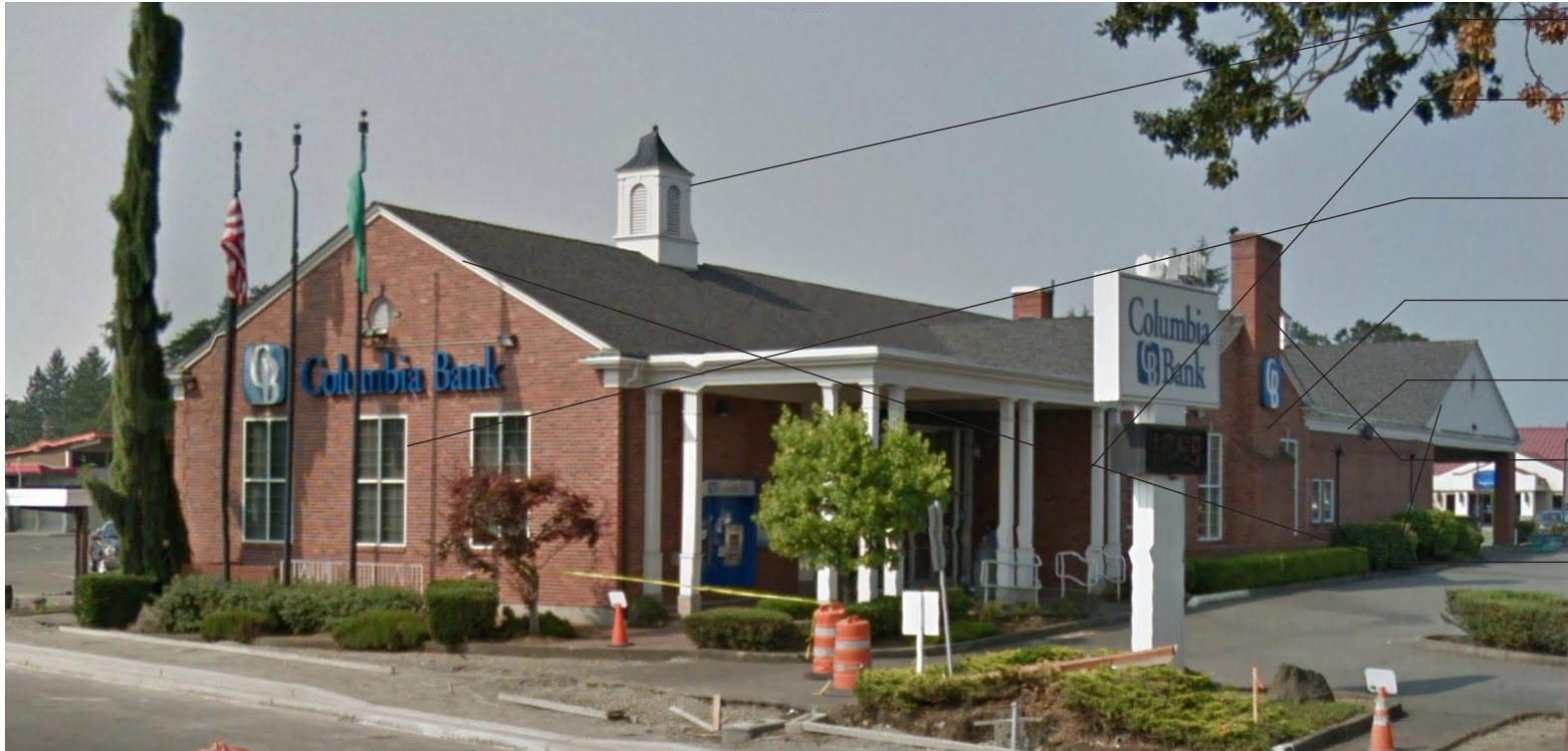
THE ELEMENTS OF COLONIAL DESIGN PROVIDE A MENU OF OPTIONS FOR THE CITY AND THE COMMUNITY TO CONSIDER IN DEVELOPING DESIGN STANDARDS FOR THE DISTRICT



DISCUSSION QUESTIONS:

- *Which elements should be the priorities and/or regulated under for the Colonial District Design Standards?
- *How do the design standards related to historic preservation, if at all? Should the Secretary of the Interiors Standards be considered as a guide?
- *Should the design standards require new buildings to be recognizable as new structures and prohibit certain historic elements to maintain authenticity?
- *Should there be additional standards/restrictions for Building Height, massing, and scale than other areas in the Downtown?

ELEMENTS



CUPOLA

COLUMNS

SYMMETRICAL
WINDOWS + PANES

BRICK

WHITE TRIM

CHIMNEY

SIDE GABLE
ROOF

DESIGN STANDARDS

Overview. American Colonial Revival Design is a part of Lakewood's history. The Lakewood Theater, constructed in 1937, is the most prominent example of Colonial Architecture in the District. Newer buildings in the district continue to exhibit Colonial Architectural elements and the community desires to reinforce the character of the district through adoption of design standards, but also provide flexibility to support other goals for activating public spaces in Downtown including along Motor Avenue adjacent to the Lakewood Theater.

Purpose and Intent. To maintain and enhance the colonial character and design elements within the district and require new development and modifications to be compatible with the scale, materials, and architectural elements of American Colonial Revival architecture. Sites and buildings should be designed to be recognizable as modern structures and not to appear as historic structures. Creativity is encouraged to design sites and buildings that represent modern interpretations of Colonial Architecture through building scale, materials, symmetry, window patterns, entry design, and other elements. Development shall be consistent with the Secretary of the Interiors Standards for the Rehabilitation of Historic Structures for new additions, exterior alterations, and related new construction (Standards 9 and 10). Additionally, the Colonial District Design Standards are intended to achieve the following:

1. To improve the image and character of the District and the Downtown.
2. To connect to Lakewood's History and identity as it relates to colonial architecture and the district's role

as a community gathering place

3. To support the community's vision for a vibrant Downtown for all and the implementation of the Downtown Subarea Plan
4. To create a great experience on Downtown Streets and in public spaces that is unique to Lakewood

Levels of Review.

1. Facade Improvements and Modifications. Require colonial design elements where practical and feasible. Consider the architectural design of the existing structure and appropriateness of adding colonial elements.
2. Additions. Additions are required to comply with the design standards where practical and consistent with the existing building architecture. Additions and modifications over 50% of the value of the existing site improvements must fully comply with the design standards.
3. New Buildings and Redevelopment. Required to fully comply with the design standards.

Design Standards.

1. Required elements:
 - Roof Type
 - Gable, Gambrel, Side-Gable, Hip
 - Materials
 - Wood, Brick, Clapboards
 - Window Design and Patterns
 - Prominent Entry Design

2. Other elements that may be approved, but are not required:

- Cupola
- Chimney
- Columns
- Portico
- Pediment
- Shutters
- Arched Windows
- Gable Dormers
- Other elements as approved by the City.

Lakewood Comprehensive Plan Amendments

To maintain consistency with the Comprehensive Plan and Downtown Plan, which will be considered a subarea plan and element of the plan, some text and policy changes are proposed. As the preferred alternative is fleshed out and implementation strategies and costs are developed, further edits to the Transportation Element and Capital Facilities Element will be needed. This document presents the basic land use and urban design related changes.

2.0 OFFICIAL LAND USE MAPS

2.3 Land Use Designations

2.3.6 Central Business District (CBD)

The CBD is the primary retail, office, social, urban residential, and government center of the City. The complementary, interactive mixture of uses and urban design provides for a regional intensity and viability with a local character. The regional focus and vitality of the district are evident in the urban intensity and composition of the uses in the district. Local character is reflected in the district's design, people-orientation, and connectivity, which foster a sense of community. The CBD is intended to attract significant numbers of additional office and retail jobs as well as new high density housing. The plan anticipates that the properties within the CBD will be developed into ~~75-percent commercial and 25-percent commercial and~~ residential mixed uses.

Table 2.1 Comprehensive Plan Designation by Density and Housing Type

Land-Use Designation	Major Housing Types Envisioned	Density ¹		Acres
		Low	High	
Residential Districts:				
Residential Estate	Larger single-family homes	1	2	1045.00
Single-Family Residential	Single-family homes	4	6	4083.43
Mixed Residential	Smaller multi-unit housing	8	14	354.21
Multi-Family Residential	Moderate multi-unit housing	12	22	313.59
High Density Multi-Family	Larger apartment complexes	22	40	442.82
Mixed Use Districts:				
Central Business District	High-density urban housing	30	54 <u>80-100</u>	289.78 <u>319</u>

Commented [LG1]: Minor acre changes in MR, NBD, etc. would also occur to address small shift to CBD

Commented [LG2]: Identify based on preferred alternative. Unlimited density may be allowed as an incentive in some locations to attract optimal implementation of plan such as public streets, mixed uses, affordable housing, etc. (limited effectively by height and bulk).



Land-Use Designation	Major Housing Types Envisioned	Density ¹		Acres
		Low	High	
Neighborhood Business District	Multi-family above commercial	12	40	299.61
Arterial Corridor	Live/work units	6	6	18.85
Air Corridor 2	Single-family homes	2	2	235.77
Non-Residential Districts:				
Corridor Commercial	N/A	--	--	471.48
Industrial	N/A	--	--	752.48
Public/Semi-Public Institutional	N/A	--	--	810.65
Air Corridor 1	N/A	--	--	376.18
Open Space & Recreation	N/A	--	--	1945.26
Military Lands	N/A	--	--	24.95
Total designated area	10969.06			
Excluded: Water & ROW	N/A	--	--	1172.14
TOTAL:				12,141.20

Commented [LG1]: Minor acre changes in MR, NBD, etc. would also occur to address small shift to CBD

¹ As expressed in the comprehensive plan for new development; existing densities are unlikely to match and may already exceed maximums in some cases.

2.4.1 Urban Center

Urban centers as relatively compact clusters of densely mixed business, commercial, and cultural activity.

Urban centers are targeted for employment and residential growth with excellent transportation, including high capacity transit service and major public amenities.

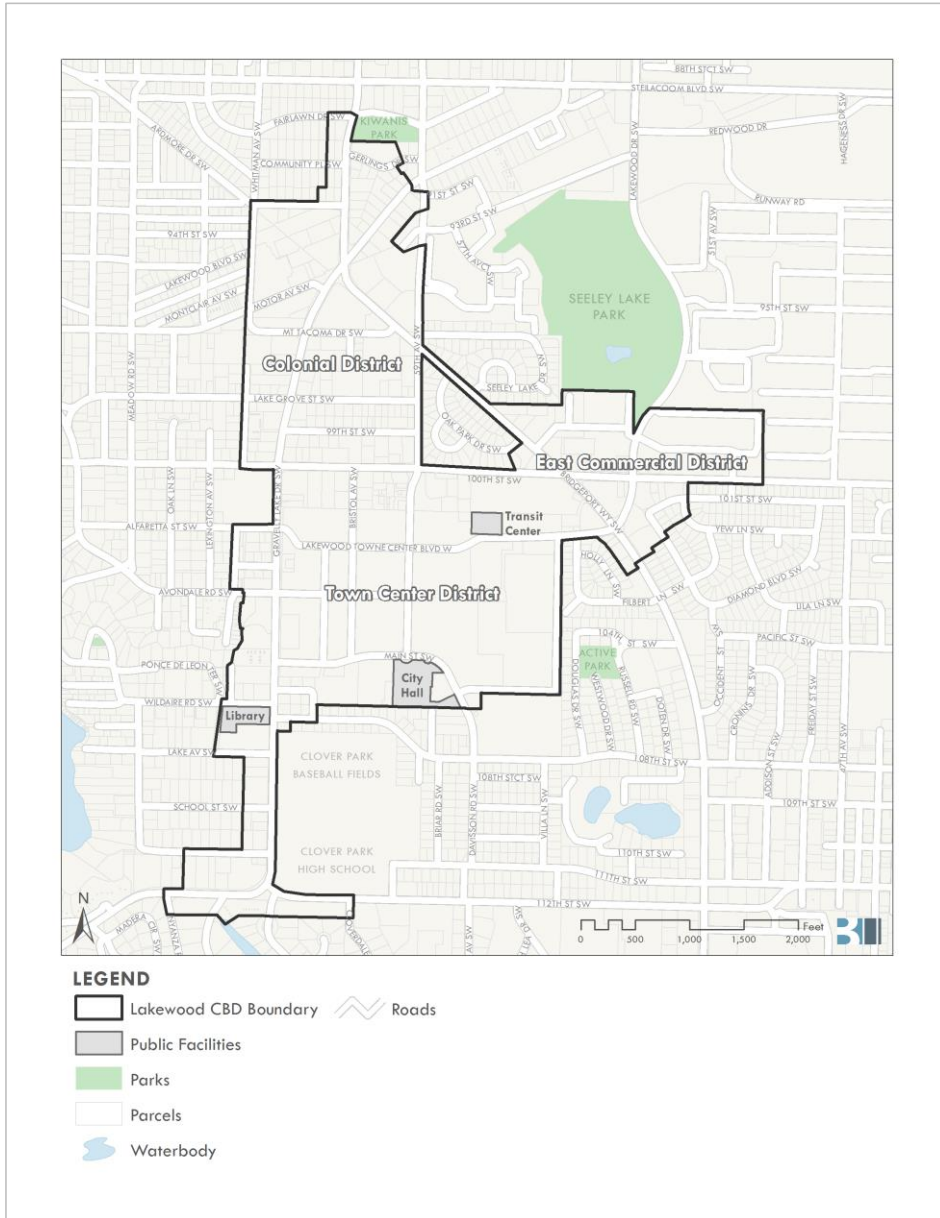
Lakewood has one Urban Center; see Figure 2.2. The boundaries of the Urban Center were drawn to include the most appropriate balance of high-density employment and housing in the City. The Urban Center includes the entire CBD ~~and the majority of the Lakewood Station District. The span of mixed residential and neighborhood commercial connecting the two ends of the Bridgeport corridor are also included.~~ High capacity transit is provided by the existing Pierce Transit Center in Lakewood Towne Center, ~~with connections to and~~ the Sound Transit commuter rail at Lakewood Station ~~and direct high occupancy vehicle (HOV) access to I-5 for bus service outside the center. In addition to the commuter rail station, there is direct high occupancy vehicle (HOV) access to I-5 for bus service, as well as general vehicle on-ramps located nearby at the intersection of Bridgeport Way and I-5.~~ Major public amenities will include improved pedestrian facilities such as design treatments, trails, and parks to be developed concurrent with implementation of the comprehensive plan. Policy language addressing designation of the urban center is located in Section 3.5 of this plan.



OLD



NEW



3.0 LAND USE

3.2.6 Lakewood's 2030 Housing Capacity

In 2014, Pierce County Planning and Land Services prepared a capacity analysis for Lakewood based on their buildable lands methodology. That model is based on existing land inventories, and a calculation of underutilized parcels based on transportation and land use demand. The accompanying map, Figure 3.1, which originates from the Pierce County 2014 Buildable Lands Report, identifies vacant, vacant single family, and underutilized properties. The analysis shows that by 2030, Lakewood would need to provide 9,565 new housing units. The data is described in Table 3.1. Current "built-in" capacity based on existing zoning densities and shown in Table 3.2 shows a new housing unit capacity of ~~11,885-12,563~~10,915.

*** See plan for Table 3.1 ***

Table 3.2 City of Lakewood: Housing Unit Capacity

Zoning District	Adjusted Net Acres	Assumed Density	Unit Capacity	Plus 1 Dwelling Unit per Vacant (single-unit) Lot	Housing Capacity
R-1	47.97	1.45	70	3	73
R-2	132.76	2.2	292	12	304
R-3	376.08	4.8	1,805	43	1,848
R-4	71.28	6.4	456	5	461
MR-1	21.65	8.7	188	0	188
MR-2	60.65	14.6	885	3	888
MF-1	46.54	22	1,024	0	1,024
MF-2	67.44	35	2,360	0	2,360
MF-3	31.44	54	1,698	0	1,698
ARC	13.23	15	198	0	198
NC-1	1.59	22	35	2	37
NC-2	15.02	35	526	7	533
TOC	12.78	54	690	0	690
CBD	17.46	54	943	0	613 [1,579-2,257]
Total Housing Capacity					10,915 [11,885-12,563]

Commented [LG4]: I had a slightly diff total from PCBLR 10,919



3.3.1 General Commercial Goals and Policies

LU-17.2: Promote the CBD as the primary location for businesses serving a Citywide market.

3.3.2 Central Business District

GOAL LU-19: Promote redevelopment of the CBD as a mixed-use urban center that creates a downtown and bolsters Lakewood's sense of identity as a City.

LU-19.1: Promote the CBD as the primary center for retail, office, public services, cultural activities, urban residential, and civic facilities of Lakewood.

LU-19.2 Encourage neighborhood businesses that provide daily goods and services in the CBD.

LU-19.3: Promote the CBD as a daytime and nighttime center for social, entertainment, cultural, business and government activity.

LU-19.4: Promote cultural institutions, performing arts uses, and recreational activities within the CBD.

LU-19.5: Remove underlying deed restrictions and/or covenants that prohibit office development, open space, high density residential development and/or mixed use development in the Towne Center.

LU-19.6: Acquire lands and construct community-gathering destinations such as plazas, open space or community facilities within the Towne Center.

LU-19.7: Support the formation of a Towne Center association or similar organization to establish economic improvement strategies and to sponsor social and safety events.

LU-19.8: Consider the use of the City's eminent domain powers to establish public streets and public open spaces in the Towne Center.

LU-19.9: Revise land use and development regulations to require mixed use development within the CBD for any new development excepting standalone commercial pads and service commercial uses.

LU-19.10: Implement the policies and strategies in the Downtown Plan, which is hereby incorporated by reference as amended, to serve as a subarea plan and to supplement the CBD policies of the comprehensive plan.

GOAL LU-20: Emphasize pedestrian and bicycle connectivity and transit use within the CBD while accommodating automobiles.

LU-20.1: Accommodate automobiles in balance with pedestrian, bicycle, and transit uses within the CBD and on individual sites.

LU-20.2: Maintain the Pierce Transit Center located in the Lakewood Towne Center.

LU-20.3: Maintain an appropriate supply of parking in the CBD as development intensifies.

LU-20.4: Encourage shared parking agreements within the Towne Center.

LU-20.5: Encourage multi-modal policies, improvements, and strategies consistent with the Downtown Plan.

3.5 Urban Center

Lakewood's Urban Center, Chapter 2, Figure 2.2 includes the entire CBD, ~~the majority of the Lakewood Station district, and a significant amount of residential and commercial land along the Bridgeport~~

Commented [LG5]: There is overlap and more detailed policies and strategies in the Downtown Plan, but I don't see particular conflicts.



~~corridor~~. Designation of this urban center is consistent with the vision of this plan and the region's VISION 2040 strategy.

GOAL LU-33: Achieve the VISION 2040 Urban Center criteria.

LU-33.1 Designate the ~~Bridgeport Corridor from Pacific Highway SW to the Colonial Center~~Future Land Use Map "Downtown" designation with an implementing CBD Zone as an Urban Center.

LU-33.2 Adopt by reference and implement the Countywide Planning Policies for Urban Centers.

4.0 URBAN DESIGN AND COMMUNITY CHARACTER

4.3 Relationship Between Urban Design and Transportation

See Element for framework of streets

Civic Boulevards: These are the key vehicular routes people use to travel through or to districts and neighborhoods. These road corridors should be a priority for improvements to vehicular and pedestrian functioning and safety, and for general streetscape improvements such as street trees, street lighting, landscaping, signage and pedestrian sidewalks, building orientation, and the location of on-street parking. They have been identified as civic boulevards due to the prominent role they play in carrying people into the city and therefore creating an image of the city. The urban design framework plan identifies the following arterials as civic boulevards: the full length of Bridgeport Way, Gravelly Lake Drive from Nyanza Boulevard to Steilacoom Boulevard, 100th Street from South Tacoma Way to Gravelly Lake Drive, and the entirety of S. Tacoma Way and Pacific Highway Southwest, the entire length of Steilacoom Blvd., Veterans Drive from Vernon Ave. to Gravelly Lake Drive, Washington Blvd. from Military Road to Gravelly Lake Drive, and Military Road from 107th Ave. to Washington Blvd, as well as N. Thorne Lane and Union Avenue in Tillicum (Table 4.2).

Table 4.2: Civic Boulevards.

Civic Boulevards	Locations
Bridgeport Way	Full length
Gravelly Lake Drive 100th Street	from Nyanza Boulevard to Steilacoom Boulevard from South Tacoma Way to Gravelly Lake Drive See also Downtown Plan for street sections
S. Tacoma Way/ Pacific Hwy SW	All (except So. Tac. Way extension)
N. Thorne Lane	from I-5 to Union Avenue
Union Avenue	from N. Thorne Lane to Berkeley Street
Veterans Drive	Vernon Ave SW to Gravelly Lake Drive
Steilacoom Blvd SW	South Tacoma Way to Far West Drive
Washington Blvd.	Military Road to Gravelly Lake Drive
Military Road	107th Avenue to Washington Blvd.



Key Pedestrian Streets or Trails (“Green Streets”): This term identifies streets that function as preferred pedestrian routes between nodes of activity, trails that link open space areas, or streets with a distinctive pedestrian oriented character, such as a shopping street. Key pedestrian streets should have wide sidewalks; streetscape features such as street trees, benches, way-finding signage, and pedestrian-oriented street lighting; and safe street crossings. The framework plan identifies pedestrian-friendly green streets in several areas including the CBD where they are important to create a downtown atmosphere. Lastly, Lakewood’s Legacy Parks Plan identifies a system of off-street trails to be developed that link the city’s major open spaces.

Table 4.3: Key Pedestrian Routes.

Green Streets	Neighborhood	Extents
83rd Ave.	Oakbrook	Steilacoom Blvd. to Garnett
Onyx Drive	Oakbrook	Oakbrook Park to 87th Ave.
Phillips Road	Oakbrook	Steilacoom Blvd. to 81st St.
87th Ave SW	Oakbrook	Onyx Drive to Fort Steilacoom Park
Hipkins Road		104th to Steilacoom Blvd.
<u>Green Street Loop with Arterial and Local Streets in Downtown</u>	<u>CBD</u>	<u>See Downtown Plan for extent and street sections</u>
Lakewood Town Center	CBD	Various pedestrian links within LTC property
Lakewood Drive	CBD	Bridgeport Way to Steilacoom Blvd.
Steilacoom Blvd.	CBD	Lakeview Drive to 63rd Ave.
63rd Ave.	CBD	Steilacoom Blvd. to Motor Ave.
Motor Avenue	CBD	Gravelly Lake Dr. to Whitman
72nd Ave.	Lakewood Center	Steilacoom Blvd. to Waverly Dr.
Waverly Drive	Lakewood Center	72nd Ave. to Hill Grove Lane
Hill Grove Lane	Lakewood Center	Waverly Drive to Mt. Tacoma Drive
Mt. Tahoma Drive	Lakewood Center	Dekoven to Bridgeport Way
108th Street	Lakeview	Pacific Hwy. to Davisson Road
Kendrick Street	Lakeview	Entire length
San Francisco Ave.	Springbrook	Bridgeport Way to 49th Ave.
49th Ave.	Springbrook	San Francisco Ave. to 127th St.
127th St.	Springbrook	49th Ave. to 47th Ave.
Bridgeport Way	Springbrook	123rd St. to McChord Gate



Green Streets	Neighborhood	Extents
123rd St.	Springbrook	Entire length
47th Ave.	Springbrook	From Pacific Hwy. SW to 127th St.
Washington Ave.	Tillicum	W. Thorne Lane to N. Thorne Lane
Maple Street	Tillicum	Entire length
Custer Road	Flett	Bridgeport Way to Lakewood Drive

4.5 Focus Area Urban Design Plans

Three areas of the city were selected for a focused review of urban design needs: the CBD, the Lakewood Station district, and Tillicum. These areas were singled out for their prominence, for the degree of anticipated change, and for the rich mixture of land uses within a limited space, calling for a higher level of urban design treatment. Each area is discussed in terms of a vision for that area, its needs, and proposed actions to fulfill those needs and realize the vision. A graphic that places those identified needs and proposed actions in context accompanies the discussion.

4.5.1 Central Business District

A major goal of this comprehensive plan is to create a downtown in the CBD, redeveloping it into a rich urban area with civic amenities, walkable streets, and a mix of uses including housing, entertainment, restaurants, and retail. The CBD has significant economic assets such as the Lakewood Towne Center, historic and cultural assets such as the Colonial Center, nearby open space assets such as Seeley Lake, civic assets such as Clover Park High School and City Hall, and other major retail and entertainment assets. There is a strong street pattern, including the intersection of three of the city's major civic boulevards: Bridgeport Way, Gravelly Lake Drive, and 100th Street.

To create a downtown atmosphere, a number of land use and infrastructure changes will be needed, including:

- **Green Street Loop:** To address the lack of park space, improve public streets, and improve circulation for pedestrians and bicyclists the green loop will include park like elements, green infrastructure, and support redevelopment in Downtown.
- **New Public Streets:** The Downtown lacks a dense and walkable street grid to support urban development, circulation, and an active public realm.
- **Central Park:** A new urban park of between two to four acres is proposed just north of City Hall to serve as the main gathering space for the community and to include a variety of features and programming.
- **Revised Gravelly Lake Drive:** As part of the Green Street Loop, a revised road design for Gravelly Lake Drive SW is proposed. The revision will allow for expanded sidewalks and a multi-use path on the east side of the street.
- **Catalyst Sites:** Catalyst sites are the best opportunities to weave together public improvements in infrastructure and amenities with infill and redevelopment by the private sector. The best opportunities for redevelopment based on vacant and underutilized sites, and large surface parking areas, and surrounding context have been identified as catalyst sites in the near term to further the implementation of this Plan.



- **Motor Avenue Festival Street:** The City intends to move forward with creating a festival street along Motor Avenue consistent with the adopted concept plan. The plan includes a large central plaza, a pedestrian promenade, a farmer's market and event structure, street trees, landscaping, and public art opportunities.

- *—intensification of land use within the CBD, including some higher density residential infill;
- *—development of more urban civic amenities, including park space, civic plazas, and recreation opportunities;
- *—establishment of pedestrian linkages between the Colonial Center and Lakewood Towne Center; and
- *—creation of an urban streetscape with pedestrian-oriented spaces, buildings that define street edges, and high quality design in the streetscape.

Key to this vision for the CBD is continuation of the successful and creative evolution of Lakewood Towne Center. Specific actions the City can take in support of this redevelopment include assistance with strengthening the street grid within the CBD, including specific streetscape improvements along major civic boulevards; good transportation planning, including a strong transit link between the CBD and the new commuter rail station; and good land-use planning, working with the development community to promote residential growth within the CBD where it is close to available jobs and services.

The urban design framework plan depicting some of the potential land use and urban design changes in the CBD is shown in Figure 4.2. Some of the specific urban design actions shown in that figure that may occur as the CBD develops are as follows:

Landmarks/Activity Nodes: Streetscape enhancements to the intersection of Gravelly Lake Drive and Bridgeport Way would create a positive image of the city, with new landscaping, crosswalks, signal poles, central island, signage, and other treatments.

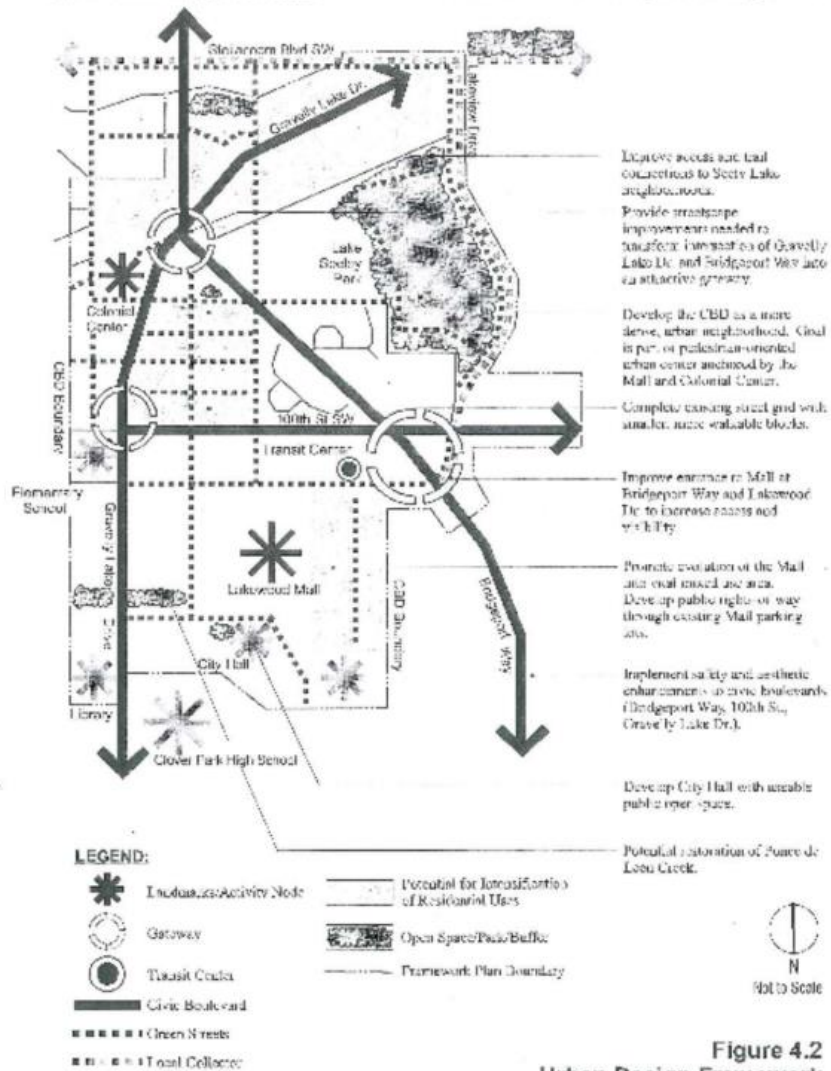
Civic Boulevards: The framework plan identifies various safety and image-oriented streetscape improvements to Bridgeport Way, Gravelly Lake Drive, and 100th Street, including the use of landscaped medians in the current turning lanes, crosswalks, undergrounding of utilities, and general aesthetic improvements. Improvements to the intersection of Bridgeport Way with Lakewood Boulevard and 100th Street would improve visibility and access to the Towne Center.

Green Streets: For the network of pedestrian-oriented streets identified in between the Colonial Center and the Towne Center, improvements would be made to increase pedestrian interest and safety, such as curb ramps, street trees, crosswalks, and lighting.

Open Space: Improved access and recreational opportunities are shown for Seeley Lake Park. The development of smaller urban parks within the CBD could occur through density bonuses to private developers in exchange for development of public open space. Integrated park/plaza spaces are a priority in the CBD, particularly in the Towne Center and Colonial Center areas. Such spaces should provide for the display of public art, other cultural and festive celebration, and for visitors and workers to relax and enjoy.

OLD

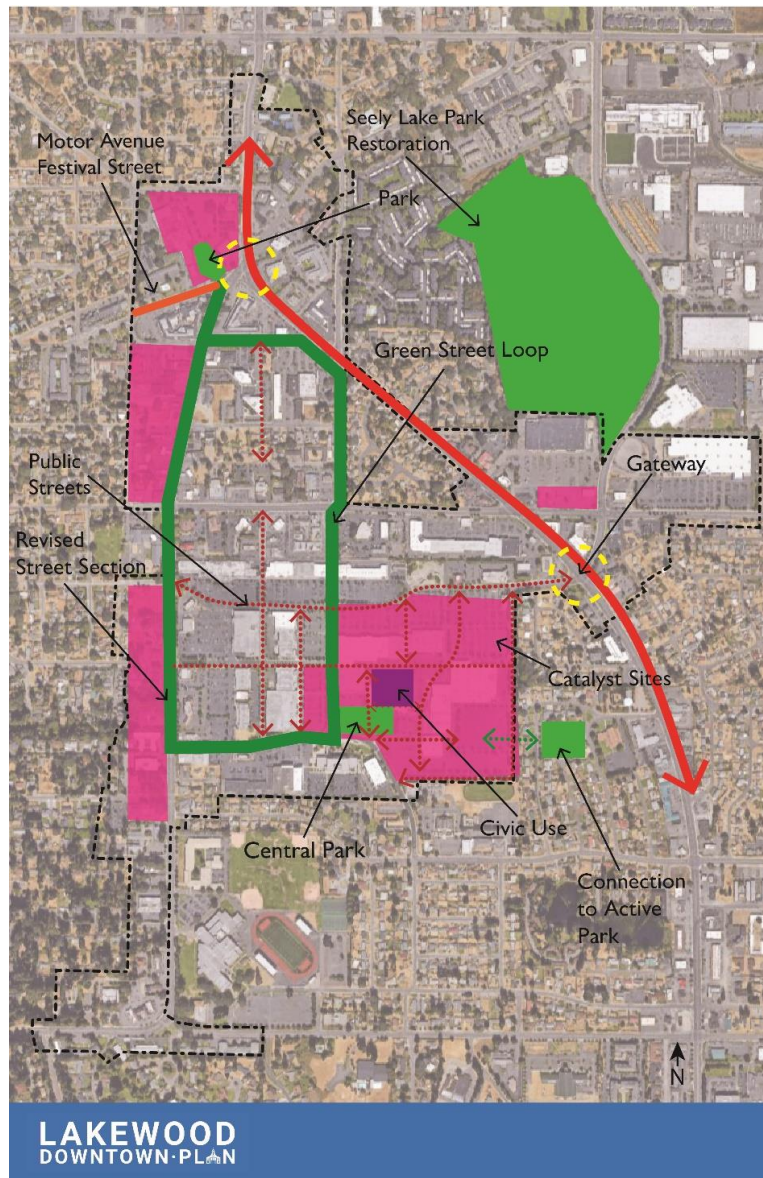




SOURCE: ED&M, Inc. 1989.
July 2000
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NEW

Figure 4.2 Downtown Plan Concept



Framework, 2017



Downtown Plan Adjustments

Integration of Preferred Alternative and Responses to Comments

Proposed Updates and Adjustments

The public, Planning Commission, and City Council have provided some feedback on the Draft Subarea Plan and Draft Environmental Impact Statement (EIS) that are helpful in creating a Preferred Plan. This document identifies potential updates to the March 2018 Draft Downtown Plan for Planning Commission consideration.

STUDY AREA

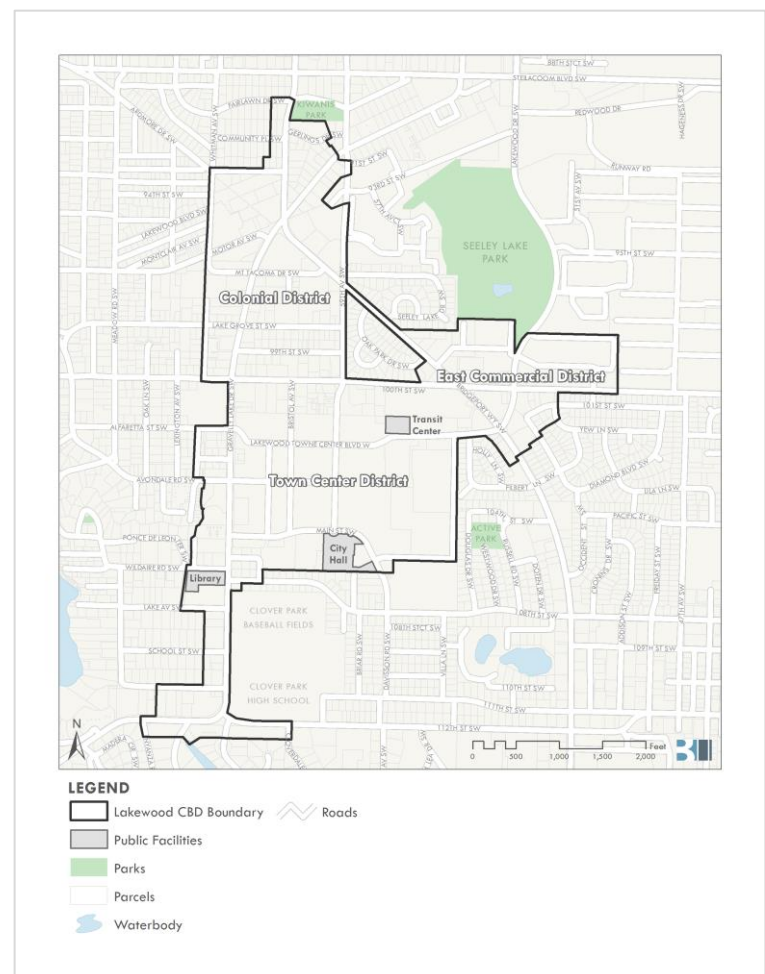
Figure 7 in the Downtown Plan identifies some potential study area adjustments for land use purposes. As recommended in the Draft EIS, areas designated Mixed Residential are included in the Downtown boundary.

Based on City staff review, other refinements to the study area are recommended for consistent streetscape. The amended study area map adjusts centerline boundaries to ensure full rights of way are included to provide for a common consistent landscaping. Likewise, the right of way for Bridgeport Way between the East Commercial Area and Colonial District is included for consistent landscaping. Abutting land use is not included in that segment.

PLAN CONCEPT

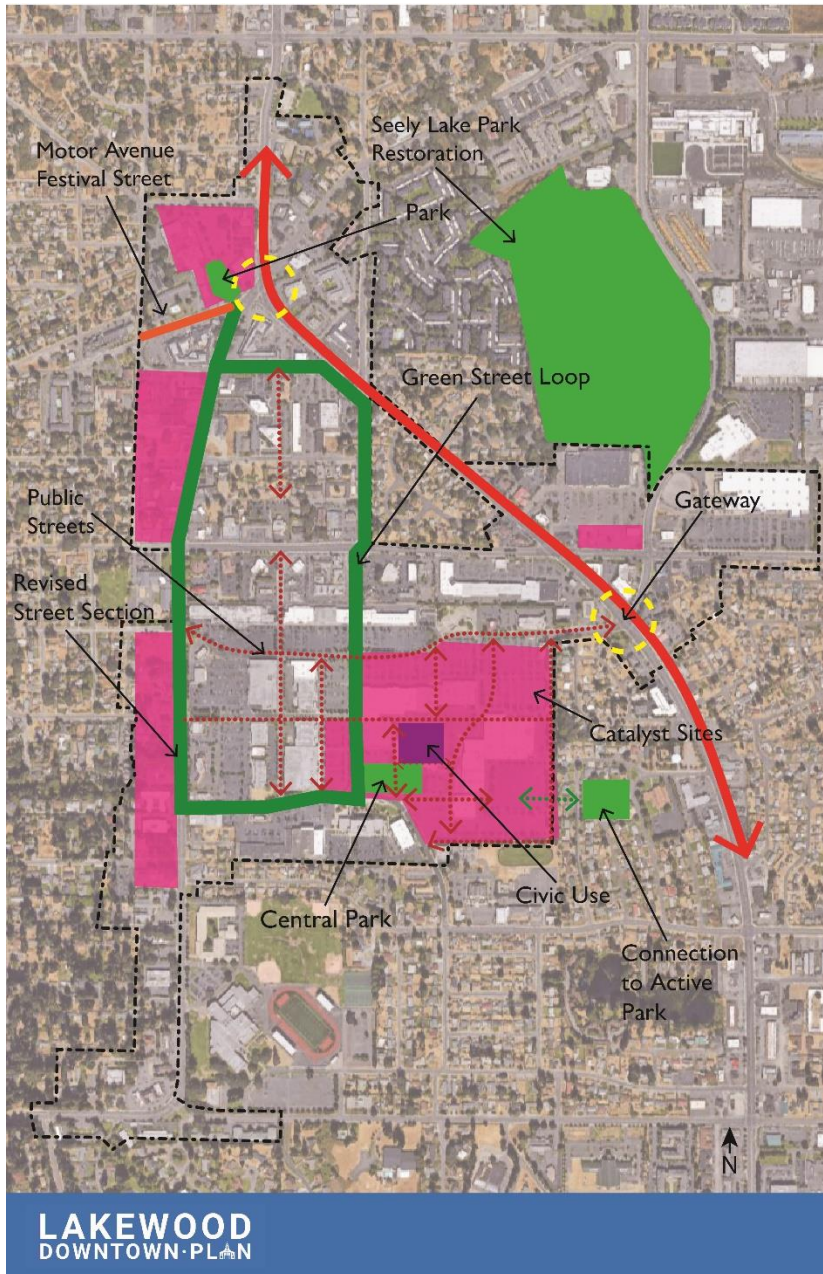
The Plan Concept dated March 2018 showed the Green Loop bisecting the Central Park in Figure 3 Downtown Plan Concept. To maintain a north-south street alignment and retain full use of the current roundabout in front of City Hall, the Central Park is now shown east of the Green Loop. See figure on the next page.

Exhibit 1. Revised Study Area



Source: BERK Consulting 2018

Exhibit 2. Revised Downtown Plan Concept April 2018



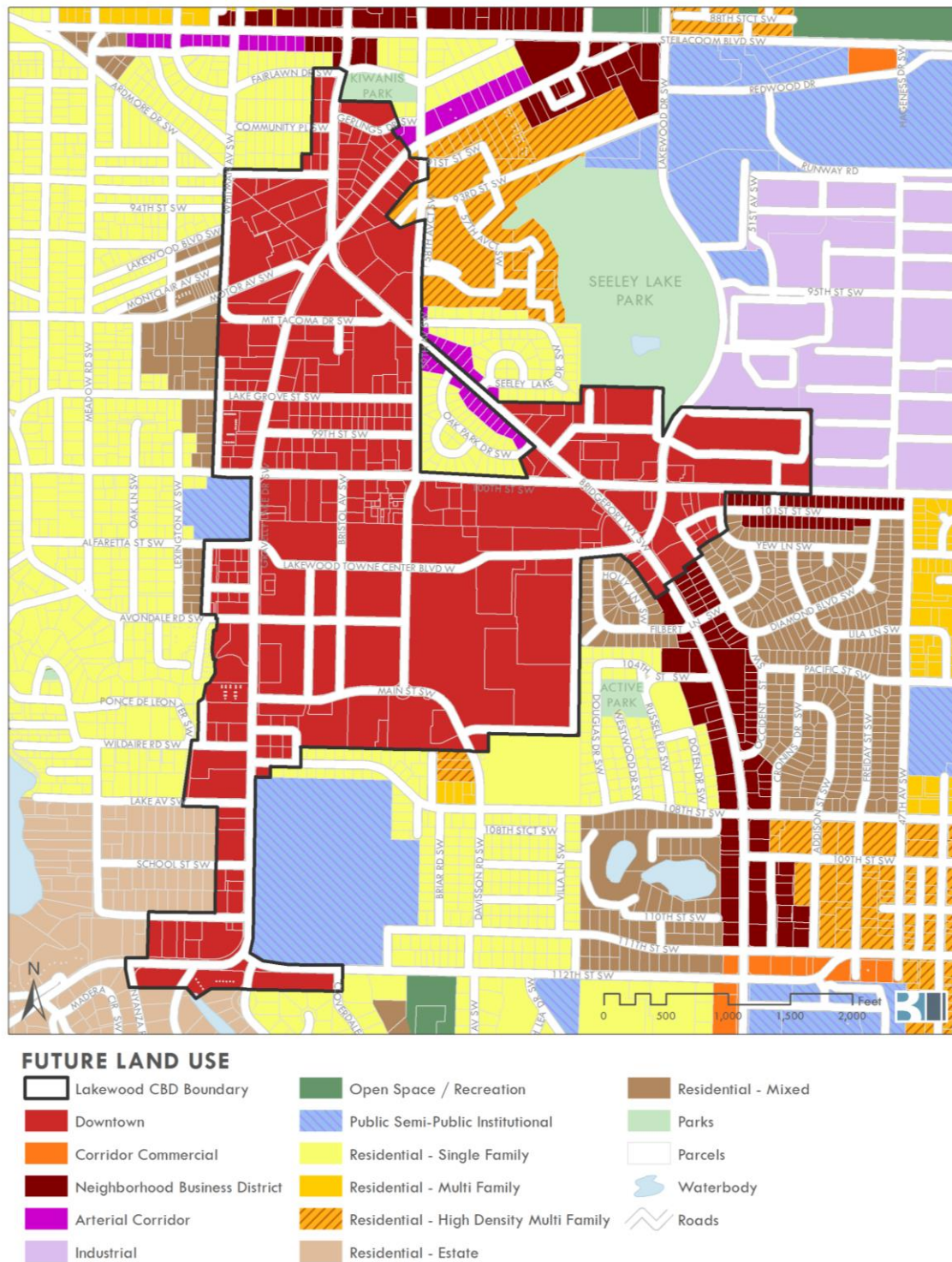
Source: Framework 2018

FUTURE LAND USE PLAN AND ZONING

Figures 10 and 11 of the Draft Downtown Plan were placeholders for the Future Land Use Plan and Zoning. In the text on page 18 of the Draft Downtown Plan, the Future Land Use designation was described as Downtown.

“Downtown” is proposed as a Future Land Use Designation, as suggested in the Draft Plan text. See Exhibit 3.

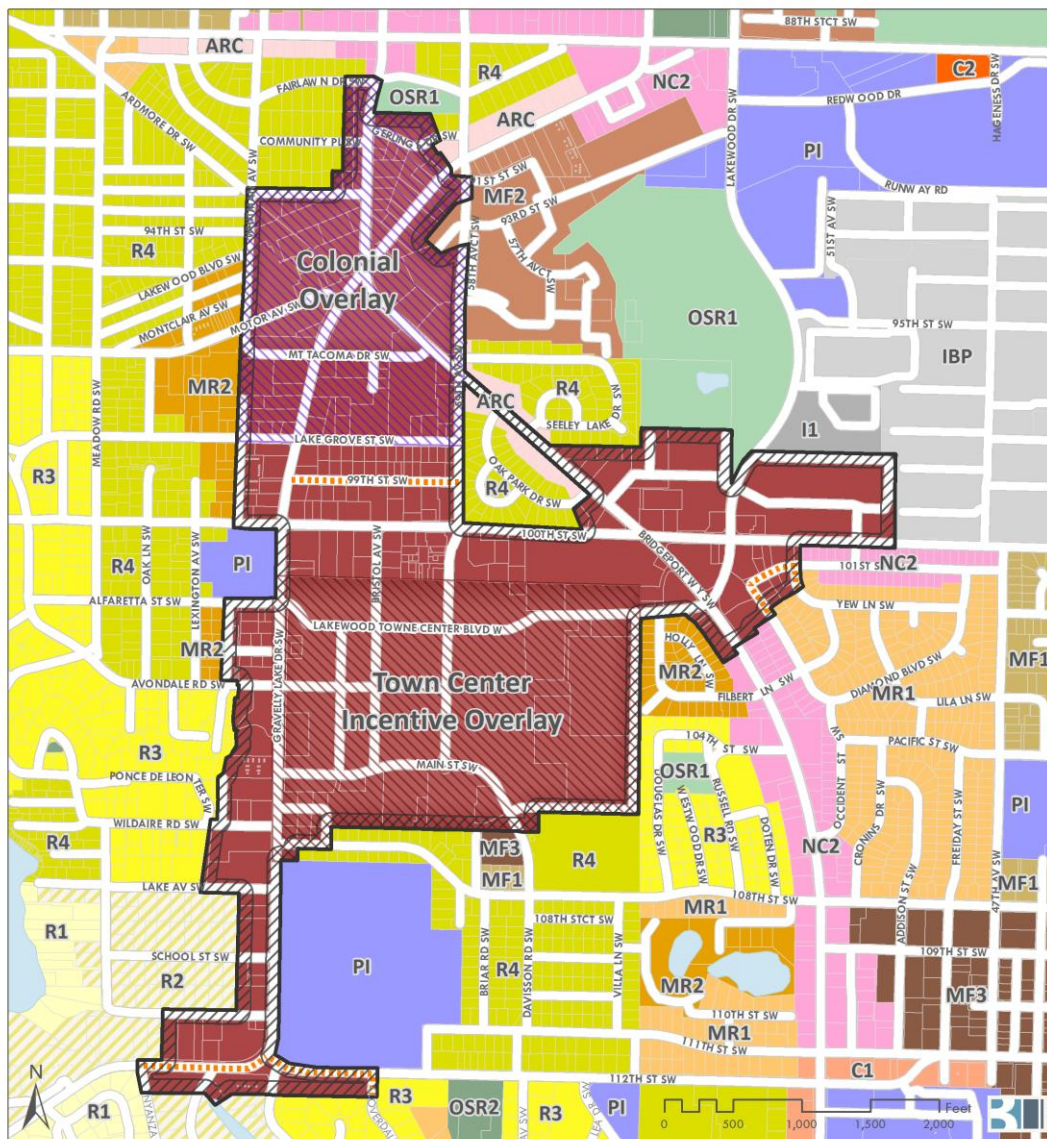
Exhibit 3. Proposed Future Land Use Plan



Source: City of Lakewood 2017, BERK Consulting 2018

The proposed zoning map would consistently apply Central Business District (CBD) as the implementing zone. A series of overlays would require design standards (Colonial), ensure compatible height and landscaping (Transition), or offer incentives for optimal implementation of the plan (Town Center).

Exhibit 4. Proposed Zoning Map



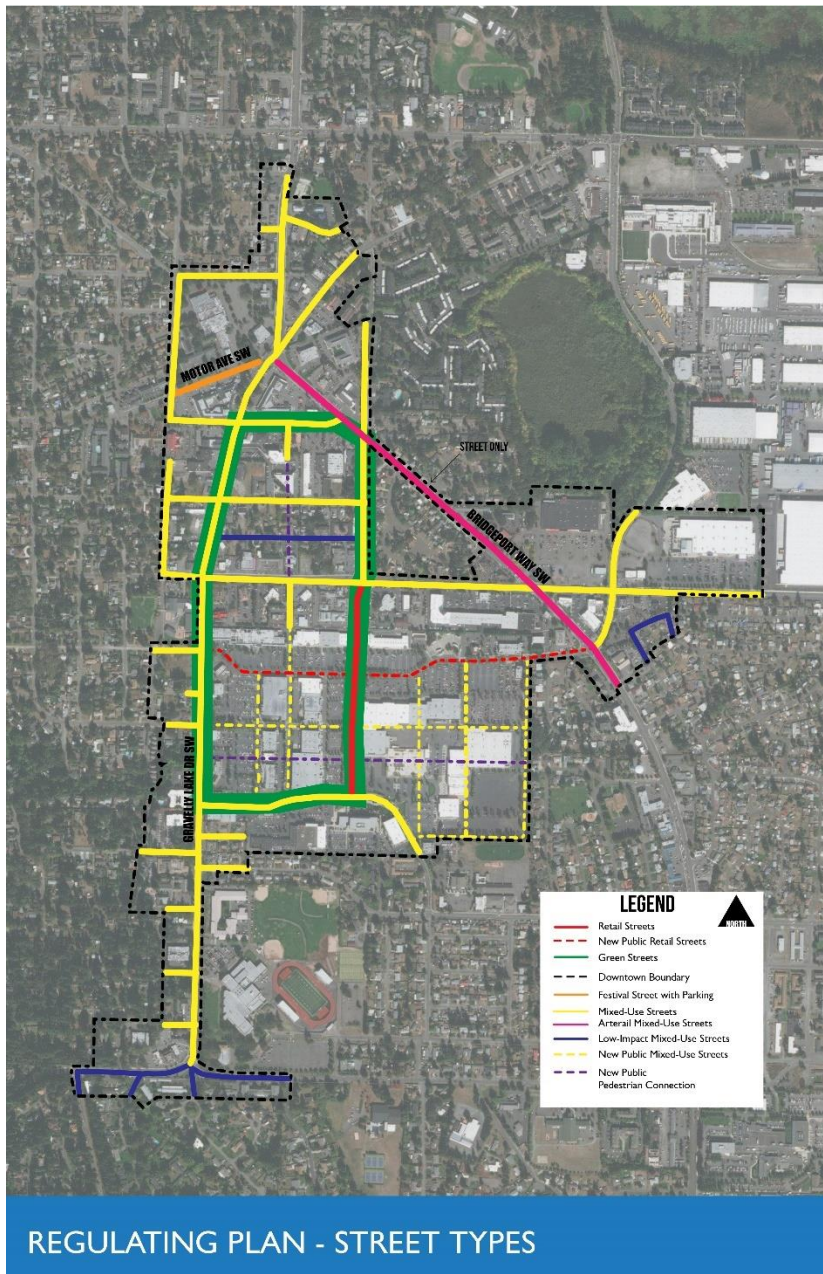
LEGEND

Source: City of Lakewood 2017, BERK Consulting 2018

REGULATING PLAN

The Downtown Plan at page 33, Figure 28, included a regulating plan with street types. It focused on street types for particular streets. Uncategorized streets would have followed standard City classifications in the Transportation Plan. To ensure consistent streetscapes and for ease of implementing the development code, each street is typed in this updated Regulating Plan.

Exhibit 5. Regulating Plan

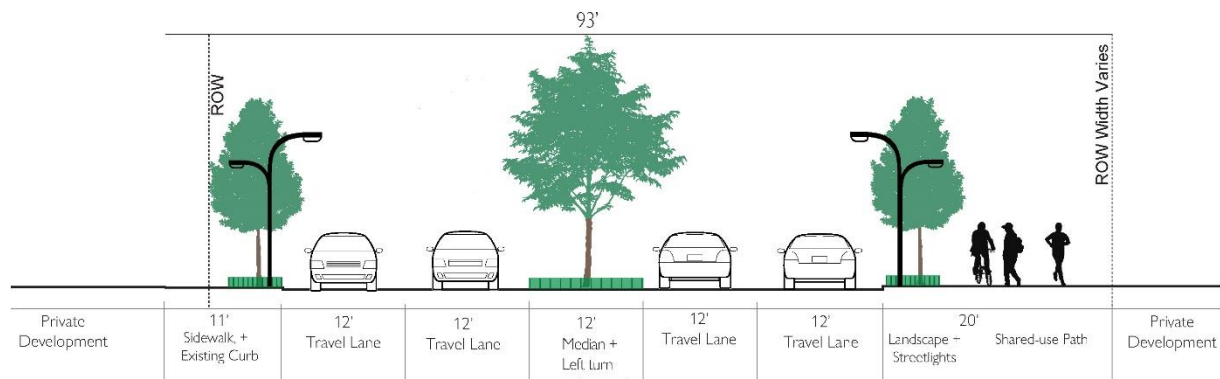


Source: Framework 2018

CROSS SECTION SELECTION

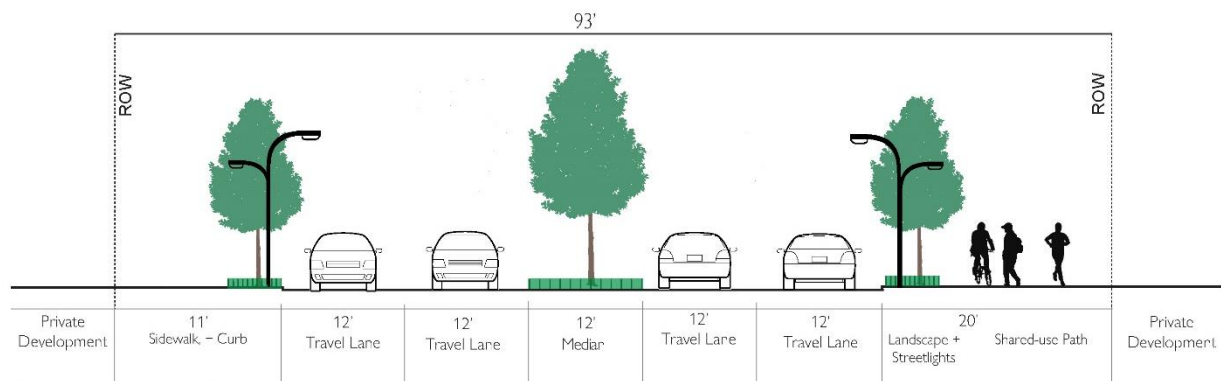
To protect recent sidewalk investments and limit the need for right of way, as well as to limit new intersection improvements, a 4-lane street section with center turn lane and median is proposed for Gravelly Lake Drive between 100th and 112th Streets SW. This is consistent with Figures 34 and 35 in the Draft Downtown Plan.

Exhibit 6. Gravelly Lake Drive SW Revision – Concept #3A (Looking north)



Source: KPG and Framework 201

Exhibit 7. Gravelly Lake Drive SW Revision – Concept #3B (Looking north)



Source: KPG and Framework 2018

CAPITAL FACILITY PLAN

This section provides a draft capital facility plan for transportation and parks improvements. It would be appended to the Downtown Subarea Plan and would be a basis for amending the City's Transportation and Capital Facilities Elements of the Comprehensive Plan.

Downtown Plan Appendix – Capital Facilities Plan Text

The Draft EIS and Downtown Plan identified new transportation and park improvements. This capital plan identifies priorities for public investments based on City levels of service and the Downtown Plan Vision and concepts. It identifies available funding sources including local, state, and federal funds in addition to grant opportunities, and considers the City's budget and revenue projections and the Transportation Improvement Program (TIP).

Transportation Improvements

The list below, together with Exhibit 8, summarizes the transportation network assumptions for the Downtown Plan including projects in the City's Six-Year TIP and additional projects.

Transportation Improvement Program Improvements: 2018-2023

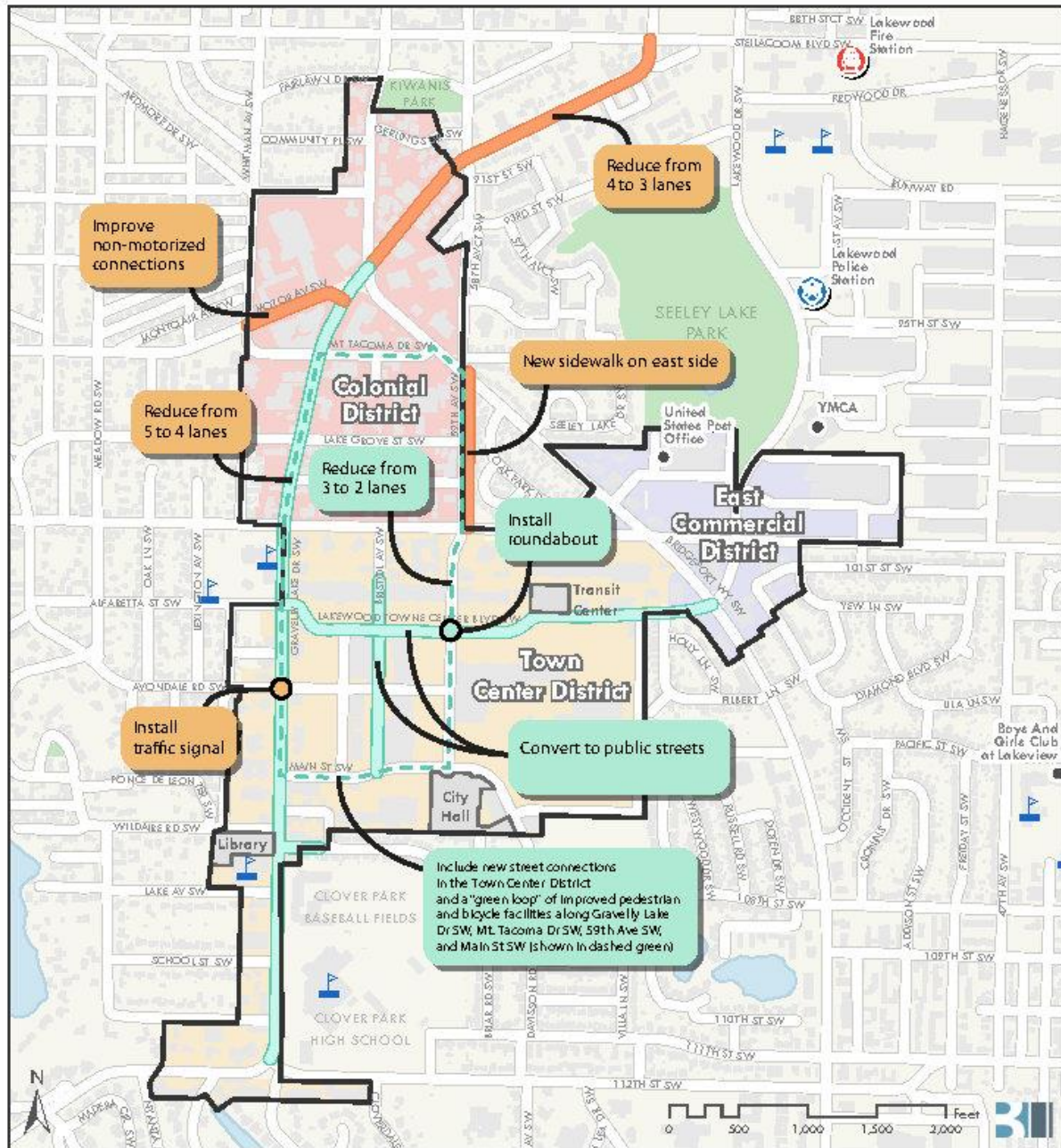
- 2.69B – Reduce Gravelly Lake Drive SW from four lanes to three lanes (with bicycle lanes) between Bridgeport Way SW to Steilacoom Blvd SW;
- 3.13 – Install a traffic signal at the Gravelly Lake Drive SW/Avondale Road SW intersection;
- 5.7 – Improve non-motorized connections on Motor Avenue SW between Whitman Avenue SW and Gravelly Lake Drive SW; and
- 2.82 – Construct sidewalk on the eastern side of 59th Avenue SW between Bridgeport Way SW and 100th Street SW.
- 2.72 – 100th St. & Lakewood Dr. curb, gutter, sidewalks, new signal
- 9.16 – 59th Ave pavement restoration from Main Street to 100th St
- 9.22 – 100th Street pavement restoration from 59th Ave to Lakeview Ave

Preferred Alternative Network Changes

- Consideration of reducing Gravelly Lake Drive SW from five lanes to ~~three-four lanes with center turn~~ lane/~~median~~ between Bridgeport Way SW and 112th Street SW and construct improved pedestrian and bicycle facilities ~~(extending the three-lane section south of the currently planned project);~~
- Convert Lakewood Towne Center Blvd SW and Bristol Avenue SW to public streets within Lakewood Towne Center;
- Reduce 59th Avenue SW from three lanes to two lanes between Main Street SW and 100th Street SW and construct bicycle facilities;
- Install a one-lane roundabout at the 59th Avenue SW/Lakewood Towne Center Blvd SW intersection; and

- Construct more street connections to support walkability (~~Alternative 2 would have more street connections than Alternative 1).~~

Exhibit 8. Transportation Network Assumptions [Map is updated for GLD Lanes]



LEGEND

	All Alternatives		Lakewood CBD Boundary		Schools
	Action Alternative		Public Facilities		Waterbody
	Parks		Roads		

Additional Intersection Improvements

Based on the Planned Action EIS, in addition to the Six-Year TIP and additional Network Improvements described above, additional intersection improvements will be required as listed in Exhibit 9.

Exhibit 9. Proposed Mitigation Measures.

INTERSECTION	NO ACTION	ALT 1	ALT 1 MITIGATED	ALT 2	ALT 2 MITIGATED
Gravelly Lake Dr SW/59th Ave SW					
Signalize intersection	E/38	E/46	B/19	F/82	B/19
100th St SW/Bridgeport Way SW					
Add westbound right turn pocket, convert existing westbound through-right lane to through-only, and prohibit east and westbound left turns	E/68	F/85	C/34	F/102	D/49
100th St SW/Lakewood Dr SW					
Signal timing revisions to provide more green time to protected left turn phases and reduce time for eastbound and southbound through phases	D/50	E/56	D/49	E/56	D/54
Lakewood Dr SW/Bridgeport Way SW					
Convert westbound through-left lane to left only to remove split phase or move the pedestrian crossing to the north side of the intersection coincident with the WB phase *	C/34	E/66	D/39	E/67	D/48
108th St SW/Bridgeport Way SW**					
Add northbound right turn pocket	D/48	D/51	D/47	E/58	D/52
112th St SW/Gravelly Lake Dr SW**					
Add second westbound left turn pocket and combine through and right turn movements into outside lane	E/31	E/61	C/34	E/65	C/35

Notes: * The LOS results are slightly better if the split phasing is removed (D/48) than if the pedestrian crossing is relocated (D/54)

~~**These intersections remain within the City's LOS standard of D if the Gravelly Lake Drive SW Revision is not implemented.~~

Source: Fehr & Peers, 2018.

Transportation Costs

The table below identifies the cost for proposals in the Six-Year TIP. The total is about \$5.8 million. The City has funded about 40% of these improvements. About 58% is covered by grants, and the final 3% by Developer contributions.

Exhibit 10. Six-Year Transportation Improvement Program (2018-2023) in Downtown Study Area

PROJECT	COST	YEARS	FUNDING SOURCES
2.69B – Reduce Gravelly Lake Drive SW from four lanes to three lanes (with bicycle lanes) between Bridgeport Way SW to Steilacoom Blvd SW;	\$1,300,000	2018-2019	City: \$200,000 Grant: \$1,100,000
3.13 – Install a traffic signal at the Gravelly Lake Drive SW/Avondale Road SW intersection;	\$250,000	2022	City: \$100,000 Other (Developer): \$150,000
5.7 – Improve non-motorized connections on Motor Avenue SW between Whitman Avenue SW and Gravelly Lake Drive SW; and	\$930,000	2018-2019	City: \$100,000 Grant: \$830,000
2.82 – Construct sidewalk on the eastern side of 59th Avenue SW between Bridgeport Way SW and 100th Street SW.	\$125,000	2019	City: \$25,000 Grant: \$100,000
2.72 – 100th St. & Lakewood Dr. curb, gutter, sidewalks, new signal	\$1,680,000	2018-2019	City: \$330,000 Grant: \$1,350,000
9.16 – 59th Ave pavement restoration from Main Street to 100th St	\$450,000	2020	City: \$450,000
9.22 – 100th Street pavement restoration from 59th Ave to Lakeview Ave	\$1,100,000	2022	City: \$1,100,000
Total	\$5,835,000		City \$2,305,000 Grant \$3,380,000 Other \$150,000

Note: Other includes Developer contributions.

Source: City of Lakewood 2017

The Planned Action EIS describes potential improvements to the network and impacted study intersections in addition to the City's 2018-2023 6-Year Transportation Improvement Program; see Exhibit 9. Implementation of improvements would occur through a SEPA fair share fee program such that new development contributes its share of the cost for these projects.

Planned Action EIS traffic modeling identified approximately 45% pass-through traffic in the study area; to support citywide or regional travel the City would provide some funding and much of it would come from grants or other funding sources. The responsibility of cumulative planned action development would equal 55% unless the combination of City and Grant resources was more successful. The proportionate share of costs of the Planned Actions would be determined based on their proportionate share of trips identified in the Planned Action Ordinance. [This approach is being reviewed by Fehr & Peers.]

Exhibit 11. Transportation Improvements in addition to Six-Year Transportation Improvement Program

PROJECT	TITLE	COST (100%)	PLANNED ACTION SHARE 55%
1	Gravelly Lake Dr SW Revised Section: 4-lane section plus median/turn lane shown in the March 2018 Subarea Plan concept #3A on page 38	\$19,410,488	\$10,675,768
2.1	Conversion of Lakewood Towne Center Blvd as Public Street	\$11,756,511	\$6,466,081
2.2	Conversion of Bristol Ave as Public Street	\$7,357,170	\$4,046,444
3	Lakewood Towne Center Blvd at 59th Ave SW, Roundabout	\$2,401,959	\$1,321,077
4	Reduce 59th Ave SW to two lanes, allowing for bicycle facilities (sharrows)	\$188,559	\$103,708
5	Gravelly Lake Dr / Avondale Rd SW New Signalized Intersection	\$1,177,548	\$647,651
6	100th St SW / Bridgeport Way SW	\$648,673	\$356,770
7	100th St SW / Lakewood Dr SW	\$8,000	\$4,400
8	Option A: 100th St SW / Lakewood Dr SW: Convert westbound through-left lane to left only to remove split phase	\$13,000	\$7,150
	Option B: 100th St SW / Lakewood Dr SW: Move the pedestrian crossing to the north side of the intersection coincident with the WB phase	\$268,688	\$147,779
9	108th St SW / Bridgeport Way SW [remove from list]	\$1,093,206	\$601,263
10	112th St SW / Gravelly Lake Dr SW [remove from list]	\$73,000	\$40,150
Total	with 8A	\$44,128,114	\$24,270,463
-	with 8B	\$44,383,803	\$24,411,091
Total	with 8A minus 9 and 10 [Proposed]	\$42,961,908	\$23,629,049
	with 8B minus 9 and 10 [Proposed]	\$43,217,596	\$23,769,678

Source: KPG, BERK 2018

Potential Funding Sources

The City would need to blend funding sources to pay for infrastructure improvements. Traditional funding sources include sales, property, and utility taxes, state and federal competitive grants and legislative allocations, and mitigation from development similar to the above. The City is also considering several sources in its Six-Year Financial Forecast Update (January 2018), including: an additional \$20 vehicle licensing fee (total \$40 VLF), property tax levy lid lift and capital bond.

The City could also consider specialized funding options like community revitalization financing, community facility districts, Local Improvement Districts or Road Improvement Districts, and latecomer agreements. These various sources of revenue are described below.

Opportunities to Capture Contributions from New Development

- **Sales Tax Generated on Development.** Sales tax is generated from the taxable sales of goods occurring within the city boundaries. Sales tax impacts from potential site development in the Downtown study area will be generated in two ways:
 - The initial construction of the development will generate sales tax for the full cost of supplies, material, and labor used in construction.
 - Retail and hotel development will generate significant ongoing sales and use tax revenues.
- **Property Tax Generated on Development.** Assessed value (AV) from new construction is the only way for a jurisdiction to increase its property tax base and revenues beyond the 1% per year cap on the property tax levy.
- **Utility Tax Generated on Development.** Utility taxes and franchise fees are charged against total utility revenues, and revenue from utility taxes scales in proportion with the quantity of utilities purchased by the study area's future tenants. The development in the study area would generate utility tax revenue for the City, based on the total utility billing generated by the area occupants.

In addition to the general tax benefits described above, there are funding mechanisms that provide opportunities to more directly tap the value increase in the land to support infrastructure development for the Downtown properties, summarized as follows:

- **Community Facility Districts.** Allow jurisdictions (including cities and counties) to finance infrastructure improvement through establishing a special assessment district for a variety of improvements including water, sewer, roads, storm drainage, sidewalks, and other forms of infrastructure. The formation of a district requires 100% of property owners within the district to sign a petition to form the district.
- **Road Improvement Districts (RID).** Levy a special assessment on properties that would benefit from roadway improvements to pay for those improvements. This mechanism can be particularly effective when: (1) there are significant and demonstrable benefits to the property values associated with the road improvements; and, (2) there are relatively few large property owners within the assessment area and they see the benefit of participating in the RID.

Finally, there are mechanisms that provide opportunities to address some of the equity balancing issues associated with allocating some of the funding responsibility to future development.

- **Latecomer Agreements.** Funding agreements that allow property owners who have paid for capital improvements to recover a portion of the costs from other property owners in the area who later develop property that will benefit from those improvements. This approach reflects the reality that it is difficult to phase some of these infrastructure investments which can result in the early participants carrying a larger financial burden to get the project off the ground. Latecomers agreements would offer a mechanism for the early commitments to recover some of their investment.

Community Contributions

Development of the Downtown area will result in general tax revenue and economic benefits. As a result, there is an appropriate role for public funding to build some of the infrastructure necessary to generate these broader community benefits. Investing in infrastructure with public funds (City or other agency) can result in several benefits:

- **Economic Opportunity.** The range of employment opportunities and the real wage gains of employees.
- **Constituent Tax Burdens.** Efficient land use and public services and high-value development opportunities can keep tax burdens lower than they would otherwise be.
- **Productive and Efficient Returns on Infrastructure.** Infrastructure is by nature a capacity building asset. Effectively leveraging infrastructure capacity and targeting new investments to open up economic opportunities are integral to supporting private investment in the community.

The following is a brief discussion of the mechanisms available to local jurisdictions seeking to generate public funding to support infrastructure development in the area.

- **Transportation Benefit District (TBD).** Funding districts that may be established for the construction and operation of improvements to roadways within their jurisdiction. TBDs have two available funding mechanisms:
 - **Sales and Use Tax (RCW 82.14.0455).** TBDs can levy up to a 0.2% local sales and use tax with voter approval. This tax must be authorized by voters, and may not be in effect longer than 10 years unless reauthorized by voters.
 - **Motor Vehicle Excise Tax (MVET) (RCWs 81.100 and 81.104).** TBDs can levy up to a \$100 fee for each new vehicle weighing less than 6,000 pounds registered in its jurisdiction. Initially, \$20 of this fee can be leveraged without a public vote. After two years that amount increases to \$40, and later to \$50.

On September 15, 2014, the Lakewood City Council, acting as the Transportation Benefit District Board, voted to enact a \$20 vehicle license fee. In 2015, the legislature increased the allowable nonvoted vehicle license fee up to a \$50 maximum. However, a TBD may only impose a nonvoted vehicle license fee above \$20 as follows:

- Up to \$40, but only if a \$20 fee has been in effect for at least 24 months.
- Up to \$50, but only if a \$40 fee has been in effect for at least 24 months. Any nonvoted fee higher than \$40 is subject to potential referendum.
- **Property Tax Levy Lid Lift.** A taxing jurisdiction that is collecting less than its maximum statutory levy rate may ask a simple majority of voters to “lift” the total levy amount collected from current assessed valuation by more than 1% (RCW 84.55.050; WAC 458-19-045). With a single-year lid lift, a jurisdiction can exceed the 1% annual limit for one year only, and then future increases are limited to 1% (or inflation) for the remainder of the levy. With a multi-year lid lift, a jurisdiction can exceed the 1% annual limit for up to 6 consecutive years. A multi-year levy lid lift may be used for any purpose, but the ballot must state the limited purposes for which the increased levy will be used (unlike a single-year lid lift, where there is no requirement to state the purpose). (MRSC 2018)

- **Grants and Loans.** There are state and federal grant and revolving loan programs, which could provide some funding. These programs are extremely competitive; however, any grant funding that could be made available would significantly improve the funding and economic feasibility of the Downtown development, since these funds would reduce the amount that needs to come from development and local public sources.
- **Legislative Allocation.** In addition to the grant programs, some infrastructure funding is allocated through the state budget process. Since there are investments required for state transportation facilities, a contribution through the state budget would have the same benefits as a grant. As with grants, these discretionary funds are limited, subject to state appropriation, and very competitive.
- **Community Revitalization Financing.** A form of tax increment financing from local property taxes generated within the area authorized by Chapter 39.89 RCW. The law authorizes counties, cities, towns, and port districts to create tax increment areas within their boundaries where community revitalization projects and programs are financed by diverting a portion of the regular property taxes imposed by local governments within the tax increment area. The law allows local governments raise revenue to finance public improvements that are designed to “encourage economic growth and development in geographic areas characterized by high levels of unemployment and stagnate employment and income growth.” Use of the funds is expected to “encourage private development within the increment area and to increase the fair market value of real property within the increment area.” The law requires there be a signed, written agreement among taxing districts, a public hearing, and adoption of an ordinance. The agreement indicates that taxing districts in the aggregate will levy at least 75 percent of the regular property tax within the increment area.

Parks Costs

The roadway improvements above address the Green Loop, a linear park and non-motorized travel improvement. In addition to the Green Street Loop the Downtown Plan supports a Central Park. A two to four-acre park has been evaluated. It is likely that with the Preferred Alternative: Modified Alternative 1 level of growth the City would initially pursue a two-acre Central Park. This would also result in less potential disruption to future public road improvements and retain more area for private redevelopment.

The capital costs (not including cost of land and design) will be in a range of \$3 to \$5 million. For reference, Downtown Redmond’s construction cost is \$11 million for 2.2 acres.

Exhibit 12. Park Size and Costs Excluding Acquisition and Design

	TWO-ACRE PARK	FOUR-ACRE PARK
Capital Cost	\$10,000,000	\$20,000,000

Source: KPG, BERK 2018

The Downtown Development Code allows a developer to pay an in-lieu fee for up to half of the required private common and unit-specific open space, and instead contribute to the Central Park or the Green Loop.

Citywide the City is considering park financing options and exploring metropolitan park district options.

Draft Planned Action Lakewood Downtown

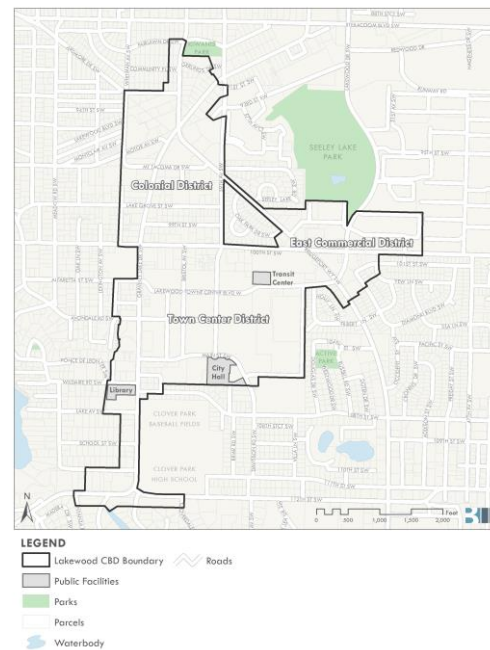
Discussion Guide

Planned Action

The City is proposing that future development within the Downtown be designated by the City as a Planned Action, pursuant to SEPA (RCW 43.21c.440 and WAC 197-11-164 to 172). A planned action provides more detailed environmental analysis during an areawide planning stage rather than at the project permit review stage. Designating a planned action streamlines environmental review for development proposals consistent with Environmental Impact Statement (EIS) mitigation measures that are adopted in a planned action ordinance. Planned actions would be allowed if they meet or exceed proposed land use and environmental performance standards. This tool has been used elsewhere by local governments in Washington State.

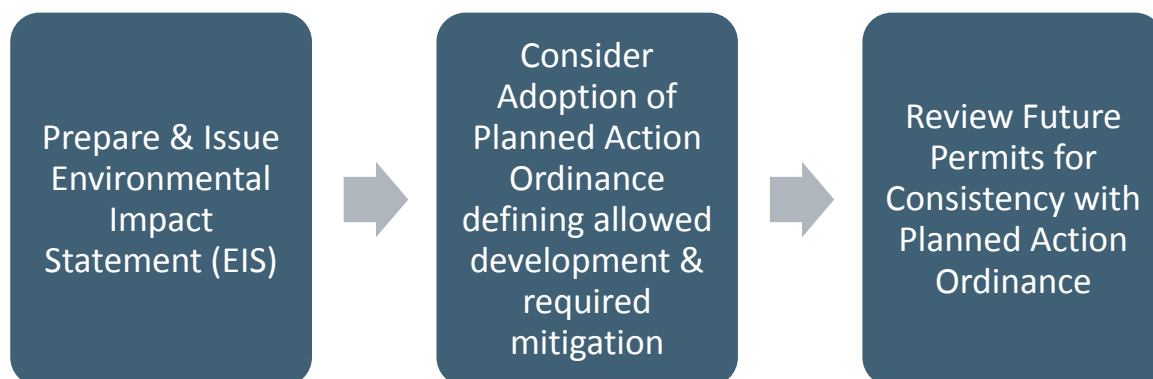
The Proposed Planned Action Boundary is included in Exhibit 1. A diagram of the Planned Action process is included in Exhibit 2.

Exhibit 1. Proposed Planned Action Boundary



Source: BERK Consulting 2018

Exhibit 2. Planned Action Process



Review of a planned action is intended to be simpler and more focused than for other projects. If the PAO is adopted, the City would follow the applicable procedures contained in the ordinance to determine if the proposed project impacts are consistent with the EIS. When a permit application and environmental checklist are submitted for a project that is being proposed as a planned action project, the City must first verify the following:

- The project meets the description of any project(s) designated as a planned action by ordinance or resolution;
- The probable significant adverse environmental impacts were adequately addressed in the EIS; and
- The project includes any conditions or mitigation measures outlined in the ordinance or resolution.

If the project meets the above requirements, the project qualifies as a planned action project and a SEPA threshold determination is not required. However, City actions (i.e., the permit process) are still applicable.

Alternatives

The City has developed three land use alternatives for consideration by the Planning Commission and City Council:

- **No Action.** This alternative assumes growth according to current trends and under current City Plans and development regulations, including over 450 housing units, and over 1,660 jobs.
- **Action Alternative 1**, assuming a moderate level of development, with over three times the housing and over two times the jobs as the No Action Alternative, based on targeted infrastructure and civic investments and plan and code changes. Investments include a “Green Loop” of street and trail improvements, more public streets, and a 2-acre central park. Development evaluated include nearly 1,580 housing units and over 4,150 jobs.
- **Action Alternative 2**, assuming a high level of growth, with five times the housing and jobs compared with No Action and with the greatest level of civic and infrastructure investments, (including a Green Loop, added public streets, and a 4-acre central park). With Alternative 2, over 2,250 housing units would be developed and nearly 7,370 jobs.

Exhibit 3. Comparison of Alternatives Population, Housing, Employment

Alternative	Population	Housing	Jobs
Base Year Units	909	419	5,248
Net Growth			
No Action	990	456	1,667
Action Alternative 1	3,426	1,579	4,147
Action Alternative 2	4,898	2,257	7,369
Total Units 2035			
No Action	1,899	875	6,915
Action Alternative 1	4,336	1,998	9,395
Action Alternative 2	5,807	2,676	12,617

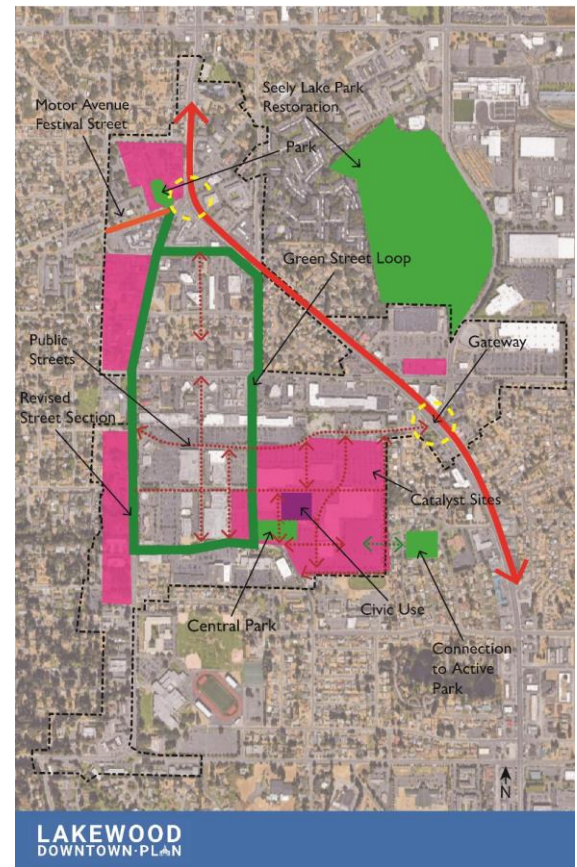
Notes: No Action estimates based on City of Lakewood Transportation Model; assumes limited redevelopment
Persons per Household, Census Tract 719.01, 1-Year Estimates 2016

Key concepts included in the Action Alternatives are described below.

The overall concept plan was initially developed during the 2017 charrette and informed by the public design exercise, public input to date, and insights from the planning and design team based on best practices and experience on similar projects (See Exhibit 4). The following are highlights from the concept plan:

- **Green Street Loop:** To address the lack of park space, improve public streets, and improve circulation for pedestrians and bicyclists the green loop will include park like elements, green infrastructure, and support redevelopment in Downtown.
- **New Public Streets:** The Downtown lacks a dense and walkable street grid to support urban development, circulation, and an active public realm.
- **Central Park:** A new urban park of between two to four acres is proposed just north of City Hall to serve as the main gathering space for the community and to include a variety of features and programming.
- **Revised Gravelly Lake Drive:** As part of the Green Street Loop, a revised road design for Gravelly Lake Drive SW is proposed. The revision will allow for expanded sidewalks and a multi-use path on the east side of the street.
- **Catalyst Sites:** Catalyst sites are the best opportunities to weave together public improvements in infrastructure and amenities with infill and redevelopment by the private sector. The best opportunities for redevelopment based on vacant and underutilized sites, and large surface parking areas, and surrounding context have been identified as catalyst sites in the near term to further the implementation of this Plan.
- **Motor Avenue Festival Street:** The City intends to move forward with creating a festival street along Motor Avenue consistent with the adopted concept plan. The plan includes a large central plaza, a pedestrian promenade, a farmer's market and event structure, street trees, landscaping, and public art opportunities.

Exhibit 4. Downtown Plan Concept



Framework, 2017

The City may choose to implement an alternative under study, a combination of alternatives, or another alternative in the range of alternatives studied in the EIS.

Proposed Planned Action Ordinance

The proposed draft Planned Action Ordinance (PAO) is included in the Draft Environmental Impact Statement (EIS) published in March 2018, and includes the following sections:

- Recitals: The recitals identify facts and procedures the City followed in developing the PAO.
- Purpose: The overall purposes are to streamline and expedite the land use permit review process in the PAO and ensure that environmental analysis, land use plans, development regulations, City codes and ordinances together with the mitigation measures in the Planned Action EIS and Addendum mitigate environmental impacts.
- Findings: The findings indicate the PAO meets the criteria in SEPA Rules.
- Procedures and Criteria for Evaluating and Determining Planned Action Projects within Planned Action Area: This section establishes thresholds for growth, land use, and transportation. This section also establishes criteria by which the City would review planned action applications.
- Monitoring and Review: Establishes a review process to monitor the progress of the Planned Action.
- Exhibit A: Identifies the boundary of the Planned Action Area.
- Exhibit B: Identifies Planned Action EIS Mitigation Measures that apply to new development. Mitigation addresses topics such as natural environment, population/employment/housing, land use, transportation, public services, and utilities, plus topics addressed in the SEPA Checklist such as cultural resources and human health.

Contact

For more information, please contact:

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Planning Manager, Special Projects
City of Lakewood
6000 Main St SW, Lakewood, WA 98499
253.983.7702
tspeir@cityoflakewood.us

Note: This document shows changes and additions to the Ordinance framework included in the Draft Environmental Impact Statement for the Downtown Plan.

~~APPENDIX B~~

ORDINANCE NO. _____

AN ORDINANCE of the City Council of the City of Lakewood, Washington, establishing a planned action for Downtown Lakewood pursuant to the State Environmental Policy Act.

WHEREAS, the State Environmental Policy Act (SEPA) and its implementing regulations provide for the integration of environmental review with land use planning and project review through the designation of planned actions by jurisdictions planning under the Growth Management Act (GMA), such as the City of Lakewood ("City"); and

WHEREAS, Section 43.21C.440 of the Revised Code of Washington (RCW), Sections 197-11-164 through 172 of the Washington Administrative Code (WAC) allow for and govern the adoption and application of a planned action designation under SEPA, and Section 14.02.030 of the Lakewood Municipal Code (LMC) adopts Chapter 197-11 WAC by reference as amended; and

WHEREAS, the designation of a planned action expedites the permitting process for projects of which the impacts have been previously addressed in an environmental impact statement (EIS); and

WHEREAS, a subarea of the City commonly referred to as the "Downtown", as depicted on the map attached hereto as Exhibit A and incorporated herein by this reference, has been identified as a planned action area for future redevelopment to a mixed-use center ("Planned Action Area"); and

WHEREAS, the City has developed and adopted a subarea plan complying with the GMA (RCW 36.70A), dated [REDACTED] 2018, to guide the redevelopment of the Planned Action Area ("Downtown Plan"); and

WHEREAS, after extensive public participation and coordination with all affected parties, the City, as lead SEPA agency, issued the Downtown Planned Action Final Environmental Impact Statement ("Final EIS") dated [REDACTED] 2018, which identifies the impacts and mitigation measures associated with planned development in the Planned Action Area as identified in the Downtown Plan; the Final EIS includes by incorporation the Downtown Planned Action Draft Environmental Impact Statement issued on [REDACTED] 2018 (collectively referred to herein as the "Planned Action EIS"); and

WHEREAS, the City desires to designate a planned action under SEPA for the Downtown ("Planned Action"); and

WHEREAS, adopting a Planned Action for the Downtown with appropriate standards and procedures will help achieve efficient permit processing and promote environmental quality protection; and

WHEREAS, the City has adopted development regulations and ordinances that will help protect the environment and will adopt regulations to guide the allocation, form, and quality of development in the Downtown; and

WHEREAS, the City Council finds that adopting this Ordinance is in the public interest and will advance the public health, safety, and welfare;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON DO ORDAIN as follows:

Section I. Purpose. The purpose of this Ordinance is to:

- A.** Combine environmental analysis, land use plans, development regulations, and City codes and ordinances together with the mitigation measures in the Planned Action EIS to mitigate environmental impacts and process Planned Action development applications in the Planned Action Area;
- B.** Designate the Downtown shown in Exhibit A as a Planned Action Area for purposes of environmental review and permitting of designated Planned Action Projects pursuant RCW 43.21C.440;
- C.** Determine that the Planned Action EIS meets the requirements of a planned action EIS pursuant to SEPA;
- D.** Establish criteria and procedures for the designation of certain projects within the Planned Action Area as “Planned Action Projects” consistent with RCW 43.21C.440;
- E.** Provide clear definition as to what constitutes a Planned Action Project within the Planned Action Area, the criteria for Planned Action Project approval, and how development project applications that qualify as Planned Action Projects will be processed by the City;
- F.** Streamline and expedite the land use permit review process by relying on the Planned Action EIS; and
- G.** Apply applicable regulations within the City’s development regulations and the mitigation framework contained in this Ordinance for the processing of Planned Action Project applications and to incorporate the applicable mitigation measures into the underlying project permit conditions in order to address the impacts of future development contemplated by this Ordinance.

Section II. Findings. The City Council finds as follows:

- A.** The Recitals above are adopted herein as Findings of the City Council.
- B.** The City is subject to the requirements of the GMA.
- C.** The City has adopted a Comprehensive Plan complying with the GMA and is amending the Comprehensive Plan to incorporate text and policies specific to the Downtown.
- D.** The City is adopting zoning and development regulations concurrent with the Downtown Plan to implement said Plan, including this Ordinance.
- E.** The Planned Action EIS adequately identifies and addresses the probable significant environmental impacts associated with the type and amount of development planned to occur in the designated Planned Action Area.
- F.** The mitigation measures identified in the Planned Action EIS, attached to this Ordinance as Exhibit B and incorporated herein by reference, together with adopted City development regulations are adequate to mitigate significant adverse impacts from development within the Planned Action Area.
- G.** The Downtown Plan and Planned Action EIS identify the location, type, and amount of development that is contemplated by the Planned Action.
- H.** Future projects that are implemented consistent with the Planned Action will protect the environment, benefit the public, and enhance economic development.

I. The City provided several opportunities for meaningful public involvement and review in the Downtown Plan and Planned Action EIS processes, including a community meeting consistent with RCW 43.21C.440; has considered all comments received; and, as appropriate, has modified the proposal or mitigation measures in response to comments.

J. Essential public facilities as defined in RCW 36.70A.200 are excluded from the Planned Action as designated herein and are not eligible for review or permitting as Planned Action Projects unless they are accessory to or part of a project that otherwise qualifies as a Planned Action Project.

K. The designated Planned Action Area is located entirely within an Urban Growth Area.

L. Implementation of the mitigation measures identified in the Planned Action EIS will provide for adequate public services and facilities to serve the proposed Planned Action Area.

Section III. Procedures and Criteria for Evaluating and Determining Planned Action Projects within the Planned Action Area.

A. Planned Action Area. This “Planned Action” designation shall apply to the area shown in Exhibit A of this Ordinance.

B. Environmental Document. A Planned Action Project determination for a site-specific project application within the Planned Action Area shall be based on the environmental analysis contained in the Planned Action EIS. The mitigation measures contained in Exhibit B of this Ordinance are based upon the findings of the Planned Action EIS and shall, along with adopted City regulations, provide the framework the City will use to apply appropriate conditions on qualifying Planned Action Projects within the Planned Action Area.

C. Planned Action Project Designated. Land uses and activities described in the Planned Action EIS, subject to the thresholds described in Subsection III.D of this Ordinance and the mitigation measures contained in Exhibit B of this Ordinance, are designated “Planned Action Projects” pursuant to RCW 43.21C.440. A development application for a site-specific project located within the Planned Action Area shall be designated a Planned Action Project if it meets the criteria set forth in Subsection III.D of this Ordinance and all other applicable laws, codes, development regulations, and standards of the City, including this Ordinance, are met.

D. Planned Action Qualifications. The following thresholds shall be used to determine if a site-specific development proposed within the Planned Action Area was contemplated as a Planned Action Project and has had its environmental impacts evaluated in the Planned Action EIS:

(1) Qualifying Land Uses.

(a) **Planned Action Categories:** The following general categories/types of land uses are defined in the Downtown Plan and can qualify as Planned Actions:

- i. Townhome dwelling units
- ii. Multi-family dwelling units
- iii. Commercial Office
- iv. Services,
- v. Medical
- vi. Hotel and Lodging

- vii. Retail and Eating and Drinking Establishments
- viii. Open Space, Parks, Plazas, Trails, Gathering Spaces, Recreation
- ix. Cultural Facilities
- x. Governmental Facilities

(b) Planned Action Project Land Uses: A primary land use can qualify as a Planned Action Project land use when:

- i. it is within the Planned Action Area as shown in Exhibit A of this Ordinance;
- ii. it is within one or more of the land use categories described in Subsection III.D(1)(a) above; and
- iii. it is listed in development regulations applicable to the zoning classifications applied to properties within the Planned Action Area.

A Planned Action Project may be a single Planned Action land use or a combination of Planned Action land uses together in a mixed-use development. Planned Action land uses may include accessory uses.

- (c) Public Services: The following public services, infrastructure, and utilities can also qualify as Planned Actions: onsite roads, utilities, parks, trails, and similar facilities developed consistent with the Planned Action EIS mitigation measures, City and special district design standards, critical area regulations, and the Lakewood Municipal Code.

(2) Development Thresholds:

(a) Land Use: The following thresholds of new land uses are contemplated by the Planned Action:

FEATURE	PHASE 1: ALTERNATIVE 1, MODIFIED	PHASE 2: ALTERNATIVE 2
Residential Dwellings (units): Net 2018-2035	1,579 1,725	2,257
Commercial Square Feet: Net 2018-2035	1.65 million square feet	2.85 million square feet
Jobs: Net 2018-2035	4,147 4,531	7,369

Note: Under Alternative 1, to achieve 45 activity units per acre, the density would need to increase to nearly 95 units per acre instead of 80 units per acre. Alternatively, the job capture would need to increase to 60%. A combination approach of 85 units per acre, and job capture of 55% would also create sufficient activity units. This combination approach is assumed above. If uses are housing and office/service oriented it would have less impact on traffic trips than retail.

- (b) Shifting development amounts between land uses in identified in Subsection III.D(2)(a) may be permitted when the total build-out is less than the aggregate amount of development reviewed in the Planned Action EIS; the traffic trips for the preferred alternative are not exceeded; and, the development impacts identified in the Planned Action EIS are mitigated consistent with Exhibit B of this Ordinance.
- (c) Further environmental review may be required pursuant to WAC 197-11-172, if any individual Planned Action Project or combination of Planned Action Projects exceeds the development thresholds specified in this Ordinance and/or alter the assumptions and analysis in the Planned Action EIS.

(3) Transportation Thresholds:

(a) Trip Ranges & Thresholds. The number of new PM peak hour trips anticipated in the Planned Action Area and reviewed in the Planned Action EIS for 2035 is as follows:

Land Use	QUANTITY			ITE GROSS PM PEAK HOUR TRIPS		
	Existing	No Action	Alternative 1	Alternative 2	No Action	Alternative 1 2
Total Dwelling Units						
Total Commercial Sq. Ft. (ksf)						
Mainstreet Adjustments						
Internal Capture (% / % / %)						
External Trips - All Modes						
External Walk/Bike (% / % / %)						
External Transit (% / % / %)						
External Vehicle Trips						
Total Reduction Applied to Travel Model						

Source: Fehr & Peers, 2018. **[To Be Filled in with Phase 1: Alternative 1 Modified]**

- (b) Concurrency. All Planned Action Projects shall meet the transportation concurrency requirements and the Level of Service (LOS) thresholds established in LMC 18A.50.195.
- (c) Traffic Impact Mitigation. Transportation mitigation shall be provided consistent with mitigation measures in ~~Exhibit B, Attachment Exhibit B-1~~ and Exhibit D of this Ordinance, attached hereto and incorporated by this reference.
- (d) The responsible City official shall require documentation by Planned Action Project applicants demonstrating that the total trips identified in Subsection III.D(3)(a) are not exceeded, that the project meets the concurrency and intersection standards of Subsection III.D(3)(b), and that the project has mitigated impacts consistent with Subsection III.D (3)(c).
- (e) Discretion.
- The responsible City official shall have discretion to determine incremental and total trip generation, consistent with the Institute of Traffic Engineers (ITE) Trip Generation Manual (latest edition) or an alternative manual accepted by the City's Public Works Director at his or her sole discretion, for each project permit application proposed under this Planned Action.
 - The responsible City official shall have discretion to condition Planned Action Project applications to meet the provisions of this Planned Action Ordinance and the Lakewood Municipal Code.

iii. Planned Action Project applicants shall pay a proportionate share of the costs of the projects identified in Exhibit D. The responsible City official shall have the discretion to adjust the allocation of responsibility for required improvements between individual Planned Action Projects based upon their identified impacts.

- (4) Elements of the Environment and Degree of Impacts. A proposed project that would result in a significant change in the type or degree of adverse impacts to any element(s) of the environment analyzed in the Planned Action EIS would not qualify as a Planned Action Project.
- (5) Changed Conditions. Should environmental conditions change significantly from those analyzed in the Planned Action EIS, the City's SEPA Responsible Official may determine that the Planned Action Project designation is no longer applicable until supplemental environmental review is conducted.

E. Planned Action Project Review Criteria.

- (1) The City's SEPA Responsible Official, or authorized representative, may designate as a Planned Action Project, pursuant to RCW 43.21C.440, a project application that meets all of the following conditions:
- (a) the project is located within the Planned Action Area identified in Exhibit A of this Ordinance;
 - (b) the proposed uses and activities are consistent with those described in the Planned Action EIS and Subsection III.D of this Ordinance;
 - (c) the project is within the Planned Action thresholds and other criteria of Subsection III.D of this Ordinance;
 - (d) the project is consistent with the Lakewood Comprehensive Plan including the policies of the Downtown Plan incorporated into the Comprehensive Plan and the regulations of the Downtown Plan integrated into the Lakewood Municipal Code;
 - (e) the project's significant adverse environmental impacts have been identified in the Planned Action EIS;
 - (f) the project's significant impacts have been mitigated by application of the measures identified in Exhibit B of this Ordinance and other applicable City regulations, together with any conditions, modifications, variances, or special permits that may be required;
 - (g) the project complies with all applicable local, state and/or federal laws and regulations and the SEPA Responsible Official determines that these constitute adequate mitigation; and
 - (h) the project is not an essential public facility as defined by RCW 36.70A.200, unless the essential public facility is accessory to or part of a development that is designated as a Planned Action Project under this Ordinance.
- (2) The City shall base its decision to qualify a project as a Planned Action Project on review of the SEPA Checklist form in WAC 197-11 and review of the Planned Action Project submittal and supporting documentation, provided on City required forms.

F. Effect of Planned Action Designation.

- (1) Designation as a Planned Action Project by the City's SEPA Responsible Official means that a qualifying project application has been reviewed in accordance with this Ordinance and found to be

consistent with the development parameters and thresholds established herein and with the environmental analysis contained in the Planned Action EIS.

- (2) Upon determination by the City's SEPA Responsible Official that the project application meets the criteria of Subsection III.D and qualifies as a Planned Action Project, the project shall not require a SEPA threshold determination, preparation of an EIS, or be subject to further review pursuant to SEPA. Planned Action Projects will still be subject to all other applicable City, state, and federal regulatory requirements. The Planned Action Project designation shall not excuse a project from meeting the City's code and ordinance requirements apart from the SEPA process.

G. Planned Action Project Permit Process. Applications submitted for qualification as a Planned Action Project shall be reviewed pursuant to the following process:

- (1) Development applications shall meet all applicable requirements of the Lakewood Municipal Code and this Ordinance in place at the time of the Planned Action Project application. Planned Action Projects shall not vest to regulations required to protect public health and safety.
- (2) Applications for Planned Action Projects shall:
 - (a) be made on forms provided by the City;
 - (b) include the SEPA checklist in WAC 197-11;
 - (c) meet all applicable requirements of the Lakewood Municipal Code and this Ordinance.
- (3) The City's SEPA Responsible Official shall determine whether the application is complete and shall review the application to determine if it is consistent with and meets all of the criteria for qualification as a Planned Action Project as set forth in this Ordinance.
- (4) (a) If the City's SEPA Responsible Official determines that a proposed project qualifies as a Planned Action Project, he/she shall issue a "Determination of Consistency" and shall mail or otherwise verifiably deliver said Determination to the applicant; the owner of the property as listed on the application; and federally recognized tribal governments and agencies with jurisdiction over the Planned Action Project, pursuant to Chapter 1, Laws of 2012 (Engrossed Substitute Senate Bill (ESSB) 6406).
 - (b) Upon issuance of the Determination of Consistency, the review of the underlying project permit(s) shall proceed in accordance with the applicable permit review procedures specified in Chapter 18A.02 LMC, except that no SEPA threshold determination, EIS, or additional SEPA review shall be required.
 - (c) The Determination of Consistency shall remain valid and in effect as long as the underlying project application approval is also in effect.
 - (d) Public notice and review for qualified Planned Action Projects shall be tied to the underlying project permit(s). If notice is otherwise required for the underlying permit(s), the notice shall state that the project qualifies as a Planned Action Project. If notice is not otherwise required for the underlying project permit(s), no special notice is required by this Ordinance.
- (5) (a) If the City's SEPA Responsible Official determines that a proposed project does not qualify as a Planned Action Project, he/she shall issue a "Determination of Inconsistency" and shall mail or otherwise verifiably deliver said Determination to the applicant; the owner of the property as listed on the application; and federally recognized tribal governments and agencies with jurisdiction over

the Planned Action Project, pursuant to Chapter 1, Laws of 2012 (Engrossed Substitute Senate Bill (ESSB) 6406).

(b) The Determination of Inconsistency shall describe the elements of the Planned Action Project application that result in failure to qualify as a Planned Action Project.

(c) Upon issuance of the Determination of Inconsistency, the City's SEPA Responsible Official shall prescribe a SEPA review procedure for the non-qualifying project that is consistent with the City's SEPA regulations and the requirements of state law.

(d) A project that fails to qualify as a Planned Action Project may incorporate or otherwise use relevant elements of the Planned Action EIS, as well as other relevant SEPA documents, to meet the non-qualifying project's SEPA requirements. The City's SEPA Responsible Official may limit the scope of SEPA review for the non-qualifying project to those issues and environmental impacts not previously addressed in the Planned Action EIS.

(6) To provide additional certainty about applicable requirements, the City or applicant may request consideration and execution of a development agreement for a Planned Action Project, consistent with RCW 36.70B.170 et seq.

(7) A Determination of Consistency or Inconsistency is a Process I land use decision and may be appealed pursuant to the procedures established in Chapter 18A.02 LMC. An appeal of a Determination of Consistency shall be consolidated with any pre-decision or appeal hearing on the underlying project application.

Section IV. Monitoring and Review.

A. The City should monitor the progress of development in the designated Planned Action area as deemed appropriate to ensure that it is consistent with the assumptions of this Ordinance and the Planned Action EIS regarding the type and amount of development and associated impacts and with the mitigation measures and improvements planned for the Planned Action Area.

B. This Planned Action Ordinance shall be reviewed by the SEPA Responsible Official no later than five (5) years from its effective date in conjunction with the City's regular Comprehensive Plan review cycle, as applicable. The timing of subsequent reviews after the first review shall be determined with the completion of the first review. The review shall determine the continuing relevance of the Planned Action assumptions and findings with respect to environmental conditions in the Planned Action Area, the impacts of development, and required mitigation measures (Exhibit B) and Public Agency Actions and Commitments (Exhibit C). Based upon this review, the City may propose amendments to this Ordinance or may supplement or revise the Planned Action EIS.

Section V. Conflict. In the event of a conflict between this Ordinance or any mitigation measures imposed thereto, and any ordinance or regulation of the City, the provisions of this Ordinance shall control.

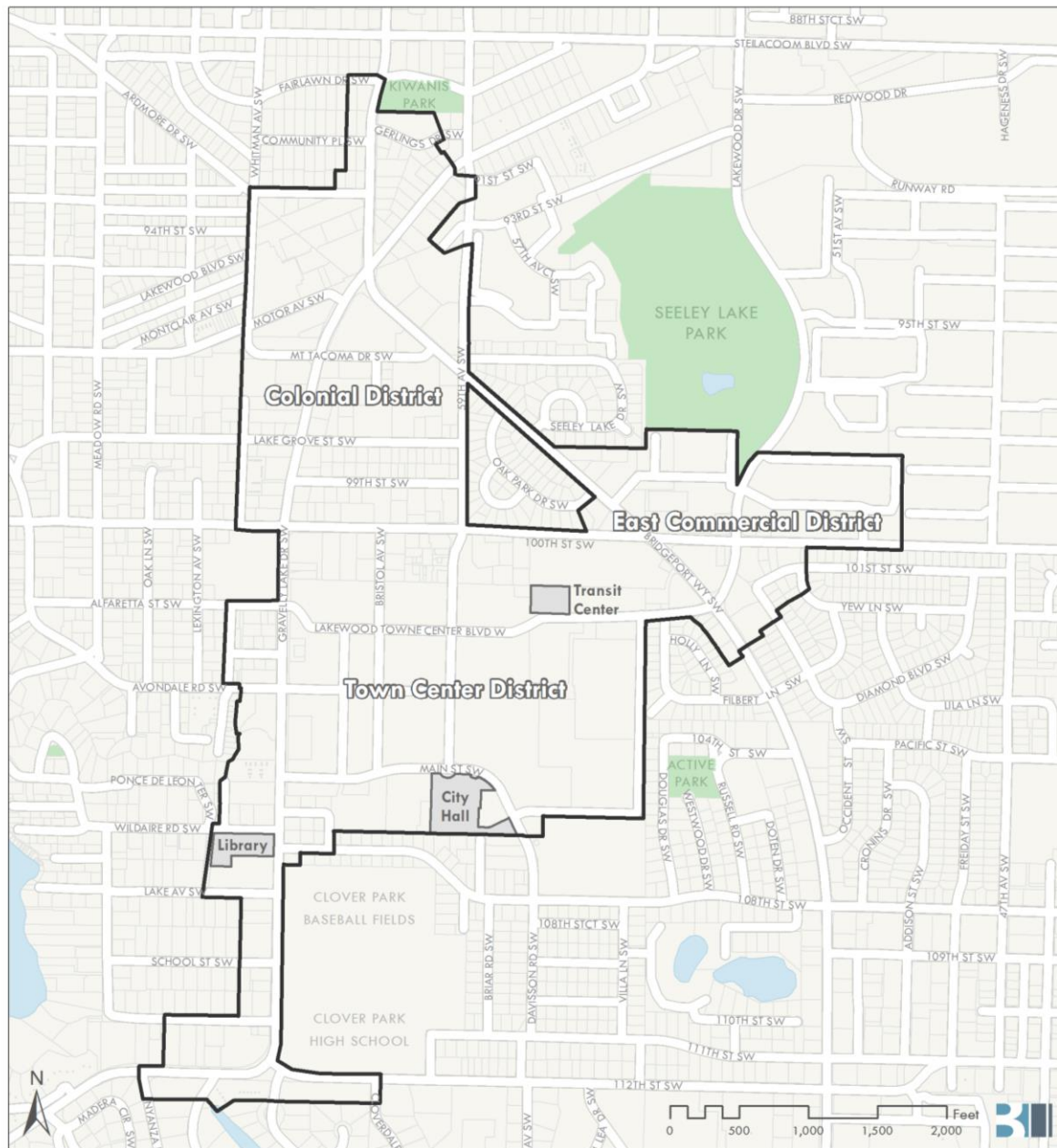
Section VI. Severability. If any one or more sections, subsections, or sentences of this Ordinance are held to be unconstitutional or invalid such decision shall not affect the validity of the remaining portions of this Ordinance and the same shall remain in full force and effect.

Section VII. Effective Date. This Ordinance shall take effect and be in force **ten (10)** days after publication as provided by law.

ADOPTED by the City Council this ____ day of _____, 2018. CITY OF LAKEWOOD.

	<hr/> Don Anderson, Mayor
Attest: <hr/> Alice M. Bush, MMC, City Clerk	
Approved as to Form: <hr/> Heidi A. Wachter City Attorney	

Exhibit A. Planned Action Area



LEGEND

- Lakewood CBD Boundary
- Roads
- Public Facilities
- Parks
- Parcels
- Waterbody

Note: Map adjusts centerline boundaries to ensure full rights of way are included to provide for a common consistent landscaping. Likewise, the right of way for Bridgeport Way between the East Commercial Area and Colonial District is included for consistent landscaping. Abutting land use is not included in that segment.

EXHIBIT B. MITIGATION DOCUMENT

Note: This is filled in based on the Draft EIS. Tracks on mitigation measures are included to show differences.

The Planned Action EIS has identified significant beneficial and adverse impacts that are anticipated to occur with the future development of the Planned Action Area, together with a number of possible measures to mitigate those significant adverse impacts. Please see Final EIS Chapter 1 Summary for a description of impacts, mitigation measures, and significant unavoidable adverse impacts.

A Mitigation Document is provided in this Exhibit B-1 to establish specific mitigation measures based upon significant adverse impacts identified in the Planned Action EIS. The mitigation measures in this Exhibit B-1 shall apply to Planned Action Project applications that are consistent with the Preferred Alternative range reviewed in the Planned Action EIS and which are located within the Planned Action Area (see Exhibit A).

Where a mitigation measure includes the words “shall” or “will,” inclusion of that measure in Planned Action Project application plans is mandatory in order to qualify as a Planned Action Project. Where “should” or “would” appear, the mitigation measure may be considered by the project applicant as a source of additional mitigation, as feasible or necessary, to ensure that a project qualifies as a Planned Action Project. Unless stated specifically otherwise, the mitigation measures that require preparation of plans, conduct of studies, construction of improvements, conduct of maintenance activities, etc., are the responsibility of the applicant or designee to fund and/or perform.

Any and all references to decisions to be made or actions to be taken by the City’s SEPA Responsible Official may also be performed by the City’s SEPA Responsible Official’s authorized designee.

Section B-1. Mitigation Required for Development Applications

Natural Environment

1. With major redevelopment that would propose activities that could involve groundwater discharge or potential changes to groundwater flow (such as underground structures), the City ~~could~~shall require site specific evaluation of groundwater protection. The susceptibility and vulnerability of the critical aquifer recharge area ~~should~~shall be evaluated by a licensed hydrogeologist. All stormwater ~~should~~shall be treated appropriately to avoid any potential groundwater contamination. Stormwater improvements should be designed to improve aquifer recharge.
2. The City ~~could~~shall require a conservation easement or other regulatory structure for piped streams to ensure that the possibility of creek daylighting is not precluded by future redevelopment.

Transportation

3. Implementation of transportation improvements identified as mitigation measures ~~could~~shall occur through a SEPA fair share fee program such that new development contributes its share of the cost for these projects. See Appendix D.

Environmental Health (SEPA Checklist Draft EIS Appendix A)

4. Applicants for development shall conduct a site assessment to determine if contamination is present from past use.

Section B-2. Advisory Notes to Applicants: Applicable Regulations and Commitments

The Planned Action EIS identifies specific regulations that act as mitigation measures. These are summarized in this section by EIS topic. All applicable federal, state, and local regulations shall apply to Planned Action Projects. Planned Action Project applicants shall comply with all adopted regulations where applicable including those listed in the Planned Action EIS and those not included in the Planned Action EIS. In addition, this section identifies voluntary water and energy conservation measures that may be implemented by new development.

Natural Environment

Planned Actions shall comply with applicable regulations:

- City of Lakewood Critical Area Regulations (Title 14A), which includes protection of:
 - Aquifer recharge areas;
 - Fish and wildlife habitat areas (including streams) and their buffers;
 - Flood hazard areas;
 - Wetlands and their buffers;
- City of Lakewood Engineering Standards Manual (City of Lakewood, 2016);
- 2012 Stormwater Management Manual for Western Washington (as amended in 2014) (Washington Department of Ecology, 2014);
- Pierce County Stormwater Management and Site Development Manual (Pierce County, 2015) ; and
- WSDOT Highway Runoff Manual (Washington State Department of Transportation, 2014)

Planned Actions shall comply with the Downtown Development Code:

- 18A.35. 540.E: Native and/or drought tolerant landscaping shall be incorporated into required landscape plans. **[Proposed]**
- 18A.35. 540.F: The City may require educational signage for aboveground stormwater facilities and/or added natural features. **[Proposed]**

Population, Employment, and Housing

The City allows for tax exemptions for development projects including low and moderate-income housing units in “Tax Incentive Urban Use Centers” in Chapter 3.64 in the Lakewood Municipal Code. As defined in 3.64.010, such a center means “a compact, identifiable district where urban residents may obtain a variety of products and services” and which has businesses, adequate public facilities, and a mix of uses including housing, recreation, and cultural activities. The Downtown Study Area is included in this boundary. Planned actions are encouraged to implement this voluntary incentive. **[Suggest boundary adjustment to match Downtown Plan boundary.]**

Land Use

- Planned Actions shall comply with the Downtown Development Code: Title 18A.35. [Proposed]

Transportation

- Washington State Commute Trip Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip reduction plans and work toward meeting their mode share targets through internal programs and monitoring.

Public Services and Utilities

Planned Actions shall comply with applicable regulations:

- City of Lakewood Comprehensive Plan including the Capital Facilities and Utilities elements, and Legacy Plan for Parks, regarding levels of service.
- Downtown Development Code: 18A.35.530 Common and Open Space Standards. [Proposed]
- Lakewood Municipal Code standards for water, sewer, and stormwater infrastructure for new development. (LMC Title 12A)
- Lakewood Municipal Code requires application of the national energy code (LMC Chapter 15A.25).

Following are voluntary measures that result in water and energy efficiency and are encouraged in new development:

- Developments may reduce water demand by using new technologies that would reduce per-capita water use (and therefore wastewater service demand) by using newer, low- or no-flow plumbing fixtures and equipment.
- Implementation of sustainable requirements including the construction and operation of LEED-compliant (or similar ranking system) buildings could reduce the increase required in power systems.
- Implementation of conservation efforts and renewable energy sources to conserve electricity in new developments, including energy efficient equipment (i.e., light bulbs, appliances, and heating and air conditioning), could reduce energy consumption.

Environmental Health (SEPA Checklist)

The State Model Toxics Control Act (MTCA) sets standards for cleanup of lower levels of contaminants that are incorporated into new development and redevelopment parcels noted to have contamination potential. The City applies relevant standards regarding hazardous materials handling in the International Fire Code and Zoning Codes.

EXHIBIT C. PUBLIC AGENCY ACTIONS AND COMMITMENTS

Under some elements of the Planned Action EIS, specific City or other agency actions are identified. Generally, incorporation of these actions is intended to provide for consistency within the City's Comprehensive Plan, Downtown Plan, or between the Downtown Plan and implementing regulations; to document pending City actions; to establish a protocol for long-term measures to provide for coordination with other agencies; or to identify optional actions that the City may take to reduce impacts. These actions are listed in Exhibit 1.

Exhibit 1. Public Agency Actions and Commitments

MITIGATION MEASURES	PROPOSED SYNCHRONOUS AMENDMENTS	SHORT TERM: NEXT DOCKET OR WITHIN 5 YEARS	LONG TERM	OTHER AGENCY	ESTIMATED YEAR OF IMPLEMENTATION AND RESPONSIBLE DEPARTMENT
Natural Environment					
The ecological benefits of daylighting a portion of Ponce de Leon Creek could be evaluated by the City. An evaluation could include leaving the stream piped but identifying its historic location, as well as considering water quality treatments that benefit the nearby open channel stream, and serve as landscape amenities in the Study Area.			X		Public Works
The Downtown Plan can offers support for Pierce County efforts to address potential habitat, stormwater, and recreation improvements to Seeley Lake Park.		X		X	Parks and Community Development
Population, Employment, and Housing					
The City works with the Economic Development Board for Tacoma-Pierce County on business retention, expansion, and recruitment activities, as well as the Lakewood Chamber of Commerce. If small business relocation assistance is needed, the City could work with these agencies or others to develop strategies and solutions.			X	X	Economic Development
Land Use					
Amend the Comprehensive Plan Land Use Designation for the Study Area and create a new implementing "form-based code". Amend capital facility and transportation elements.	X				Community Development
<u>In collaboration with Pierce County, the 2014 Buildable Lands Report methods for Lakewood should be updated at the next Buildable Lands Report Update to reflect an alternative method to the jobs per acre approach. The analysis should also reflect a higher density in the Downtown.</u>		X		X	Community Development

MITIGATION MEASURES	PROPOSED SYNCHRONOUS AMENDMENTS	SHORT TERM: NEXT DOCKET OR WITHIN 5 YEARS	LONG TERM	OTHER AGENCY	ESTIMATED YEAR OF IMPLEMENTATION AND RESPONSIBLE DEPARTMENT
Transportation					
The City of Lakewood <u>has included</u> policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote TDM strategies.		X			Public Works
Public Services					
Implement the Legacy Plan and Downtown Plan to promote urban nodal and urban linear parks meeting distance standards. [See Downtown Plan for proposed.]	X		X		Parks
Utilities					
City of Lakewood Comprehensive Plan including the Capital Facilities and Utilities element that set levels of service and coordination policies with service providers.	X		X		Community Development
Ongoing updates to Comprehensive Water System Plan by the Lakewood Water District and the Unified Sewer Plan by Pierce County would address the increases in density in the Study Area and ensure services are in place to meet the growing demand.			X	X	Community Development in association with Lakewood Water District and Pierce County
Power service providers conduct regular electric utility resource planning to address service demand and conservation.			X	X	Community Development in association with Tacoma Public Utilities and Lakewood Light and Power

EXHIBIT D. TRANSPORTATION COST ESTIMATES

Note: Tracks show changes to EIS text.

The City could make capital improvements to increase the capacity of impacted intersections and roadways in the Study Area. Exhibit 3.4-21 The Planned Action EIS describes potential improvements to the network and impacted study intersections in addition to the City's 2018-2023 6-Year Transportation Improvement Program. Implementation of improvements identified in Exhibit 2 could shall occur through a SEPA fair share fee program such that new development contributes its share of the cost for these projects.

Planned Action EIS traffic modeling identified approximately 45% pass-through traffic in the study area. The maximum responsibility of cumulative planned action development is 55%. The proportionate share of costs of the Planned Actions shall be determined based on their proportionate share of trips identified in Section III.D(3) of this ordinance. [This approach is being reviewed by Fehr & Peers.]

For a conservative test of alternative transportation improvements, the EIS authors modeled the greatest shift in traffic for Gravelly Lake Drive at three lanes and then compared it to no change in the section (five lanes). The table of mitigation measures below shows the full list of improvements if Gravelly Lake Drive were modified to a cross section of three lanes.

If five lanes were retained, the following intersections would not require change:

- ~~108th St SW / Bridgeport Way SW~~
- ~~112th St SW / Gravelly Lake Drive SW~~

Exhibit 2. Transportation Improvements in addition to Six-Year Transportation Improvement Program

PROJECT	TITLE	COST (100%)	PLANNED ACTION SHARE 55%
1	Gravelly Lake Dr SW Revised Section: 4-lane section plus median/turn lane shown in the March 2018 Subarea Plan concept #3A on page 38	\$19,410,488	\$10,675,768
2.1	Conversion of Lakewood Towne Center Blvd as Public Street	\$11,756,511	\$6,466,081
2.2	Conversion of Bristol Ave as Public Street	\$7,357,170	\$4,046,444
3	Lakewood Towne Center Blvd at 59th Ave SW, Roundabout	\$2,401,959	\$1,321,077
4	Reduce 59th Ave SW to two lanes, allowing for bicycle facilities (sharrows)	\$188,559	\$103,708
5	Gravelly Lake Dr / Avondale Rd SW New Signalized Intersection	\$1,177,548	\$647,651
6	100th St SW / Bridgeport Way SW	\$648,673	\$356,770
7	100th St SW / Lakewood Dr SW	\$8,000	\$4,400
8	Option A: 100th St SW / Lakewood Dr SW: Convert westbound through-left lane to left only to remove split phase	\$13,000	\$7,150
-	Option B: 100th St SW / Lakewood Dr SW: Move the pedestrian crossing to the north side of the intersection coincident with the WB phase	\$268,688	\$147,779
Total	with 8A [Proposed]	\$42,961,908	\$23,629,049
-	with 8B [Proposed]	\$43,217,596	\$23,769,678

PLANNING COMMISSION AGENDA ITEM

CITY OF LAKEWOOD, WASHINGTON

AGENDA TITLE:	6-YEAR TIP (2019-2024) – FINAL DRAFT AND BACKGROUND INFORMATION	
DEPARTMENT:	Public Works Department	
PRESENTED BY:	Weston Ott, P.E., Public Works Engineering Capital Projects Division Manager <i>W/O 4/25/18</i>	
<input type="checkbox"/> Public Hearing	<input type="checkbox"/> Study Session	<input type="checkbox"/> Recommendation
<input checked="" type="checkbox"/> Discussion	<input type="checkbox"/> Update	<input type="checkbox"/> Other

Background:

Chapter 35.77.010 RCW requires that the City annually update its Six Year Comprehensive Transportation Improvement Program (6-Year TIP) and file a copy with the Secretary of the Washington State Department of Transportation within 30 days of adoption.

The primary objective of the Program is to produce a comprehensive program for the orderly development and preservation of the City's street system. Only those projects identified in the adopted Program are eligible for state or federal grant funding.

Adoption of the Program does not irreversibly commit the City of Lakewood to construct identified projects. Projects in the early years of the Program have, a higher probability that they will be constructed as scheduled, at least those with significant grant funding therein versus projects in the later years, which are subjected to more flexibility and may be accelerated, delayed, or canceled as funding and conditions change. The usual reasons for canceling a project are that it is either environmentally unacceptable or contrary to the best interests of the community as a whole or its funding just didn't materialize. The Program may also be revised by a majority of the City Council at any time, but only after a public hearing.

Discussion:

City Council has directed that the Planning Commission to review and recommend Council adoption related to the 6-Year TIP. The Planning Commission reviewed background information and the first draft of the 6-Year TIP at their April 4, 2018 meeting.

During the May 2, 2018 Planning Commission meeting, Public Works Engineering staff will be on-hand to answer any questions regarding the Final Draft of the 6-Year TIP (2019-2024). The final draft addresses minor changes to project timelines and estimated costs as discussed on April 4, 2018. The review and adoption schedule is as follows:

Date	Description	Planning Commission 6:30 PM	Council Meeting 7:00 PM
4/4/2018	Planning Commission - Review background data and 1st draft of 2019-2024 TIP	X	
5/2/2018	Planning Commission - Review Final Draft of 2019-2024 TIP	X	
5/18/2018	Send Final Draft of 2019-2024 TIP for Review (City Council, PRAB, Other jurisdictions/agencies, Service Groups, Etc.)		
5/18/2018	Notice of Public Hearing (5/29/18)		X
5/29/2018	Review of 6-Year TIP Presented to Council - Study Session		X
6/18/2018	6-Year TIP - Public Hearing (by City Council)		X
7/2/2018	6-Year TIP - Adoption (by City Council)		X

Appendix A

Final Draft of 6-Year(2019-2024) Transportation Improvement Program for Review



CITY OF LAKEWOOD

SIX-YEAR COMPREHENSIVE TRANSPORTATION IMPROVEMENT PROGRAM 2019-2024 -- Draft: 5/2/2018 --

PREFACE

Chapters 35.77.010 of the Revised Code of Washington (RCW) provide that each city shall annually update its Six-Year Comprehensive Transportation Program (Program) and file a copy of the adopted Program with the Secretary of the Washington State Department of Transportation (WSDOT) by July 1 of each year. The Program is necessary to allow cities and counties to obtain State and Federal funding. For a project to obtain funding from the State, it must appear in the agency's current Program. Because the state also disperses federal highway funds, this requirement applies to federally funded projects as well.

RCW 35.77.010 also requires each city to specifically set forth those projects and programs of regional significance for inclusion in the transportation improvement program for that region.

The Program is based upon anticipated revenues versus desirable projects. There are always more projects than available revenues. Therefore, a primary objective of the Program is to integrate the two to produce a comprehensive, realistic program for the orderly development and preservation of our street system.

Several important points must be considered during the review of the proposed Program. The early years of the Program are fairly definite; that is, it can be assumed that those projects will be constructed as scheduled. Projects in the later years are more flexible and may be accelerated, delayed or canceled as funding and conditions change.

It is also important to note that the adoption of the Program does not irreversibly commit the City of Lakewood to construct the projects. A project may be canceled at any time during the course of study or design. The usual reasons for canceling a project are that it is environmentally unacceptable or contrary to the best interests of the community as a whole. The Program may at any time be revised by a majority of the City Council, but only after a public hearing.

CONSISTENCY WITH LAND USE MANAGEMENT PLAN

The State's Growth Management Act (GMA) requires local governments to develop and adopt comprehensive plans covering land use, housing, capital facilities, utilities, and transportation. These comprehensive plans must balance the demands of growth with the provision of public facilities and services and, in particular, transportation facilities and services. The City of Lakewood was required to develop and adopt a comprehensive plan that is in conformance with the requirements of the GMA.

The City of Lakewood has, as part of its Comprehensive Plan, a Transportation Element with a Master Goal to "Ensure that the transportation and circulation system is safe, efficient and serves all segments of the population and reduces reliance on single-occupant vehicles and increase use of other modes of transportation." 24 of 274

Specific goals include the following.

1. To provide a safe, comfortable and reliable transportation system.
2. To reduce consumption of energy through an efficient and convenient transportation system.
3. To enhance options for future improvements to the transportation system by taking advantage of advances in technology and transportation research.
4. To keep travel times for people and goods as low as possible.
5. To emphasize the movement of people and goods, rather than vehicles, in order to obtain the most efficient use of transportation facilities.
6. To establish a minimum level of adequacy for transportation facilities through the use of consistent and uniform standards.
7. To protect the capital investment in the transportation system through adequate maintenance and preservation of facilities.

The projects in the Six-Year Comprehensive Transportation Program are intended to conform to the goals within the City's Comprehensive Plan.

GRANT APPLICATIONS AND LEVERAGING LOCAL DOLLARS

The need to leverage local dollars through grant applications is very important to the City, especially in light of the decrease in funding available for transportation related capital improvements. The intent of this Program is not only to list and program projects for funding, but to establish City Council approval to submit grant applications on those projects contained in the Program.

FUNDING SOURCES

A. Motor Vehicle Fuel Tax Funds

The Motor Vehicle Fuel Tax Funds have been programmed to provide matching funds for federal aid and urban arterial projects and for projects to be implemented with Motor Vehicle Fuel Tax Funds only.

By law, each city receives a proportionate share of the total state motor vehicle fuel tax. Money received is a monthly allocation based on population. The dollars shown in this year's Program reflect the revenues from this source expected to be received by the City of Lakewood. It is anticipated that revenue received from gas tax for the Streets Capital Projects Fund will be: \$335,000 (FY 2015).

B. Federal Aid Funding Programs

Each of the Federal aid programs listed below has specific requirements a project must meet to qualify for funding under the individual program. For a project to receive funding from any of these sources it must compete with other public agency projects.

On December 4, 2015, President Obama signed the Fixing America's Surface Transportation Act (FAST ACT). The Act authorizes \$305 billion over fiscal years 2016 through 2020 for the Department's highway, highway and motor vehicle safety, public transportation, motor carrier safety, hazardous materials safety, rail, and research, technology and statistics programs. The ACT essentially continues on with a number of specific funding programs that were funded under the previous Federal Transportation program (MAP 21). These include the following:

1. STP Surface Transportation Program: This is a regionally competitive program.
2. CMAQ Congestion Mitigation and Air Quality: This is a regionally competitive program intended for projects that significantly improve air quality.
3. HSIP Highway Safety Improvement Program: Statewide competition for federal funds targeted at safety improvements at high accident locations.
4. TAP Transportation Alternatives Program: This is a regionally competitive program and focuses on pedestrian and bicycle facilities (on and off road); safe-routes to schools, etc.; and other non-highway focused programs.

Much of the above said Federal grant funds are funneled thru the regional MPOs which for Lakewood that's Puget Sound Regional Council (PSRC). Presently PSRC has call out for projects where in \$206,000,000 in grant funding is presently available throughout its 4 county region. Typically Lakewood projects are most competitive at County Wide level for which under this present call there is \$21,870,000.

C. Washington State Transportation Improvement Board (TIB)

The TIB has a number of statewide competitive programs which use criteria developed by the TIB for prioritization of projects. The tow TIB programs in which the City can compete are as follows:

1. UAP Urban Arterial Program. This program is for arterial street construction with primary emphasis on safety and mobility.
2. SP Sidewalk Program. This program is for the improvement of pedestrian safety, and to address pedestrian system continuity and connectivity.

D. Community Development Block Grants (CDBG)

This is a program to provide physical improvements within low-income census tracts or to promote economic development within the City. Through the years 2019-2024 it is anticipated that a minimum of \$250,000 (on average) per year will be made available for pavement preservation, street lighting, and pedestrian improvements in eligible neighborhoods.

E. City Funding Sources

1. Real Estate Excise Tax (REET). This funding source comes from the two ¼% REET's charged by the City on the sale of real estate within the City limits. The City's REET is designated entirely for transportation related capital improvements. Revenue from REET has averaged around \$1,100,000 in the past few years. The REET is estimated to be \$1,100,000 annually.
2. General Fund Transfer In. This funding source comes from several different sources that make up the General Fund revenue including: property tax, sales tax, and utility tax and fees. The Street Capital Projects Fund is budgeted to receive approximately \$500,000 annually (on average) over the next 5 years in support of the pavement preservation program.

3. Transportation Benefit District (TBD). In 2014, the TBD Board implemented a \$20 per vehicle tab fee to provide funds toward a specific list of pavement preservation projects to be implemented between 2015 through 2020. The anticipated revenue is approximately \$775,000 per year.

F. Washington State Department of Transportation

1. Pedestrian and Bicycle Program: This is a statewide competitive program specifically oriented toward the elimination of hazards to the pedestrian and bicyclists. The recent call for projects has expanded the program's scope to emphasize "complete streets" – accommodation of all roadway users from vehicles to bicyclists to pedestrians. The programs focus for "complete streets" is for "main street" urban arterials and corridors. Historically, the city has not received much funding from this program. However, given the change in the grant scope, there may be opportunities from this source in the future.
2. Safe Routes to Schools Program: This is a statewide competitive program specifically oriented toward pedestrian and bicycle safety near schools.
3. Surface Water Management Program:

The City's Surface Water Management (SWM) Program pays for all drainage facilities constructed in conjunction with street improvements. The revenue from SWM is directly related to the amount of capital improvement projects constructed. SWM participation in roadway projects averages about \$300,000 annually.

PROJECT NUMBERING SYSTEM

Project numbers were revised in 2018 to match the City's BARS numbering system to have a consistent numbering system through all documents to maintain consistency.

Most sections of the Program will have discontinuous project numbering, as projects are completed and removed from the list. Projects carried forward from previous year(s) retain the same project numbers from the previous year(s).

BUDGET DOLLARS

Costs shown are planning level estimates and are reflected in each year as FY2018 dollars with no accounting for inflation.

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN			NOTE: BOLD and ITALICIZED numbers denote grant is secured						TOTAL FUNDS
SECTION 1 NEW CONSTRUCTION ARTERIAL STREET PROJECTS			2019	2020	2021	2022	2023	2024	2019-2024
302.0040 North Gate Access Improvements Total Estimated Cost \$2,132 1.25	Improve access to Lewis North including: intersection improvements (Edgewood / North Gate Road); non-motorized improvements (Edgewood Dr. and North Gate Rd)	City Grant Other Total		289 1,932 0					289 1,932 2,221
									Grant GO Bond Proceeds
302.0053 123rd ST SW - Bridgeport Way to 47th Ave. Total Estimated Cost \$1,000 1.20	Realign 123rd ST SW as it enters Bridgeport. Design and initial construction to begin in 2018.	City Grant Other Total							0 500 167 667
									CDBG SWM
302.0073 150th Street Corridor Capacity Total Estimated Cost \$1,600 Notes: Assume multiple phases; multiple years	Provide capacity for Woodbrook Industrial development: widening of 150th Street; bike/pedestrian facilities; structural pavement section improvements	City Grant Other Total				80	80 1440		160 1,440 0
									SWM TIB
302.0085 Murray Road Corridor Capacity Total Estimated Cost \$330 Notes: Assume multiple phases; multiple years	Provide capacity for Woodbrook Industrial development: widening of Murray Road; bike/pedestrian facilities; structural pavement section improvements	City Grant Other Total							0 130 200 330
									SWM/Dev. Contr.
302.0090 96th Street - 2-way left turn lane Total Estimated Cost \$500	Widen 96th St. from 500' east of So. Tac. Wy to I-5 underpass to provide 2-way left turn lane. Does not include sidewalks or HMA overlay.	City Grant Other Total				100			100 0 400 500
									Dev. Contr.
302.0091 Gravelly Lake Drive @ I-5 Right Turn Lane Total Estimated Cost \$1,600	Widen GLD from Nyanza to I-5 SB on-ramp to provide dedicated right-turn lane. Traffic signal upgrades; bridge widening; r/w acquisition.	City Grant Other Total			50 200	350 1,000			400 1,200 1,600
									Grant
302.0095 Bridgeport Way - I-5 Ramp to Pacific Hwy Total Estimated Cost \$900 302.0038, 1.27	Turn lane extension to improve capacity and queuing capability. Road / shoulder widening; sidewalks; walls for widening.	City Grant Other Total			50 100	50 100	100 400 100		200 600 100 900
									Dev. Contr.
302.0096 Union Avenue - W. Thorne Ln. to Spruce St. Total Estimated Cost \$3,225 Notes: Limits revised to reflect recent improvements at Berkeley/Union.	Widen to add turn lane, shared bike/travel lane, sidewalks, street lighting. Intersection improvements.	City Grant Other Total				125 375 75	250 2,250 150		375 2,625 225 3,225
									SWM

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN								TOTAL FUNDS	
SECTION 1 NEW CONSTRUCTION ARTERIAL STREET PROJECTS			2019	2020	2021	2022	2023	2024	2019-2024
302.0107 Interstate 5 through Lakewood (WSDOT led project - coordination only) Total Estimated Cost \$332,477	Planning and design coordination only.	City	10	10	10				30
		Grant							0
		Other							0
		Total	10	10	10	0	0	0	30
302.0108 Gravelly to Thorne Connector (WSDOT led project - coordination only) Total Estimated Cost \$25,000	Two-way connector road between Tillicum and Gravelly Lake Drive. Signalization.	City		5	5	5			15
		Grant							0
		Other							0
		Total	0	5	5	5	0	0	15
302.01xx Grade separation 100th St. Total Estimated Cost \$100,000	Grade separation between the rail and road at 100th St. Comprehensive Plan chapter 6 goal T-13.8 and T-18.9	City							0
		Grant							0
		Other				2000	2000	96,000	100,000
		Total	0	0	0	2,000	2,000	96,000	100,000
TOTALS		City	10	304	115	2,585	2,180	96,000	1,194
		Grant	250	250	430	3,100	3,840	96,000	3,870
		Other	83	2,016	200	2,400	2,100	96,000	2,799
		Total	343	2,570	745	8,085	8,120	288,000	7,863

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN			NOTE: BOLD and ITALICIZED numbers denote grant is secured						
SECTION 2 ROADWAY IMPROVEMENTS			2019	2020	2021	2022	2023	2024	2019-2024
302.0024 Steilacoom Blvd -Farwest to Phillips <i>Joint project with Town of Steilacoom - Design only</i>	Curb, gutter, sidewalks, sharrows, turn lanes, street lighting, drainage, overlay.	City	100						100
		Grant							0
		Other							0
		Total	100	0	0	0	0	0	100
302.0056 Steilacoom Blvd - Weller to Phillips ROW Acquisition <i>ROW Acquisition - grant funded</i>	Curb, gutter, sidewalks, sharrows, turn lanes, street lighting, drainage, overlay.	City	97						97
		Grant	605						605
		Other							0
		Total	702	0	0	0	0	0	702
302.0056 Steilacoom Blvd - Farwest to Weller ROW Acquisition <i>ROW Acquisition - 2018 PSRC Grant Application</i>	Curb, gutter, sidewalks, sharrows, turn lanes, street lighting, drainage, overlay.	City		98					98
		Grant		617					617
		Other							0
		Total	0	715	0	0	0	0	715
302.0029 Washington Blvd - Edgewood Ave to Vernon Ave. Total Estimated Cost \$2,000 2.77	Curb, gutter, sidewalks, bike lanes, street lighting, drainage, overlay.	City		382					382
		Grant		903					903
		Other							0
		Total	0	1,285	0	0	0	0	1,285
302.0058 Washington Blvd - Vernon Ave. to Gravelly Lake Dr Total Estimated Cost \$4,525 2.77	Curb, gutter, sidewalks, bike lanes, street lighting, drainage, overlay.	City		325					325
		Grant		4,200					4,200
		Other							0
		Total	0	4,525	0	0	0	0	4,525
302.0030 Lake City Business District Sidewalks (American Lake Park to Veterans Dr / Alameda) Total Cost Interlaaken \$2,310	Curb, gutter, sidewalks, sharrows, street lighting, drainage, overlay.	City		230					230
		Grant		2,000					2,000
		Other		65					65
		Total	0	2,295	0	0	0	0	2,295
302.0044 Steilacoom Blvd - Weller Road to Phillips Rd. Ph.1 <i>SRTS: Design and right-of-way through project 302.0024</i> <i>Notes: split into two phases. Safe Routes to Schools Grant received. Project is on the north side of the roadway</i>	Curb, gutter, sidewalks, bikeway/buffer, street lighting, drainage on north side.	City	100						100
		Grant	552						552
		Other							0
		Total	652		0	0	0	0	652
302.0044 Steilacoom Blvd - Weller Road to Phillips Road Ph.2 <i>Design through project 302.0024</i> <i>Notes: Project is on the south side of the roadway</i>	Curb, gutter, sidewalks, bikeway, street lighting, drainage, overlay.	City		100					100
		Grant		1350					1,350
		Other		100					100
		Total	0	1,550	0	0	0	0	1,550
302.0055 Lakewood Drive - Steilacoom Blvd to N. City Limits	Add turn lanes, curb, gutter, sidewalk, bike way, street lighting, pavement rehab	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN			NOTE: BOLD and ITALICIZED numbers denote grant is secured						
SECTION 2 ROADWAY IMPROVEMENTS				2019	2020	2021	2022	2023	2024
									2019-2024
302.0065 Gravelly Lake Drive - Bridgeport to Steilacoom Road Diet <i>Design through 2018, construction 2019</i>	Reduce 4 travel lanes to 3. Curb, gutters, sidewalks, bike lanes, street lighting, drainage, overlay.	City	50						0
		Grant	450						0
		Other							0
		Total	500	0	0	0	0	0	0
302.0069 112th / 111th - Bridgeport to Kendrick Total Estimated Cost \$2,200 <i>302.0015 Design</i>	Curb, gutter, sidewalks, sharrows, street lighting, drainage, overlay.	City		165	165				330
		Grant		1,100	1,100				2,200
		Other							0
		Total	0	1,265	1,265	0	0	0	2,530
302.0071 Phillips Road West Side - Agate to Onyx Total Estimated Cost \$1,010	Curb, gutter, sidewalks, bike lanes, street lighting, drainage, overlay.	City	8	127					135
		Grant	112	753					865
		Other				0			0
		Total	120	880	0	0	0	0	1,000
302.0072 59th Ave SW Sidewalk - 100th to Bridgeport Wy SW	Sidewalk east side of roadway.	City	125						125
		Grant							0
		Other							0
		Total	125	0	0	0	0	0	125
302.0074 South Tacoma Way - 88th to North City Limits Total Estimated Cost \$3,700	Curb, gutter, sidewalks, bike lanes, street lighting, signal at 84th, drainage, overlay.	City		205	3,500				3,705
		Grant							0
		Other							0
		Total	0	205	3,500	0	0	0	3,705
302.0075 Interlaaken Drive SW / Mt. Tacoma Drive Non-Motorized Improvements - Short Lane to Whitman Avenue SW Total Cost Interlaaken \$3,505 -- SPLIT INTO TWO PROJECTS --	Provide curb and gutter, sidewalk and a shared travel/bike lane on one side of Interlaaken / Mt. Tacoma Dr.	City							0
		Grant				500	3,005		3,505
		Other					0	0	0
		Total	0	0	0	500	3,005	0	3,505
302.01XX Interlaaken Drive SW Non-Motorized Improvements - Short Lane to Holly Hedge Ln. SW Total Cost Interlaaken \$	Provide curb and gutter, sidewalk and a shared travel/bike lane on one side of Interlaaken Dr.	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0
302.01XX Mt. Tacoma Drive Non-Motorized Improvements - Holly Hedge Ln. SW to Whitman Ave. SW Total Cost Motor Ave. \$	Provide curb and gutter, sidewalk and a shared travel/bike lane on one side of Mt. Tacoma Dr.	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0

Grant
S.T.SWM
Grant

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN			NOTE: BOLD and ITALICIZED numbers denote grant is secured						
SECTION 2 ROADWAY IMPROVEMENTS			2019	2020	2021	2022	2023	2024	2019-2024
302.0081 Steilacoom Blvd - 83rd to Weller Road Total Estimated Cost \$3,215 Design through project 302.0024	Curb, gutter, sidewalks, street lighting, drainage, overlay.	City					200		200
		Grant					2,000		2,000
		Other					300		300
		Total	0	0	0	0	2,500	0	2,500
302.0083 Hipkins Rd. 104th to Steilacoom Blvd. Total Estimated Cost \$3,355 2.65	Curb, gutters, sidewalks, street lighting, drainage, overlay.	City							0
		Grant							0
		Other			655	2,700			3,355
		Total	0	0	655	2,700	0	0	3,355
302.0086 Steilacoom Blvd - 87th to 83rd Total Estimated Cost \$2,015 Design through project 302.0024	Curb, gutter, sidewalks, street lighting, drainage, overlay.	City				15	200		215
		Grant				200	1,400		1,600
		Other					200		200
		Total	0			215	1,800	0	2,015
302.0087 Veterans Drive SW-Gravelly Lake Dr. to Alameda	curb, gutter, sidewalks, sharrows street lighting, drainage, overlay	City						0	0
		Grant				925		0	925
		Other							0
		Total	0	0	0	925	0	0	925
302.0088 & 302.0089 Oakbrook Sidewalks & Street Lighting Onyx Dr W (97th to 87th); Onyx Dr E (Garnet to Phillips) (Total Cost \$3,720)	Curb, gutter, sidewalks, sharrows, turn lanes, street lighting, drainage, overlay.	City				1,850			1,850
		Grant							0
		Other							0
		Total	0	0		1,850	0	0	1,850
302.0088 & 302.0089 Oakbrook Sidewalks & Street Lighting Onyx Dr W (97th to 87th); Onyx Dr E (Garnet to Phillips) (Total Cost \$3,720)	Curb, gutter, sidewalks, sharrows, turn lanes, street lighting, drainage, overlay.	City				1,850			1,850
		Grant							0
		Other							0
		Total	0	0	0	1,850	0	0	1,850
302.0092 Steilacoom Blvd-Bridgeport Way to Gravelly Lk. Dr. Total Estimated Cost \$2,500 Note: Preliminary design completed via previous TIB grant 2.41 Changed end location from Fairlawn to GLD.	Curbs, gutters, sidewalks, on both sides. Overlay.	City			20	150			170
		Grant			130	2,000			2,130
		Other				200			200
		Total	0	0	150	2,350		0	2,500
302.0093 Gravelly Lake Dr. - Pacific Hwy to Nyanza (south)	Curb, gutter, sidewalk, bike way, street lighting, pavement rehab	City				50	75	250	375
		Grant				100	175	800	1,075
		Other							0
		Total	0	0		150	250	1,050	1,450

State
SWM

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN			NOTE: BOLD and ITALICIZED numbers denote grant is secured						
SECTION 2			2019	2020	2021	2022	2023	2024	TOTAL FUNDS
ROADWAY IMPROVEMENTS									2019-2024
302.0097 Lakewood Station - Non-Motorized Access Improvements Total Estimated Cost \$3,200	Curb, gutters, sidewalks, and street lighting improvements per Lakewood NMTP and Sound Transit Access Improvement Study.	City Grant Other Total			100 100 100	100 400 500	100 400 500	100 400 500	300 1,300 1,600
			0	0	200	1,000	1,000	1,000	3,200
302.00xx 100th Street <i>Bridgeport Way to 400 feet north of 100th Street</i>	Curb, gutter, sidewalks, sharrows, replace 100th/Lakewood signal, street lighting, drainage, overlay.	City Grant Other Total							0 0 0
			0	0	0	0	0	0	0
302.0057 Steilacoom Blvd. SW - Weller Rd. SW to 88th St. SW	curb, gutter, sidewalks, sharrows street lighting, drainage, overlay	City Grant Other Total		2,243					0 2,243 0
			0	2,243	0	0	0	0	2,243
302.01xx Military Rd. Sidewalk Improvements Military Rd. SW to 200' South of intersection Military/112th and Military Rd. SW to Farwest Dr. SW.	curb, gutter, sidewalks, sharrows street lighting, drainage, overlay. This connect Military Rd. to sidewalks constructed as part of development on	City Grant Other Total							0 0 0
			0	0	0	0	0	0	0
302.01xx farwest Dr. SW Sidewalks: Military Rd. SW to 200' South of intersection Military/Farwest	curb, gutter, sidewalks, sharrows street lighting, drainage, overlay. This connect Military Rd. to sidewalks constructed as part of development	City Grant Other Total							0 0 0
			0	0	0	0	0	0	0
302.01xx 112th Sidewalks: Gravelly Lk. Dr. SW to Bridgeport Way SW	curb, gutter, sidewalks, bike lanes, street lighting, drainage, overlay	City Grant Other Total							0 0 0
			0	0	0	0	0	0	0
302.01xx Davisson Rd. SW and Highland Ave SW: 112th St. SW to 108th St. SW	curb, gutter, sidewalks, sharrows, street lighting, drainage, overlay	City Grant Other Total							0 0 0
			0	0	0	0	0	0	0
302.01xx Custer Rd. SW: Bridgeport Way - Lakewood Dr. SW (East City Limits/74th St.)	curb, gutter, sidewalks, sharrows, street lighting, drainage, road reconstruction, utility relocation	City Grant Other Total							0 0 0
			0	0	0	0	0	0	0
302.01xx Round-a-Bout 87th Ave. SW, Dresden Ln., and Ft. Steilacoom Park Entrance and sidewalks 87th Ave. SW Dresden Ln. to Steilacoom Blvd.	round-a-bout, curb, gutter, sidewalks, sharrows, street lighting, drainage, road reconstruction, and signage	City Grant Other Total							0 0 0
			0	0	0	0	0	0	0

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Grant

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN			NOTE: BOLD and ITALICIZED numbers denote grant is secured						TOTAL FUNDS
SECTION 2 ROADWAY IMPROVEMENTS			2019	2020	2021	2022	2023	2024	2019-2024
302.01xx Lakewood Drive - Custer/74th to N. City Limits <i>Previously Steilacoom Blvd. to N. City Limits 2.84</i>		City							0
		Grant							0
		Other							0
		Total	0		0	0	0	0	0
302.01xx Lakewood Drive - Steilacoom Blvd. to Flett Creek Limits <i>Previously Steilacoom Blvd. to N. City Limits 2.84</i>	curb, gutter, sidewalks, street lighting, drainage, overlay	City							0
		Grant							0
		Other							0
		Total	0		0	0	0	0	0
302.01xx Tye Park School Sidewalks - Seminole Rd. SW	Intersection upgrades and sidewalks to school	City		100					100
		Grant		400					400
		Other							0
		Total	0	500	0	0	0	0	500
302.01xx 112th Sidewalks: farwest Dr. SW to Holden Rd. SW	curb, gutter, sidewalks, bike lanes, street lighting, drainage, overlay	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0
302.01xx 47th Ave. SW Sidewalks: 123rd St. SW to Pacific Hwy. SW	curb, gutter, sidewalks, bike lanes, street lighting, drainage, overlay	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0
302.0012 Steilacoom Blvd. Custer to 88th Street Total Estimated Cost \$1,975	Curbs, gutters, sidewalks, street lighting, on both sides, Signal modifications, Signal replacement, Custer/Ardmore, Overlay.	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0
302.0026 Roadway Safety Improvements at 40th Ave. SW and 96th St Under construction	Curb, gutter, sidewalk, sharrows, guard rail, street lighting, pavement reconstruction	City	0						0
		Grant	0						0
		Other	0						0
		Total	0	0	0	0	0	0	0
302.0043 Phillips Road West Side- Steilacoom Blvd. to Agate Total Estimated Cost \$550 SRTS	Curb, gutter, sidewalks, bike lanes, street lighting, drainage, overlay.	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0
302.0000 High Accident Location Safety Improvements <i>2016-2017 Funds reallocated to 2.81 Roadway Safety Improvements to 40th Ave. SW and 96th St. SW and 3.20 Military Rd. and 112th St. Safety Improvement.</i>	May include sight distance corrective measures, signal modifications, etc. at one of top 25 accident locations.	City	25	25	25	25	25	25	150
		Grant							0
		Other							0
		Total	25	25	25	25	25	25	150

FED

Grant

257 of 274
15

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 3 TRAFFIC SIGNALS			2019	2020	2021	2022	2023	2024	2019-2024
302.0059 Steilacoom / Durango New Traffic Signal 3.1	Intersection meets warrants for traffic signal. Signal needed with new development in area. Special concern with adjacent train crossing becoming active.	City							0
		Grant							0
		Other			642				642
		Total	0	0	642	0	0		642
302.0060 100th Street & Lakewood Drive SW Traffic Signal Replacement	Replace 100th/Lakewood signal, street lighting, drainage, overlay.	City	325						325
		Grant							0
		Other							0
		Total	325	0	0	0	0	0	325
302.0070 Washington Blvd. and Interlaaken Drive Signal and intersection improvement Total Estimated Cost \$375 3.7	Install new signal at intersection.	City							0
		Grant							0
		Other	642						642
		Total	642	0	0	0	0	0	642
302.0078 So. Tacoma Way / 92nd Street 3.14	New warranted signal, improvements include associated ADA upgrades and pavement patching.	City		65					65
		Grant		585					585
		Other							0
		Total	0	650	0	0	0	0	650
302.0082 City-Wide Traffic Signal Management System 3.11	City-hall based Traffic Management Center. Fiber optic interconnect. PTZ major corridors. Active traffic management including web based info.	City				30			30
		Grant				300			300
		Other							0
		Total	0	0	0	330	0	0	330
302.0094 Gravelly Lake Drive / Avondale Traffic Signal 3.13	Intersection meets warrants for traffic signal. Increased volumes in and around Towne Center. Increase in accidents.	City		125					125
		Grant		400					400
		Other							0
		Total	0	525	0	0	0	0	525

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PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 3 TRAFFIC SIGNALS			2019	2020	2021	2022	2023	2024	2019-2024
302.0098 84th St. Pedestrian Crossing Signal at Pine St 3.21	Install pedestrian signal, connection to Pine street intersects Tacoma's Water Ditch Trail.	City		50					50
		Grant		150					150
		Other							0
		Total	0	200	0	0	0	0	200
302.01xx Holden/Military Rd. New Traffic Signal	Intersection meets warrants for traffic signal. Increased volumes in and around Mann Middle School.	City		125					125
		Grant		400					400
		Other							0
		Total	0	525	0	0	0	0	525
302.01xx Lakewood Dr./Custer Rd./74th St. Traffic Signal Replacement	Replace existing traffic signal with pole and mast arm signal.	City			125				125
		Grant			400				400
		Other							0
		Total	0	0	525	0	0	0	525
302.01xx Lakewood Dr./75th St. New Traffic Signal	Intersection meets warrants for new traffic signal.	City				125			125
		Grant				400			400
		Other							0
		Total	0	0	0	525	0	0	525
302.01xx Custer Rd. and 88th Traffic Signal Replacement	Replace existing traffic signal with pole and mast arm signal.	City					325		325
		Grant							0
		Other							0
		Total	0	0	0	0	325	0	325
302.01xx Steilacoom Blvd./Briggs Traffic Signal Replacement Associated with 302.0081	Replace existing traffic signal with pole and mast arm signal.	City						325	325
		Grant							0
		Other							0
		Total	0	0	0	0	0	325	325

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 3 TRAFFIC SIGNALS			2019	2020	2021	2022	2023	2024	2019-2024
302.01xx Steilacoom Blvd./83rd Traffic Signal Replacement <i>Associated with 302.0083</i>	Replace existing traffic signal with pole and mast arm signal.	City						325	325
		Grant							0
		Other							0
		Total	0	0	0	0	0	325	325
302.01xx Washington Blvd./Edgewood Ave. Traffic Signal Replacement <i>Associated with 302.0040</i>	Replace existing traffic signal with pole and mast arm signal.	City						325	325
		Grant							0
		Other							0
		Total	0	0	0	0	0	325	325
302.0012 Steilacoom Blvd / Western State Hospital Signal Replacement	Replace existing signal	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0
302.0012 Steilacoom Blvd / Lakeview Ave Signal Replacement	Replace existing signal	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0
302.0025 Military Rd. and 112th St. Safety Improvement	Replace existing traffic signal to current standards. Update phasing to yellow flashing arrow operation. ADA ramp upgrades. Repave intersection	City							0
		Grant	0						0
		Other							0
		Total	0	0	0	0	0	0	0
302.0000 Traffic Signal Timing Upgrades (on-going technical support) <i>incl. turning movement counts</i>	Upgrade traffic signal timing and coordination.	City	10	10	10	10	10	10	60
		Grant							0
		Other							0
		Total	10	10	10	10	10	10	60
302.0000 Traffic Signal Asset Management System	Purchase software; develop asset management system	City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0
TOTALS		City	325	365	125	155	325	975	2,270
		Grant	0	1,535	400	700	0	0	2,635
		Other	642	0	642	0	0	0	1,284
		Total	967	1,900	1,167	855	325	975	6,189

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 4 TRANSPORTATION PLANNING			2019	2020	2021	2022	2023	2024	2019-2024
101.0000 Pavement 4.1	Semi-Annual evaluation of pavement condition	City	30	5	30	5	30	5	105
		Grant							0
		Other							0
		Total	30	5	30	5	30	5	105
302.0000 Lakewood City Center Sub-Area Plan 4.8	Review access and circulation for vehicles, transit, and non-motorized transportation.	City	5	5	5	5	5	5	30
		Grant							0
		Other							0
		Total	5	5	5	5	5	5	30
302.0000 Non-Motorized Transportation Plan Update 4.9	Update NMTP to include relevant policy updates and capital improvement projects. (original plan adopted June 2009)	City	50	10	10	10	10	50	140
		Grant							0
		Other							0
		Total	50	10	10	10	10	50	140
302.0000 ADA Transition Plan Update 4.10	Update ADA transition plan to address ADA deficiencies of existing curb ramps; signal access / operations; etc.	City	5	5	5	5	5	5	30
		Grant							0
		Other							0
		Total	5	5	5	5	5	5	30
302.0000 Transportation Model	On-going updates of travel demand model.	City	10	10	10	10	10	10	60
		Grant							0
		Other							0
		Total	10	10	10	10	10	10	60
TOTALS		City	100	35	60	35	60	75	365
		Grant	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Total	100	35	60	35	60	75	365

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PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 5 BIKEWAYS AND TRAILS			2019	2020	2021	2022	2023	2024	2019-2024
302.0063 Colonial Plaza <i>Motor Avenue - Whitman to Gravelly Lake Dr.</i> 5.7	Provide non-motorized path including lighting and landscaping. Colonial Center Plaza and Revitalization	City	1,500						1,500
		Grant							0
		Other	500						500
		Total	2,000	0	0	0	0	0	2,000
302.0076 Gravelly Lake Non-Motorized Trail - Phase 2 (Nyanza Blvd) <i>Notes: Project split into three phases. Grant received for 302.0039</i> 5.6B	Provide non-motorized path around Gravelly Lake along Gravelly Lake Drive and Nyanza Drive. Existing roadway cross section shifted to outside and overlaid. Lighting.	City	45	455					500
		Grant	255	3,775					4,030
		Other							0
		Total	300	4,230	0	0	0	0	4,530
302.0077 Gravelly Lake Non-Motorized Trail - Phase 3 (GLD - Nyanza (S) to Wash.) Construction 2022+. <i>TOTAL Cost \$3.8 Mill. - 5.6C</i> <i>Notes: Project split into three phases. Grant received for 302.0039</i>	Provide non-motorized path around Gravelly Lake along Gravelly Lake Drive and Nyanza Drive. Existing roadway cross section shifted to outside and overlaid. Lighting.	City	45	335					380
		Grant	255	3,545					3,800
		Other							0
		Total	300	3,880	0	0	0	0	4,180
302.0108 North Thorne Lane to Gravelly Lake Drive (WSDOT led project - coordination only) <i>Total Estimated Cost \$25,000</i> 5.5	Provide non-motorized path between Tillicum and Gravelly Lake Drive "Gravelly to Thorne Connector" construction.	City	5	5	5	5			20
		Grant							0
		Other							0
		Total	5	5	5	5	0	0	20
302.0039 Gravelly Lake Non-Motorized Trail - Phase 1 (Washington Blvd to Nyanza (N)). <i>Amendment 1: Project split into three phases. Grant received for 5.6A. - WILL BE BUILT IN 2018</i>	Provide non-motorized path around Gravelly Lake along Gravelly Lake Drive and Nyanza Drive. Existing roadway cross section shifted to outside and overlaid. Lighting.	City	0						0
		Grant	0						0
		Other							0
		Total	0	0	0	0	0	0	0
302.00xx Miscellaneous Bikeway Markings / Signage		City	20	20	20	20	20	20	120
		Grant							0
		Other							0
		Total	20	20	20	20	20	20	120

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 5 BIKEWAYS AND TRAILS			2019	2020	2021	2022	2023	2024	2019-2024
302.00xx Miscellaneous Bike Lane Construction		City		50		50		50	150
		Grant							0
		Other							0
		Total	0	50	0	50	0	50	150
		City	1,615	865	25	75	20	70	2,670
		Grant	510	7,320	0	0	0	0	7,830
		Other	500	0	0	0	0	0	500
		Total	2,625	8,185	25	75	20	70	11,000

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 6 STREET LIGHTING			2019	2020	2021	2022	2023	2024	2019-2024
302.0002 New Street Lighting 6.2	Install street lighting in requested areas based on ranking criteria.	City	170	175	175	175	180	180	1,055
		Grant							0
		Other							0
		Total	170	175	175	175	180	180	1,055
TOTALS		City	170	175	175	175	180	180	1,055
		Grant	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Total	170	175	175	175	180	180	1,055

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 7 BRIDGES			2019	2020	2021	2022	2023	2024	2019- 2024
101.0000 Bridge Inspection 7.1	On-going biennial bridge inspection.	City	5	5	5	5	5	5	30
		Grant							0
		Other							0
		Total	5	5	5	5	5	5	30
302.01xx Structural guardrail replacement Clover Ck. Gravelly Lake Drive: 112th to Nyanza		City	10	50					60
		Grant							0
		Other							0
		Total	10	50	0	0	0	0	60
TOTALS		City	15	55	5	5	5	5	90
		Grant	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Total	15	55	5	5	5	5	90

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 8 BEAUTIFICATION PROJECTS			2019	2020	2021	2022	2023	2024	2019-2024
302.0000 Gateway Improvements 8.10		City	50	50	50	50	50	50	300
		Grant							0
		Other							0
		Total	50	50	50	50	50	50	300
TOTALS		City	50	50	50	50	50	50	300
		Grant	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Total	50	50	50	50	50	50	300

General

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN			NOTE: BOLD and ITALICIZED numbers denote grant is secured						
SECTION 9 ROADWAY RESTORATION PROJECTS				2019	2020	2021	2022	2023	2024
									2019-2024
302.0004 Minor Capital Improvements Total Estimated Cost \$250	Roadway patching and repair, sidewalk, signage, markings, and striping.	City	250	250	260	260	270	270	1,560
		Grant							0
		Other							0
		Total	250	250	260	260	270	270	1,560
302.0005 Chip Seal Resurfacing Program 9.7	Projects in various locations may include pavement preservation contribution to planned utility projects to facilitate full roadway overlays.	City	360	360	380	380	390	390	2,260
		Grant							0
		Other							0
		Total	360	360	380	380	390	390	2,260
302.0052 Steilacoom Boulevard - Weller Road to Custer Rd Note: Overlay from 88th St to Custer included in project 2.29		City							0
		Grant	1,295	1,020					2,315
		Other							0
		Total	1,295	1,020	0	0	0	0	2,315
302.0066 Custer - Steilacoom to John Dower 9.18		City				0			0
		Grant	540						540
		Other							0
		Total	540		0	0	0	0	540
302.0067 100th - Lakeview to South Tacoma Way 9.21		City	200						200
		Grant	329						329
		Other							0
		Total	529	0	0	0	0	0	529
302.0068 Pacific Hwy - 108th to SR512 9.20		City	165		0				165
		Grant	430		0				430
		Other							0
		Total	595	0	0	0	0	0	595
302.0079 100th - 59th to Lakeview 9.22		City					0		0
		Grant			1,320				1,320
		Other							0
		Total	0	0	1,320	0	0	0	1,320
302.0080 108th - Bridgeport Way to Pacific Hwy 9.17		City			661				661
		Grant							0
		Other							0
		Total	0		661	0	0	0	661

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN			NOTE: BOLD and ITALICIZED numbers denote grant is secured						TOTAL FUNDS
SECTION 9 ROADWAY RESTORATION PROJECTS			2019	2020	2021	2022	2023	2024	2019-2024
302.0101 & 302.0102 88th - Steilacoom to Custer (Design \$ amount NOT included)		City		0	275				275
		Grant							0
		Other							0
		Total	0	0	275	0	0	0	275
302.0000 Steilacoom Boulevard - 87th to Weller Road		City			20	350			370
		Grant				750			750
		Other							0
		Total	0	0	20	1,100	0	0	1,120
302.01xx Custer Rd.: John Dower to 500' west of Bridgeport Way	Roadway Overlay, signage, and striping.	City				0			0
		Grant		500					500
		Other							0
		Total	0	500	0	0	0	0	500
302.0051 Lakewood Drive - Steilacoom Blvd. to N. City Limits <i>Construct Overlay: Flett Ck. To Custer/74th</i>		City							0
		Grant							0
		Other							0
		Total	0		0	0	0	0	0
302.0049 59th Ave - Main Street to 100th Street		City							0
		Grant							0
		Other							0
		Total	0		0	0	0	0	0
TOTALS		City							0
		Grant							0
		Other							0
		Total	0	0	0	0	0	0	0

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 10 NEIGHBORHOOD TRAFFIC MANAGEMENT			2019	2020	2021	2022	2023	2024	2019-2024
302.0003 Neighborhood Traffic Safety Traffic Calming Various Locations 10.1	May include speed humps, traffic circles, signage, radar feedback signs, etc.	City	25	25	27	27	30	30	164
		Grant							
		Other							
		Total	25	25	27	27	30	30	164
TOTALS		City	25	25	27	27	30	30	164
		Grant	0	0	0	0	0	0	0
		Other	0	0	0	0	0	0	0
		Total	25	25	27	27	30	30	164

PROJECT COSTS IN THOUSANDS OF DOLLARS									
EXPENDITURE PLAN									TOTAL FUNDS
SECTION 11 OTHER			2019	2020	2021	2022	2023	2024	2019-2024
302.0001 On-call technical assistance 11.1	Various professional services including traffic counts, surveying, structural, geotechnical, environmental to support various projects.	City	50	50	50	50	50	50	300
		Grant							0
		Other							0
		Total	50	50	50	50	50	50	300
TOTALS		City	50	50	50	50	50	50	300
		Grant							0
		Other							0
		Total	50	50	50	50	50	50	300

Notes:

LID = Property owner participation through a Local Improvement District (LID).

Dev. Contr. = Funds provided through private (developer) contribution

TIB = Transportation Improvement Board grant funding

TEA-21 = Transportation Efficiency Act grant funds.

State = other state grant funding programs

CDBG = Community Development Block Grant funds.

FED = Federal Grant dollars (TEA-21, FAST, SAFETEA, Enhancement, etc.)

SWM = Surface Water Management funds

S.T. = Sound Transit

TBD = Transportation Benefit District

MAP-21 = Moving Ahead for Progress in the 21st Century (Federal Transportation Act)

Appendix B

Draft 2017 Accident Data

City of Lakewood, Washington
Top 20 Collision Locations
 Collisions Per Million Entering Vehicles (MEV)
 2017

#	Intersection	Most Severe Collision Type					Total	ADT St 1	ADT St 2	Total Entering Vehicles (day ave)	Total Entering Vehicles (yrly ave)	Accidents & MEV
		Fatal	Serious Injury	Evident Injury	Possible Injury	Unknown or No Injury						
1	Mt Tacoma Dr & Meadow Rd				2	4	6	3300	2200	5500	2007500	2.989
2	Portland Ave & W Thorne Ln*				1	1	2	1,315	1000	2315	844975	2.367
3	Gravelly Lake Dr & 59th Ave		1		1	5	7	11506	3700	15206	5550190	1.261
4	Lakeview Dr & 108th St				1	6	7	5026	11035	16061	5862265	1.194
5	Gravelly Lake Dr & Veterans Dr				2	7	9	13300	7400	20700	7555500	1.191
6	Steilacoom Blvd & Lakeview Dr			2	4	4	10	20400	6100	26500	9672500	1.034
7	Bridgeport Way & Pacific Hwy			1	3	9	13	23600	12400	36000	13140000	0.989
8	Veterans Dr & Vernon Ave				1	2	3	6040	2919	8959	3270035	0.917
9	South Tacoma Way & 88th St				4	5	9	25300	1800	27100	9891500	0.91
10	84th st & 33rd Ave			1	2	3	6	15860	2500	18360	6701400	0.895
11	Washington Blvd & Interlaaken Dr				1	5	6	18025	2006	20031	7311315	0.821
12	Gravelly Lake Dr & Washington Blvd				3	7	10	14707	21270	35977	13131605	0.762
13	Bridgeport Way & Custer Rd			1	5	8	14	21000	29900	50900	18578500	0.754
14	Gravelly Lake Dr & Steilacoom Blvd				2	5	7	11800	14100	25900	9453500	0.74
15	South Tacoma Way & 100th St				1	9	10	22900	14100	37000	13505000	0.74
16	Pacific Hwy & New York Ave					2	2	7054	1000	8054	2939710	0.68
17	South Tacoma Way & Steilacoom Blvd	1	1		4	9	15	35800	26100	61900	22593500	0.664
18	Washington Blvd & Vernon Ave				3	2	5	16482	4334	20816	7597840	0.658
19	Bridgeport Way & Lakewood Dr & Lakewood Towne Center		1		3	5	9	25861	11867	37728	13770720	0.654
20	112th St & 34th Ave				1	3	4	15348	1500	16848	6149520	0.65

Intersections only, roadway sections not included.

* Data prior to gate relocation. New I-5 interchanges will affect volumes.

Note: Complete 2017 collision data was received later than normal, therefore these results have not been inspected. Culmination 3 and 5 year reports are not available to date.

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J. Howe
4/25/2018

City of Lakewood, Washington
Fatal Collision History
 2005 thru 2017

Year	Fatalities	Intersection	Description	Report	Date
2017	1	108th St & Douglas Dr	Street Light Pole or Base, Exceeding Reasonable Safe Speed	E671874	5/17/2017
	1	7800 Block Grange St	Vehicle Turning Right Hits Pedestrian, Inattention	E720304	10/7/2017
2016	1	S. Tacoma Way & Stellacom Blvd	Exceeding Reasonable Safe Speed, Disregard Stop and Go Light	E696573	7/29/2017
	1	3200 Block 92nd St S	Vehicle going straight hits pedestrian	E586714 162560031 E531233 16-001612	9/12/2016 4/1/2016
2015	1	Gravelly Lake Dr & Avondale Rd	Enter at angle	E499569	12/28/2015
	1	Washington Blvd & Vernon Ave	Vehicle turning left hits pedestrian	E480333	11/9/2015
	1	Gravelly Lake Dr & Veterans Dr	From same direction - both going straight - one stopped - rear end	E453038	8/17/2015
2014	1	Dekoven Dr se/o Blossom Ln	Utility Pole	E408304	3/5/2015
	1	Stellacom Blvd w/o Angle Ln	Retaining Wall (concrete, rock, brick, etc.)	E322254	4/20/2014
2013	1	40th Ave (west of 38th Ave Ct)	Vehicle going straight hits pedestrian	E228121	02/19/13
2012	1	108th St & 47th Ave	Vehicle going straight hits pedestrian	E197496	10/08/12
2011	0	Washington Blvd (West of Gravelly Lake Dr)	Tree or Stump (stationary)	-	-
2010	1	7800 Block Custer Rd (Ref Bridgeport Way)	Passenger Car, Boulder, Exceeding Reasonable Safe Speed, From West to East	E080019	8/27/2010
	1	Stellacom Blvd & Woodbourne Rd	Passenger Car, Vehicle Going Straight Hits Pedestrian, From West To East	E044968	3/7/2010
2009	0	-	-	-	-
2008	2	11600 Block Gravelly Lake Dr (Ref Tower Rd)	From opposite direction - all others	3010852	4/20/2008
	1	12500 Block Bridgeport Way (Ref San Francisco Ave)	From opposite direction - both moving - head on, Over center line	2922654	4/4/2008
2007	1	14500 Block Murray Rd (Ref 146th St)	Tree or stump (stationary), Exceeding reasonable safe speed	2437940	1/29/2008
	0	-	-	-	2007
2006	1	12300 Block Bridgeport Way (Ref Seattle Ave)	Vehicle going straight hits pedestrian, Fail to yield ROW to pedestrian, Inattention	2438233	11/15/2006
	1	Washington Blvd & Lake City Blvd	Entering at angle, Disregard stop sign - Flashing red, None	2079425	4/30/2006
2005	1	6000 Block 99th St (Ref 54th Ave)	One parked - one moving, Under influence of alcohol	909835	2/23/2005
	1	9600 Block Stellacom Blvd (Ref Farwest Dr)	Retaining Wall (concrete, rock, brick, etc.), Other	2079051	4/8/2005

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