



LAKEWOOD CITY COUNCIL AGENDA

Monday, April 19, 2021

7:00 P.M.

City of Lakewood

The State Legislature passed a Concurrent Resolution modifying the Open Public Meetings Act (OPMA) during the COVID-19 pandemic until the end of the emergency. As a result, City Hall Council Chambers will **NOT** be open for City Council meetings.

Residents can virtually attend City Council meetings by watching them live on the city's YouTube channel: <https://www.youtube.com/user/cityoflakewoodwa>

Those who do not have access to YouTube can participate via Zoom by either visiting <https://us02web.zoom.us/j/86872632373> or calling by telephone: Dial +1(253) 215- 8782 and enter participant ID: 868 7263 2373.

To participate in Public Comment and/or Public Testimony:

Public Comments and Public Testimony on Public Hearings will be accepted by mail, email or by live virtual comment. Send comments by mail or email to Briana Schumacher, City Clerk at 6000 Main Street SW Lakewood, WA 98499 or bschumacher@cityoflakewood.us. Comments received up to one hour before the meeting will be provided to the City Council electronically.

Virtual Comments: If you would like to provide live Public Comments or Public Testimony during the meeting, you will need to join the Zoom meeting as an attendee by calling by telephone Dial +1(253) 215- 8782 and enter participant ID: 868 7263 2373 or visiting <https://us02web.zoom.us/j/86872632373>.

By Phone: For those participating by calling in by telephone (+1(253) 215- 8782 and enter participant ID: 868 7263 2373), the Mayor will call on you during the Public Comment and/or Public Hearings portions of the agenda. When you are unmuted please provide your name and city of residence. Each speaker will be allowed (3) three minutes to speak during the Public Comment and at each Public Hearing.

By ZOOM: For those using the ZOOM link (<https://us02web.zoom.us/j/86872632373>), upon entering the meeting, please enter your name or other chosen identifier. Use the "Raise Hand" feature to be called upon by the Mayor during the Public Comments and/or Public Hearings portions of the agenda. When you are unmuted please provide your name and city of residence. Each speaker will be allowed (3) three minutes to speak during the Public Comment and at each Public Hearing.

Outside of Public Comments and Public Hearings, all attendees on ZOOM will continue to have the ability to virtually raise your hand for the duration of the meeting. You will not be acknowledged and your microphone will remain muted except for when you are called upon.

Page No.

CALL TO ORDER

ROLL CALL

PLEDGE OF ALLEGIANCE

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

<http://www.cityoflakewood.us>

PROCLAMATIONS AND PRESENTATIONS

- (5) 1. Recognition of Boys and Girls Clubs of South Puget Sound Youth of the Year Azalia Riviere.
- (6) 2. Proclamation declaring April 24, 2021 as Parks Appreciation Day.
– *Mr. Jason Gerwen, Parks and Recreation Advisory Board*
- (7) 3. Proclamation recognizing JP Bega, Cody Orcutt, Mokia Tennant and Alex Thomason for attaining the rank of Eagle Scout.
- 4. Youth Council Report.

PUBLIC COMMENTS**C O N S E N T A G E N D A**

- (8) A. Approval of the minutes of the City Council study session of March 22, 2021.
- (11) B. Approval of the minutes of the City Council retreat of March 27, 2021.
- (13) C. Motion No. 2021-23

Authorizing the execution of a cooperative use agreement with the Clover Park School District.
- (20) D. Motion No. 2021-24

Authorizing the execution of a purchase and sale agreement for parcel 6785000211 located at 9203 Veterans Drive SW for future parking for American Lake Park.
- (22) E. Items filed in the Office of the City Clerk:
 - 1. Lakewood's Promise Advisory Board meeting minutes of March 4, 2021.
 - 2. Planning Commission meeting minutes of March 24, 2021.

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

R E G U L A R A G E N D A**PUBLIC HEARINGS AND APPEALS**

- (27) This is the date set for a public hearing on the FY2021 Community Development Block Grant (CDBG) Annual Action Plan.

ORDINANCE

- (32) Ordinance No. 751

Adopting the Lakewood Station District Subarea Plan as an element of the Comprehensive Plan, amending the Lakewood Municipal Code by establishing a new Title 18C Lakewood Station District Development Code and amending Lakewood Municipal Code Chapters 18A.10 Basic Provisions and 18A.20 Administration.

- (157) Ordinance No. 752

Adopting a SEPA Planned Action related to the Lakewood Station District Subarea.

RESOLUTION

- (261) Resolution No. 2021-04

Expressing a commitment to valuing diversity, equity and inclusion.
– *continued from March 15, 2021*

- (263) Resolution No. 2021-05

Adopting a statement on Equity.

- (266) Resolution No. 2021-06

Adopting the Region 5 All Hazard Mitigation Plan (2020-2025) Edition and the 2021 Addendum to the Region 5 Hazard Mitigation Plan.

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

UNFINISHED BUSINESS

NEW BUSINESS

- (477) Review of Community Development Block Grant (CDBG) CARES Act CDBG-CV2 Funding.

REPORTS BY THE CITY MANAGER

- (488) Review of 2021 SSMCP Work Plan.
- (490) Review of Transportation Benefit District (TBD) Annual Report.

CITY COUNCIL COMMENTS

ADJOURNMENT

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

Certificate of Recognition

Awarded to

LAKEWOOD BRANCH BOYS & GIRLS CLUB MEMBER

AZALIA RIVIERE

2021 BOYS AND GIRLS CLUBS OF SOUTH PUGET SOUND YOUTH OF THE YEAR

PRESENTED ON THIS 19TH DAY OF APRIL, 2021



BOYS & GIRLS CLUBS
OF SOUTH PUGET SOUND



Don Anderson
Mayor, City of Lakewood



CITY OF LAKEWOOD



PROCLAMATION

WHEREAS, parks, playgrounds, nature trails, open spaces, community and cultural centers, and historic sites make a community attractive and a desirable place to live, work, play and visit; and

WHEREAS, parks are a place where people can reflect, re-energize or socialize; a place where everyone is welcome regardless of age, race, beliefs or social and economic status; and a place where a sense of community is created; and

WHEREAS, use of Lakewood's parks significantly increased during the last year when our city, our nation and the world were affected by the coronavirus pandemic, and

WHEREAS, local businesses and areas of the community may have been closed but the parks were open for business and people flocked there to escape the fear and isolation of the disease, to boost their mental health, to add a recess period to their virtual school day or for a variety of other reasons to stay and play close to home; and

WHEREAS, the City of Lakewood Parks, Recreation and Community Services Department encourages citizens to celebrate the value and enhanced quality of life that parks bring to our communities; and

WHEREAS, park supporters of all ages will volunteer their time to clean-up and beautify Fort Steilacoom Park, Springbrook Park, the Lakewood Community Garden and other specific beautification projects throughout Lakewood on Saturday, April 24, 2021.

NOW, THEREFORE, the Lakewood City Council do hereby proclaim April 24, 2021 as

PARKS APPRECIATION DAY

in the City of Lakewood and urges all citizens to show support by masking up and safely visiting, beautifying and protecting our parks.

PROCLAIMED this 19th day of April, 2021.

Don Anderson, Mayor

CITY OF LAKEWOOD



PROCLAMATION

WHEREAS, the City of Lakewood takes pride in the collaborative work of our community and the volunteer efforts of our citizens; and

WHEREAS, the vision of the Boy Scouts of America is to prepare every eligible youth in America to become responsible, participating citizens and leaders, who are guided by the Scout Oath and Law; and

WHEREAS, the Boy Scouts of America encourages Eagle Scout candidates to complete worthy projects to improve their neighborhoods, community and the region; and

WHEREAS, an Eagle Scout devotes endless hours fulfilling many requirements for the advancement in scouting, including attaining 21 merit badges; holding prerequisite ranks and offices in advance of coordinating their eagle scout project and completing an examination by the Eagle Scout Board of Review; and

WHEREAS, Eagle Scout is the highest award a scout can receive; few move through the ranks of scouting. In 2019 only 8% of all scouts make it all the way to Eagle and even more rare is a Quartermaster project, where nationally only 35 accomplish this in conjunction with the Sea Scout program; and

WHEREAS, for his Eagle Scout community service project JP Bega from Troop #436 directed volunteers for 365 hours in 2016 to create community garden planting beds and then returned in 2019 and created garden benches in the Springbrook neighborhood to complete his Quartermaster Project for the Sea Scout program; and

WHEREAS, for his community service project, Cody Orcutt from Troop #436 directed volunteers in the creation of a community library and food pantry kiosk in the Springbrook neighborhood; and

WHEREAS, for his community service project, Mokoia Tennant from Troop # 436 directed volunteers for 123 hours working on the creation of a Western State Hospital Historic Cemetery Monument Path; and

WHEREAS, for his community service project, Alex Thomason from Troop # 436 directed volunteers for 113 hours working on a Western State Hospital Historic Cemetery Sign Replacement Project; and

NOW, THEREFORE, the Lakewood City Council do hereby recognize JP Bega, Cody Orcutt, Mokoia Tennant and Alex Thomason on their personal achievements of attaining the rank of Eagle Scout, the highest rank in the Boy Scouts of America.

PROCLAIMED this 19th day of April, 2021.

Don Anderson, Mayor



LAKEWOOD CITY COUNCIL STUDY SESSION MINUTES

Monday, March 22, 2021

City of Lakewood

<https://www.youtube.com/user/cityoflakewoodwa>

Telephone via Zoom: +1(253) 215- 8782

Participant ID: 868 7263 2373

CALL TO ORDER

Mayor Anderson called the meeting to order at 7:00 p.m.

ROLL CALL

Councilmembers Present: 7 – Mayor Don Anderson, Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, Patti Belle, Paul Bocchi and Linda Farmer.

ITEMS FOR DISCUSSION:

Contaminated Properties Update.

Economic Development Manager Becky Newton shared that the State of Washington has over 13,000 sites on the Department of Ecology Confirmed and Suspected Contaminated Sites list of which 36 are in Lakewood. She shared that there are three types of contaminated sites in Lakewood which are Superfund, Brownfields and Underground Storage Tank (UST).

She reviewed the status and location of contaminated sites in Lakewood and she highlighted a list of the 18 sites that have been cleaned up and the date that no further action was required for the site. She then highlighted the cleanup process, funding options through grant programs and the city's obligations for monitoring and regulating sites for redevelopment activities. She shared that next steps include continuing to evaluate the sites, obtaining site access authorizations and prioritizing sites that are in target areas for redevelopment. Discussion ensued.

Review FY 2021 Annual Action Plan.

Program Manager Jeff Gumm provided an update on housing related assistance programs and funding that is available to Lakewood residents. He noted that the American Rescue Plan Act provides additional HOME funding allocations in the amount of \$1.2 Million with a four-year expenditure timeline.

He highlighted the FY 2021 Annual Action Plan which covers the period of July 1, 2021 through June 30, 2022. He shared that a public hearing was held on February 10th, where six individuals testified related to permanent and affordable housing

within the city and the proposed Annual Action Plan was also presented to the Community Services Advisory Board on February 17th. He reviewed funding allocations for Community Development Block Grant (CDBG) which totals \$573,352 and for the HOME Program which totals \$321,730 and priorities, proposed funding recommendations and activities.

He shared that next steps include a thirty-day public comment period from April 1, 2021 through April 30, 2021 followed by a City Council public hearing scheduled for April 19th followed by adoption of the FY2021 Annual Action Plan on May 3, 2021. Discussion ensued.

Review of Chapter 9.06 of the Lakewood Municipal Code related to Controlled Substances.

City Attorney Heidi Wachter, who was joined by Police Chief Zaro, shared that the Washington State Supreme Court issued a ruling rendering the state's current drug possession law unconstitutional. She shared that arrests for drug possession are not being made because there is no law in effect upon which to base such arrest. Discussion ensued and it was recommended that the Ordinance come forward for City Council review at the April 5, 2021 City Council regular meeting.

ITEMS TENTATIVELY SCHEDULED FOR THE APRIL 5, 2021 REGULAR CITY COUNCIL MEETING:

1. Proclamation recognizing Town of Steilacoom Mayor Ron Lucas.
2. Presentation of the City of Lakewood 25th Anniversary Art Contest.
3. Youth Council Report.
4. Clover Park School District Report.
5. Authorizing the execution of an agreement with Hemisphere Design, in the amount of \$141,500, for the Build Your Better Here campaign. – (Motion – Consent Agenda)
6. Appointing Linn Larsen to serve on the Planning Commission through December 15, 2021. – (Motion – Consent Agenda)
7. Public Hearing regarding the Lakewood Station District Subarea Plan. – (Public Hearing and Appeals – Regular Agenda)
8. Amending Lakewood Municipal Code Chapter 9.06 related to Controlled Substances. – (Ordinance – Regular Agenda)
9. Review joint facility use agreement with the Clover Park School District. – (Reports by the City Manager)

REPORTS BY THE CITY MANAGER**Review of March 27, 2021 City Council Retreat Agenda.**

City Manager Caulfield provided an overview of the draft agenda for the City Council Retreat scheduled for Saturday, March 27th starting at 8:30 a.m. in the Council Chambers. Discussion ensued related to adding a diversity, equity and inclusion discussion item specific to violence against the Asian American community.

CITY COUNCIL COMMENTS

Councilmember Brandstetter shared that he testified at the State Legislature in support of increasing authority of utilities and transportation commission in terms of rail safety and the behavioral health and involuntary commitment of Veterans.

Councilmember Belle shared that she looks forward to the City Council Retreat.

Councilmember Farmer shared that she will testify in support of HB 5163 regarding the treatment and placement of sex offenders.

Deputy Mayor Whalen shared that last week he attended the Pierce Transit Board of Commissioners meeting and the Pierce County Regional Council meeting. He also expressed his support for the Asian American community.

ADJOURNMENT

There being no further business, the meeting adjourned at 9:13 p.m.

DON ANDERSON, MAYOR

ATTEST:

BRIANA SCHUMACHER
CITY CLERK



LAKEWOOD CITY COUNCIL RETREAT MINUTES

Saturday, March 27, 2021
Lakewood City Hall
Council Chambers
6000 Main Street SW
Lakewood, Washington 98499

CALL TO ORDER

Mayor Anderson called the meeting to order at 8:37 a.m.

ROLL CALL

Councilmembers Present: 7 – Mayor Don Anderson; Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, Patti Belle, Linda Farmer and Paul Bocchi.

City Manager Caulfield welcomed the City Council and shared that today's meeting is a follow up to the March 7, 2020 retreat and the purpose is to identify and set City Council goals and priorities for 2021-2022. He then introduced facilitator Ms. Shannon Kelley-Fong, Assistant City Manager for the City of Lacey.

Ms. Kelley-Fong led the City Council through an icebreaker activity, which was a team activity through a socially distanced obstacle course.

She then reviewed the ground rules for today's meeting. City Council members then reflected on the past year sharing what they have learned personally and on the City Council.

City Manager Caulfield reviewed the City's 2020 accomplishments highlighting the responsiveness and communications related to COVID-19, treatment of alum at American Lake, economic development initiatives, the launching of the Build Your Better Here campaign.

Ms. Kelley-Fong then reviewed responses to City Council priorities that were identified at the March, 2020 City Council Retreat.

At 9:45 a.m., the City Council recessed and reconvened at 9:55 a.m.

Planning Manager Tiffany Speir provided an overview of the American Rescue Plan Act (ARPA). She reviewed options and strategies for allocating ARPA funding noting that the City has until December 31, 2024 to expend these funds.

Discussion ensued related to funding priorities and the City Council each identified key projects and areas of focus for both internal and external uses of the funding.

At 11:57 a.m., the City Council recessed and reconvened at 12:10 p.m.

Ms. Kelley-Fong highlighted the city's diversity, equity, and inclusion efforts to date. Discussion ensued related to the City Council's diversity, equity and inclusion goals and priorities. After further discussion, the City Council requested that a follow-up Retreat be scheduled for Saturday, May 22, 2021.

ADJOURNMENT

There being no further business, the meeting adjourned at 1:02 p.m.

DON ANDERSON, MAYOR

ATTEST:

BRIANA SCHUMACHER
CITY CLERK

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: April 19, 2021	TITLE: City / CPSD Cooperative Use Agreement	TYPE OF ACTION: — ORDINANCE NO. — RESOLUTION NO. <u>X</u> MOTION NO. 2021-23 — OTHER
REVIEW: April 5, 2021	ATTACHMENTS: City / CPSD Cooperative Use Agreement	



SUBMITTED BY: Mary Dodsworth, Park, Recreation and Community Services Director

RECOMMENDATION: It is recommended that the City Council approve and authorize the City Manager to execute the City / CPSD Cooperative Use Agreement.

DISCUSSION: Representatives from the City and Clover Park School District (CPSD) met over a period of time to discuss and develop a cooperative use agreement. The proposed agreement aligns w/current plans and policies, will ensure efficient use of public resources, improves access and opportunities for students, families and community members (taxpayers), creates equity and contributes to a safe and healthy district and community. It is understood that the success of this agreement (and partnership) includes ongoing communication and trust. The city and the CPSD have a positive working relationship and memorializing current operations as well as providing updates based on changes in use and access to publically owned facilities will help educate representatives from each agency as well as the community regarding public use and accessibility of City and CPSD facilities. The CPSD Board approved the agreement at their April 12 Board meeting.

ALTERNATIVE(S): Not approve the recommended agreement or provide alternative language. Changes to the document would require review from both parties and would delay or postpone the implementation of the agreement.

FISCAL IMPACT: No impact

Prepared by  Department Director	  City Manager Review
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**COOPERATIVE USE AGREEMENT
CLOVER PARK SCHOOL DISTRICT AND THE CITY OF LAKEWOOD
INTERLOCAL AGREEMENT**

This Interlocal Agreement, made this April 12, 2021 day of August 31, 2024, is by and between Clover Park School District No 400, a Washington municipal corporation (hereinafter referred to as the "District"), and the City of Lakewood, a Washington municipal corporation (hereinafter referred to as the "City"), for the purpose of efficient use of public resources, improving local or student services and providing more opportunities for the community members they serve.

WHEREAS, The District recognizes that public school facilities are community facilities that should be utilized to their maximum potential for the benefit of all community members, the Board of Directors further recognizes that the primary purpose of these facilities is the educational program of the Clover Park School District. Within the guidelines specified by law, the superintendent or designee is authorized to develop administrative regulations and rental fees as applicable to administer the non-commercial use of school district facilities at times they are not in use by school district programs.

WHEREAS, The City has a mission of creating a healthy and vibrant community where opportunities for all are available through established programs, services, events and partnerships; and, within the guidelines specified by law, the City Manager or designee is authorized to implement City Council approved facility use policies and fees as applicable to use City facilities at times they are not in use for City purposes.

WHEREAS, the District and the City have shared facilities and resources for many years and have a positive history of cooperative use along with a desire to be effective and efficient stewards of public resources and assets; and,

WHEREAS, the District and the City have a mutual interest in supporting the needs of students, youth and families living in the City of Lakewood and surrounding areas; and

WHEREAS, the District and the City are mutually interested in supporting programs and services at facilities located in or adjacent to the City of Lakewood; and

NOW, THEREFORE, pursuant to Chapter 39.34 "Interlocal Cooperation Act" of the Revised Code of Washington, the City and District agree as follows:

I. Purpose

- A. The City and District acknowledge that the intent of this Agreement is to address the community use of District and City facilities.
- B. This Agreement addresses the City and the District's desire to memorialize this partnership, clarify expectations and ensure consistency and continuity for current and future agency representatives.

II. General Provisions

- A. This Agreement shall be continuous, with procedures for modification to meet the needs of the District and City.
- B. The execution and modification of this Agreement must be authorized by the Clover Park School Board or designee and the Lakewood City Council or designee.
- C. The District and City shall act in good faith to implement the terms of this Agreement.
- D. Access to District and City facilities is subject to change based on local, state or national issues or disasters. The reopening of City or District facilities will follow local, state or national regulations, guidelines and best practices to ensure a healthy and safe environment.

III. Scheduling

A. Schedule: Clover Park School District

- i. The District shall create and maintain the master schedule of facility use.
- ii. The District will utilize their own current scheduling and reservation systems for scheduling their facilities. All efforts will be made to inform each other of any changes to the scheduling processes.
- iii. The District reserves the right to cancel or postpone any activity due to conflicts, disregard of policies, or other uncontrollable circumstances. Efforts will be made to provide adequate notice as to not disrupt program continuity.

B. Schedule: City of Lakewood

- i. The City shall create and maintain the master schedule for City and District use of City Facilities.
- ii. The City will utilize their own current scheduling and reservation systems for scheduling their facilities. All efforts will be made to inform each other of any changes to the scheduling processes.
- iii. The City reserves the right to cancel or postpone any activity due to conflicts, disregard of policies, or other uncontrollable circumstances. Efforts will be made to provide adequate notice as to not disrupt program continuity.
- iv. The City will provide an annual calendar with dates the facilities are unavailable.

IV. Access and Priority Use

A. Access: Clover Park School District

- i. The schedule of available times for community use of District facilities, which are not in conflict with District use, shall be:
September-June (academic year)
Monday through Friday 6:00 P.M. to 8:30 P.M. or 30 min after school programs have ended and
June - August (summer months)
Monday through Friday 8:00 A.M. to 5:00 P.M.
Saturday as scheduled
- ii. Consideration will be given for special events.

B. Access: City of Lakewood

- i. The schedule of available times for community use of City park facilities
Monday – Sunday – 7:00 A.M. to dusk or 9:00 P.M. whichever is earlier.
The schedule of available times for community use of City Hall
Monday – Friday – 8:30 A.M. to 5:00 p.m.

Saturday – Sunday – as scheduled

C. Priority Use: Clover Park School District

- i. Clover Park School District owned athletic fields and facility priority shall be given to District sponsored programs. District programs and activities shall have the right to preempt other users upon giving advance notice, except in extraordinary circumstances when advance notice is not possible. The District reserves the right to add or remove facility options to accommodate district mission and vision.
- ii. Clover Park School District facility use is prioritized in the following order:
 - a. District sponsored school-related activities and events. District sponsored educational or interscholastic activities limited to student and school related functions will only incur charges if custodial or other personnel are required.
 - b. Non-Profit Organizations, Community Parks and Recreation, Youth-Oriented Community Service Groups. Community organizations promoting youth programs, local youth parks and recreation groups and affiliated organizations (PTSA/PTA, booster clubs, city, Girl/Boy Scouts, Boys & Girls Club, etc.) may be charged for incurred expenses for use of facilities, playfields, custodial overtime and/or personnel fees.
 - c. Fee-Based Non-Profit Organizations, Non-Community Parks and Recreation, Adult Service Groups, Religious and For-Profit/Commercial Groups. Meetings of religious, fee-based non-profit, and profit-making organizations or corporations whose gatherings are open to the public for which adequate facilities are not otherwise readily available in the community shall incur charges as required by the district fee schedule.
- iii. The District agrees to give the City second priority of District owned facilities with the exception of the following that will become available on a case by case basis, following regular scheduling policies and procedures:
 - a. Lakes High School; not included
 - b. Clover Park High School; not included
 - c. Harry Lang Stadium; not included
 - d. Thomas Middle School facility use available after the following dates:
 1. Gymnasium available after August 31, 2021
 2. Outdoor fields available after August 31, 2022
- iv. The City may request district facility use:
 - a. Elementary Schools ten (10) school days after the start of school
 - b. Middle Schools
 1. Fall activities after August 15
 2. Winter activities after October 15
 3. Late Winter activities after December 15
 4. Spring activities after February 15
- v. Requests are limited as follows; two (2) weekdays Monday through Thursday at four (4) or less elementary schools and two (2) or less middle school facilities per season. Friday and Saturday use scheduled as needed for games at one (1) middle school and one (1) elementary school per weekend. Requests outside of these dates will be given the same priority as traditional users. Consideration will be given to special requests.

- vi. In the event it is determined that facilities are not fully utilized there may be adjustments in allocation the following year.

D. Priority Use City of Lakewood

- i. The District agrees that first priority for the use of the City owned athletic fields and facilities shall be given to City managed programs. City programs and activities shall have the right to preempt other users upon giving advance notice, except in extraordinary circumstances when advance notice is not possible.

Use of City public facilities shall have the following priority:

- 1. Programs, governmental functions, recreational activities or special events sponsored by the City of Lakewood will have first priority in scheduling.
 - 2. Lakewood non-profit organizations, Lakewood community groups, or Clover Park School District will have the second priority.
 - 3. Residents of the Lakewood area, employees and employers based in the Lakewood area, may reserve facilities and parks for group or personal use and will have third priority.
 - 4. Nonprofit groups, organizations and individuals who reside outside of the Lakewood area will have fourth priority.
- ii. In conjunction CITY facility use is prioritized in the following order:
 - a. City use, government functions, and sponsored events, including recreational and community programs provided by the City.
 - b. District use and District sponsored programs provided for District staff and students.
 - c. All other uses as determined and prioritized by the City.
 - iii. The City agrees to give the District second priority of City owned facilities with the exception of the following that will become available on a case by case basis, following regular scheduling policies and procedures:
 - a. Colonial Plaza, Motor Ave SW
 - b. Pavilion at Fort Steilacoom Park, 9107 Angle Ln SW
 - iv. The District may request dates for meetings and special events in October for the following scheduling period of January 1 – December 31. In November available space for all use is on a space available basis. Every effort will be made to accommodate existing established schedules. Sports fields may be unavailable December – the end of February annually for field repair and maintenance.

V. Supervision:

- A. Agencies acknowledge that the community has invested significant resources in City and District facilities and that proper use, management, security and supervision are required to reduce negative impacts and potential overuse of facilities. Joint use should not create additional work or burden for agency employees. An agreed upon joint agency code of conduct for staff, volunteers and participants will be adhered to when using City or District facilities. The City and District reserves the right to cancel or postpone any activity due to conflicts, disregard of policies, or other uncontrollable circumstances.

IV: Fees and Cost Sharing

- A. Generally, facilities will be available at no cost unless additional equipment, staffing, access or operational support is needed outside of established hours or operation or because new regulations, guidelines or changes in operations are required to meet local, state, national issues or mandates. Each agency may request reimbursement for additional support needed to ensure a healthy and safe environment. . Examples include scheduled use taking place during a weekend or after established business hours could require additional staff to supervise or secure buildings or additional labor could be needed to clean and sanitize an area to meet Department of Health guidelines.
- B. Registration and use fees will be retained by the collecting and managing agency.
- C. Fee would be charged for direct costs incurred as a result of a particular activity, such as when a given use results in non-scheduled labor costs or other direct costs attributed to a specific use of a facility, or when in the view of the facility owner a facility was left unkempt or damaged. In this case, the City and District agree to reimburse one another for their share of expenses upon written invoice for direct costs that are a consequence of facility use. Fees for indirect costs shall not be reimbursed.
 - i. "Direct Costs" are those costs that are incurred directly such as the cost of labor, an object or specific service.
 - ii. "Indirect Costs" are those costs that cannot be readily and specifically attributed to a particular use, such as depreciation, normal "wear and tear" of facilities, overhead or administrative expenses.

V: Term, Renewal and Termination

- A. Agreement shall be from Apr.12, 2021 through and including Aug. 31, 2024. At the end of this three (3) year period, the Agreement shall automatically renew with (3) one-year renewal terms existing on this contract, unless any Party gives the others written notice of its intent not to renew the Agreement at least ninety (90) days before the expiration of this Agreement.

VI: Capital Improvements

- A. Joint Projects will be addressed in a separate agreement.

VII: Responsibility and Liability

- A. The District shall indemnify and hold the City and its agents, employees, and/or officers, harmless from and shall process and defend at its own expense any and all claims, demands, suits, at law or equity, actions, penalties, losses, damages, or costs, of whatsoever kind or nature, brought against the City arising out of, in connection with, or incident to the execution of this Agreement and/or the District's performance or failure to perform any aspect of this Agreement; provided, however, that if such claims are caused by or result from the concurrent negligence of the City, its agents, employees, and/or officers, this indemnity provision shall be valid and enforceable only to the extent of the negligence of the District; and provided further, that nothing herein shall require the District to hold harmless or defend the City, its agents, employees and/or officers from any claims arising from the sole negligence of the City, its agents, employees, and/or officers. No liability shall attach to the City by reason of entering into this Agreement except as expressly provided herein.
- B. The City shall indemnify and hold the District and its agents, employees, and/or officers, harmless from and shall process and defend at its own expense any and all claims, demands,

suits, at law or equity, actions, penalties, losses, damages, or costs, of whatsoever kind or nature, brought against the District arising out of, in connection with, or incident to the execution of this Agreement and/or the City's performance or failure to perform any aspect of this Agreement; provided, however, that if such claims are caused by or result from the concurrent negligence of the District, its agents, employees, and/or officers, this indemnity provision shall be valid and enforceable only to the extent of the negligence of the City; and provided further, that nothing herein shall require the City to hold harmless or defend the District, its agents, employees and/or officers from any claims arising from the sole negligence of the District, its agents, employees, and/or officers. No liability shall attach to the District by reason of entering into this Agreement except as expressly provided herein.

VIII: Conflict Resolution

- A. If either party believes that the other party is not fulfilling the performance obligations established by this Agreement, that party shall give written notice of its complaint to the other party. The party receiving the complaint shall, within ten (10) calendar days, correct the situation and confirm the correction in writing or reject the complaint while explaining the mitigating circumstances and why a remedy cannot be achieved.
- B. If the City and District representatives are unable to resolve the complaint, the District's Director of Operations & Maintenance and the City's Parks and Recreation Director agree to meet to resolve the complaints. If they are unable to resolve the complaints, the issue shall be referred to the District Superintendent and the City Manager for resolution.

IN WITNESS WHEREOF the parties hereto have executed this Agreement as of the day and year first above written.

CLOVER PARK SCHOOL DISTRICT

By: _____
Superintendent

CITY OF LAKEWOOD

By: _____
John Caulfield, City Manager

Attest: _____
City Clerk, Briana Schumacher

Attest: _____
Lakewood City Attorney

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: April 19, 2021	TITLE: Motion authorizing the City Manager to execute a purchase and sale agreement for parcel 6785000211, a.k.a. 9203 Veteran's Dr SW.	TYPE OF ACTION: — ORDINANCE — RESOLUTION <u>X</u> MOTION #2021-24 — OTHER
REVIEW: April 19, 2021	ATTACHMENTS: Vicinity Map	

SUBMITTED BY: Paul A. Bucich, P.E., Public Works Engineering Director/City Engineer.

RECOMMENDATION: It is recommended that the City Council authorize the City Manager to execute a purchase and sale agreement for tax parcel 6785000211 also known as 9203 Veteran's Drive in the amount of \$220,000 plus applicable closing costs estimated at \$3,000.

DISCUSSION: American Lake Park experiences periods of heavy use during summer time resulting in overflow parking along adjacent city streets. With the construction of the Veteran's Drive roadway improvements, a significant portion of the on-street parking is no longer available. The City was approached by a representative of a parcel across Veteran's Drive asking if the City was interested in purchase of the property. The parcel is approximately ½ acre and vacant. At Council direction, negotiations were conducted and an agreed upon value of \$220,000 was reached plus closing costs estimated at \$3,000. After purchase, the site will be cleared and gravel applied for the summer of 2021 while a detailed study is conducted to determine the best mix of parking for the site. The site will be secured except during high usage of the park when it will be opened for park user parking.

ALTERNATIVE(S): The City can decide not to purchase the property, however there are no other vacant parcels in the vicinity available to pursue. Lack of parking is expected to hinder use of the park and will likely result in park users spilling into surrounding neighborhoods.

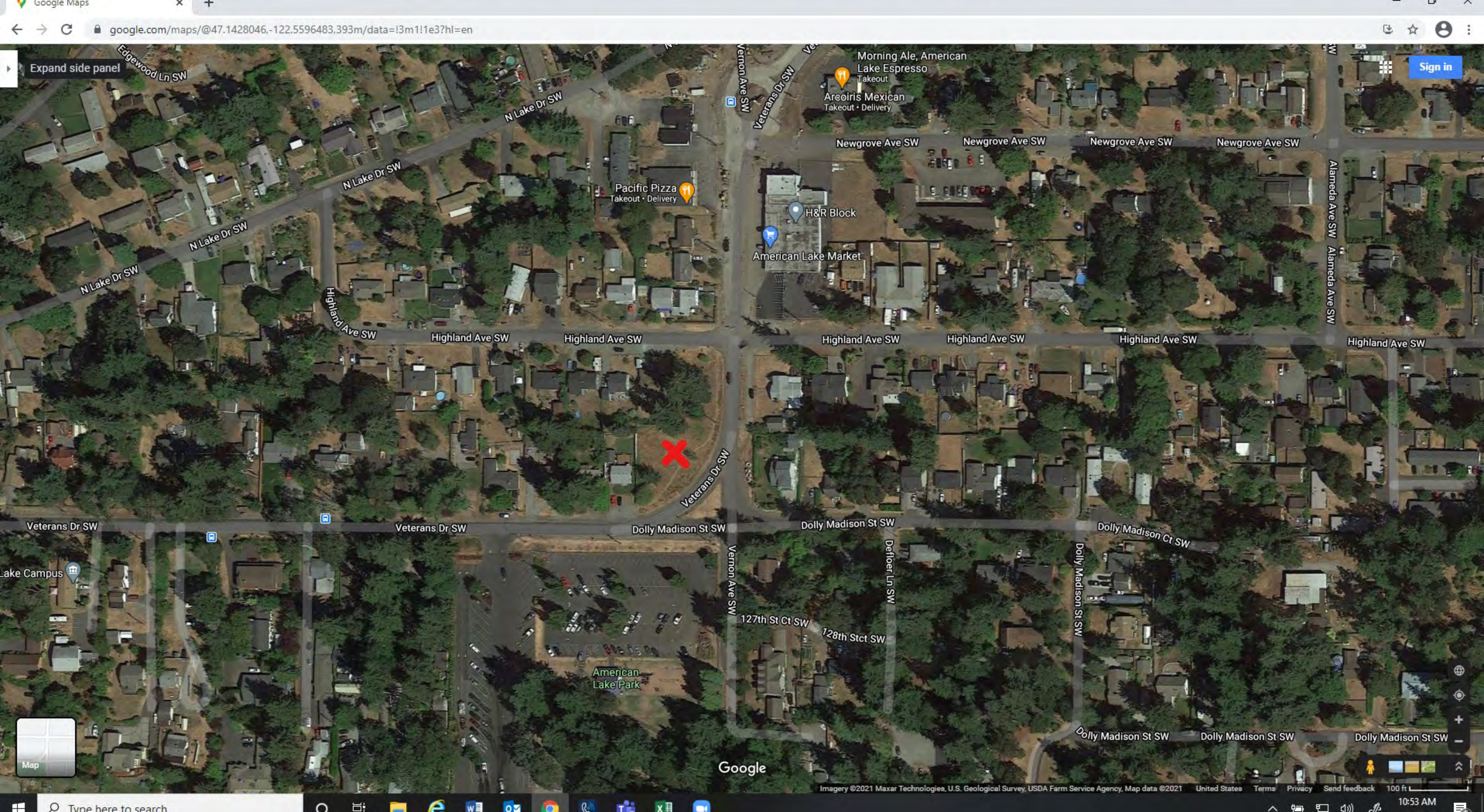
FISCAL IMPACT: This project will be budgeted as shown below:


- \$450,000 from General Government
 - \$223,000 sales and closing costs
 - \$50,000 land clearing and grading with gravel in 2021
 - \$177,000 final pavement and improvements in 2022

Paul A. Bucich
Prepared by


City Manager Review

Paul Bucich, P.E.
Department Director



	<p>LAKEWOOD'S PROMISE ADVISORY BOARD Thursday, March 4th, 2021 Zoom Virtual Meeting Lakewood, WA 98499 7:30 am – 8:30 am</p>
--	--

CALL TO ORDER

Ellie Wilson called the meeting to order at 7:32 am.

ATTENDANCE

Promise Advisory Members Present: Ellie Wilson, Andie Gernon, Mary Dodsworth, Dr. Michele Johnson, Superintendent Ron Banner, Leanna Christian, Dr. Joyce Loveday, Elise Bodell, Beverly Howe, Dr. Wanda Elder,

City Council Liaison: Mary Moss

Staff Present: Brian Humphreys

Youth Council Liaisons: Sarah James, Hank Jones, Brandon Elliott

PUBLIC COMMENT

NONE

MEETING MINUTES

February 4th meeting minutes unanimously approved.

NEW BUSINESS

Grant Twyman, the Equity, Diversity and Inclusion Manager for Clover Park School District, shared a video about the district's Give 6 initiative. The idea is that youth thrive when adults see, hear, and understand them. Members provided feedback about the video, distribution methods, and some ideas about how to maximize awareness about the 6 assets.

The human services coordinator gave an update about the youth mental health conversation that took place with the Youth Council in January. The Give 6 initiative was acknowledged as an appropriate way to address many of those mental health issues in addition to the workforce development efforts to support families and communities. He also gave an update about workforce development efforts, which will include a pilot event in Springbrook sometime in the Spring. Further details will be presented at next month's meeting.

BOARD MEMBER UPDATES

Mary Moss gave an update that the Lakewood City Council is full again now that Patti Belle is on the Council. Also, on March 1st the City Council recognized Captain Rudolph Dambeck with a Proclamation designating him as Honorary City Attorney. The Lakewood Multicultural Coalition website is also almost complete.

Joyce Loveday and Michele Johnson shared the colleges are preparing for the Fall enrollment process and received some additional COVID funding.

Elise Bodell and Mary Dodsworth shared that this is the Pierce County Library System's 75th anniversary. The Lakewood Library is also open for limited services now.

Beverly Howe shared that the hospital system is now called Virginia Mason Franciscan health.

Ron Banner shared about the school district's efforts to continue to safely transition students back to the classroom and to help facilitate the distribution of the COVID vaccine.

ADJOURN

Ellie Wilson adjourned the meeting at 8:39 am.

Ellie Wilson (digitally signed) 4/1/2021

Ellie Wilson, Chair

Date



**PLANNING COMMISSION
REGULAR MEETING MINUTES
March 24, 2021
Zoom Meeting
6000 Main Street SW
Lakewood, WA 98499**

Call to Order

Mr. Don Daniels, Chair called the ZOOM meeting to order at 6:30 p.m.

Roll Call

Planning Commission Members Present: Don Daniels, Chair; Connie Coleman-Lacadie, Paul Wagemann, James Guerrero and Phillip Combs

Planning Commission Members Excused: Ryan Pearson

Commission Members Absent: None

Staff Present: Tiffany Speir, Long Range & Strategic Planning Manager; and Karen Devereaux, Administrative Assistant

Council Liaison: Councilmember Paul Bocchi (not present)

Approval of Minutes

The minutes of the meeting held on February 17, 2021 were approved as amended by voice vote M/S/C Wagemann/Combs. The motion passed unanimously, 4 - 0.

Agenda Updates

None

Public Comments

This meeting was held virtually to comply with Governor Inslee's Emergency Proclamations 20-28 and its addendums. Citizens were encouraged to virtually attend and to provide written comments prior to the meeting. No public comments were received.

Public Hearings

None

Unfinished Business

None

New Business

2021 Comprehensive Plan Amendments 2021-01, 02, -03, -05, -07, and -09.

Ms. Tiffany Speir presented the background and CEDD recommendation on proposed 2021 Comprehensive Plan Amendments -01, -02, -03, -05, -07, and -09.

2021-01 Replacement of Comprehensive Plan Sustainability Chapter with an Energy and Climate Change Chapter

CEDD Recommendation: Approval

2021-02 Updates to reflect 2020 rezone of certain Springbrook parcels to Industrial Business Park (IBP) Zone

CEDD Recommendation: Approval

2021-03 Updates to reflect adoption of 2020 City Parks Legacy Plan

CEDD Recommendation: Approval

2021-05 Updates related to Western State Hospital (WSH) and Public and Semi-Public Institutional Uses

CEDD Recommendation: Defer to 2020 Comprehensive Plan amendment cycle.

2021-07 Updates related to establishing a new industrial Center of Local Importance (CoLI) and removing existing COLIs (Industrial Business Park / Clover Park Technical College and 5 (South Tacoma Way.)

City Council Action via Resolution: Remove 2021-07 from 2021 Docket; no direction to include it in future Comprehensive Plan amendment cycles.

2021-09 Text amendments to Comprehensive Plan Goal (LU-18.5) related to highest and best use of commercial lands.

CEDD Recommendation: Approval

The Commission's discussion would continue at the April 7th meeting to include the remaining proposed 2021 Comprehensive Plan Amendments; 2021-04, -06, -08, and -10. The public hearing would be held virtually on April 21st at 6:30 pm.

Report from Council Liaison

None.

Reports from Commission Members and Staff

Future Planning Commission Agenda Topics

April 7:

- Review of proposed Comprehensive Plan Amendments 2021-04, -06, -08, and -10
- Annual Shoreline Restoration Report

April 21:

- 2021 Comprehensive Plan Amendment Package Public Hearing

May 5:

- Action on 2021 Comprehensive Plan Amendment Package

Regional Planning Land Use Updates

None

Other

Ms. Tiffany Speir informed members that Mayor Anderson is scheduled to appoint the next Planning Commissioner, Mr. Linn Larsen, to the commission on Monday, April 5, 2021. Mr. Larsen's first meeting would be on April 7, 2021.

At its March 27 retreat, the City Council would discuss how to spend the roughly \$15M awarded to the City of Lakewood from the American Rescue Plan Act (ARPA.)

Mr. Paul Wagemann queried where the statistics were found that helped develop the proposed Energy & Climate Change Chapter for Lakewood's Comprehensive Plan. (Ms. Speir subsequently provided the 2015 Report (revised in 2018) from the Puget Sound Clean Air Agency Greenhouse Gas Emissions Inventory to all commissioners.)

Next Regular Meeting: The next regular meeting would be held on April 7, 2021.

Meeting Adjourned at 7:00 p.m.



Don Daniels, Chair
Planning Commission 04/07/2021




Karen Devereaux, Recording Secretary
Planning Commission 04/07/2021



TO: Mayor and City Councilmembers

FROM: Jeff Gumm, Program Manager

THROUGH: John J. Caulfield, City Manager 

DATE: April 19, 2021

SUBJECT: Public Hearing on FY 2021 Annual Action Plan and Amendment to FY 2017, 2019, and 2020 Annual Action Plans

Introduction: This memorandum will provide the City Council with a brief description of the review and approval process associated with the Community Development Block Grant (CDBG) and HOME Investment Partnership Act (HOME) program funding allocations for FY 2021 (July 1, 2021 – June 30, 2022), including amendment to the FY 2017, 2019, and 2020 Annual Action Plans.

Public Participation Process: As part of the CDBG and HOME consolidated planning process, grantees are required to include a public participation component as it determines how funding is be allocated to meet housing and community development needs for low and moderate income individuals. This public process involves two steps- a public hearing during development of the plan, and a minimum 30 day comment period to receive comments from citizens on the consolidated plan specifics. The process began on February 10, 2021 when the Community Services Advisory Board held a public hearing to receive testimony on housing, human services, and community development needs. To complete the public participation component, the City is providing for a 30-day public comment period (April 1, 2021 – April 30, 2021) and is holding a public hearing on April 19, 2021 to accept comments on the City's proposed use of funds for FY 2021 (2021 Annual Action Plan and amendment to FY 2017, 2019, and 2020 Annual Action Plans).

Timeline: The timeline below provides milestones and upcoming actions relating to the FY 2021 Annual Action Plan.

- February 1, 2021 – City Council review of CDBG/HOME priorities and funding options for FY 2021.
- February 10, 2021 – Public hearing to receive input on housing, human services and community development needs.

- March 10, 2021 – CSAB review of Draft FY 2021 Annual Action Plan and amendment to FY 2017, 2019, and 2020 Annual Action Plans.
- March 22, 2021 – Council review of Draft FY 2021 Annual Action Plan and amendment to FY 2011, 2019, and 2020 Annual Action Plans.
- April 1 - April 30, 2021 – Formal citizen review & comment period for Plan.
- *April 19, 2021* – *Lakewood City Council conducts public hearing on the Draft Annual Action Plan.*
- May 3, 2021 – Lakewood City Council adopts FY 2021 Annual Action Plan, including amendment to FY 2017, 2019, and 2020 Annual Action Plans.
- May 14, 2021 – Submit Annual Action Plan to HUD.

FY 2021 Annual Action Plan and Amendment to FY 2017, 2019, and 2020 Annual Action Plans:

Tables 1 and 2 below provide a listing of CDBG and HOME funds available and proposed use of funds for the upcoming fiscal year.

TABLE 1 CDBG FUNDING RECOMMENDATIONS – FY 2021 (Includes Reprogrammed CDBG)					
Activity	CDBG	Reprogrammed FY 2017 CDBG	Reprogrammed FY 2019 CDBG	FY 2020 CDBG	Total
Housing – Major Home Repair/Sewer Loan	\$264,106.92	\$0	\$0	\$0	\$264,106.92
Housing – CDBG Administration of HOME Housing Services	\$15,000	\$0	\$0	\$0	\$15,000
Housing – Emergency Assistance for Displaced Residents (EADR)	\$0	\$28,708.76	\$0	\$0	\$28,708.76
Public Service – Housing/Foreclosure Counseling & Fair Housing	\$50,000	\$0	\$0	\$0	\$50,000
Infrastructure – COL Sidewalk Project – Phillips Rd.	\$129,574.68	\$0	\$170,425.32	\$0	\$300,000
Administration	\$114,670.40	\$0	\$0	\$0	\$114,670.40
Total Funding	\$573,352	\$28,708.76*	\$170,425.32**	\$0***	\$772,486.08
<i>*FY 2017 - Reprogrammed \$28,708.76 in EADR funds to current fiscal year.</i> <i>**FY 2019 – Reprogrammed funding \$170,425.32 (FY 2019 Low Income Street Lighting \$164,257.17; FY 2019 Administration \$3,392.96; and FY 2019 Section 108 Repayment – Interest \$2,775.19).</i> <i>***FY 2020 – Annual Action Plan Amendment addresses CDBG allocation incorrectly reported to Lakewood on October 22, 2020. Corrected allocation provided on November 9, 2020. Initial allocation reported as \$596,006; corrected allocation reported as \$595,915 (\$91 less). Amendment to decrease FY 2020 Administration allocation by \$91 to \$119,110.20. No other changes to 2020 AAP.</i>					

TABLE 2 HOME FUNDING RECOMMENDATIONS – FY 2021	HOME
Affordable Housing Fund	\$289,557
Housing Rehabilitation	\$0
Administration (Tacoma 10%)	\$32,173
Total Funding	\$321,730



TO: City Council

FROM: Tiffany Speir, Long Range & Strategic Planning Manager

THROUGH: John Caulfield, City Manager *John P. Caulfield*
Dave Bugher, Assistant City Manager for Development Services

DATE: April 19, 2021

SUBJECT: Lakewood Station District Subarea (LSDS) Plan, Development Code and SEPA Planned Action Ordinance

ATTACHMENTS:

- **Attachment A (Agenda Bill** for LSDS Plan and Hybrid Form-Based Code, with Exhibit A (Ordinance 751 and LSDS Plan), Exhibit B (LSDS Code, proposed LMC Title 18C) and Exhibit C (Amendments to LMC Title 18A to reflect adoption of LSDS Subarea Plan and LMC Title 18C));
 - **Attachment B (Agenda Bill** for LSDS SEPA Planned Action and Ordinance 752 with Exhibit A (Ordinance) and Exhibit B (LSDS Planned Action Checklist));
 - **Attachment C** (Identified pending 2021 amendments to Comprehensive Plan text reflecting adoption of LSDS package); and
 - **Attachment D** (Public comments received at City Council and Planning Commission public hearings and CEDD responses and Exhibits A-C)
-

BACKGROUND

On March 8, 2021, the City Council reviewed the draft Lakewood Station District Subarea (LSDS) Plan and the implementing Hybrid Form-Based Code as recommended by the Planning Commission, as well as the Planned Action. The City Council held a public hearing on April 5.

Attached to this memorandum (**Attachments A and B**) are agenda bills for the ordinances to adopt the LSDS Plan, Development Code and Planned Action as well as exhibits thereto. Also attached (**Attachment C**) is a summary list of the amendments to the Comprehensive Plan reflecting the adoption of the LSDS Plan. Finally, attached (**Attachment D**) is a table of the public comments received at the City Council and Planning Commission public hearings and the City responses to those comments.

RECOMMENDATION

It is recommended that the City Council adopt the ordinances approving the LSDS Plan, Code and SEPA Planned Action after amending the Commission-recommended parking requirements at LMC 18C.600.610 to reinsert the originally proposed requirements.

DISCUSSION

The Lakewood Station District Subarea (LSDS) Plan, Development Code and Planned Action ("LSDS Package") were developed between December 2019 and November 2020.

The website www.lakewoodstation.org went live in December 2019 as has been kept up to date with the latest information and draft documents throughout the process. Beginning in February 2020, stakeholders (property owners, neighborhood associations, agencies, utilities, and developers) were engaged for in-person and then virtual sessions to provide their perspective, expertise, and opinions regarding the vision, policies and development requirements within the subarea. An on-line public survey was conducted in the summer of 2020, and 95 responses were received. Three (3) mailings were sent to LSDS residents and businesses to keep them updated on progress on the LSDS package.

The LSDS Package responds to the vision and priorities expressed in the Comprehensive Plan, to stakeholder and public input provided while it was drafted, and by those involved in the legislative process beginning with the Planning Commission public hearing in November 2020. It does not rezone any parcel; rather, the Package encourages higher density in residential and commercial redevelopment via the hybrid form-based development code that allows for a wider range of uses in buildings designed and placed on their parcels as required for the type of street upon which they are located.

The LSDS Package plans for 1,722 dwelling units and 2,276 jobs in the subarea over the next twenty years. It acknowledges and prioritizes the provision of housing affordable to, and avoiding displacement of, current subarea residents. The Package encourages opportunities for “missing middle” homeownership. It also includes consideration of and planning for Lakewood Landing, a key economic development mixed use project located near the Lakewood Station and the Hwy 512 and I-5 interchanges.

The LSDS package includes the development of private park and open space as well as the use of “excess” right-of-way (ROW) (i.e., that beyond a 60’ width) for linear open space over time. Pedestrian and cycle routes as well as improved street design are included as well. Much of the stakeholder and public hearing discussion addressed on- and off-site parking requirements in the subarea, and the original drafts presented to the Planning Commission reflect a balancing of the views and policies considered. (The Planning Commission amended several parking requirements, and those amendments are included in the draft Development Code provided to the Council with this memorandum.)

Much of the structure of the LSDS Plan, Development Code and Planned Action is similar to that of the Downtown Subarea Plan, Code, and Planned Action, and they are intended to work in conjunction with each other and the rest of the Lakewood Development Code. The intent is to better connect the Downtown with the Lakewood Station District to maximize the benefits of so many transit hubs and employment opportunities within the City.

Like the Downtown Subarea Plan, Code and Planned Action, the LSDS Package includes an implementation plan (See Section 8 of the LSDS Plan) and sets up biennial reviews of the progress of development within the subarea and the continued relevance of the Planned Action assumptions by the City Council (See the Planned Action Ordinance at Section 4.) Future Downtown Subarea reviews will occur in even-numbered years, and future LSDS reviews will occur in odd-numbered years.

ATTACHMENT A

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: April 19, 2021	TITLE: An Ordinance adopting Lakewood Station District Subarea (LSDS) Plan, Development Code (new LMC Title 18C), and amendments to LMC Title 18A.	TYPE OF ACTION: <div style="display: flex; justify-content: space-between;"> <u> X </u> ORDINANCE NO. 751 </div> <div style="display: flex; justify-content: space-between;"> — RESOLUTION NO. </div> <div style="display: flex; justify-content: space-between;"> — MOTION NO. </div> <div style="display: flex; justify-content: space-between;"> — OTHER </div>
REVIEW: March 8 Study Session; April 5 Public Hearing	ATTACHMENTS: Exhibit A (Ordinance and LSDS Subarea Plan), Exhibit B (LSDS Development Code, new LMC Title 18C), and Exhibit C (Amendments to LMC Title 18A)	

SUBMITTED BY: David Bugher, Assistant City Manager for Development Services
 Tiffany Speir, Long Range & Strategic Planning Manager

RECOMMENDATION: It is recommended that the City Council amend the proposed LSDS Development Code to reflect the originally included parking standards in LMC 18C.600.610. It is also recommended that the City Council approve the attached Ordinance 751: adopting the Lakewood Station District Subarea (LSDS) Plan; adopting the LSDS Development Code (new LMC Title 18C) (with or without the City Council’s amendments); and adopting amendments to LMC Title 18A.

DISCUSSION: Lakewood has recognized the need for, and memorialized initial design for, a subarea plan in the Lakeview Neighborhood/Lakewood Station District since adopting its first Comprehensive Plan in 2000. In November 2019, the State Department of Commerce awarded Lakewood a grant under E2SHB 1923 to prepare a Lakewood Station District Subarea (LSDS) Plan, an implementing Hybrid Form-Based Code and a SEPA-based Planned Action. In-person and virtual stakeholder outreach was conducted between December 2019 and spring 2021 to help develop the proposed package.

ALTERNATIVE(S): The Council could amend the proposed Ordinance and attachments; the Council could also not adopt the Ordinance and attachments.

FISCAL IMPACT: N/A

Prepared by
Tiffany Speir, Long Range & Strategic Planning Manager

Department Director
Dave Bugher, Assistant City Manager for Development Services



 City Manager Review

EXHIBIT A

ORDINANCE NO. 751

AN ORDINANCE of the City Council of the City of Lakewood, Washington, adopting the Lakewood Station District Subarea Plan as an element of the Comprehensive Plan, amending the Lakewood Municipal Code by establishing a new Title 18C (Lakewood Station District Development Code), and amending Lakewood Municipal Code Chapters 18A.10 (Basic Provisions) and 18A.20 (Administration.)

I. RECITALS

WHEREAS, the City of Lakewood is a non-charter optional municipal code city as provided in Title 35A RCW, incorporated under the laws of the State of Washington, and planning pursuant to the Growth Management Act (GMA), Chapter 36.70A RCW; and

WHEREAS, the City has adopted a Comprehensive Plan under the GMA and, as provided in RCW 36.70A.080(2), is authorized to adopt a subarea plan as an optional planning element; and

WHEREAS, RCW 36.70A.130(2)(a)(i) exempts the initial adoption of a subarea plan from the GMA's limitation on Comprehensive Plan amendments to once per year; and

WHEREAS, the City's Comprehensive Plan includes text and policies authorizing and anticipating the adoption of a subarea plan for the Lakewood Station District (e.g., Section 1.3.1, 1.6.7, 2.3.7, 2.3.16, 2.4.1, Policy LU-17.3, Goal LU-21, Section 3.3.5, Goal LU-25, Policy LU-25.5, Goal LU-27 and Policies LU-27.1 – 27.3, Section 4.4, Section 4.5.2, Goal UD-9, Policy UD-9.5, Goal ED-5, and Land-Use Implementation Strategy 11.3.1); and

WHEREAS, In November, 2019, the State Department of Commerce awarded the City of Lakewood, WA a grant under ESB 1923 to: prepare a subarea plan for the Lakewood Station District per RCW 43.21C.420; prepare a planned action using a threshold determination process per RCW 43.21C.440 (1)(b)(ii); and prepare a hybrid form-based development code to implement the Lakewood Station District Subarea (LSDS) Plan; and

WHEREAS, the City prepared the LSDS Plan, Code and Planned Action after an extensive public participation and review process for the subarea plan including an online survey, open houses, community meetings, stakeholder meetings, and study sessions and public meetings before the Planning Commission and the City Council in 2020 and 2021; and

WHEREAS, the City held the LSDS Planned Action Ordinance Community Meeting on November 4, 2020 as required by RCW 43.21C.440 (3)(b); and

WHEREAS, implementation of the LSDS Plan and amendments to the Lakewood Land Use and Development Code to adopt a new Chapter 18C (LSDS Development Code), would improve conditions for Lakewood residents and businesses. Design regulations would include standards related to: integration of the natural environment including new parks, building design, pedestrian experience and streetscapes, mixed-use building features, site planning, parking, lighting, screening, and signage; and

WHEREAS, the LSDS would increase housing capacity and commercial space and further support business investment with more flexible zoning and civic and infrastructure investments. Tax exemptions for low and moderate-income housing units in “Residential Target Areas” (RTAs) are allowed per LMC Chapter 3.64; most of the LSDS is within a RTA. Respondents to an online survey about the LSDS Plan showed a strong interest in: well designed and attractive development; sidewalks and pathways; new retail, restaurant and entertainment; new jobs in proximity to the Sounder Station; better transit, cycle and pedestrian connections to the Downtown; new mixed development to include homes (“missing middle” and affordable units), jobs, retail and services; and new open spaces and parks; and

WHEREAS, the LSDS job mix would change to more services jobs and less retail. Office and professional services jobs offer higher wages than typical retail jobs and support the increase of a jobs-housing balance. The Plan envisions and the Code encourages a dense mix of commercial

and medical office, regional and local retail, services and hospitality, and high-density residential uses offering ownership and rental housing opportunities; and

WHEREAS, the Planning Commission, held a duly noticed public hearing on November 18, 2020 regarding the “LSDS Packet” (including the LSDS Plan, amendments to the Lakewood Municipal Code to adopt a new Chapter 18C (LSDS Development Code), associated amendments to the City's Comprehensive Plan text and Land Use Map, and associated amendments to the Lakewood Municipal Code at Chapters 18A.10 (Basic Provisions), and 18A.20 (Administration), reviewed the public record and made a recommendation to the City Council on February 3, 2021; and

WHEREAS, the City Council held a duly noticed public hearing on April 5, 2021 regarding the “LSDS Packet” (including the Plan, Code and Planned action) and reviewed the public record; and

WHEREAS, pursuant to RCW 36.70A.370, the City has utilized the process established by the Washington State Attorney General so as to assure the protection of private property rights; and

WHEREAS, pursuant to RCW 36.70A.106, on January 14, 2021, the City provided the Washington State Department of Commerce with a 60-day notice of its intent to adopt the amendments to City's Comprehensive Plan, and it was assigned number 2021-S-2258; and

WHEREAS, pursuant to RCW 43.21C and WAC 197-11-508, on November 12, 2020 the City submitted information to the Department of Ecology related to the Determination of Non-Significance for the LSDS SEPA Environmental Impact Checklist via the SEPA register and the materials were assigned #202005805; and

WHEREAS, on March 30, 2021, the City submitted a Revised LSDS Determination of Non-Significance (DNS) and SEPA Environmental Checklist to the Department of Ecology via the SEPA register and the materials were assigned #202101613.

II. FINDINGS

The procedural and substantive requirements of the State Environmental Policy Act (RCW 43.21C) have been complied with.

The procedural requirements of the Growth Management Act (RCW 36.70A) have been complied with.

The proposed amendments are consistent with the requirements of Revised Code of Washington (RCW), and the Washington Administrative Code (WAC.)

The proposed amendments are consistent with the City of Lakewood Comprehensive Plan.

The proposed amendments have been reviewed and processed in accordance with the requirements of Lakewood Municipal Code (LMC) Title 14 Environmental Protection, Title 14A Critical Areas, and Title 18A Land Use and Development of the City of Lakewood Municipal Code.

All of the facts set forth in the Recitals are true and correct, and are incorporated herein by reference.

All necessary public meetings and opportunities for public testimony and comment have been conducted in compliance with State law and the City's municipal code.

The Lakewood City Council finds and determines that the regulation of development and land use within the Downtown is within the City's regulatory authority.

The Lakewood City Council finds and determines that approval of such amendments to the Comprehensive Plan and Land Use and Development Code is in the best interests of the residents of Lakewood, and will promote the general health, safety and welfare.

The Lakewood City Council finds and determines that regulation of land use and development is subject to the authority and general police power of the City, and the City reserves its powers and authority to appropriately amend, modify and revise such land use controls in accordance with applicable law;

The documents and other materials that constitute the record of the proceedings upon which the City Council's action is based, which include, but are not limited to, the staff reports for the Project and all of the materials that support the staff reports for the Project, are located in the City of Lakewood, Community and Economic Development Department at 6000 Main Street SW, Lakewood, Washington, 98499-5027. The custodian of these documents is the Assistant City Manager for Development Services of the City of Lakewood;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON, DO ORDAIN as Follows:

Section 1. Adoption of the Lakewood Station District Subarea Plan. The Lakewood Station District Subarea Plan, filed with the Community and Economic Development Department and attached hereto as **Exhibit A**, is adopted as an optional element of the Comprehensive Plan.

Section 2. Adoption of Lakewood Station District Subarea Development Code. The City of Lakewood's Municipal Code is amended to include a new Title 18C, Lakewood Station District Subarea Development Code, filed with the Community and Economic Development Department and attached hereto as **Exhibit B**.

Section 3. Amendment of the Comprehensive Plan and Land Use Map. The City of Lakewood's Comprehensive Plan text and Land Use Map will be amended to reflect the adoption of the Lakewood Station District Subarea Plan as part of the 2021 Lakewood Comprehensive Plan amendment cycle.

Section 4. Amendment of Lakewood Municipal Code Title 18A. The City of Lakewood's Municipal Code Title 18A is amended at Chapter 18A.10 (Basic Provisions) and 18A.20 (Administration), filed with the Community and Economic Development Department and attached hereto as **Exhibit C**.

Section 5. Severability. If any portion of this Ordinance or its application to any person or circumstances is held invalid, the remainder of the Ordinance or the application of the provision to other persons or circumstances shall not be affected.

Section 6. Effective Date. That this Ordinance shall be in full force and effect thirty (30) days after publication of the Ordinance Summary.

ADOPTED by the City Council this 19th day of April, 2021.

CITY OF LAKEWOOD

Attest:

Don Anderson, Mayor

Briana Schumacher, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney

EXHIBIT A

Lakewood Station District Subarea Plan

EXHIBIT B

Lakewood Station District Subarea Development Code (new LMC Title 18C)

EXHIBIT C

LMC Title 18A Amendments

STATION DISTRICT SUBAREA PLAN



City of Lakewood – DRAFT March 29, 2021

Prepared for the City of Lakewood

Prepared by: BERK, Fehr & Peers, Framework, and Skipstone



Acknowledgements

Lakewood's elected and appointed officials and City staff wish to acknowledge those in the community that participated and contributed their valuable assistance in the preparation and production of this document.

Lakewood City Council (2020)

Don Anderson, Mayor
 Jason Whalen, Deputy Mayor
 Paul Bocchi
 Michael Brandstetter
 Linda Farmer
 Mary Moss
 John Simpson

Lakewood City Council (2021)

Don Anderson, Mayor
 Jason Whalen, Deputy Mayor
 Patti Belle
 Paul Bocchi
 Michael Brandstetter
 Linda Farmer
 Mary Moss

Lakewood Planning Commission (2020)

Don Daniels, Chair
 Connie Coleman-Lacadie, Vice-Chair
 James Guerrero
 Nancy Hudson-Echols
 Ryan Pearson
 John Paul Wagemann

City of Lakewood

John Caulfield, City Manager
 M. David Bugher, Community & Economic Development Director
 Tiffany Speir, Long Range and Strategic Planning Manager
 Becky Newton, Economic Development Administrator
 Paul Bucich, Public Works Director
 Weston Ott, Transportation Division Manager

Consulting Team

Jeff Arango, Framework
 Ariel Davis, Fehr & Peers
 Kevin Gifford, BERK Consulting
 Lisa Grueter, BERK Consulting
 A-P Hurd, Skipstone
 Erika Rhett, BERK Consulting
 Josh Steiner, Fehr & Peers

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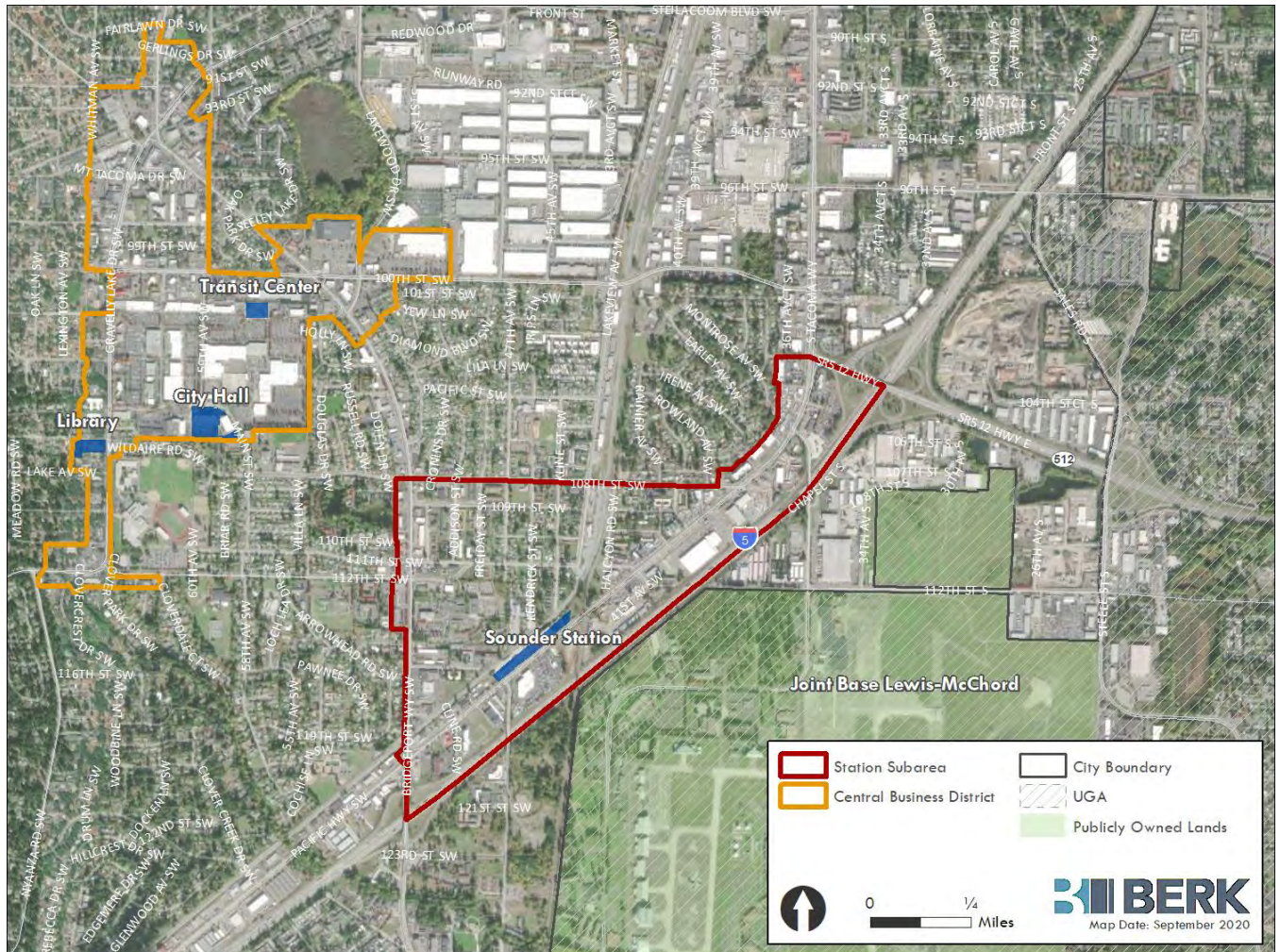
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The Lakewood Station District

The Lakewood Station District Subarea (LSDS) is an area of opportunity for future growth and development within Lakewood's Urban Center. It is located adjacent to I-5 and home to the terminus of the Sounder, which provides passenger rail service to Tacoma, Seattle, and Everett and. It already has a strong employment base that provides jobs in medical services, hospitality, retail, and restaurants. Proximity to high capacity transit and employment also make this a feasible area for compact residential development that can take advantage of such amenities. These factors make this a desirable area for a portion of Lakewood's future development.

Generally, the LSDS boundary incorporates the area within a half mile of the Sounder station but does not include areas southeast of I-5, since the freeway provides a significant barrier. Exhibit 1 shows the boundaries of the planning area for the LSDS. To capture the full transportation corridor to the north and south of the station, the subarea spans the area from the interchange with Bridgeport Way to the interchange with SR-512. Just outside the LSDS is Joint Base Lewis-McChord (JBLM), which relies on retail and services within the study area boundary. Lakewood's Town Center District is also about one mile away.

Exhibit 1. Lakewood Station District Subarea and Vicinity

Source: BERK, 2020; Pierce County Assessor, 2020.

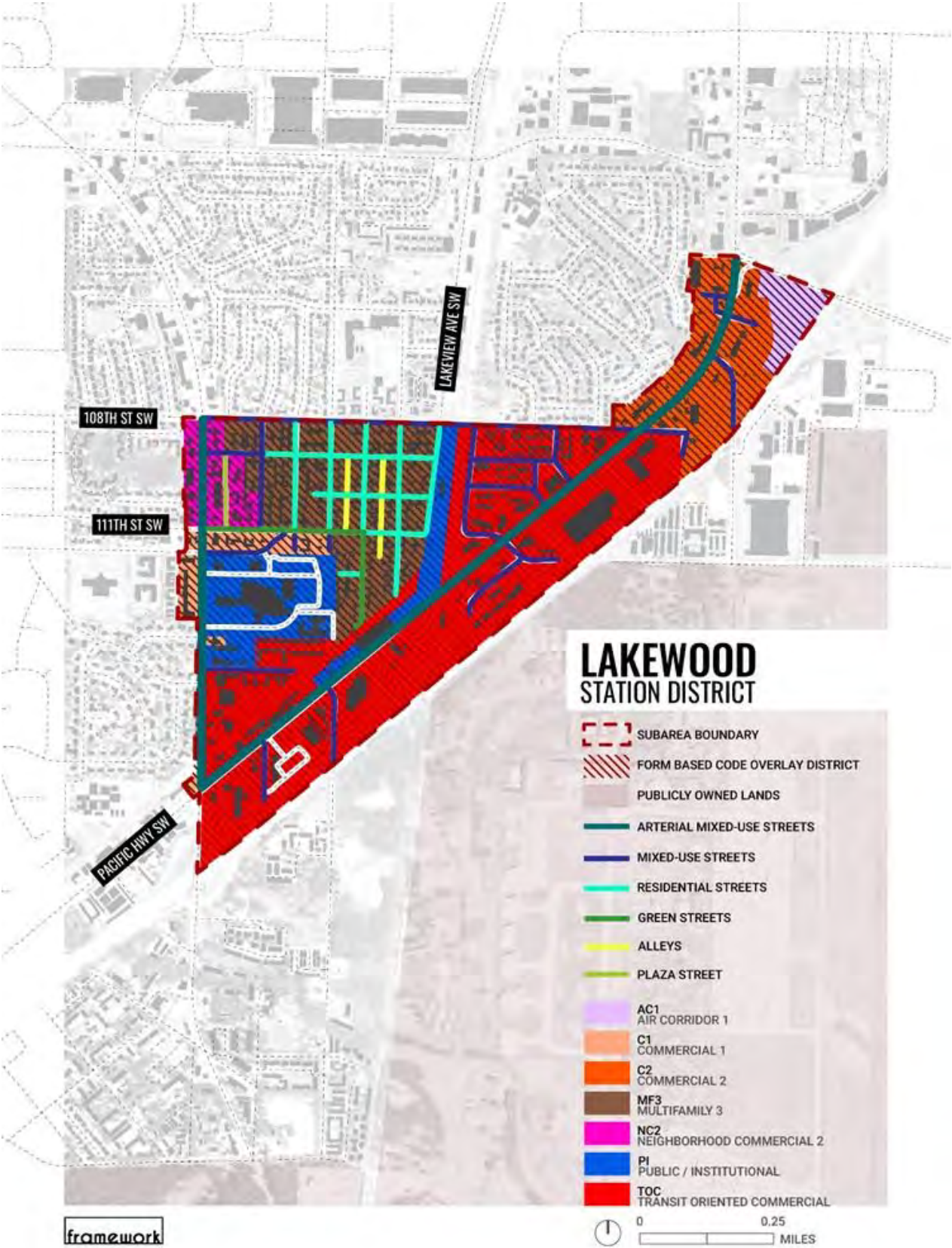
Vision and Concept

The Lakewood Station District is a multi-modal commuter hub of Lakewood and the southern terminus of Sound Transit's commuter rail service. The Lakewood Station District provides an amenity-rich, transit-oriented development node surrounding the Lakewood Station. This District offers a mixture of intensive land uses and activities supportive of direct regional transportation access via the Lakewood commuter rail station and I-5. The District implements development standards to foster a high quality, pedestrian-oriented urban environment including incentives to encourage a dense mix of commercial and medical office, regional and local retail, services and hospitality, and high-density residential uses offering ownership and rental housing opportunities, all supported by direct regional transportation access.

Exhibit 2 shows the land use concept for the LSDS. Key features of the concept for the LSDS include:

- **Transit-Oriented Development.** Residential and employment growth can be supported here because of the proximity to local and regional transit. Residential densities would be up to 40 units per acre in the residential zone (MF3) and up to 54 units per acre in the mixed-use zone (TOC). Commercial development will add employment opportunities, retail, and restaurants to serve residents and workers. As amenities and more compact urban forms are added to the LSDS, those who live and work in this area are less likely to be auto-dependent.
- **Hybrid Form-Based Code.** Land use regulations will include a hybrid form-based code. Under such a system, zoning directs the appropriate areas for residential, commercial, and mixed-use. Building and site design is based on a series of identified streetscape typologies. This allows for the continuation of a variety of uses but with a uniformed development standard that creates an identifiable look and feel for the district.
- **Affordable Housing and Homeownership.** Current residents rely on this area for low and moderate income housing, and displacement is a concern. Residential development and preservation will target housing serving households at 65%-110% of the area median income. Rowhouse residential development allows for compact residential development at an affordable price point. Ground-related units provide private and semi-private outdoor space and the opportunity for zero-lot line platted development. This provides homeownership opportunities and the chance to build wealth and equity for moderate income households in the subarea.
- **Walkable District.** New streetscapes and sidewalk improvements will make getting around the LSDS safer and more comfortable for people on foot. As housing and other amenities are added to this area, increased walkability makes it much more feasible to live and work in the subarea and reduces automobile dependency.

Exhibit 2. Lakewood Station District Subarea



Source: BERK, 2020; Pierce County Assessor, 2020.

Lakewood Landing is a 30-acre site for master planned development. The site is likely to include a mix of residential, retail, entertainment, and employment uses. Conceptually, an initial mixed-use plan with 760 units and 87,000 square feet of retail are part of Phase 1 in the “Central Core” on 11.5 acres of the 30-acre site (see Exhibit 3). Water features, carefully placed structures and sound-absorbing walls will help to mitigate noise. Future phases are under consideration. Development of the site is supported by the subarea plan but more detailed master planning will proceed on a separate track.

Exhibit 3. Lakewood Landing Location and Concept



Exhibit 4a. Lakewood Station District Location and Access



Source: Sound Transit, 2021.

Subarea Growth

Net residential growth within the LSDS is estimated to be 1,772 dwelling units, which assumes 760 units in Lakewood Landing and 962 new units from residential growth in the rest of the subarea over a 20-year period. Employment capacity would include 1,105 new jobs, plus 171 jobs for Lakewood Landing.¹ This complements the growth anticipated for the city center in the Lakewood Downtown Subarea Plan, which plans for about 2,257 dwelling units and 7,369 jobs over roughly the same time period.

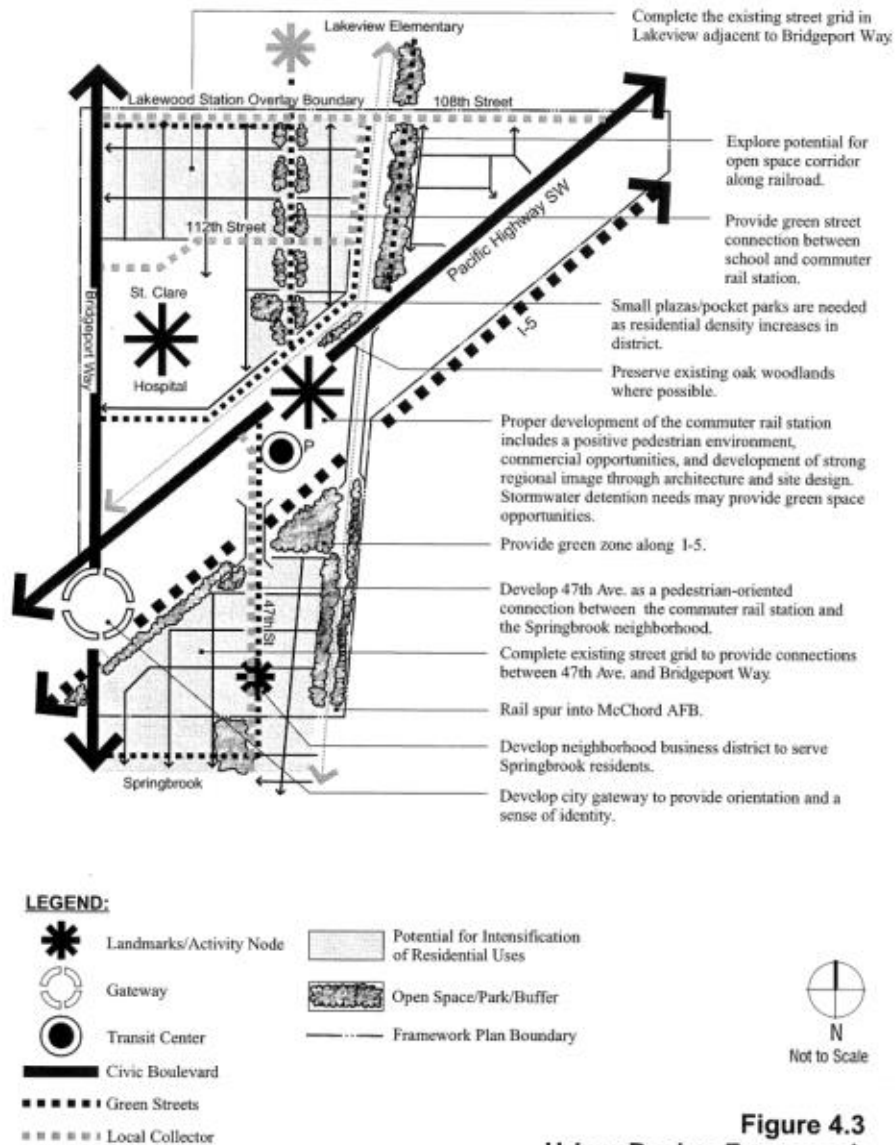
Lakewood Comprehensive Plan

Section 3.3.5 of Lakewood's Comprehensive Plan focuses on the LSDS. Goal LU-25 identifies LSDS as Lakewood's multi-modal commuter hub with supporting policies that call for a transit-oriented development district, development of a subarea plan, coordination with other agencies, and the use of bonus densities and incentives to achieve this goal. A rich mix of land uses around the station is the goal of LU-26 including regional offices, major institutions, high density residential, neighborhood businesses, and open space. The Comprehensive Plan also supports the citywide economic goal to promote a dynamic local economy with diverse housing stock and transportation options.² Transportation linkages between the LSDS and Downtown Subarea will connect people to the amenities of both places and to the region.³ An urban design framework to guide livable and attractive development is the focus of Goal LU-27, which include design guidelines, open spaces, and pedestrian connections. Both the station itself and the I-5 interchanges are major gateways into the city that should have a distinctive look and feel that continue along Pacific Highway, Bridgeport Way, and throughout the rest of the subarea.

¹ Growth numbers include anticipated development of Phase I of Lakewood Landing. Planning for future phases will occur when such phases are fully defined.

² *Lakewood Comprehensive Plan* Economic Development objectives 1.3 and 1.5.

³ *Lakewood Comprehensive Plan* section 2.4.

Exhibit 5. LSDS Planning Concept

SOURCE: EDAW, Inc. 1999

Source: Lakewood Comprehensive Plan, 2014.

Development of a special district around the Lakewood Station has been a consideration since the community's first Comprehensive Plan. It remains a part of the policy framework of Lakewood's Comprehensive Plan and is identified as a future community landmark.⁴ The 1999 high-level concept for the LSDS was included in the 2014 Comprehensive Plan as shown in Exhibit 5.

⁴ Lakewood Comprehensive Plan section 4.4

Community Input

Since the vision and concept for the Lakewood Station District had been a longstanding part of Lakewood's Comprehensive Plan, as a starting point for developing the LSDS the City tested this direction with the community to see if updates were needed. An in-person meeting conducted in late February, before public health orders were issued, with over 24 attendees invited from 17 agency and organizational stakeholders. This meeting collected information about existing conditions and planning efforts occurring in the study area. In addition, the launch of this project coincided with the early stages of the global COVID-19 pandemic. Knowing that the residents and workers in the subarea were vulnerable to the economic effects of the pandemic⁵ the City hoped to better understand local impacts to identify potential short term and long term responses.

In compliance with public health orders to the limit the spread of COVID-19, the City used remote outreach engagement options to solicit and collect community input. The City conducted an online survey during the summer of 2020. Outreach for the survey included a variety of techniques over an engagement period of about six weeks. Subarea residents received postcards and posters were hung in essential businesses and community locations to advertise the survey. Partner organizations, such as Sound Transit, shared the survey with their local mailing lists. Social media messages encouraged survey participation as well as a self-guided walkshop exercise that asked participants to walk the subarea and record their

Kid's Walkshop Response

Lakewood Station District Subarea Plan
WHAT DO YOU THINK?
 First Name: Jo Age: 12
 After your adventure, write or draw answers to these questions:

What did you LIKE BEST?
 how there's places with room for more people
 (Drawing of a building with 'For Rent' signs)

What did you LIKE LEAST?
 1. how some places look nice but they don't take care of that landscape and public park
 (Drawing of a park area with a tree and a person)

What do you WISH FOR?
 A park for kids to play at. A public were it's not a danger to get to. so probably in a neighborhood.
 (Drawing of a park with a tree, a person, and a building)

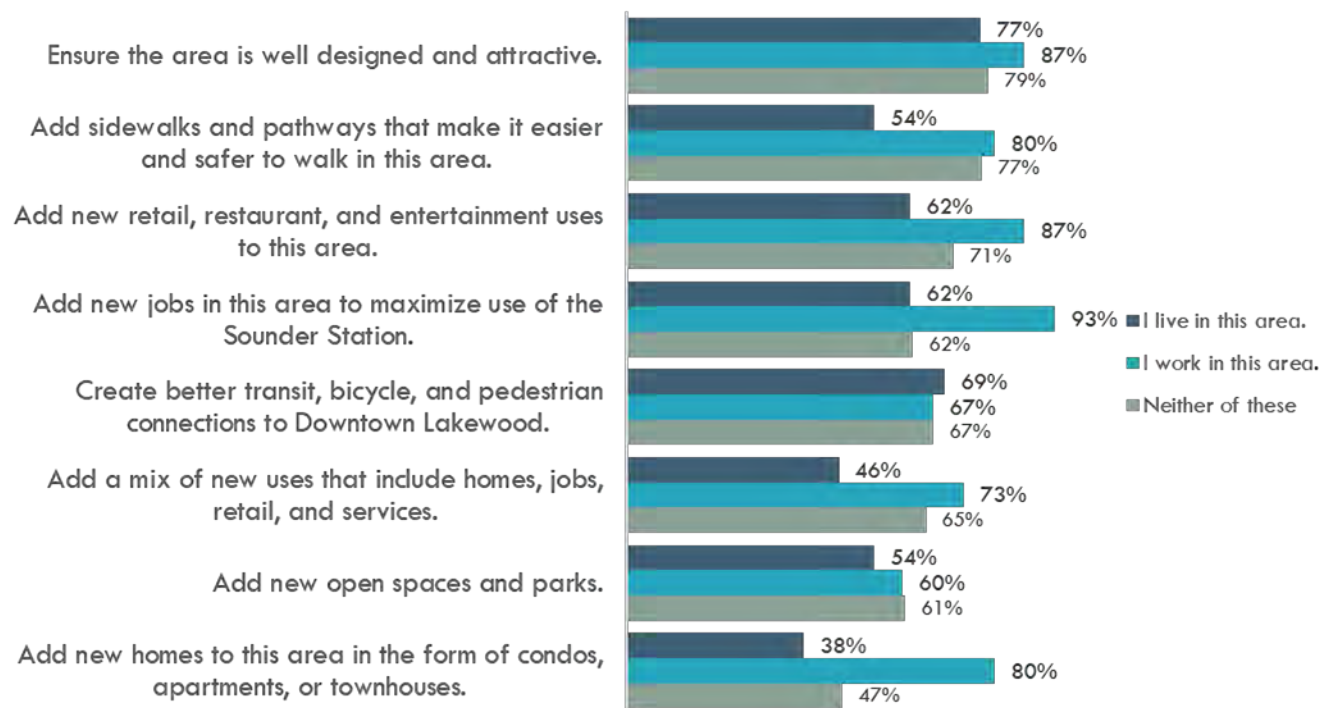
⁵ Based on socio-economic data collected in the *Situation Assessment*

impressions. The walkshop materials included an opportunity for kids to submit their ideas as well, as shown in the Kid's Walkshop Response sidebar.

The survey was offered in both English and Spanish, but the City received no responses in Spanish. Responses were analyzed as a whole and also disaggregated to look for differences in opinion based on whether the respondent lived in the subarea, worked in the subarea, or identified as neither. About 31% of the 91 respondents live or work in the subarea. Of those who identified as working in the area, 20% are business owners. More than half (58%) of the respondents live in Lakewood but outside of the study area.

Exhibit 6. Survey Responses - Future LDS Priorities

Percentage of Group Rating Priority as Important or Very Important



Source: BERK, 2020.

Survey responses summarized in Exhibit 6 confirmed the vision and concept for the LDS from the Comprehensive Plan. The following bullets highlight key results:

- Those who work in the subarea more strongly favored adding new homes to the area (80%) than those who live in the subarea (38%).
- Respondents identified the presence of the Sounder Station as the most important asset of the area, contributing to an

easy commute for residents and employees. The station's location was an asset for those working in the subarea because employees or customers live close by or it is easy to get there. An important asset to residents was the ability to have a home with some outdoor space.

- Changes that respondents would most like to see in the subarea included: more green space or parks, better walkability, more and new businesses, and additional housing options. More affordable housing was a desire of those who worked in the area and more community or neighborhood events were important to current residents.

Future Planned Engagement

The Draft LSDS plan will go through a public engagement process to share and invite feedback on the draft vision, goals, policies, and environmental review information. This will include workshops with the Lakewood Planning Commission, an online open house, and a stakeholders meeting. The City of Lakewood will hold a public hearing and a 30-day comment period to take comment on the draft Plan and Planned Action. Comments received will be used to revise the Plan, which the Planning Commission will review prior to voting on a recommendation to City Council. City Council will also hold a public hearing prior to deciding on adoption.

Note: once the engagement is completed this section will be updated prior to finalization of the plan.

Summary of Existing Conditions

Lakewood completed a *Situation Assessment* to identify existing conditions in the LSDS at the initiation of plan development. Information from the assessment is integrated into the analysis supporting the Planned Action and Environmental Checklist for this project. Exhibit 7 summarizes the results from the *Situation Assessment*.

Exhibit 7. Existing Conditions Summary Table

Topic	Summary
Demographics and Housing	<p>The subarea is a racially and ethnically concentrated area of poverty. There are low rates of homeownership and housing in the area is characterized by low values. These conditions place existing residents at risk of economic displacement.</p> <p>The majority of subarea residents work in service and retail industries and the majority of current employment is in these sectors. However, only about 10% of those who work in the subarea live in Lakewood. Additional moderate income housing in the subarea could provide the opportunity for some workers to live closer to work.</p> <p>In theory, the median household income in the LSDS should be able to pay the typical housing cost in the area. However, households in the subarea have high levels of housing cost burden, meaning they are paying more than 30% of their income for housing. Affordable housing and subsidized housing units could help alleviate the cost burden of existing residents.</p> <p>New multifamily housing is needed in Lakewood due to low vacancy rates. There is also demand for housing to serve military personnel and their families stationed at Joint Base Lewis McChord.</p>
Land Use	<p>Land use policies are in place to support the development of the subarea plan. Current land use patterns are auto-oriented, so transitional patterns of development will be necessary to achieve the desired land use.</p> <p>Residential and commercial land uses are in close proximity to each other. Although they are not currently integrated, this presents opportunities for mixed-use development. There are also a number of vacant and underutilized properties zoned for commercial and residential uses that provide opportunities for redevelopment.</p> <p>The subarea has few natural features and sensitive areas and does not include natural hazards that may limit future development. However, there are some environmental health hazards that will need to be addressed with future redevelopment.</p>
Zoning, Regulation, and Incentives	<p>Zoning is consistent with future land use but zoning and regulatory changes will be needed to support the transition of land use from its current condition to the desired future condition. Temporary activation of spaces in the subarea may be helpful during the transition period.</p> <p>Housing incentives may help the LSDS remain an area for households with low and moderate incomes while increasing the quality of housing through redevelopment.</p> <p>Attached residential development that can be subdivided for ownership can increase density and offer homeownership options, but parking and other development standards should be addressed.</p> <p>Proximity to Joint Base Lewis-McChord brings additional regulations to maintain safe airspace, but the City's current integrated review process minimizes complexity for applicants.</p>
Transportation	<p>Intersections currently operate at an acceptable Level of Service.</p> <p>The area is well served by two transit hubs: the Lakewood Station and the SR 512 Park and Ride.</p> <p>The City of Lakewood, Sound Transit, and Pierce Transit have plans to enhance transit connectivity and multimodal mobility in the subarea in the next decade. <i>Lost revenues due to COVID may affect these plans, but details are currently not available.</i></p>
Utilities and Public Services	<p>Infrastructure upgrades will be needed for some redevelopment plans. This includes the upgrade of water mains for capacity and fire flow, and capacity upgrades for sewer. Coordination with the Lakewood Water District and Pierce County Wastewater Utility during the update of their system plans is needed.</p> <p>Sponsoring the upfront costs of infrastructure investment may be a strategy to support new development in the LSDS.</p>



Land Use and Urban Design

The Lakewood Comprehensive Plan established land use designations based on the vision and concept for the Lakewood Station District. Implementing zoning was adopted consistent with the land use designations. These basic land use policy and regulatory structures are carried on through the LSDS Plan. In addition, the Plan applies an LSDS regulatory overlay. This overlay includes the adoption of a hybrid form-based code to direct new development according to street and building typologies. The use of these typologies will guide urban design to give the LSDS a unique feel and identity and create a thriving environment for those who live and work in the district.

Context

Existing Land Use and Design

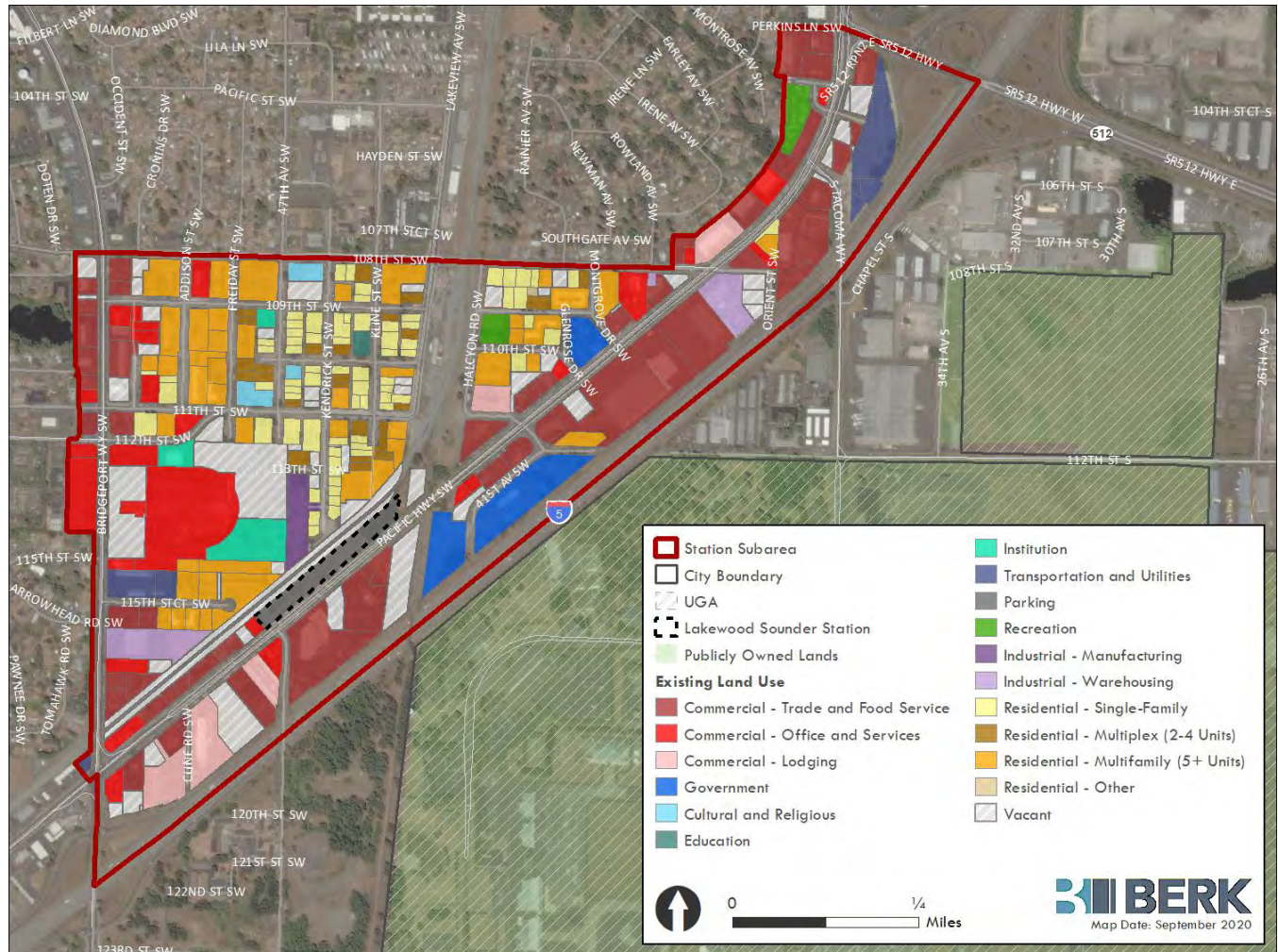
The LSDS vision and development concepts are not reflected in the existing land use or urban design. Development intensity is lower than what is planned for the subarea, as shown in Exhibit 8. Wide streets prioritize the movement of automobiles. This can make major arterials like Bridgeport Way or Pacific Highway act as barriers for pedestrian and bicyclists. Residential streets also sport rights of way over 50 ft wide with few traffic calming measures to buffer automobile traffic.



Current Urban Form in the LSDS

Sources: City of Lakewood, 2020; Google Earth, 2020.

The variety of commercial developments in the LSDS is one of its assets. Companies include a range of small unique businesses, local chains, and national companies in a range of building types that include low rise office buildings, strip commercial developments, corporate architecture, and commercial pads. Most of these building sites and adjacent streetscapes are built for auto-access with entries set far back from the sidewalk and limited pedestrian connectivity except through the parking lot. This type of streetscape and site planning is not designed at a human-scale that feels safe for walking. During public engagement walkability improvements were a top desire of residents and employees currently in the subarea.

Exhibit 8. Existing Land Use

Note: Recreation includes RV park and recreation activities.

Sources: BERK, 2020; Pierce County Assessor, 2020.

Most of the subarea is in commercial use, but there is a significant amount of residential use and some vacant lands as well. Residential uses include single-family homes (60%), small duplex or multiplex buildings (26%), and low-rise multifamily units (14%). Parcel size in the subarea is varied, as shown in Exhibit 9. Half the parcels are a half-acre or less in size, but these only account for about 40 acres in the 339-acre subarea. The 13 largest parcels account for 38% of the land in the subarea. This variety of parcels provides different types of opportunities for small and large scale redevelopment.

Exhibit 9. Parcel Size in the LSDS

Parcel Size	Parcel Count	Sum of Acreage
0.5 acre or less	169	40.25
0.5 - 1 acre	71	51.37
>1 - 5 acres	61	119.55
Greater than 5 acres	13	128.09
Total	314	339.26

Sources: Pierce County Assessor, 2020; BERK Consulting, 2020.

Although residential development is allowed at up to 54 units per acre in the LSDS currently, the average density of existing development is 11.6 units per acre. Policies and strategies in this plan aim to better utilize land for housing in the area around Lakewood Station.

Environmental Conditions

The Lakewood Station District Subarea is a highly developed urban environment that contains few natural features. It is characterized by parcelized land, pavement, and existing vegetation mostly planted for ornamental purposes. A summary of environmental conditions is shown in Exhibit 10. Additional detail can be found in the *Situation Assessment* or *SEPA Environmental Checklist*.

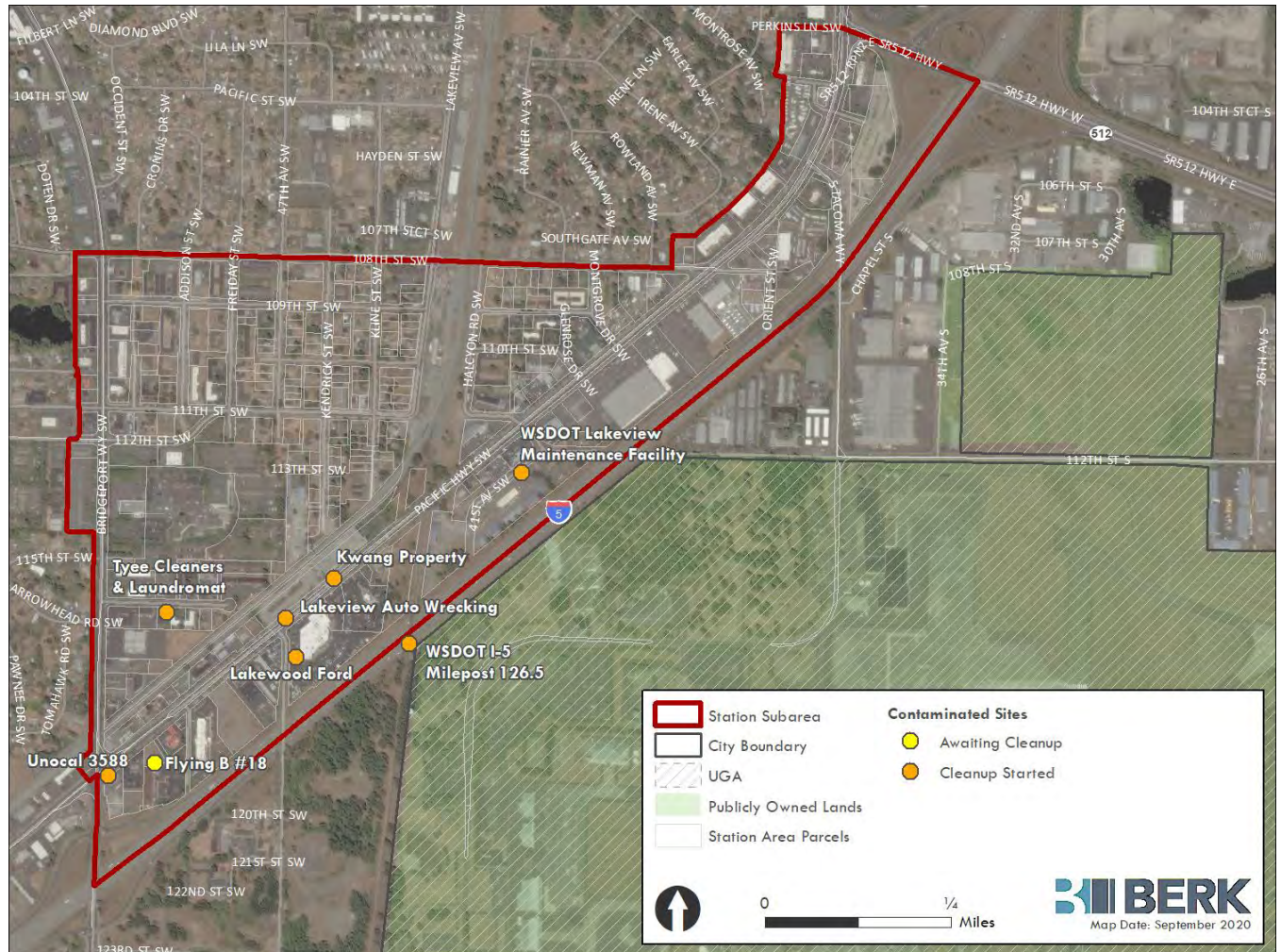
Exhibit 10. Summary of Environmental Conditions in the LSDS

Environmental Feature	LSDS Conditions	Effects on the LSDS
Waterbodies and Shorelines	There are no significant features within the LSDS.	Not applicable.
Wetlands	There are three small, isolated areas of mapped wetlands adjacent to I-5. There is a delineated wetland outside the LSDS west of Bridgeport Way.	The isolated, mapped wetlands will need to be investigated at the time of development and follow critical areas regulations. A limited number of LSDS parcels may be subject to buffer requirements identified for the wetland outside the LSDS west of Bridgeport Way.
Floodplains	A portion of the study area is a moderate (or 500-year) flood area. This is mapped along Bridgeport Way and between Pacific Highway and I-5 west of Lakeview Avenue SW.	Areas of moderate flooding are not regulated by the City's flood hazard regulations.
Aquifer Recharge Area	All of Lakewood is within the Central Pierce County Aquifer Area. This is a sole source aquifer.	New land uses that could result in groundwater contamination are limited, including things such as storage tanks, dry cleaning, and auto repair.
Geological Hazards	There are no significant features within the LSDS.	Not applicable.
Environmental Clean-up Sites	There are eight sites identified for environmental clean-up in the LSDS, as shown in Exhibit 11.	Contamination from petroleum products, metals, solvents, and lead is the result of current or past activities on these sites. ⁶ Clean-up is started on all the sites except the Flying B #18 site. Sites are in various stages of the clean-up process and subject to Washington State's Model Toxics Control Act. ⁷

Sources: Pierce County Assessor, 2020; BERK Consulting, 2020.

⁶ Specific information on each site and the clean-up efforts that are underway can be found at: <https://ecology.wa.gov/Spills-Cleanup/Contamination-cleanup/Cleanup-sites/Locate-contaminated-sites>.

⁷ RCW 70.105D.

Exhibit 11. Environmental Clean-up Sites in the LSDS

Sources: BERK, 2020; Pierce County Assessor, 2020.

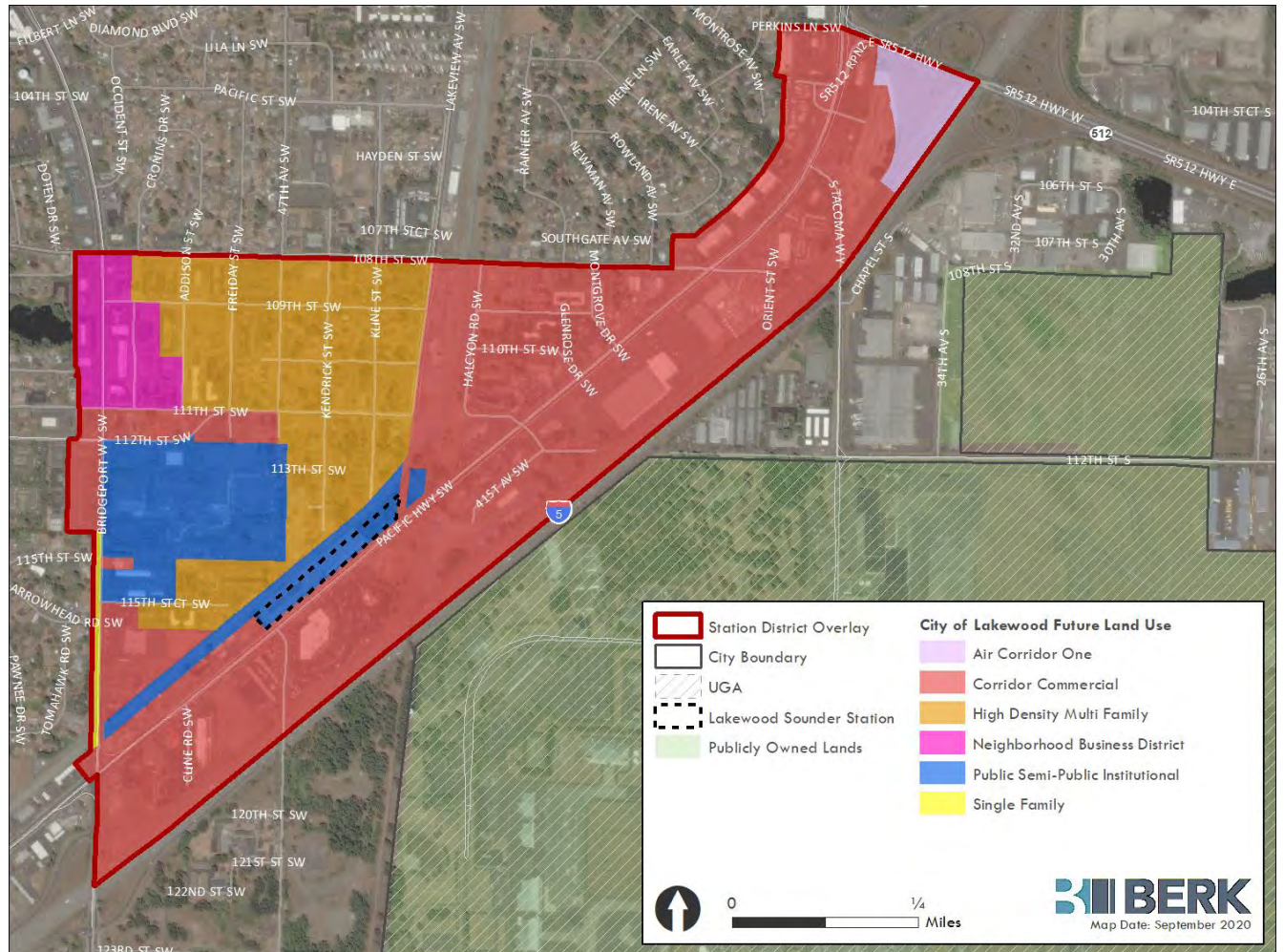
Future Land Use

The Comprehensive Plan identifies the following land use designations in the subarea, as shown in Exhibit 12:

- Corridor Commercial.** This land use is designated along Pacific Highway SW (Pacific Highway) and just north of the Hospital. It recognizes Lakewood's pattern of existing strip commercial development and is implemented by the Transit-Oriented Commercial, Commercial 1, and Commercial 2 zones. Within the LSDS, this district will remain and include opportunities for mixed-use development.

- **Neighborhood Business District.** This designation is implemented through the Neighborhood Commercial 1 zoning along Bridgeport Way SW (Bridgeport Way) that allows for smaller or strip commercial business areas to transform into compact urban development over time. It allows commercial development that serves surrounding neighborhoods and beyond, and allows for mixed-use residential development.
- **Public & Semi-Public Institutions.** St. Clare Hospital and the Lakewood Station are in this land use designation that recognizes essential moderate and large scale facilities that serve all of Lakewood.
- **High Density Multi Family.** The existing residential areas zoned Multi Family 3 are in the High Density Multi Family future land use designation. The purpose of this designation is to integrate a variety of high density housing types into adopted subareas and business districts. The LSDS Plan envisions rowhouse and other residential developments that are integrated with surrounding commercial and mixed-use development through pedestrian connections and urban design.

This Subarea Plan adds the Station District Overlay to the Comprehensive Plan Future Land Use Map. The purpose of the overlay is to recognize the area in which the LSDS vision will be applied.

Exhibit 12. Future Land Use in the LSDS

Sources: BERK, 2020; Pierce County Assessor, 2020.

Land Use Regulation

Land use regulation in the LSDS is driven by a hybrid form-based code. Assigned zoning identifies the types of uses allowed in each zoning and is consistent citywide. The form-based code creates a development regulation overlay that is unique to the LSDS. It regulates building forms and land use in a way that is unique to the subarea. Regulations to maintain land use compatibility with airfield operations at Joint Base Lewis-McChord remain in effect.⁸

⁸ Within the LSDS these regulations primarily implement lighting standards to prevent interference with aircraft operations. Additional detail is available in the *Situation Assessment* and the Lakewood Municipal Code 18.10.135.

Zoning

Zoning in the LSDS is shown in Exhibit 13. These zones are consistent with the Future Land Use map and include:

- **TOC – Transit-Oriented Commercial.** TOC zoning is shown along most of Pacific Highway in the subarea around Lakewood Station and the proposed Lakewood Landing site. This zone is unique to the LSDS. The purpose of TOC is *“an interactive mixture of uses which focus on regional transportation networks while providing for urban design, people orientation, and connectivity between uses and transportation routes.”*⁹ The mix of uses allowed in the TOC is very similar to those allowed in the Central Business District. They focus on retail and services, prohibiting space-intensive uses like auto sales, furniture and appliance stores, or industrial uses that may cause compatibility issues in a compact urban environment such as manufacturing or recycling stations. Mixed-use and multifamily residential uses are allowed at densities up to 54 units per acre.
- **C1 - Commercial One and C2 - Commercial Two.** C1 and C2 are commercial corridor districts that incorporate employment, shopping, services, offices, and light industrial uses near major arterials. A small strip of C1 is located north of the hospital, just off Bridgeport Way. Along Pacific Highway near the intersection of SR 512 is an area of C2. Both districts allow a range of businesses as permitted uses. Hotels and motels are allowed in both districts, permitted in C2, but a conditional use in C1. Commercial recreation, heavy manufacturing, shopping centers, and recycling and transfer stations characterize the type of uses that are prohibited. Residential uses are not



Lakewood Station is at the heart of the TOC zone

Source: HewittSeattle.com

⁹ Lakewood Municipal Code 18A.10.120D.5

allowed, except for allowing a caretaker's unit.

- **NC2 - Neighborhood Commercial Two.** The commercial area on Bridgeport is zoned NC2 with the intent to create a sense of urban community that serves surrounding neighborhoods that may also attract people from other areas. This zone allows a mix of residential, retail, office, and services. Residential may be multifamily or mixed-use development up to 35 units per acre. Permitted commercial uses tend to be small or mid-sized. Most light industrial and larger commercial uses are prohibited. The few that may be considered, such as auto sales or breweries, are conditional uses to help mitigate impacts and ensure district and neighborhood compatibility.
- **MF3 - Multi Family Three.** The existing residential area of attached and detached single-family homes and low rise multifamily is zoned MF3. MF3 zoning is in areas where there is both an arterial and a nearby commercial or mixed-use district. This is intended to be a high density multifamily environment with multi-story housing with densities up to 54 units per acre. The LSDS overlay will provide form-based regulations that effectively limit densities to 30-40 units per acres and incentivize rowhouse development. Where multifamily development occurs within the LSDS, ground floor commercial use is allowed.¹⁰ Many of the existing uses in the LSDS are non-conforming in this zone. Non-conforming structures may be maintained but not altered or enlarged.¹¹
- **PI - Public/ Institutional.** This zone recognizes the sites of St. Clare hospital and Lakewood Station, which are major facilities serving all of Lakewood and beyond.



St. Clare provides regional medical service

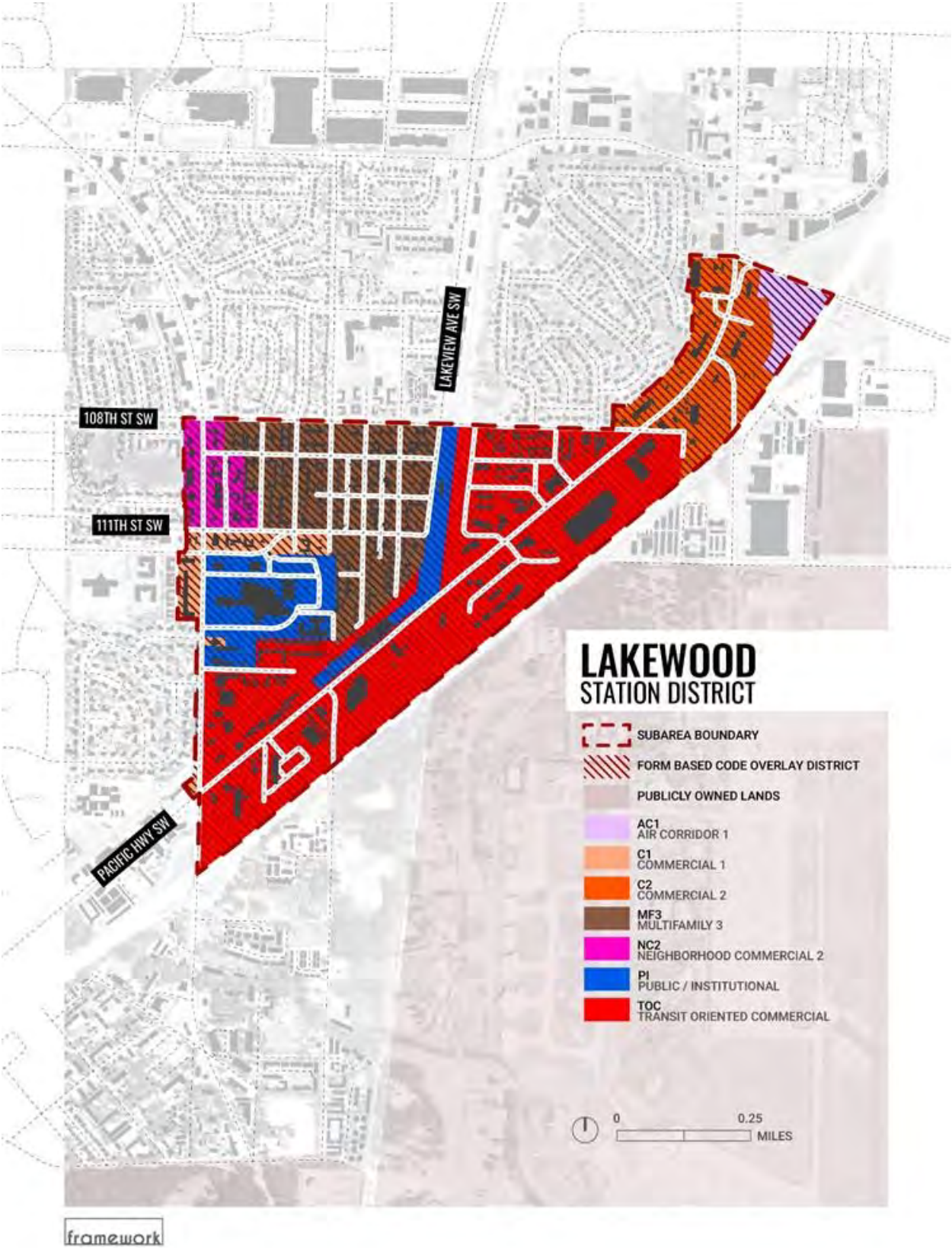
Source: CHIFrancisican.org

The zoning map adds an overlay for the Lakewood Station District (see Exhibit 13). This area links the vision to specific regulating provisions in the Lakewood Municipal Code. It allows for the implementation of a hybrid form-based code, incentive programs, and other measures within this area.

¹⁰ Lakewood Municipal Code 18A.40.040B.1

¹¹ Lakewood Municipal Code 18A.20.200

Exhibit 13. Zoning in the LSDS



Sources: BERK, 2020; Pierce County Assessor, 2021.

Hybrid Form-Based Code

Form-based codes address the relationship between buildings and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. This code type works best when applied to targeted areas that have a defined vision, such as the LSDS. Lakewood adopted a hybrid form-based code for its Downtown. The form-based code for the LSDS draws on the work done for Downtown, but is customized for the vision, goals, and conditions of the Station District.

While uses are still regulated by zone, the hybrid form-based code shapes building design to reinforce a human, neighborhood scale that feels safe and welcoming. The use of a hybrid form-based code will standardize streetscapes to provide a consistent look and feel as well as facilitate the walkability desired by the community. Off-street parking, which is necessary to support retail as the area transitions to a more compact form, will be moved behind or under buildings that are located closer to the street. New development will include 8-14' wide sidewalks and pedestrian connections between the sidewalk and the building to enhance walkability.

Transitioning from the current land use and urban form to a more compact and pedestrian-friendly environment will occur over time. The hybrid form-based code focuses retail along designated streetscapes with high traffic. This helps preserve existing retail until the area is ready for higher intensity mixed-use redevelopment with ground floor commercial.

Exhibit 14. LSDS Hybrid Form-Based Code Overlay



Sources: Lakewood Municipal Code, 2020; Framework, 2021.

The hybrid form-based code is administered through an overlay that covers the entire LSDS as shown in Exhibit 14. It overrides zoning and development regulations in the Lakewood Municipal Code if there are areas of conflict. Uses are defined by the underlying zoning designation, with some additional prohibitions intended to limit uses at a scale or impact that is incompatible with the LSDS vision. Maximum densities and height limits are also set by the underlying zoning code.

This code uses street typologies to guide the development of building types and development regulations within the subarea as shown below. The hybrid form-based code can be found in Lakewood Municipal Code Title 18C.

Street improvements in this area will work within the existing right-of-way (ROW) to ensure the safe and efficient movement of vehicles, bicycles, and pedestrians, as well as to facilitate travel by transit. As a result, street sections may vary and are subject to the approval of the Public Works Department.

Arterial Mixed-use Streets

Arterial mixed-use streets maintain the existing vehicle capacity and allow a range of land use and building frontage types and include surface parking lots. This streetscape applies to Bridgeport Way and Pacific Highway SW.

Mixed-Use Streets

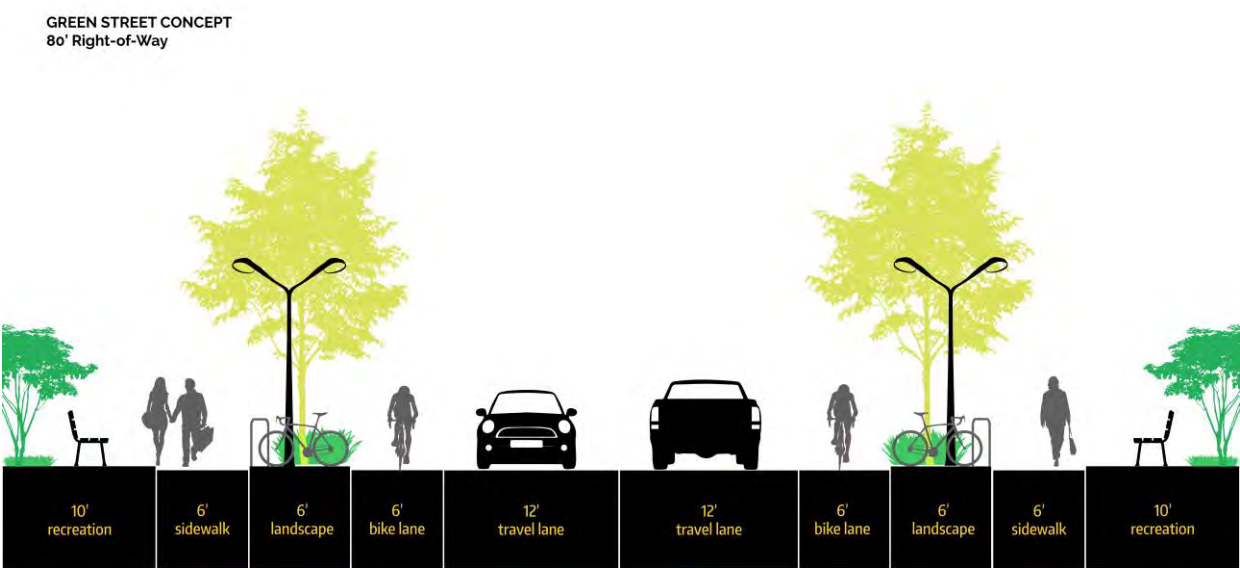
Mixed-use streets support a variety of activities and functions both in the public right-of-way and development along the street edges. Street level retail is permitted, but not required, and a wider range of building frontage types are permitted, including street-level residential and office uses. Mixed-use streets also require pedestrian-oriented design and requirements may vary based on the location within Station District.

Green Streets

Green streets include vehicle travel lanes, sidewalks, bike lanes, and open space for stormwater management, landscaping, and gathering space. Green streets contribute both functional and amenity features to the streetscape to make an attractive and welcoming district as shown in Exhibit 15 and Exhibit 16. The Boulevard street in Exhibit 17 is a variety of green street that

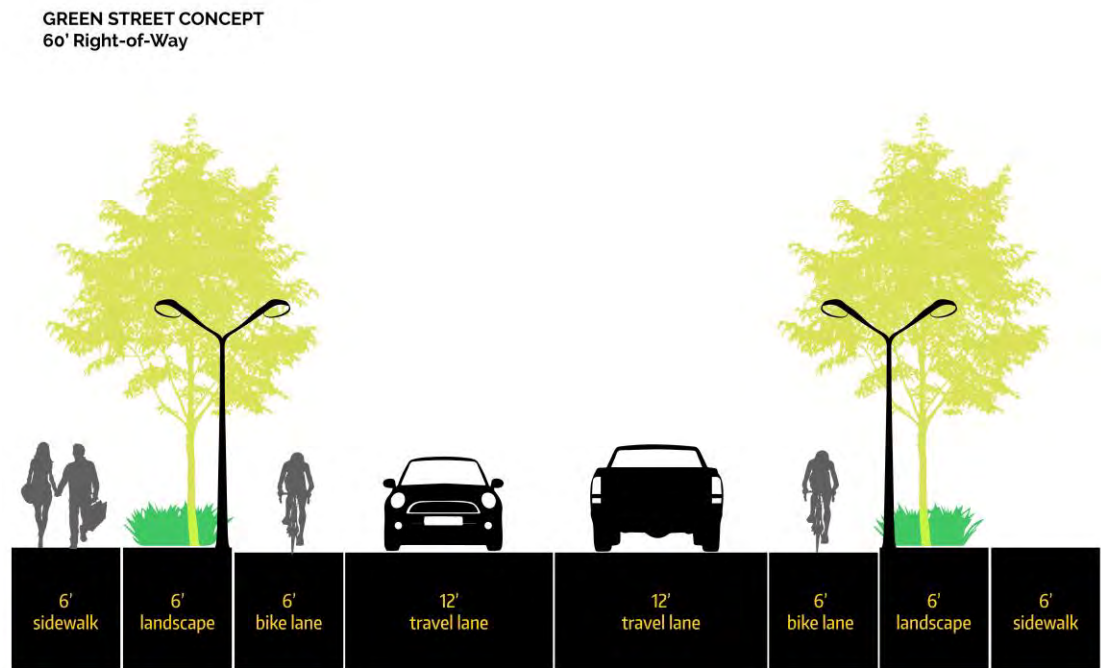
may be carried beyond the district, highlighting connections to Downtown or other nearby areas.

Exhibit 15. Green Street Concept #1 – 80' Right-of-Way



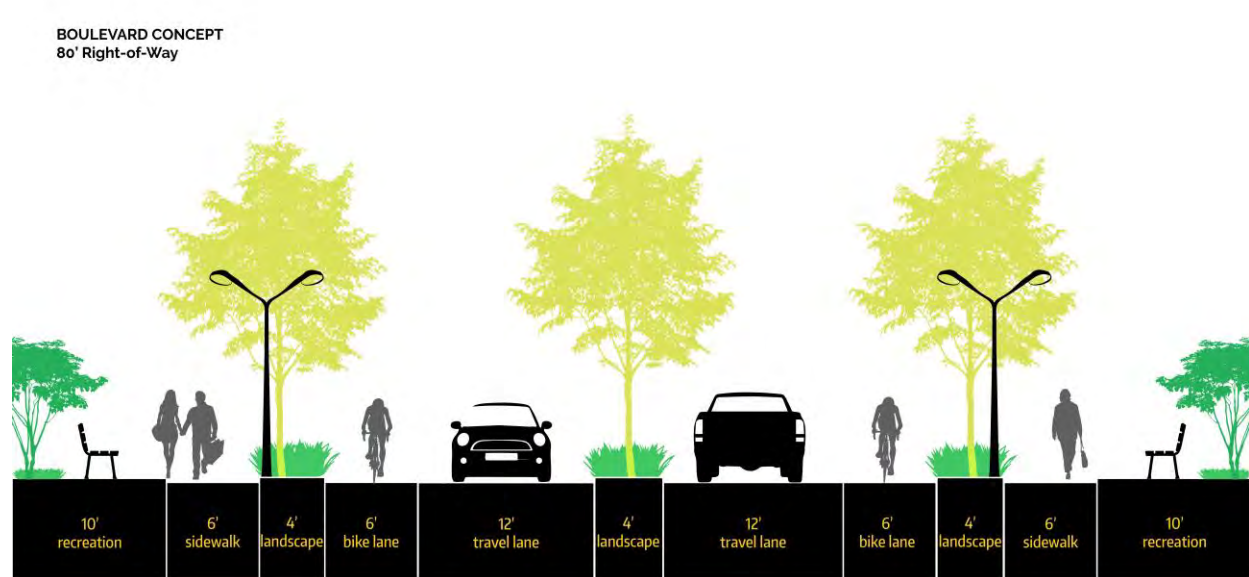
Source: Framework, 2020.

Exhibit 16. Green Street Concept #2 – 60' Right-of-Way



Source: Framework, 2020.

Exhibit 17. Boulevard Street Concept – 80' Right-of-Way



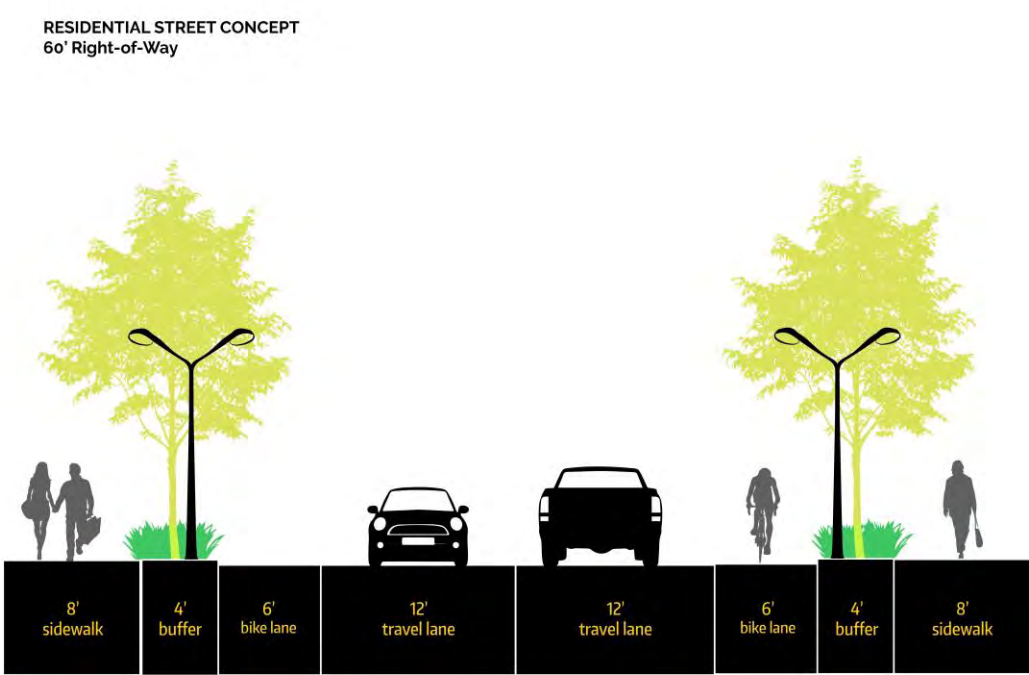
Source: Framework, 2020.

Residential Streets

Residential Streets serve residential neighborhoods and include vehicle travel with sidewalks and bike lanes. Many of the current streets identified as Residential Streets do not have curbs, gutter, or sidewalks, and there is no definition between the public right-of-way and private property. Parking occurs along the street edge, often in gravel shoulders and without a consistent pattern.

The residential street ROW in the LSDS area varies between 60' and 80' based on historical platting and construction. Exhibit 18 and Exhibit 19 show a standard residential street sections with either a 60' right-of-way or 80' right-of-way that include travel lanes, sidewalks, and bike lanes, and landscaping. In the future, bike lanes could be converted to on-street parking.

Exhibit 18. Residential Street Typology – 60' Right-of-Way



Source: Framework, 2020.

Exhibit 19. Residential Street Typology - 80' Right-of-Way



Source: Framework, 2020.

Alleys and Plazas

Alleys are encouraged to provide secondary access to properties, de-emphasize parking lots, and to promote continuous building frontages. Alleys would meet Public Works engineering standards. Along Occidental, a special Plaza designation identifies acceptable development frontages similar to Mixed Use Streets. The Plaza street concept is shared below.

Exhibit 20. Plaza Street Concept – 80’ Right-of-Way



Framework, 2020

Exhibit 21. Plaza Street Concept – 60’ Right-of-Way



Framework, 2020

Public Spaces

Green spaces and public open spaces should be located throughout the LSDS. This will result in an attractive district, provides gathering or recreation space, and is a desire of the community. The *Legacy Plan 2020*, Lakewood's plan for parks, recreation, and open space, aims to provide a public park or open space facility within a 10-minute walk of each resident.¹² Most of the LSDS is outside of a 10-minute walkshed from a City facility.¹³

Land consolidation would be needed to create a public park or open space in this area. While that opportunity is being explored, there are options to create linear open spaces and green corridors using available ROW. Such opportunities include:

- A linear park along Kendrick Street south of 111th Avenue SW that would bring green space to the heart of the district's residential area. This would better define the pedestrian connection between Lakewood Station and the Lakeview Hope Academy and facilitate the use of the playground there as a neighborhood park as envisioned in the *Legacy Plan 2020*.
- Green corridors with enhanced landscaping in areas with planned pedestrian and bicycle improvements such as 111th /112th Avenue SW.
- A pocket of open space in the alley near Occidental Street would be a plaza street as shown in Exhibit 22.
- A linear park connecting Lakewood Station to the Springbrook Area through improvements along 47th Avenue as envisioned in the *Legacy Plan 2020*.

¹² *Legacy Plan 2020*, Goal 2, Action Strategy 2.

¹³ *Legacy Plan 2020*, Figure 3-14.

Development standards will require street frontage improvements and landscaping that will increase greenery in the public realm. Mixed-use development standards will include provisions for public spaces and open spaces. Lakewood Landing will include a landscaped public plaza in its first phase to support community gathering and provide urban open space.



Conceptual Landscaped Public Plaza for Lakewood Landing

Source: Lee & Associates.2019.

Exhibit 22. Green Space Opportunities in the LSDS



Source: Framework, 2021.

Policies and Strategies

Policies

- Support transit-oriented development in the LSDS to capitalize on access to regional transit and proximity to Lakewood Station.
- Support and incentivize mixed-use development near Lakewood Station and in the commercial areas along Pacific Highway and Bridgeport Way where there is capacity for increased intensity of development.
- Integrate public open spaces into new development to create opportunities for public gathering and green spaces in the LSDS.
- Maintain land use and zoning designations that support high density residential development as part of mixed-use developments and residential redevelopment.
- Apply a hybrid-form based code to create an identifiable look and feel for the LSDS and to support safe pedestrian spaces.
- Adopt standards, uses and incentives that allow the subarea to transition from its present condition to its desired vision.
- Consider the designation of new Green Streets as opportunities arise, in coordination with public input.

Strategies

- Support the master planning of the Lakewood Landing site for mixed-use development so it will provide opportunities for high density housing, employment, business, and public space.
- Pursue opportunities to enhance landscaping or add linear parks within the public right-of-way to integrate green spaces and open spaces into the LSDS.
- Work through the *Legacy Plan 2020* to identify opportunities for public parks facilities within a 10-minute walk of subarea residents.
- Identify standards and guidelines that support transitional

parking arrangements so space used for parking can be transitioned to other uses as the area becomes less auto-dependent.

- Monitor the impact of the Lakewood Station District Overlay and amend the Plan as needed to improve outcomes.



Housing

Context

There are approximately 900 housing units in the LSDS. The majority (69%) of the units are renter-occupied.¹⁴ Vacancy rates are 13%. However, a 2018 study of the citywide Lakewood rental market showed lower vacancies for multifamily units of 3% or less.¹⁵ Market rate multifamily units had a 97% occupancy rate, while affordable units had a 99% occupancy rate citywide. Low multifamily vacancy had the effect of increasing the average costs of rent.

Market rate rental prices ranged from about \$900 to \$1,650 a month in fall 2018, depending on the unit size. Rates for affordable housing in Lakewood ranged from \$600 to \$1,800 a month. Although approximately 10% of renters within about a half mile of Lakewood Station receive housing subsidies, more than 50% experience cost burden, meaning they pay more than 30% of their household income on housing costs and earn less than 80% of the area median income.¹⁶

Median home values are about \$165,000, which is significantly lower than Lakewood's median value of \$292,000 or Pierce County's \$333,000. It is estimated that more than 30% of local homeowners experience cost-burden.

¹⁴ Estimated by ESRI Business Analyst (2019). This source makes estimates based on proportions of census blocks.

¹⁵ McCament & Rogers, 2018 *Lakewood Rental Market Survey*, October 2018.

¹⁶ City of Lakewood *Draft Consolidated Plan*, March 2020, p.13.

About 94% of subarea residents were employed with about half working in service industries, 18% in retail, and 10% in construction.¹⁷ Median income in the LSDS is \$34,673, which is lower than the \$52,002 median for Lakewood or the \$70,635 median for Pierce County.¹⁸ A household making the LSDS median income could afford to pay about \$900 in housing costs based on the housing burden definition.

Joint Base Lewis-McChord has a 99% housing occupancy rate for more than 5,000 housing units. The 70% of military households that live off-base receive a subsidy to offset rental or purchase costs. Typically, these households spend about \$1,500 to \$2,000 a month on housing.¹⁹ Despite its proximity, less than 10% of the base's workforce lived in Lakewood. This is attributed to a lack of suitable housing in good condition that is affordable to the base workforce.

Low rates of vacancy, rising rental costs, and a steady demand for off-base housing all indicate a need for low and moderate income housing in Lakewood. The LSDS is well suited to provide such housing because there is ample space for redevelopment.²⁰ Proximity to local and regional transit resources and areas of employment in the LSDS and Downtown would also allow households to manage transportation costs.

Existing housing stock in the LSDS already includes a variety of housing types, as shown in Exhibit 23.

Rental Housing Licensing Program

Lakewood has a rental housing licensing program to ensure that existing rental stock is safe and maintained and to prevent deterioration and blight. It is codified in Chapter 5.60 of the Lakewood Municipal Code (LMC). This program requires rental units more than ten years old to pass a compliance inspection every five years as a condition of receiving a business license. If deficiencies are found, the property owner must reimburse the City for tenant costs related to relocation or transitional housing. This provision prevents displacement and reduces the financial burden tenants may experience.

¹⁷ This information came from the ESRI Community Profile in February 2020, which uses 2019 data. Unless deemed essential, these top three employment sectors for subarea residents would be sensitive to the significant slowdown in economic activity due to the COVID-19 pandemic.

¹⁸ ESRI Community Profile, 2020.

¹⁹ 42% spend \$1,501 to \$2,000 on housing.

²⁰ In 2019 the density of existing development in the LSDS is only about 11.6 units per acre, far below the maximum density of 54 units per acre allowed under the zone.

Exhibit 23. Existing Residential Structures in the LSDS

Type of Residential Structures	Sum of Units
1 1/2 Story (finished space in attic or basement is the 1/2 story)	14
1 Story	68
2 Story	4
Apartment <= 3 Stories	20
Duplex 1 1/2 Story	2
Duplex One Story	20
Duplex Two Story	10
Home for the Elderly	5
Mixed Retail w/ Res Units	15
Multiple - Residential	662
Triplex One Story	3
Triplex Two Story	3
RV Park: 1 Apartment, 11 Mobile Homes & 26 RV Spaces	38
Grand Total	864

Sources: Pierce County Assessor, BERK 2020.

Housing Concepts

Lifecycle Housing

A variety of housing types is needed to meet the needs of people throughout their lifecycle. Housing needs also can vary based on factors such as household size, income, cultural traditions, interests, and preferences. All people need to have access to housing choices that are affordable to them. A family with children may have different housing needs and desires than an elderly couple or a military service person. Even within these categories, there are differences. Some families may desire a larger home to accommodate grandparents or other extended family members; other families may desire an efficient space close to civic amenities. An older couple may prefer a home with minimal maintenance to spend time on other pursuits; another couple may enjoy access to and outdoor space for gardening.

A variety of housing types allows people to find homes that meet their needs and desires at a level that they can afford. It contributes to quality of life and the flexibility to change their housing as needed. Lifecycle housing not only includes the single-family housing, apartment buildings present in the LSDS area, but also “missing middle” housing such as duplexes, triplexes, rowhouses, and smaller apartment buildings.

Exhibit 23 shows that the LSDS already includes a mix of housing types. During engagement, residents acknowledged the importance of having access to ground-related outdoor space, such as a private yard or semi-private shared courtyard. Many housing types can include ground-related outdoor space associated with individual units or incorporated as common space. They also can be built efficiently and affordably. By providing missing middle housing types, the LSDS will continue to provide important options for lifecycle housing, including small units, housing diversity, outdoor space, and affordability to those with moderate incomes.

Rowhouses

The LSDS encourages the development of rowhouses for residential areas. This type of development can successfully blend into the existing neighborhood as infill and redevelopment occurs because it is ground-related, has modest building heights, and is made of wood. It is also a compact development type that achieves densities of 30-40 units per acre. Units can be platted to offer homeownership opportunities that help local residents build wealth and equity as the neighborhood transforms and offers additional amenities.



Example Rowhouse Style Development

Sources: CGAArchitects.com, 2020 (left); Dwell.com, 2020 (middle); TheTownhouseGuy.ca, 2020 (right).

Rowhouses have minimal off-street parking requirements, which makes them functional and affordable for families. Rather than losing the ground floor to parking, they get additional ground floor living space. They are particularly well-suited for families who want the convenience of a yard, but the advantages of a more compact, walkable neighborhood, and access to transit. Where lots can be consolidated, rowhouses can scale into perimeter block housing with access to an interior common space. Parking for rowhouses is provided in collective lots on the development site and can be reconfigured or redeveloped as the area becomes less auto-dependent.

Apartments and Mixed-Use Residential Development

Apartments, whether stand-alone stacked-flats or part of a mixed-use development, are allowed in the LSDS commercial zones located along Pacific Highway and Bridgeport Way. Placing high intensity residential development in commercial areas gives residents easy access to goods, services, and entertainment. The highest density residential development is identified for the Transit-Oriented Commercial zone due to its proximity to Lakewood Station.

Larger lot sizes near the station also support larger scale development options that improve the financial feasibility of mixed-use or apartment development. Lakewood Landing is a good example of a master planned development utilizing a large available lot to build a residential development with a mix of retail and entertainment uses. Mixed-use developments like this provide housing, make an efficient use of land, and bring in amenities that reduce auto-dependence.

Mobile Home Park

The study area also contains the Oaknoll Trailer Park, located near the SR 512 interchange, but west of Pacific Highway/South Tacoma Way. Oaknoll rents about two dozen spaces that accommodate mobile homes and RVs. Some of these units are offered for rent and others are individually owned and lease a space only. Currently a non-conforming use in the C2 zone, state law protects the park from removal based on its non-conforming status.²¹



Oaknoll Trailer Park

Source: Google Earth, 2020.

The City could consider adding incentives to improve the trailer park while retaining it as a source of affordable housing. This may include incentives for landscaping and private streetscape or allowing for tiny homes on wheels to replace standard RV or obsolete manufactured homes (e.g. units built before 1976). State law allows tiny house villages in mobile home parks.²²

Affordability

There is sufficient land capacity to replace existing housing units as well as absorb additional units as redevelopment occurs in the LSDS. Supplementary policies and programs to support existing residents as the area transitions could reduce involuntary displacement. Other communities have used notice of intent to sell ordinances, right of first refusal incentives²³, and housing navigator programs²⁴ to support neighborhoods in transition. A citywide just cause eviction protection ordinance

²¹ State law indicates that cities and counties may not order the removal or phased elimination of an existing manufactured housing community because of it being a nonconforming use. Cities also cannot prohibit the entry or require the removal of a manufactured/mobile home, park model, or recreational vehicle authorized in a manufactured housing community because the community is a nonconforming use. (See RCW 35.63.160-161, RCW 35A.63.145-146, and RCW 36.70.493.)

²² State Law (SB 5383) also expanded the subdivision statute (RCW 58.17.040(5)) to allow the creation of tiny house villages and stops cities from prohibiting tiny houses in manufactured/mobile home parks.

²³ Notice of intent to sell or right of first refusal programs provide opportunities for existing tenants to reserve units when the property they live in is redeveloped.

²⁴ Housing navigator programs provide customized assistance to households in redevelopment areas.

would target reductions in arbitrary evictions.²⁵ A policy review of these types of measures, in consideration of existing policies and programs, is a recommended strategy for the LSDS.

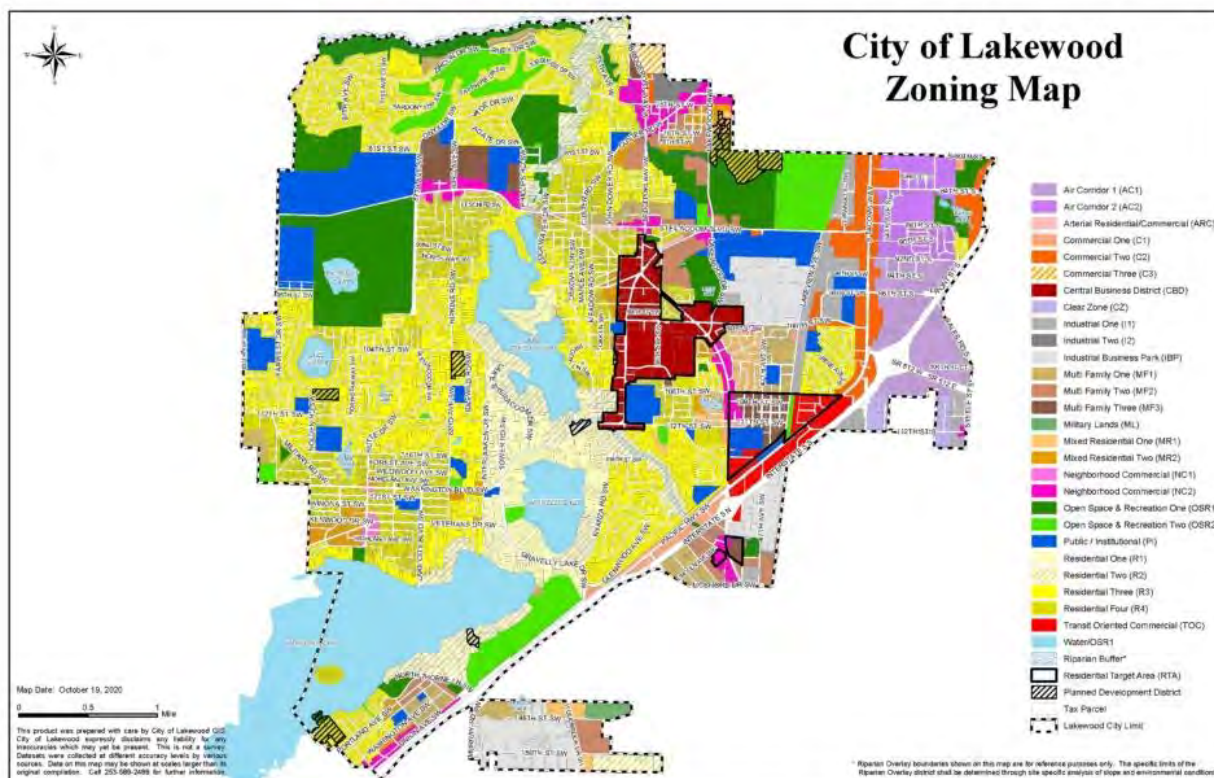
The City's Housing Incentives Program encourages the development of housing for people regardless of economic means.²⁶ Inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing are described in LMC Chapter 18A.90. Incentives are available to support the development of rental housing in all zones that allow it.²⁷ Those who create units affordable to households with very low incomes receive a bonus market rate unit or one and a half bonus market rates units for each unit affordable to households with extremely low incomes. Density bonuses are capped as a percentage of the base zoning district. This includes a 20% base density increase in MF3, a 15% increase in NC2, and a 25% increase in the TOC zone. Modifications in zone development standards such as coverage, parking, and height are allowed for projects participating in the housing incentives program. There is also a reduction in permitting fees.

Lakewood also has a multifamily property tax exemption, which exempts some types of new housing from paying ad valorem property taxes. Currently, most of the LSDS is within one of the residential target areas where the exemption may be applied. The current boundary is outlined in black as a Residential Target Area in Exhibit 24. This offers an incentive to support the types of housing envisioned for the subarea.

²⁵ Adoption would require consideration of citywide applicability and impacts.

²⁶ Lakewood Municipal Code 18A.090

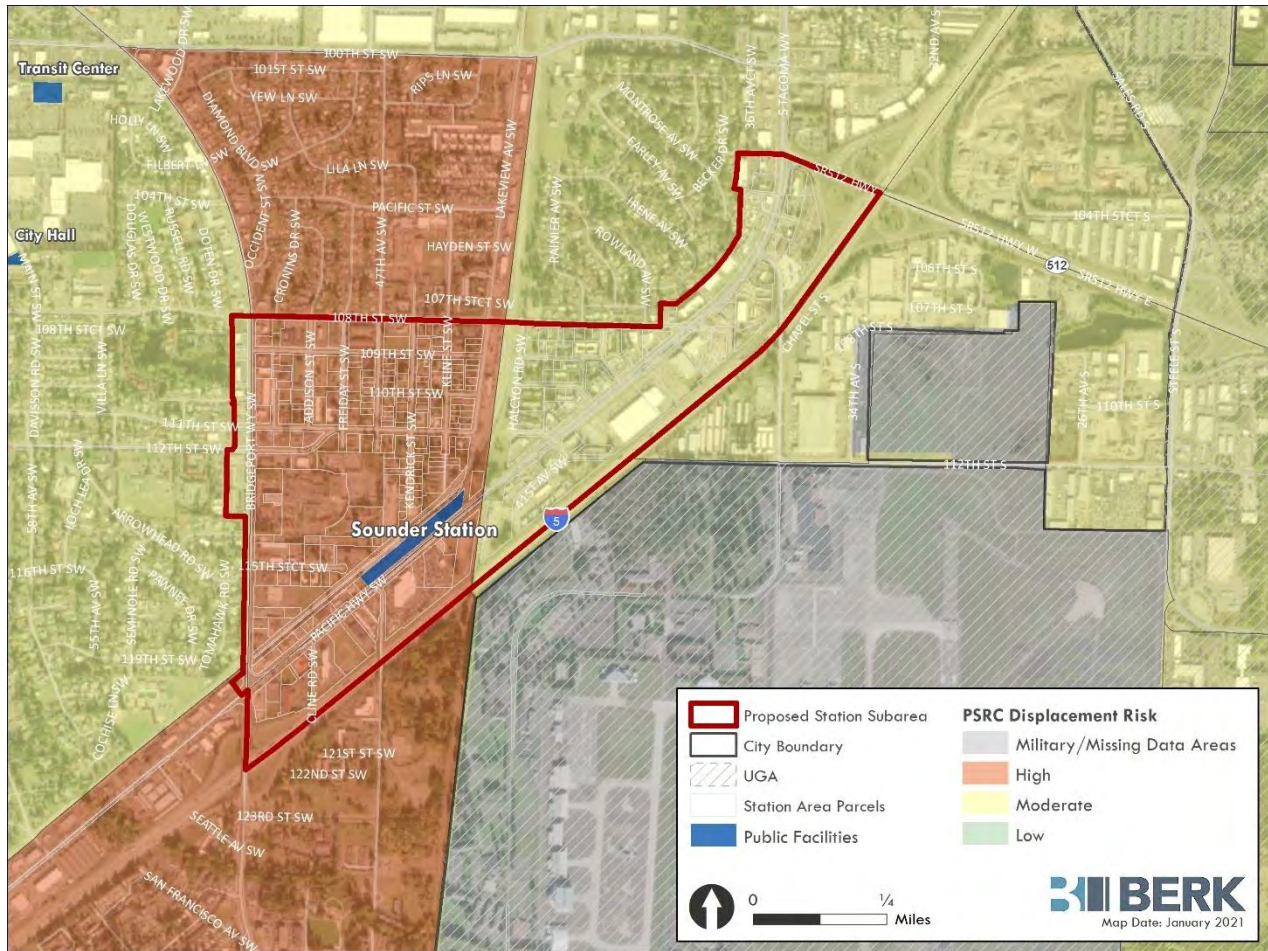
²⁷ With the exception of the construction of one single-family dwelling on one lot. 18A.090.030.

Exhibit 24. Residential Target Areas

Source: City of Lakewood, 2020.

Preventing Displacement

Current residents in the LSDS may be vulnerable to displacement with redevelopment in this area due to high rates of cost burden, low household incomes, and other factors. Exhibit 25 shows high and moderate risks of displacement in the subarea based on an analysis done by the Puget Sound Regional Council in 2017. Existing low density housing is anticipated to be replaced through small or moderate-scale redevelopment projects over the next 20 years that will increase the number of available units in the area. The focus of this plan is support additional options for low and moderate income housing, as well as a variety of housing types, is one way to prevent economic displacement.

Exhibit 25. Estimated Displacement Risk

Source: Puget Sound Regional Council, 2017; BERK, 2021.

The City of Lakewood leads or participates in plans and programs to support, create, and maintain affordable housing options for its residents. In addition, the policies and strategies in the Housing section of this plan aim to incentivize new affordable housing creation, support increased rates of homeownership, and assist residents at risk of displacement.

The following **programs support the creation of new affordable housing** in the City and study area:

- Housing Incentive Program (LMC 18.90) provides inclusionary density bonuses, development standard flexibility, and fee reductions for affordable housing development.
- Multi-family Property Tax Exemption (MFTE) gives a tax break for affordable housing development in most of the Lakewood Station District.

- Community Development Block Group (CDBG)/HOME funds from the federal government are used to support affordable housing development.
- Affordable Housing Initiative (2060) and Homeless Housing Act (2163) Programs that are joint with Pierce County and distribute funds to support affordable housing and homelessness.
- SHB 1406 Program applies a portion of the sales and use tax to use for supportive and affordable housing.

The following **programs support residents** in obtaining and maintaining high quality housing they can afford:

- Down payment assistance program by City of Lakewood and Pierce County.
- Pierce County property tax exemption for seniors and disabled.
- On-going partnership with Habitat for Humanity.
- Rental Housing Safety Program: ensures all housing units meet a consistent, minimum level of safety for tenants.

Policies and Strategies

Policies

- Encourage a variety of housing types to ensure choice for current and future residents, workers, military families, and to strengthen commercial areas.
- Support housing affordable to current LSDS residents though preservation where possible, as well as redevelopment activities.
- Incentivize the development of market rate and affordable housing options for households with incomes at 60-120% of the area median income to preserve affordability in the LSDS and avoid displacement of existing residents.
- Encourage homeownership options that allow local residents to invest in the community to gain equity and wealth.
- Attract a mix of large and small builders that can provide new housing options at a variety of scales and levels of

affordability.

Strategies

- Allow the highest density housing in mixed-use areas and in the Transit-Oriented Commercial zone where the availability of amenities within walking distance will encourage non-motorized transportation alternatives.
- Encourage and support missing middle housing types such as duplexes, triplexes, and small-scale multifamily as infill development in the residential area to improve housing choice and preserve affordability.
- Support the development of ground-related rowhouses as an affordable home ownership option for LSDS residents.
- Support the development of housing that meets the needs of military households.
- Partner with affordable housing organizations to support the expansion of subsidized housing in the LSDS by building new projects or by purchasing and rehabilitating existing housing.
- Work with the Washington Homeownership Center to find opportunities for investment in the LSDS.
- Consider expansions of existing affordable housing incentive programs and new affordability incentives in the LSDS, including the extension of the applicability area for the multi-family property tax exemption (MFTE).
- Consider programs to support residents in the LSDS as the area redevelops and reduce involuntary displacement such as a housing navigator program, a notice of intent to sell ordinance, just cause eviction protections, and right of first refusal incentives.
- Develop community benefit agreements with large investors in the LSDS.
- Provide information and support to help investors navigate affordable housing financing programs.
- Identify partners to support funding and a program that will rehabilitate existing homes such as a home repair program.



Economic Development, Business, & Employment

The LSDS is an area ripe for new investment due to its location near I-5 and the availability of local and regional transit options. Proximity to Joint Base Lewis-McChord (JBLM) provides a unique and regional market opportunity. A 2018 JBLM Regional Economic Impact Analysis shows that the base has a \$9.2 billion impact on the regional economy.²⁸ The LSDS also has the opportunity to become a health care hub with proximity to Madigan Hospital on JBLM as well as to St. Clare Hospital and Western State Hospital. Business development in the subarea could attract a greater portion of regional investment to Lakewood.

Context

The LSDS supports over 170 businesses and nearly 2,300 employees.²⁹ These numbers grow to over 640 businesses and 6,550 employees within the half mile extended study area.

²⁸ South Sound Military Partnership and University of Washington Tacoma, 2018.

²⁹ Data in this paragraph comes from ESRI Business Summary, 2019.

Retail and services are the primary businesses sectors. Services, including lodging, health care, automotive, legal, and other service industries comprise nearly half of the businesses in the subarea (46%) and employ 65% of the workers. Retail businesses, including restaurants and the sales of merchandise, gas stations, food, and other goods, comprise 29% of the businesses in the subarea and employ 22% of the workers.

Local businesses and small businesses serve the LSDS by providing opportunities for shopping, employment, and entrepreneurship. Stakeholders and public participants acknowledged the importance of small and local businesses in the LSDS visioning survey. Some provide service in Korean or Spanish to support the local community and create spaces of belonging. As the area transitions and grows, measures that prevent residential displacement and support small business will help avoid business displacement by maintaining local customer base and providing resources for continued business development.

Jobs in this area provide moderate incomes: about 44% provide an annual income above \$40,000 a year, approximately half of the median household income for Pierce County. Almost one-fifth (18%) of the workers in the subarea earned under \$15,000 a year.

The LSDS is part of the federally designated Lakeview/Kendrick Street Opportunity Zone. The opportunity zone includes two census tracts. These tracks overlap with portions of the LSDS including the Bridgeport Way interchange with I-5, St. Clare Hospital, the Sounder Station, the proposed Lakewood Landing site, and a portion of the residential area. Opportunity zones were created by the 2017 Tax Cuts and Jobs Act with the intention of supporting economic development and employment in distressed communities.³⁰ The program allows investors to defer capital gains tax for up to nine years by investing their gains in a Qualified Opportunity Zone. The federal program is funded through 2026 and supports redevelopment in the LSDS.



Local businesses serve residents and provide entrepreneurship opportunities

Source: El Mercado Latino

³⁰ Department of Commerce, 2020.

<https://www.commerce.wa.gov/growing-the-economy/opportunity-zones/>

Policies and Strategies

Policies

- Support medical office and commercial office opportunities.
- Encourage mixed-use development to expand opportunities for business and employment in the LSDS.
- Support small business development to build on the diverse offering of goods and services to residents and workers in the LSDS.

Strategies

- Promote St. Clare Hospital and satellite medical office and supportive commercial uses. Connect the residential neighborhood to the hospital with improved pedestrian and linear park opportunities.
- Support the master planning of the Lakewood Landing site for mixed-use development to expand opportunities for employment and retail development. See related Land Use policy.
- Partner with developers to identify projects eligible for participation in the Lakeview/Kendrick Street Opportunity Zone.
- Provide resources for entrepreneurs and small businesses, such as grant/loan programs, to support and recruit small, local businesses in the LSDS.
- Encourage micro-retail, or businesses that cater to specific demographic segments of the community, with business development resources and support available in other languages and flexibility in the design of cultural spaces.
- Invest in infrastructure and civic amenities consistent with this plan to attract business owners and investors.



Transportation

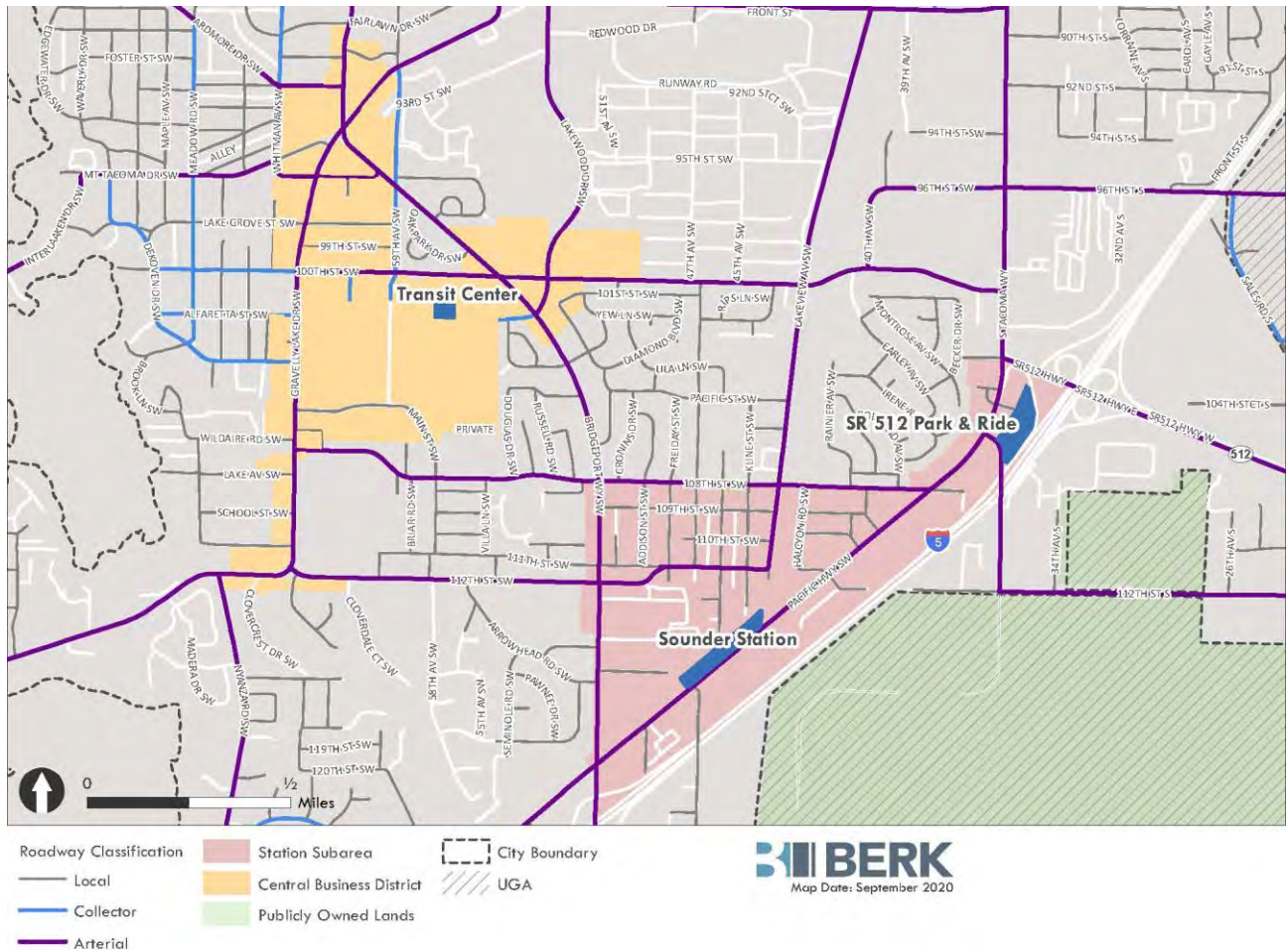
Context

The LSDS is a predominantly auto-oriented environment framed by access to I-5 to the north at the SR 512 interchange and at the Bridgeport Way interchange to the south. It is also served by major arterials such as Pacific Highway SW, Bridgeport Way, and 108th Avenue SW and a network of local access streets (see Exhibit 26).

There are two key regional transit facilities: Lakewood Station and SR 512 Park & Ride. Both Lakewood Station and SR 512 Park & Ride provide vehicle parking for transit users. The Downtown Transit Center is only about a mile away from the study area along Bridgeport Way. Enhanced streetscapes could provide better bike and pedestrian accessibility to transit resources within and nearby the LSDS.

The subarea is bisected by an existing rail line on which the Sounder and freight trains operate. While the rail line is an important regional transit and freight corridor, it has also created an environment with few roadway crossing opportunities which can affect vehicle operations and bicycle and pedestrian mobility.

Marked bicycle lanes are located in the LSDS on Pacific Highway and 108th Street SW. While sidewalks are generally located on all major streets in the project area, the residential area lacks sidewalks on most roadways. In addition to adding sidewalks and bike lanes, there is an opportunity to enhance existing street facilities. Adding landscaping that buffers non-motorized travelers from automobile traffic flows could improve safety and comfort.

Exhibit 26. Transportation Features in the LSDS and Vicinity

Sources: Fehr & Peers, 2020; Pierce County, 2020.

Level of Service

The transportation element of the Comprehensive Plan provides policy direction to address local and regional mobility. The transportation element acknowledges the increase of traffic congestion within Lakewood and seeks to mitigate it by developing a balanced multimodal system that effectively moves people, goods, and services without compromising community character. The Plan specifically calls for the incorporation of non-motorized facilities, enhanced illumination, and other pedestrian amenities into new development designs.

The City uses PM peak hour average delay to evaluate traffic operations level of service (LOS) at its intersections. The transportation element designates LOS guidelines for the city's

arterial streets and intersections. Within the study area, the City sets a LOS standard of LOS D³¹ during the weekday PM peak hour at all arterial street intersections. However, according to Policy T-20.5, the City may allow minor street stop-controlled intersections to operate below that LOS standard if those instances are thoroughly analyzed from an operational and safety perspective.

Redevelopment in the LSDS is expected to increase roadway volumes that will impact the movement of automobiles, freight, and transit. The areas where LOS may be affected, without mitigation interventions (mitigations are discussed on the following pages), include the following intersections:

- Pacific Highway / Sharondale Street SW – Increase in delay, LOS F to LOS F
- Pacific Highway / Bridgeport Way – from LOS D to LOS E
- Bridgeport Way / SB I-5 Ramp – from LOS D to LOS E

Improvements

Planned Improvements

The Six-Year Comprehensive Transportation Improvement Program (TIP) for 2021-2026 was approved by the Lakewood City Council in June 2020. This document outlines short and long term road projects, including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays, throughout the city. The following projects within the LSDS are identified in the TIP as long term improvements:

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study.
- Kendrick Street SW from 111th Street SW to 108th



111th and 112th Street Corridors

Source: City of Lakewood.

³¹ LOS D is defined in the Lakewood Comprehensive Plan as a level of traffic that is approaching unstable flow. Travel speed and freedom to maneuver are somewhat restricted, with average delays of 25 to 40 seconds per vehicle at signalized intersections. Small increases in traffic flow can cause operational difficulties at this level.

Street SW – sidewalks, street lighting, bicycle facilities.

- 112th/111th – Bridgeport to Kendrick – sidewalks, bicycle facilities, street lighting.
- 108th – Bridgeport Way to Pacific Highway – roadway patching and overlay.

Sound Transit and Pierce Transit have also identified improvements for the LSDS in their plans. The Sound Transit System Access Fund awarded funds in 2019 to support non-motorized access to Lakewood Station. These include a variety of improvements to support pedestrian and bicycle access on 11th Street SW and 112th Street SW. At this time there is no specific plan for a pedestrian crossing of Pacific Highway to connect Lakewood Station to the future Lakewood Landing site, but options should be considered. Pierce Transit's *Destination 2040* plan identifies two new Bus Rapid Transit (BRT) routes that will bring additional transit service to the subarea. A new line in 2026 would connect to Downtown Tacoma. In 2030 a BRT line would replace an existing transit line connecting Lakewood to Puyallup's South Hill.

General Mitigation Measures

Managing demand for auto travel is an important part of mitigating the traffic congestion impacts.

Transportation demand management (TDM) strategies could include subsidies or discounts for non-auto travel, education and assistance to help travelers identify non-auto commute options, rideshare and ridematch promotion, and local incentive and reward programs.



Transit near employment helps reduce auto demand

Source: City of Lakewood.

Washington state Commute Trip Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip reduction plans and work toward meeting their mode share targets through internal programs and monitoring.

The City of Lakewood included policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not

affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote transportation demand management (TDM) strategies. With a robust TDM program in place, it is expected that actual trip generation in the Study Area could be lowered beyond the levels analyzed as part of the Subarea Plan.

Specific Mitigation Measures

The City could make signal timing improvements to increase the efficiency of impacted intersections and roadways in the subarea. Exhibit 27 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right-of-way acquisition or widening, rather only changes to signal timing. The exception is intersection of Pacific Highway and Sharondale Street, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better.

Exhibit 27. Proposed Mitigation Measures

INTERSECTION	IMPROVEMENT
Pacific Highway / Sharondale Street	Install signal using existing channelization and signal length consistent with adjacent intersections
Pacific Highway / Bridgeport Way	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.
Bridgeport Way / SB I-5 Ramp	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.

Source: Fehr & Peers, 2020.

With implementation of the potential signal timing mitigation measures outlined in the previous section and future improvements at the Bridgeport Way/I-5 ramps, the magnitude of the impacts could be mitigated such that auto, freight, and transit are not significantly impacted.

Street Standards

As noted in Land Use & Urban Design, new street standards are an important tool in the transformation of the LSDS according to its vision. New development brings the opportunity to modify frontages to improve safety and comfort for pedestrians and bicyclists. It also provides opportunities to increase green space and open space. By adopting new street standards, the LSDS will improve travel conditions across all modes.

The City of Lakewood hopes to offset the costs of complying with street standards. It will identify programs and funding sources aimed at sharing or minimizing the cost of street improvements in the LSDS.

Policies and Strategies

Policies

- Provide multi-modal transportation options to improve the flow of people and goods in the LSDS and to reduce auto-dependence.
- Support pedestrians in the LSDS with sidewalk and infrastructure improvements that enhance safety and provide a more comfortable travel experience.
- Enhance non-motorized connections to Lakewood Station and Pierce Transit facilities to encourage the use of regional and local transit.
- Connect the LSDS to Downtown with enhanced pedestrian and bicycle facilities.
- Publicly invest in the development and construction of streetscapes to support redevelopment in the LSDS.
- Consider options for crossing Pacific Highway to provide a safe and efficient linkage between Lakewood Station and the Lakewood Landing development site.

Strategies

- Apply a hybrid form-based code to support improved streetscapes that accommodate pedestrian travel.
- Add signalization improvements at Pacific Highway and Sharondale Street to prevent traffic delays and at Bridgeport Way and the I-5 northbound/southbound off ramps to prevent queuing that extends onto the freeway.
- Work with Sound Transit and Pierce Transit on the timing and location of non-motorized improvements to avoid gaps and overlaps in the system.
- Require street frontage improvements that enhance walkability and support economically viable and quality residential development.
- Identify funds, funding mechanisms, grants, and programs that allow the City of Lakewood to develop and build streetscapes in the LSDS.



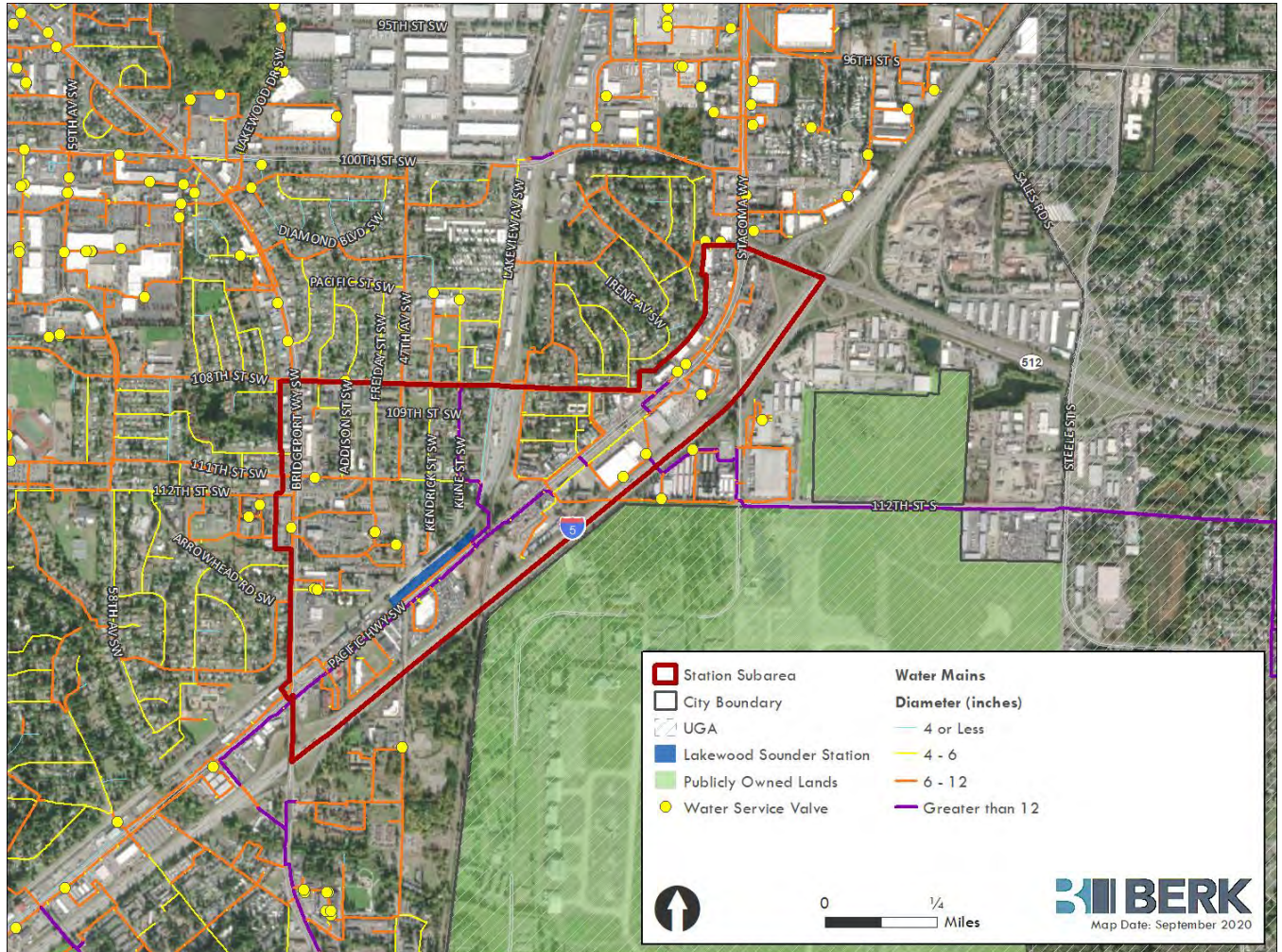
Utilities & Public Services

A variety of providers serve the LSDS with utility infrastructure and public services. Infrastructure updates and capacity changes related to the subarea plan will need to be coordinated with these providers. This should be done through participation in the updates of long-range system plans as well as shorter term capital facility improvement plans and project specific planning and permitting.

Context

Water

Water is provided by the Lakewood Water District and the subarea is fully served. As shown in Exhibit 28, the largest water mains primarily run along Pacific Highway, but a main larger than 12 inches runs roughly under Kline Street SW into the residential area. There is also a main larger than 12 inches that runs into the subarea from the southeast side of I-5. Mains 6 to 12 inches in size are well distributed throughout the subarea, running under several streets, including Bridgeport Way. These midsized mains serve St. Clare Hospital and the existing commercial development in the subarea.

Exhibit 28. Water Utility Infrastructure in LDS and Surrounding Area

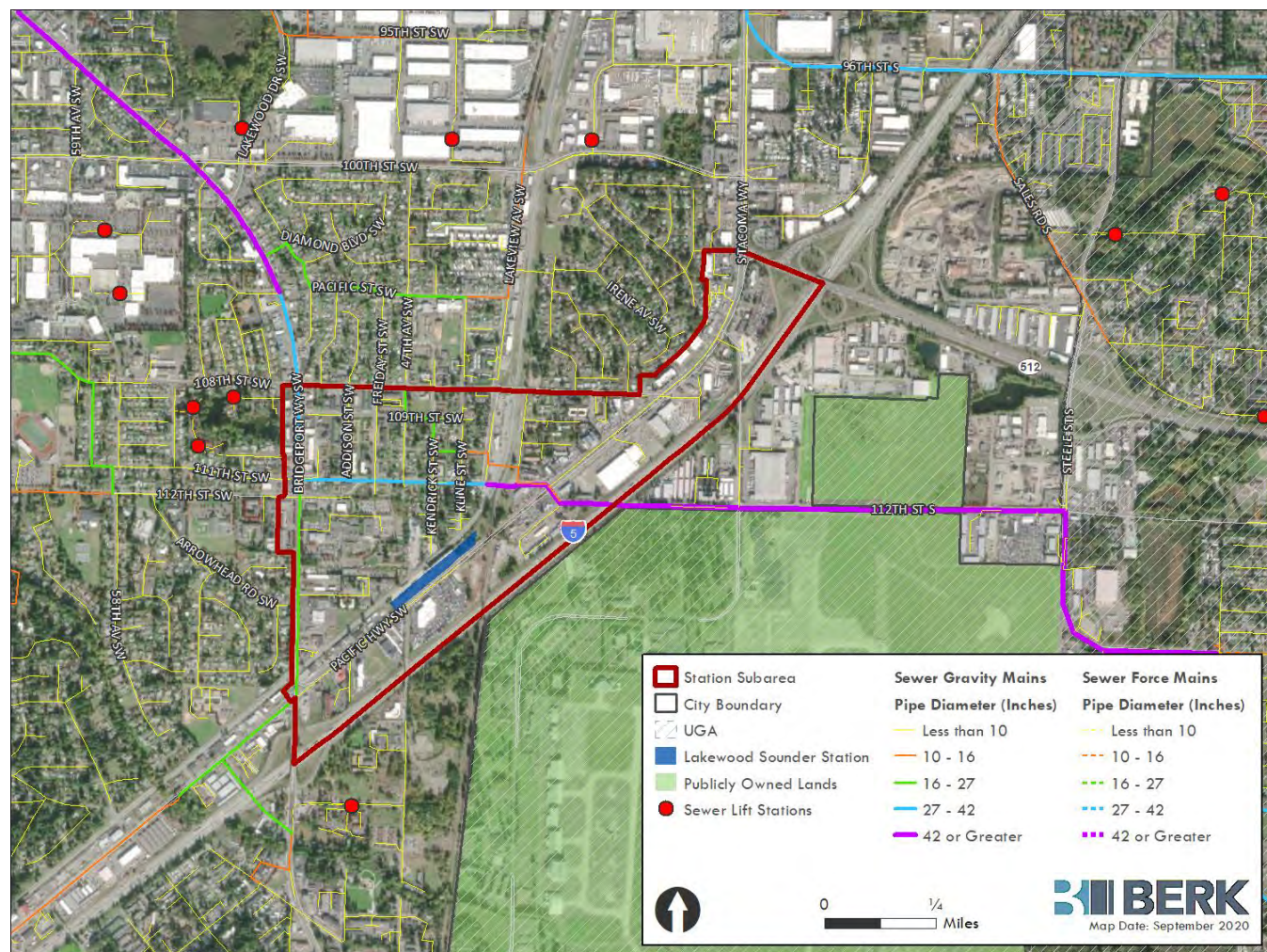
Sources: BERK, 2020; Pierce County Assessor, 2020.

The Lakewood Water District initiated a 50-year repair and replacement plan in 2014 to replace 181 miles of aging water mains. Some replacements have already occurred in the subarea, including replacements along Bridgeport Way. The repair and replacement plan focuses on the replacement of facilities that are nearing the end of their useful life and does not account for upgrades or extensions to support new development. District policy requires the developer to pay for system improvements related to new development. Depending on the location and intensity of new development in the subarea, this may include water main upgrades or line extensions to provide additional capacity or fire flow. Coordination of upgrades with the 50-year repair and replacement program is recommended and could provide cost and timing efficiency for redevelopment projects in the subarea.

Wastewater

Wastewater service in Lakewood is provided by Pierce County Planning and Public Works. Most of the sewer pipes shown in Exhibit 29 were installed in the 1980s (some later) and are primarily 8-inch diameter PVC pipes, which have a 100-year lifetime. Larger interceptors are typically made of concrete, which has a service life of 50 to 100 years. Generally, the sewer infrastructure is considered in good condition with plenty of remaining service life and no current need for large scale line replacements or upgrades.

Exhibit 29. Wastewater Infrastructure in the LSDS and Surrounding Area



Sources: BERK, 2020; Pierce County Public Works, 2020; Pierce County Assessor, 2020.

The most recent system plan is the 2010 Unified Sewer Plan, adopted in 2012. Pierce County was scheduled to begin an update in late 2020. The Plan accounted for zoning densities up to 54 units per acre, which should be adequate for most development within the LSDS. Master planned development at

Lakewood Landing may occur at a higher intensity and require wastewater system capacity upgrades. The update of the Unified Sewer Plan provides an opportunity to plan for future development at Lakewood Landing.

Pierce County coordinates quarterly with the City of Lakewood to discuss upcoming and future projects. A Sewer Improvement Plan (SIP) was adopted in September 2019, addressing capital facility planning from 2020-2040 and identifying funding for the next six years of capital facility improvements. Reviewing the SIP in light of the LSDS and Lakewood Landing development is recommended.

Energy

Lakeview Light and Power provides electrical service to the subarea and Puget Sound Energy provides natural gas service. Maps showing the locations of the lines providing natural gas service are not available and most facilities are located underground. Puget Sound Energy is working on an updated Integrated Resource Plan to ensure continued delivery of service to its existing service areas in the region and to address opportunities for enhanced conservation and reduced environmental impacts.

Lakeview Light and Power is working on increased system capacity and flexibility. There is a five-year capital budget for repowering the substation at Pacific Highway and Bridgeport Way. Improvements to the substation will allow Lakeview Light and Power to rebalance loads as needed to accommodate fluctuations in usage. Increased demand for electrical service should be possible to accommodate, but specific plans may be needed to address any heavy commercial users and electric vehicle charging capacity. Line infrastructure is located mostly above ground within the subarea. As redevelopment occurs, taller buildings will likely require utility undergrounding for safety, but Lakewood does not have a policy or regulation requiring undergrounding with new development.

Emergency Response

Emergency response services in Lakewood are provided by West Pierce Fire and Rescue and the Lakewood Police Department.

The Lakewood Police Department is one of the largest departments in the state. It operates six patrol districts as well as specialized units in criminal investigation, K9, traffic, and marine response. The subarea is part of the Pacific Patrol District, which is authorized for up to 28 sworn personnel.³² All patrol districts, including the Pacific District, are operated out of the police station in the civic campus in Downtown, less than a mile from the subarea.

Emergency management functions for Lakewood are part of the Police Department. The mission of emergency management is to assist with mitigation, preparation, response, and recovery from natural disasters and other community emergencies. This includes a variety of trainings and informational materials to help residents and businesses prepare for major emergency events.

West Pierce Fire and Rescue provides fire and emergency medical response to Lakewood, University Place, and Steilacoom. Station 20 is located within the subarea along Pacific Highway. Services offered by West Pierce Fire and Rescue include emergency medical response, fire suppression, fire prevention, and community education. They also sponsor Community Emergency Response Team (CERT) trainings that prepare residents to be the first line of response in their neighborhoods following a disaster prior to the arrival of emergency responders.



Emergency response is readily available

Source: West Pierce Fire and Rescue.

³² Lakewood Police Department 2018 Annual Report, updated in 2019.

Policies and Strategies

Policies

- Ensure there is adequate utilities infrastructure to support redevelopment of the LSDS according to the vision.
- Consider future agreements to support infrastructure development in the LSDS such as development agreements, latecomers agreements, fee assessments, improvement districts, and other tools.

Strategies

- Coordinate with public service and utility providers during the update and development of long-range plans and system improvement plans to ensure there is adequate support for anticipated growth and development of the LSDS subarea.
- Work with public service and utility providers to ensure that capital improvement plans include the projects necessary to support development in the LSDS subarea.



Implementation Plan

The following table identifies responsibilities for implementing the vision for the LSDS. It outlines the direction, responsible entity, and timeline for plan actions. The timelines include ongoing, short term (up to three years), midterm (three to five years), or long term (more than five years).

	Direction	Timeline	Department
Land Use & Design	Support the master planning of the Lakewood Landing site for mixed-use development so it will provide opportunities for high density housing, employment, business, and public space.	Ongoing	Community Development and Economic Development
	Pursue opportunities for enhancing landscaping or adding linear parks within the public right-of-way to integrate green spaces and open spaces into the LSDS.	Long term	Community Development and Parks and Recreation
	Work through the <i>Legacy Plan 2020</i> to identify opportunities for public parks facilities within a 10-minute walk of subarea residents.	Midterm	Parks and Recreation
	Identify standards and guidelines that support transitional parking arrangements so space used for parking can be transitioned to other uses as the area becomes less auto-dependent.	Short term	Community Development
	Monitor the impact of the Lakewood Station District Overlay and amend the Plan as needed to improve outcomes.	Ongoing	Community Development

	Direction	Timeline	Department
Housing	Allow the highest density housing in mixed-use areas and in the Transit-Oriented Commercial zone where the availability of amenities within walking distance will encourage non-motorized transportation alternatives.	Ongoing	Community Development
	Encourage and support missing middle housing types such as duplexes, triplexes, and small-scale multifamily as infill development in the residential area to improve housing choice and preserve affordability.	Ongoing	Community Development
	Support the development of ground-related rowhouses as an affordable home ownership option for LSDS residents.	Ongoing	Community Development
	Support the development of housing that meets the needs of military households.	Ongoing	Community Development
	Partner with affordable housing organizations to support the expansion of subsidized housing in the LSDS by building new projects or by purchasing and rehabilitating existing housing.	Ongoing	Community Development
	Work with the Washington Homeownership Center to find opportunities for investment in the LSDS.	Ongoing	Community Development
	Consider expansions of existing affordable housing incentive programs and new affordability incentives in the LSDS, including the extension of the applicability area for the multi-family property tax exemption.	Short term	Community Development
	Consider programs to support residents in the LSDS as the area redevelops and reduce involuntary displacement such as a housing navigator program, a notice of intent to sell ordinance, just cause eviction protections, and right of first refusal incentives.	Short term	Community Development
	Develop community benefit agreements with large investors in the LSDS.	Ongoing	Community Development
	Provide information and support to help investors navigate affordable housing financing programs.	Short term	Community Development and Economic Development

	Direction	Timeline	Department
Housing, continued	Identify partners to support funding and a program that will rehabilitate existing homes such as a home repair program.	Short term	Community Development
Economic Development, Business, & Employment	Promote St. Clare Hospital and satellite medical office and supportive commercial uses. Connect the residential neighborhood to the hospital with improved pedestrian and linear park opportunities.	Short term	Community Development and Economic Development
	Support the master planning of the Lakewood Landing site for mixed-use development to expand opportunities for employment and retail development. See <i>related Land Use policy</i> .	Ongoing	Community Development and Economic Development
	Partner with developers to identify projects eligible for participation in the Lakeview/Kendrick Street Opportunity Zone.	Midterm	Community Development and Economic Development
	Provide resources for entrepreneurs and small businesses, such as grant/loan programs, to support and recruit small, local businesses in the LSDS.	Ongoing	Economic Development
	Encourage micro-retail, or businesses that cater to specific demographic segments of the community, with business development resources and support available in other languages and flexibility in the design of cultural spaces	Ongoing	Economic Development
	Invest in infrastructure and civic amenities consistent with this Plan to attract business owners and investors.	Long term	Public Works
Transportation	Apply a hybrid form-based code to support improved streetscapes that accommodate pedestrian travel.	Ongoing	Community Development
	Add signalization improvements at Pacific Highway and Sharondale Street to prevent traffic delays, and at Bridgeport Way and the I-5 northbound/southbound off ramps to prevent queuing that extends onto the freeway.	Long term	Public Works
	Work with Sound Transit and Pierce Transit on the timing and location of non-motorized improvements to avoid gaps and overlaps in the system.	Ongoing	Public Works

	Direction	Timeline	Department
	Require street frontage improvements that enhance walkability and support economically viable and quality residential development.	Ongoing	Community Development and Public Works
	Publicly invest in the development and construction of streetscapes to support redevelopment in the LSDS.	Ongoing	Community Development and Public Works
Utilities	Coordinate with public service and utility providers during the update and development of long-range plans and system improvement plans to ensure there is adequate support for anticipated growth and development of the LSDS subarea.	Midterm	Public Works
	Work with public service and utility providers to ensure that capital improvement plans include the projects necessary to support development in the LSDS subarea.	Midterm	Public Works

LAKEWOOD STATION DISTRICT

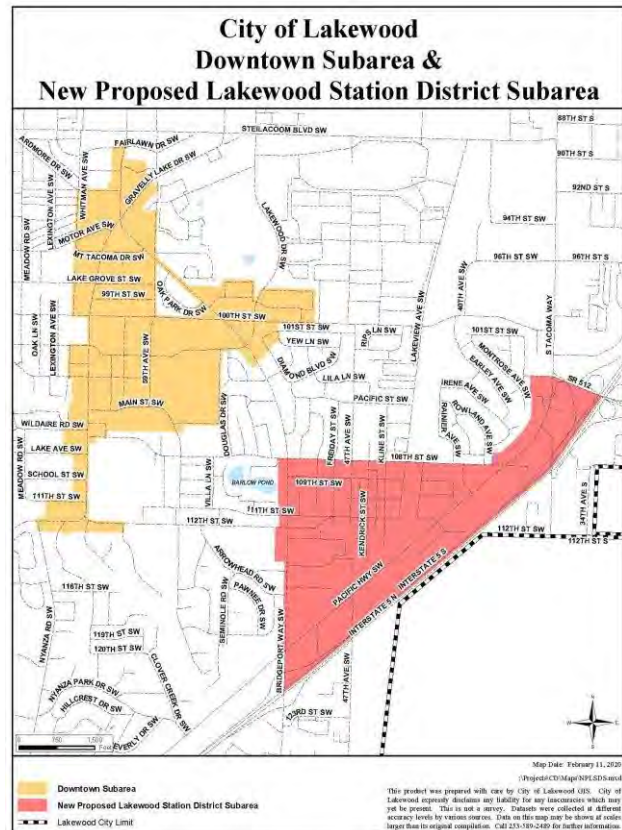
FORM-BASED CODE

Overview

The City of Lakewood is adopting a subarea plan for the Station District. The Station District is located southwest of Downtown and is centered around the Sounder Station. The City adopted a new subarea plan and hybrid form-based code for Downtown in the fall of 2018; the City intends to adopt a similar hybrid form-based code for the Station District that addresses the specific context of the Station District, including land use, streets and transportation, open space, and desired urban form. This framework will address how best to apply a hybrid form-based code to the Station District.

The draft code includes the following sections:

1. Station District
 - Purpose.
 - Vision and Objectives.
 - The Regulation Plan Map.
 - Definitions.
 - Relationship to Other Regulations.
2. Land Use and Zoning
 - Application within Station District.
 - Land Use.
 - District-Wide Development Standards.
3. Streets and Blocks
4. Site Design, Buildings, and Frontage
5. Landscaping, Open Space, and Green Infrastructure
 - Basic Standards.
 - Special Landscape Treatments.
 - Common and Open Space Standards.
 - Green Infrastructure.
6. Parking



Downtown and Station District Subareas

City of Lakewood, 2019

Title 18C

STATION DISTRICT DEVELOPMENT CODE

Chapters:

18C.100	Station District
18C.200	Land Use and Zoning
18C.300	Streets and Blocks
18C.400	Site Design, Buildings, and Frontage
18C.500	Landscaping, Open Space, and Green Infrastructure
18C.600	Parking

Chapter 18C.100

STATION DISTRICT

Sections:

18C.100.100	Background.
18C.100.105	Purpose.
18C.100.110	Vision and Objectives.
18C.100.120	The Regulating Plan Map.
18C.100.130	Definitions.
18C.100.140	Relationship to Other Regulations.

18C.100.100 Background. The Station Area District is defined by its location along major transportation corridors including Pacific Highway, I-5, and the railroad (See Figure 18C.100-1). These transportation corridors limit street, bike, and pedestrian connections. However, a pedestrian bridge connects the Sounder Station to the neighborhood north of the railroad tracks. Land use in the district varies from large auto-oriented uses to multi-family and single-family housing. Major arterial streets in the district include Pacific Highway and Bridgeport Way. Within the confines of the Regulating Plan Maps, there are several dead-end streets. Some of these streets were the result of the construction of I-5 which began in the late 1950's and culminated in November 1968. Other dead-end streets are associated with the current railroad right-of-way which existed well before the establishment of many of the residential subdivisions within the area, and, finally, prior to Lakewood's incorporation, inadequate county subdivision code regulation that did not take into account urban development.

18C.100.105 Purpose.

The purpose of the Station District is to provide for coordinated and high-quality development including new housing, commercial development, parks and open space, and multi-modal transportation infrastructure within the Subarea. The Station District Hybrid Form-Based Code Overlay is intended to guide private development and changes in the public right-of-way to support the goals and implementation of the Station District Subarea Plan.

18C.100-1. Station District



Framework, 2020

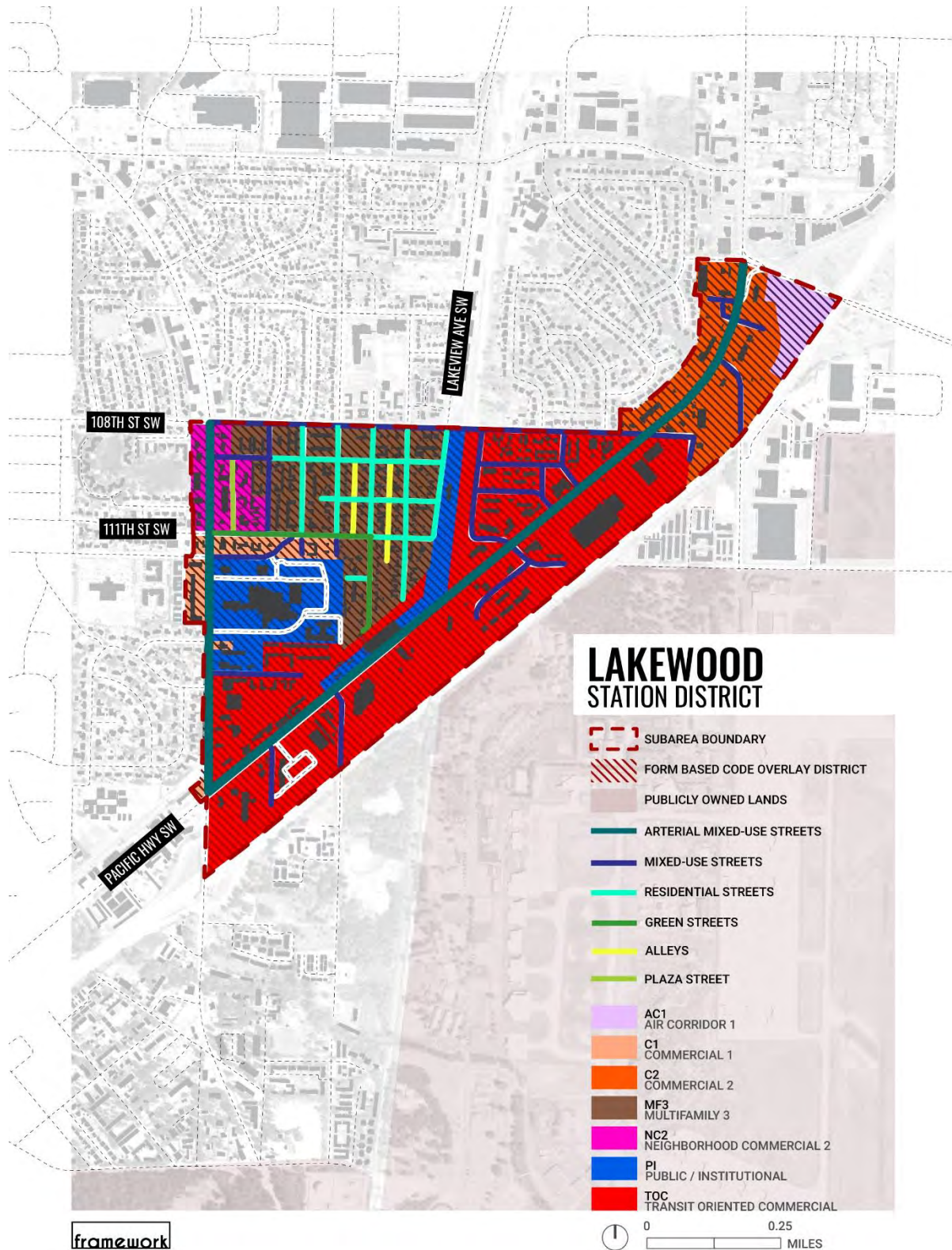
18C.100.110 Vision and Objective.

The Lakewood Station District is a multi-modal commuter hub of Lakewood and is the southern terminus of Sound Transit's commuter rail service. The Lakewood Station District provides an amenity-rich, transit-oriented development node surrounding the Lakewood Station. This District offers a mixture of intensive land uses and activities supportive of direct regional transportation access via the commuter rail station and I-5. The District implements development standards to foster a high quality, pedestrian-oriented urban environment, including incentives to encourage a dense mix of commercial and medical office, regional and local retail, services and hospitality, and high-density residential uses offering ownership and rental housing opportunities, all supported by direct regional transportation access.

18C.100.120 The Regulating Plan Map.

The regulating plan translates the community vision into a map. The regulating plan designates the locations, subdistricts, and streets that are intended to embody specific physical characteristics. It specifies the location and applicability of specific design treatments and identifies where they are required. The regulating plan works in tandem with the development standards, tables, and figures in this chapter, the rest of Title 18A and the LMC to define the shape, size, and location of streets through connections, infill blocks, buildings, and landscaping.

18C.120-1 Regulating Plan Map with Zoning



Framework, 2020

18C.120-2 Regulating Plan Map – Street Typologies



Framework, 2020

18C.100.130 Definitions.

Definitions in this chapter shall be consistent with LMC 18A.10.180, Definitions. Interpretations of certain terms and phrases shall be consistent with LMC 18A.10.040, Rules of Construction.

18C.100.140 Relationship to Other Regulations.

In the case of a conflict between the regulations in Chapter 18C.100 LMC, Station District, and the rest of the Lakewood Municipal Code, the regulations in Chapter 18C.100 LMC, Station District Form-Based Code Overlay, shall control, with the exception of LMC Title 15, which will govern issues of public health and safety.

Chapter 18C.200

LAND USE AND ZONING

Sections:

18C.200.210 Application within Lakewood Station District.

18C.200.220 Land Uses.

18C.200.230 District-Wide Development Standards.

18C.200.210 Application within Station District.

The Lakewood Station District Subarea (LSDS) has several base zones that are applied as shown in the map below. The zones are described as follows:

A. The Multifamily 3 (MF3) zoning district is intended to integrate urban, high-density, multi-story housing in close proximity to a principal or minor arterial, with commercial/residential districts. The MF 3 zoning districts are predominantly located adjacent to land zoned NC2, CBD, or SD.

B. The Neighborhood Commercial 2 (NC2) zoning district is intended to foster a sense of urban community in Lakewood. The district provides for a concentrated mix of activities, including residential, retail, office, and local services, which may serve the surrounding neighborhood or may serve more than one (1) neighborhood and attract people from other areas.

C. The Commercial 1 (C1) and Commercial 2 (C2) zoning districts promote employment, services, retail, and business uses serving and linking neighborhoods to Lakewood's major transportation networks. The geographic relationship of the corridors to major road networks and their limited integration with adjacent neighborhoods promote employment, services, retail, and business/light industrial uses linked to access the major transportation networks. The C3 zoning district is distinguished by its arterial location and focus on "big-box" type uses which form an anchor for a large-scale commercial development.

D. The Transit-Oriented Commercial (TOC) zoning district is an interactive mixture of uses which focus on regional transportation networks while providing for urban design, people orientation, and connectivity between uses and transportation routes.

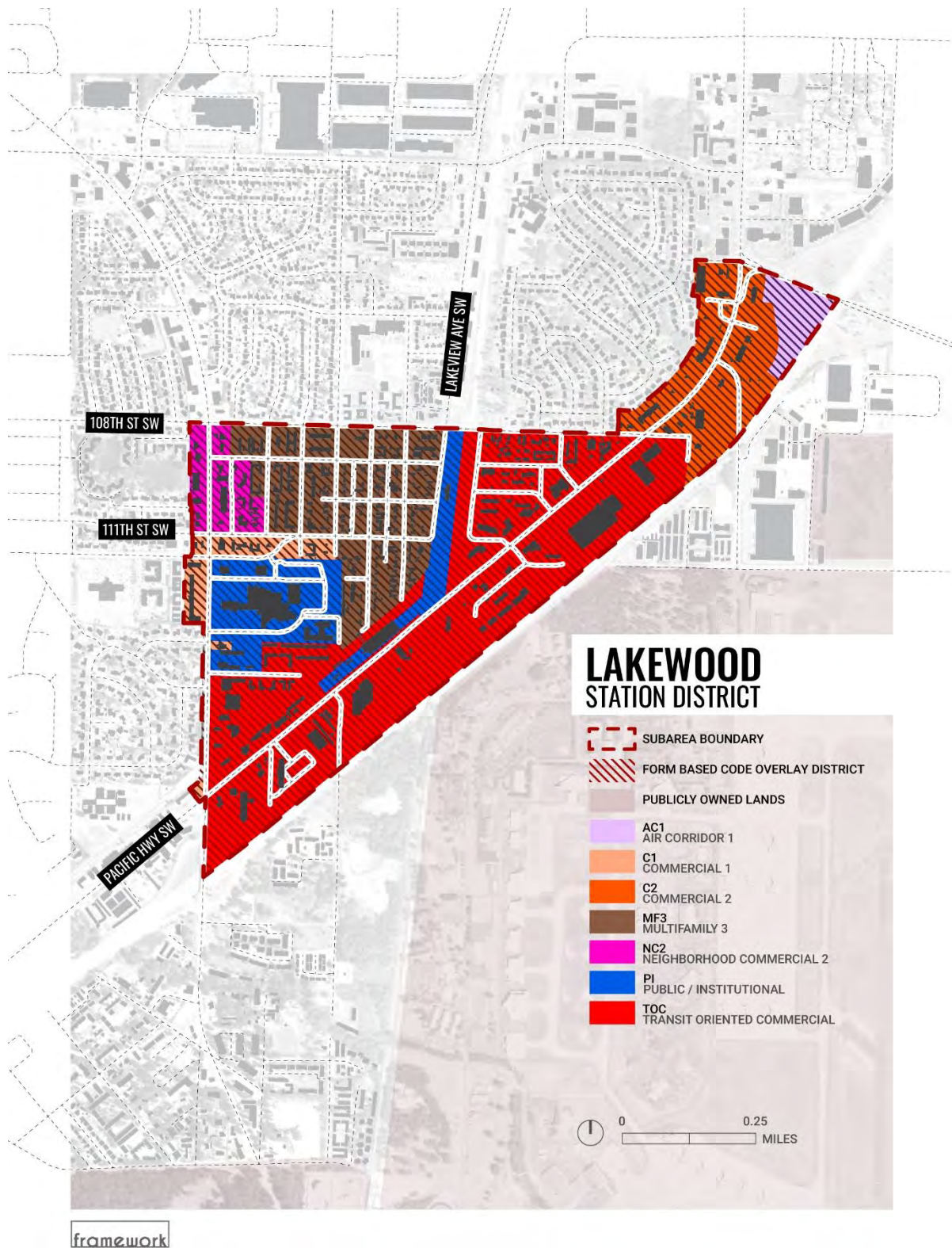
E. The Public/Institutional (PI) zoning district provides for moderate-scale and large-scale activities relating to the purpose of state and local governmental entities, except for military uses which are separately designated and zoned; special districts; and semi-public institutions providing

necessary public services. The designation allows for the specialized needs of providing public services to all areas of Lakewood.

F. The purpose of the Clear Zone (CZ), Air Corridor 1 (AC1), and Air Corridor 2 (AC2) zoning districts is to promote land use and development that is compatible with the aircraft noise and accident potential associated with the proximity to McChord Field, which is a part of the Joint Base Lewis-McChord (JBLM). McChord Field hosts three C-17 airlift squadrons. The potential risk to life and property from hazards associated with military aircraft operations necessitates control of the intensity, type, and design of land uses within the air corridor.

G. The Open Space and Recreation 1 (OSR1) and Open Space and Recreation 2 (OSR2) zoning districts provide for open space and public or semi-public recreational activities throughout the City.

18C.200-1. Station District Zoning Map



Framework, 2020

18C.200.220 Land Use.

A. Allowed and Prohibited Uses. All residential uses, civic uses, and commercial uses and their respective uses and their related accessory uses as defined in Chapter 18A.40 LMC are permitted within the Station District except for those prohibited uses listed below. Permitted uses are subject to the approval of all required development permits. Uses identified as prohibited below that legally existed prior to the adoption of this code are considered non-conforming.

1. *Prohibited uses in the AC1 zoning district:*
 - a. None.
2. *Prohibited uses in the MF3 zoning district:*
 - a. Building & landscape materials;
 - b. Convenience store;
 - c. General retail.
3. *Prohibited uses in the NC2 zoning district:*
 - a. Auto and vehicle sales/rental;
 - b. Building & landscape materials;
 - c. Solid waste transfer station;
 - d. Vehicle services, major repair/body work.
4. *Prohibited uses in the TOC zoning district:*
 - a. Accessory caretaker's unit.
5. *Prohibited uses in the C2 zoning district:*
 - a. Auto and vehicle sales/rental;
 - b. Manufacturing, assembling, & packaging, medium;
 - c. Manufacturing, assembling, & packaging, heavy;
 - d. Metal products fabrication, machine and welding;
 - e. Mobile home, RV, and boat sales;
 - f. Recycling facility – processing facility;
 - g. Repair service - equipment, large appliances.
 - h. Vehicle services, major repair/body work;
6. *Prohibited uses in the C1 zoning district:*
 - a. Auto and vehicle sales/rental;
 - b. Building & landscape materials;
 - c. Construction/heavy equipment sales and rental;

- d. Enhanced services facility;
- e. Equipment rental;
- f. Fuel dealer;
- g. Furniture/fixtures manufacturing, cabinet shop;
- h. Laundry, dry cleaning plant;
- i. Manufacturing, assembling, & packaging, medium;
- j. Manufacturing, assembling, & packaging, heavy;
- k. Mobile home, RV, and boat sales;
- l. Outdoor storage;
- m. Pawnbrokers and secondhand dealers;
- n. Recycling facility – processing facility;
- o. Repair service - equipment, large appliances;
- p. Secondhand stores;
- q. Storage, personal storage facility;
- r. Type 4 group home;
- s. Type 5 group home;
- t. Vehicle services, major repair/body work;
- u. Vehicle storage;
- v. Warehouse;
- w. Warehouse retail;
- x. Wholesaling and distribution.

B. Allowed Use Conditions. The following uses are allowed, provided the conditions listed below are met:

1. Drive-through facilities; *provided*:

- a. Drive-through facilities are limited to one drive-through lane per establishment;
- b. Drive-through facilities must have a primary customer entrance and cannot provide customer service exclusively from a drive-through or walk-up window;
- c. Drive-through facilities shall be designed so that vehicles, while waiting in line to be served, will not block vehicle or pedestrian traffic in the right-of-way;

d. Drive-through facilities shall be appropriately and attractively screened from the public right-of-way:

ii. Drive-through lanes shall only be placed parallel to a road if separated by a distance of 30 feet, or if fully screened by a 15-foot landscape setback with a designed landscape berm (six feet high at center of berm in 15-foot landscape setback) or three-and-one-half-foot decorative masonry wall;

iii. Drive-through lanes oriented perpendicular to a public right-of-way shall include landscape screening to shield headlights from shining directly into an abutting or adjacent street right-of-way.

C. Conditionally Permitted Uses.

1. Pipelines.

2. Manufacturing, assembling and packaging, light is allowed in the Station District, but such uses shall be as part of mixed-use development with commercial, retail, or residential uses.

D. Permitted Uses in the MF3 Zone within the LSDS.

1. Rowhouses.

2. Townhouses.

3. Three-family residential, attached.

E. Permitted Uses in the MF3 Zone within the LSDS with a master plan approval.

1. Multifamily use, four (4) or more units,

2. Mixed uses, when consistent with the following:

a. The commercial use is only permitted on the ground floor of the development.

b. A minimum ratio of four (4) square feet of multifamily use to one (1) square foot of commercial use.

18C.200.230 District-Wide Development Standards.

Unless otherwise specifically modified by an adopted development agreement or master plan, in addition to the regulations and requirements contained in other sections of this title and LMC Title 15, the following property development standards apply to all land and buildings in the Station District:

A. *Density*. Maximum density is based on the standards in the base zoning districts.

B. *Lot Size*. There is no minimum established lot size for the Station District. Proposed uses and the applicable design standards in this chapter shall be used to establish the minimum lot size for a project.

C. *Lot Coverage.* There is no minimum established lot size for the Station District. Proposed uses and the applicable design standards in this chapter shall be used to establish the minimum lot size for a project.

- D. *Setbacks*. The minimum distance setbacks for the Station District shall be determined by frontage type in Chapter 18C.400 LMC, except where increased setbacks due to building/fire code requirements apply.
- E. *Building Height*. The maximum building height, not including any applicable height bonus, for the Station District shall be determined by the underlying zoning districts.
- F. *Large-scale Commercial Facilities*. Large-scale commercial facilities shall meet the additional requirements specified in LMC [18A.70.050\(J\)](#).
- G. *Design*. Design features shall be required as set forth in Chapter 18C.300 LMC, Streets and Blocks, and Chapter 18C.400 LMC, Site Design, Buildings, and Frontage, and the community design standards in LMC [18A.70.040](#).
- H. *Tree Preservation*. Significant tree identification and preservation and/or replacement shall be required as set forth in Chapter [18A.70](#) LMC, Article III, Tree Preservation.
- I. *Landscaping*. Landscaping shall be provided as set forth in Chapter 18C.500 LMC and Chapter [18A.70](#) LMC, Article II, Landscaping.
- J. *Parking*. Parking shall conform to the requirements of Chapter 18C.600 LMC and Chapter [18A.80](#) LMC, Parking.
- K. *Signs*. Signage shall conform to the requirements of Chapter [18A.100](#) LMC, Sign Regulations.

Chapter 18C.300

STREETS AND BLOCKS

Sections:

18C.300.310 Streets and Blocks.

18C.300 Streets and Blocks.

A. *Street Grid and Blocks.*

1. *Street Grid.* A circulation plan must be submitted for review by the City as part of any development permit in the Station District unless waived by the City upon finding the project will not impact circulation or the enhancement of the public street grid. New development and redevelopment must demonstrate the plan supports and accommodates the expansion of the public street grid to improve circulation for vehicles, pedestrians, and bicyclists.
2. *Block Size.* Block Size. The maximum block size is 400 feet and the maximum distance permitted between public streets. New public street alignments shall be consistent with the regulating plan map. The City may approve modifications to the street alignments and waive the 400-foot maximum block size to take advantage of existing travel corridors, the location of utilities, and required improvements.
3. *Private Streets.* Private streets shall only be permitted when the City has determined there is no public benefit for circulation in the Station District. All private streets must be constructed to public standards.
4. *Mid-Block Connections/Alleys.* A minimum 20-foot-wide mid-block connection (alley) shall be provided at the midpoint along each block face or every 200 feet. The mid-block connections shall be designed to accommodate service needs and for pedestrian use and be free from permanent obstructions.
5. *Street Sections.* The typical street sections provided below are the minimum requirements for the design of public streets. The City may approve modifications to the typical street section based on localized conditions and adjacent land uses. Modifications may include adding or removing on-street parking, wider sidewalks, loading zones, bicycle facilities, and transit accommodations.

6. *Block Development.* The minimum number of buildings per 400 feet of block frontage is four, or one building per 100 feet, to create variety in the streetscape experience and support human-scale design. A single building may meet this requirement through building design and architecture that visually appears as multiple buildings. The City may approve modifications to this requirement based on site-specific conditions, including parcel ownership and configuration.

B. Street Types.

1. *Mixed-Use Streets.* Mixed-use streets support a variety of activities and functions both in the public right-of-way and development along the street edges. Street level retail is permitted, but not required, and a wider range of building frontage types are permitted, including street-level residential and office uses. Mixed-use streets also require pedestrian-oriented design and requirements may vary based on the location within Station District.

2. *Arterial Mixed-Use Street.* Arterial mixed-use streets maintain the existing vehicle capacity and allow a range of land use and building frontage types and include surface parking lots.

3. *Residential Street.* Residential streets serve residential neighborhoods and include vehicle travel with sidewalks and bike lanes.

4. *Green Street.* Green streets include vehicle travel lanes, sidewalks, bike lanes, and open space for stormwater management, landscaping, and gathering space.

C. *Street Standards and Frontage Types.* The following street standards are the minimum required and modifications to the standards may be approved by the City upon finding that the modification is consistent with the vision and objectives in LMC 18C.100.110 , supports pedestrian-oriented design, and balances the need for traffic flow to minimize congestion.

18C.300-1. Street Standard and Frontage Types

Street type	Sidewalk width	Linear	Forecourt	Plaza	Landscape	Porch /Stoop /Terrace	Parking
Mixed-Use Street	10' minimum	P	P	P/R1	P	P	X
Arterial Mixed-Use Street	As determined by Public Works	P	P	P	P	P	P
Residential Street	8' minimum	X	P	X	P	P	X
Green Street	8' minimum	X	P	X	P	P	X
Plaza Street	12' minimum or shared street	P	P	P	P	X	X

P=permitted, X=prohibited, R=required

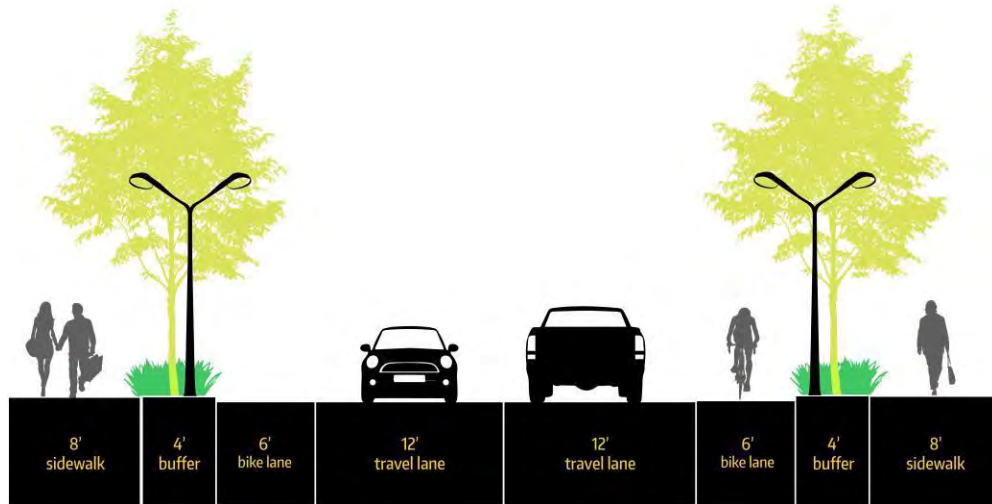
1 Required when on a corner lot.

D. *Street Sections.* The following street sections show the basic elements of the streetscape for each of the streets highlighted in Station District. The City may approve modifications to the typical cross sections based on site-specific conditions including adjacent land uses, traffic management, parking needs, and right-of-way constraints.

1. *Residential Street Typology.* Many of the streets identified as Residential Streets do not have curbs, gutter, or sidewalks and there is a lack of definition between the public right-of-way and private property. Parking occurs along the street edge often in gravel shoulders and without a consistent pattern. Established residential street ROW varies in the Station District. Figure 18C.300-2 below shows a standard residential street section with a 60' right-of-way that includes travel lanes, sidewalks, and bike lanes. In the future, bike lanes could be converted to on-street parking. Figure 18C.300-3 shows an 80' right-of-way with native plantings on either outer edge of the right-of-way.

18C.300-2. Residential Street Typology – 60' ROW

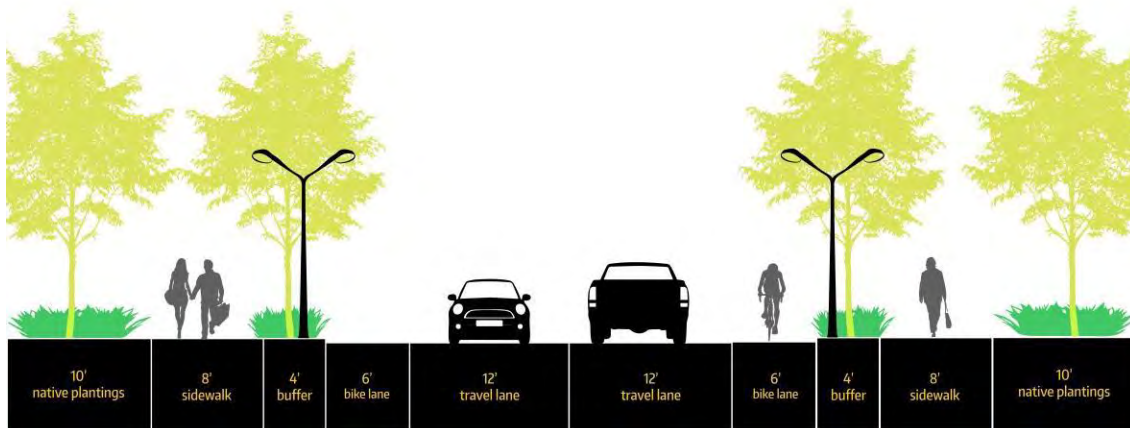
RESIDENTIAL STREET CONCEPT
60' Right-of-Way



Framework, 2020

18C.300-3. Residential Street Typology – 80' ROW

RESIDENTIAL STREET CONCEPT
80' Right-of-Way



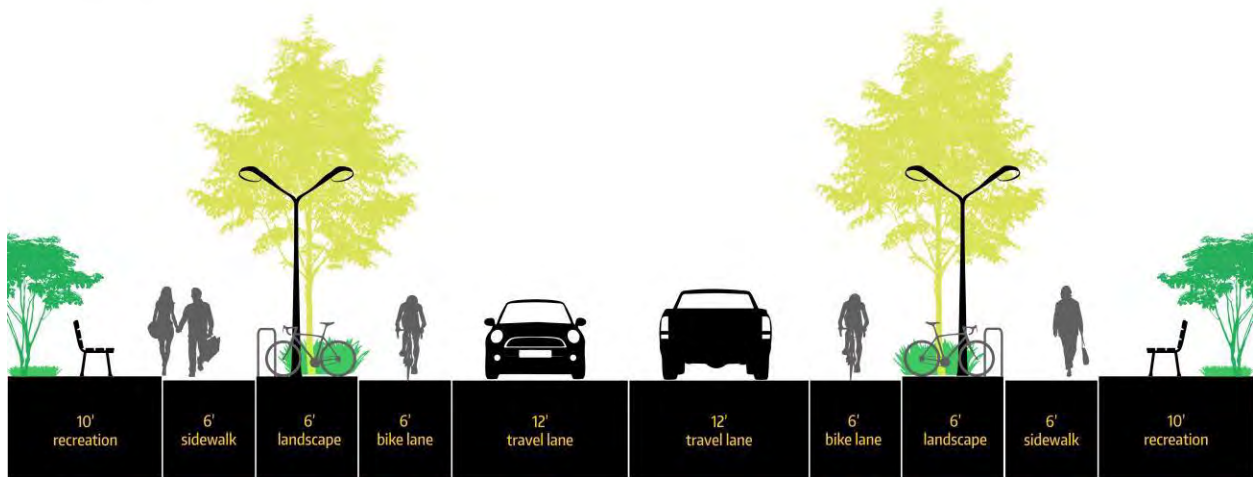
Framework, 2020

2. *Green Street Typology.* Kendrick Street SW connects to the Sounder Station to the South and to other neighborhoods to the north. The existing right-of-way width varies from between 60' and 80' and is an opportunity to integrate green street elements. The southern block of Kendrick

St SW appears to have been recently improved, so the following concepts focus on the blocks north of 111th St SW as shown in Figures 18C.300-4 through 6 below.

18C.300-4. Green Street Concept #1 – 80' Right-of-Way

GREEN STREET CONCEPT
80' Right-of-Way



Framework, 2020

18C.300-5. Boulevard Concept – 80' Right-of-Way

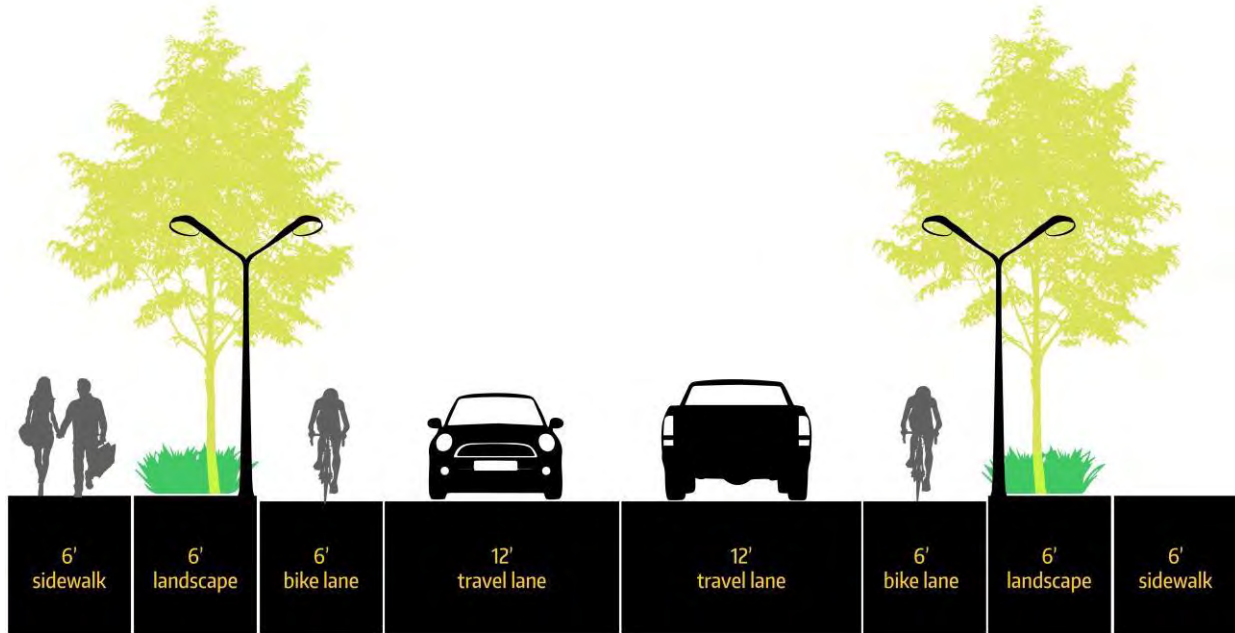
BOULEVARD CONCEPT
80' Right-of-Way



Framework, 2020

18C-300-6. Green Street Concept #2 – 60' Right-of-Way

GREEN STREET CONCEPT 60' Right-of-Way



Framework, 2020

E. *Plaza Street.* Plaza streets are located along commercial and mixed-use zoning with active uses and may vary in width. Plaza streets are designed primarily for people and activities while allowing for vehicle access to adjacent properties. Plaza streets may have dedicated sidewalks or use a shared street design with appropriate traffic calming design features. Plaza streets should include high-quality materials, integrated landscaping and trees with open space, street furniture such as places for people to sit, bike parking, lighting, public art opportunities, and a flexible design that can support a variety of events and activities such as times where the street may be temporary closed to vehicle traffic.

18C-300-7. Plaza Street Concept – 80' Right-of-Way

80' PLAZA STREET
CONCEPT



Framework, 2020

18C-300-8. Plaza Street Concept – 60' Right-of-Way

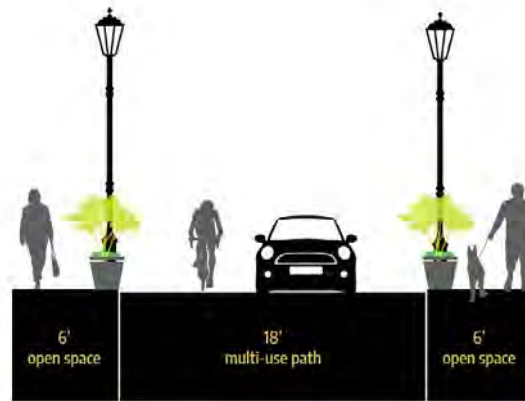
60' PLAZA STREET
CONCEPT



Framework, 2020

18C-300-8. Plaza Street Concept – 30' Right-of-Way

30' PLAZA STREET
CONCEPT



Framework, 2020

F. *Alleys.* Alleys are encouraged to provide secondary access to properties, de-emphasize parking lots, and to promote continuous building frontages. Alleys shall meet Public Works engineering standards.

G. *Weather Protection – Easements.* Weather protection or building overhangs that extend over public rights-of-way may be permitted by the Community Development Director subject to execution of an easement and requirements for maintenance by the property owner or developer.

1. Access to existing and future utilities within and under the street and boulevard must be maintained.
2. Freedom of movement of existing and future vehicular and pedestrian activity must not be restricted.
3. Weather protection depth and percentage coverage shall be consistent with LMC 18C.400.410(A) for frontage types.

4. All overhead weather protection shall be placed at a height that relates to architectural features of the building and adjacent storefront weather protection, while offering effective protection from weather. Weather protection shall have a minimum vertical clearance of eight feet, measured from the sidewalk, and should not be greater than 12 feet above the pedestrian sidewalk level.

5. The slope of the weather protection feature shall allow for proper drainage and self-cleaning action of rain and wind. Materials used should be durable and require minimum maintenance.

H. When Required. Street Frontages shall be implemented consistent with LMC 12.09.031.

Chapter 18C.400

SITE DESIGN, BUILDINGS, AND FRONTAGE

Sections:

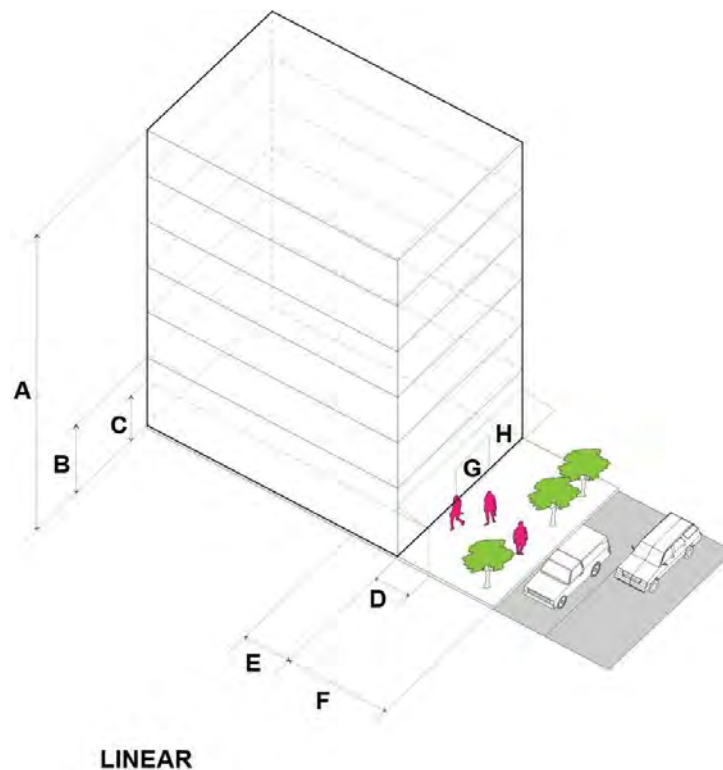
18C.400.410 Site Design, Buildings, and Frontage.

18C.400.410 Site Design, Buildings, and Frontage.

A. *Frontage Types.* The building frontage types below address the required standards for the relationship of buildings to the edge of the street and other site plan and design requirements. The permitted frontage types vary by street type as shown in LMC 18C.300.310(C). The building setback may be modified as approved by the City when necessary to expand the width of the right-of-way to accommodate the desired street design and cross section.

1. *Linear.* The linear building frontage has zero setback from the street edge and is the primary frontage type on retail streets and is also appropriate for land uses such as townhouses and row houses.

18C.400-1. Linear Frontage Type



Framework, 2020

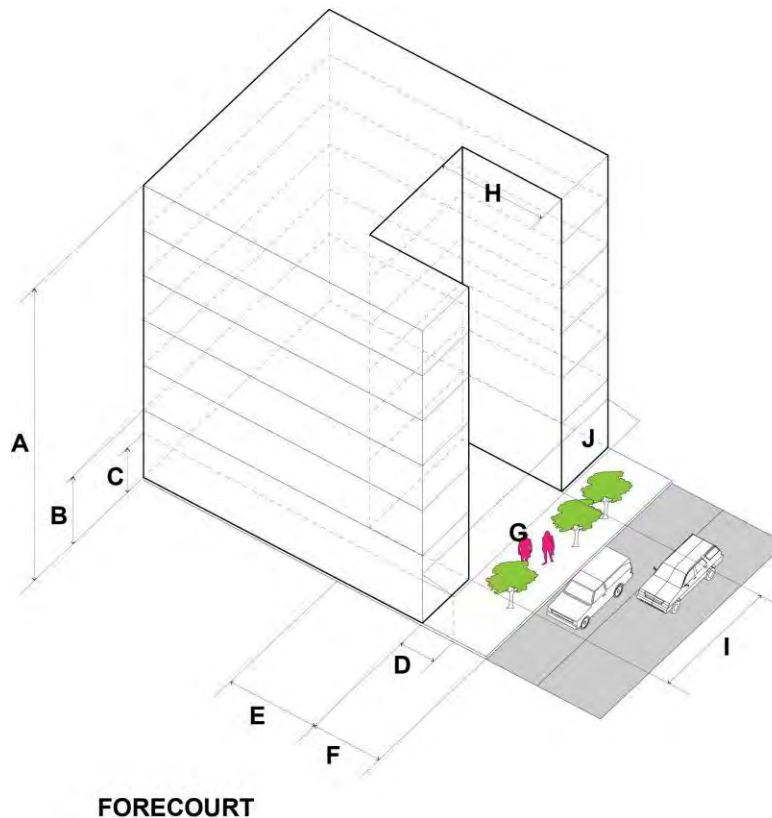
18C.400-2. Linear Frontage Standards

Standard		Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A	Building Height	Base Zone	Base Zone	n/a
B	First Floor Minimum Height	16'	16'	n/a
C	Weather Protection Height	10'	10'	n/a
D	Weather Protection Minimum Depth and Linear Frontage	10' Depth Weather protection required for minimum of 60% of frontage	10' Depth Weather protection required for minimum of 60% of frontage	n/a
E	Building Maximum Setback from Right-of-Way	10'	10'	n/a
F	Building Setback Minimum	0'	0'	n/a
G	Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	n/a
H	Window Transparency	A minimum 60% transparency required at street level between 30" and 8' for nonresidential uses	none	n/a

1 The maximum setback shall be up to 10 feet where the Public Works Engineering Director has determined property may be needed to accommodate the City's planned right-of-way at full build-out. Otherwise, the maximum setback is zero feet.

2. *Forecourt.* The forecourt building frontage type has an open area at the entrance along the street edge. This building type is applicable to a wide range of land use types and mixed-use development.

18C.400-3. Forecourt Frontage Type



Framework, 2020

18C.400-4. Forecourt Frontage Standards

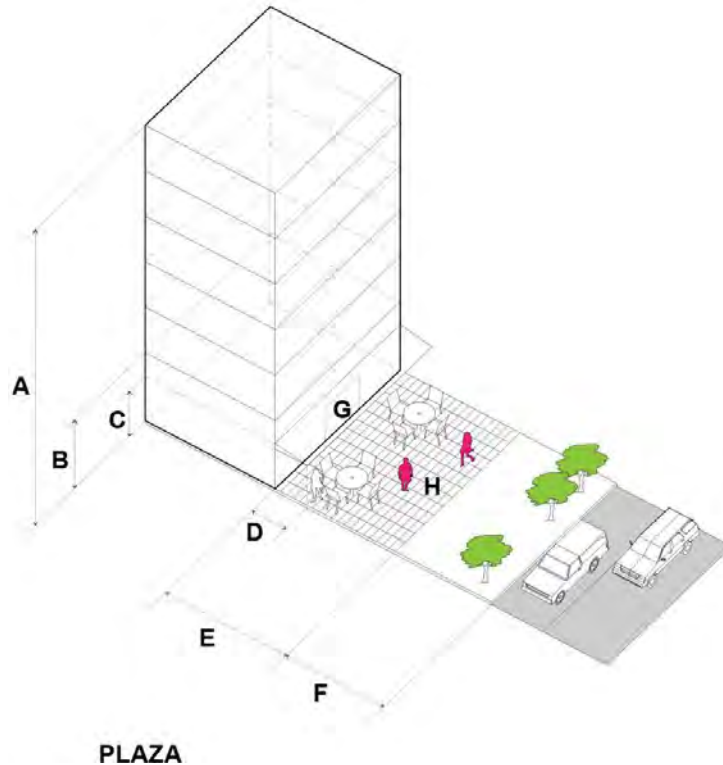
Standard	Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	Base Zone	Base Zone	Base Zone
B First Floor Minimum Height	16'	16'	n/a
C Weather Protection Height	10'	10'	n/a
D Weather Protection Minimum Depth and Forecourt Frontage	15' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	15' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	n/a
E Building Maximum Setback from Right-of-Way	10'	10'	20'
F Building Setback Minimum	0'	0'	10'
G Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required
H Forecourt Depth	10' minimum;	10' minimum;	10' minimum

Standard		Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
I	Forecourt Width	30' maximum	30' maximum	30' maximum
		10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum
J	Window Transparency	A minimum 60% transparency required at street level between 30" and 8' for nonresidential uses	none	none

1 The maximum setback shall be up to 10 feet where the Public Works Engineering Director has determined property may be needed to accommodate the City's planned right-of-way at full build-out. Otherwise, the maximum setback is zero feet.

3. *Plaza*. The plaza frontage type includes a pedestrian-oriented plaza between the building and the street edge. The plaza frontage type is applicable to retail and dining uses and can support activities such as outdoor dining, public art displays, seating, entertainment, and events. The plaza must be designed to support human activity and support a safe and inviting streetscape environment.

18C.400-5. Plaza Frontage Type



Framework, 2020

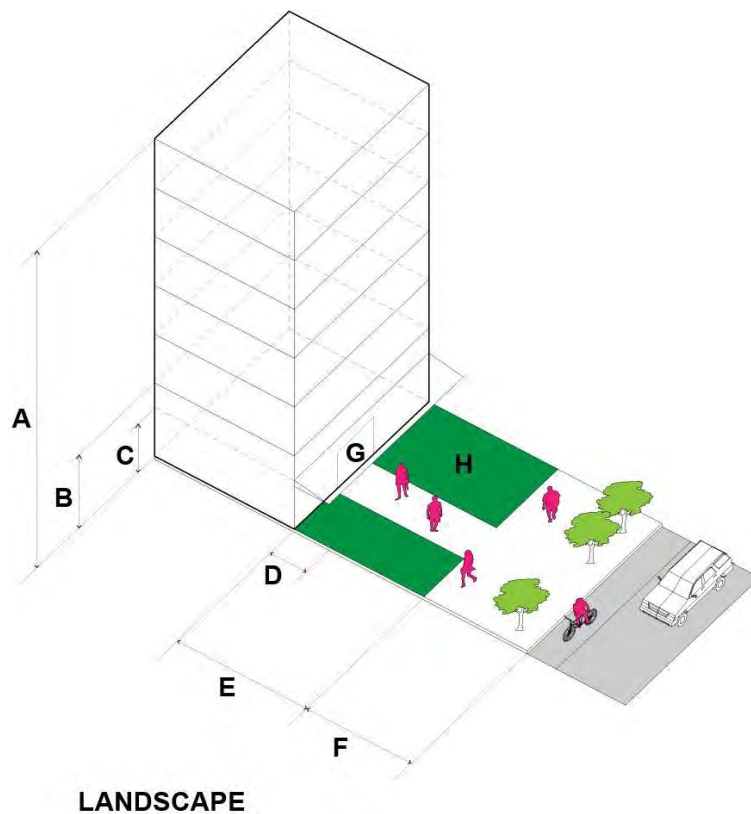
18C.400-6. Plaza Frontage Standards

Standard		Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A	Building Height	90'	90'	n/a
B	First Floor Minimum Height for depth of 30 feet from street: Commercial Uses	16'	16'	n/a
C	Weather Protection Height	10'	10'	n/a
D	Weather Protection Minimum Depth and Linear Frontage	5' min depth; 60% of frontage along sidewalk must have weather protection	5' min depth; 60% of frontage along sidewalk must have weather protection	n/a
E	Building Maximum Setback from Right-of-Way	5'-25'	5'-25'	n/a
F	Building Setback Minimum	5'	5'	n/a
G	Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	n/a
H	Plaza Design	Plaza area must be designed for human activity and include seating and other amenities	Plaza area must be designed for human activity and include seating and other amenities	n/a
I	Window Transparency	A minimum 60% transparency required at street level between 30" and 8' for nonresidential uses	none	n/a

1 The maximum setback shall be up to 25 feet where the Public Works Engineering Director has determined property may be needed to accommodate the City's planned right-of-way at full build-out. Otherwise, the maximum setback is five feet.

4. *Landscape.* The landscape frontage has landscaping between the building and street edge. The landscape frontage is not permitted on retail streets and is appropriate for office and residential uses particularly when on the ground floor.

18C.400-7. Landscape Frontage Type



Framework, 2020

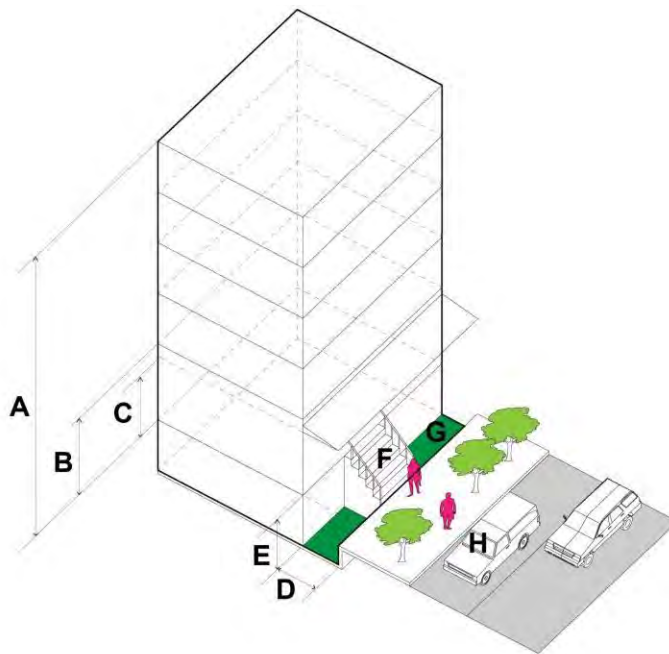
18C.400-8. Landscape Frontage Standards

Standard	Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	Base Zone	Base Zone	Base Zone
B First Floor Minimum Height for depth of 30 feet from street: Commercial Uses	16'	16'	n/a
C Weather Protection Height	10'	10'	10'
D Weather Protection Minimum Depth and Linear Frontage	5' minimum depth; required over primary entrance	5' minimum depth; required over primary entrance	5' minimum depth; required over primary entrance
E Building Maximum Setback from Right-of-Way	20'	20'	20'
F Building Setback Minimum	10'	10'	10'

Standard		Mixed-Use Street	Mixed-Use Arterial	Residential Street/Green Street
G	Front Entrance	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage
H	Landscape Requirements	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.

5. *Porch/Stoop/Terrace*. This frontage type has the building set back from the street edge to accommodate a porch, stoop, or terrace to serve as the primary access to the building. This frontage type is applicable primarily to residential and nonretail commercial uses and is not permitted on retail streets.

18C.400-9. Porch/Stoop/Terrace Frontage Type



PORCH/STOOP/TERRACE

Framework, 2020

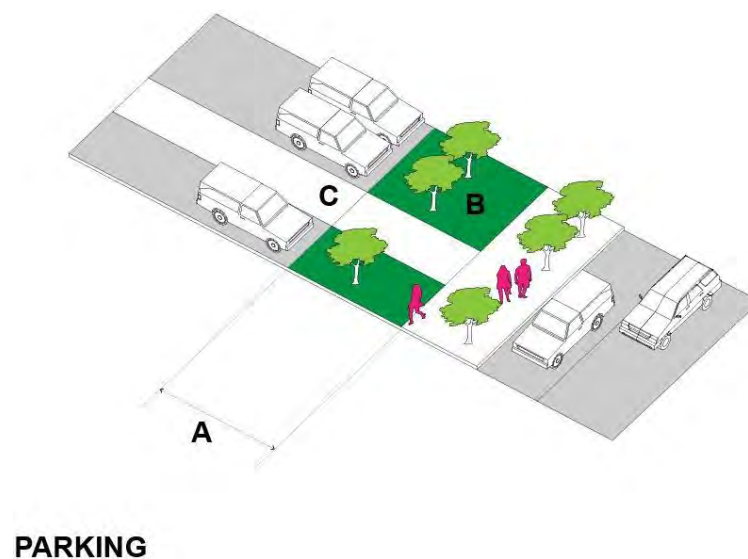
18C.400-10. Porch/Stoop/Terrace Frontage Standards

Standard	Mixed-Use Street/Plaza Street ¹	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	Base Zone	Base Zone	Base Zone
B First Floor Minimum Height for depth of 30 feet from street: Commercial Uses	16'	16'	n/a
C Weather Protection Height	10'	10'	10'
D Threshold Depth	4' minimum	4' minimum	4' minimum
E Threshold Height	Below-grade maximum 4'; Above-grade maximum 5'	Below-grade maximum 4'; Above-grade maximum 5'	Below-grade maximum 4'; Above-grade maximum 5'
F Front Entrance	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage
G Landscape Requirement	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.

¹ Prohibited on plaza street

6. *Parking.* The parking frontage type is only permitted on arterial mixed-use streets and is the only frontage type where parking is allowed between the building and the street edge. Landscape and trees are required in the landscaped area between the street edge and the parking area.

18C.400-11. Parking Frontage Type



Framework, 2020

18C.400-12. Parking Frontage Standards

Standard	Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A Landscape Buffer Width	n/a	Minimum 10'; Maximum 20'	n/a
B Landscape Requirements	n/a	The landscape area shall comply with the City's landscaping requirements.	n/a
C Pedestrian Walkway	n/a	Minimum width – <u>see LMC 18C.500.520</u>	n/a

B. *Civic Uses.* Sites and buildings where the primary use is civic should be designed to reflect a public use and therefore may deviate from the frontage standards. Civic buildings and sites should be recognizable and accessible as public buildings in Station District and to reflect civic pride in the community. The following standards address the design of civic uses:

1. Civic uses are permitted on the ground level of all street types except the low-impact mixed-use streets and may be designed in accordance with any of the frontage types except parking.
2. Civic uses are not subject to the maximum setback requirements.
3. Civic uses may provide the main entry on the side of the building when necessary to the function of the site, including safety and security. A pedestrian connection shall be provided from the sidewalk to the entry.
4. The design sites and buildings for civic uses should incorporate public open space when feasible and practical and provide for a variety of activities such as public art, seating, play equipment, games, and events.

C. *Townhouse/Rowhouse Development.* The following standards apply to townhouse and rowhouse development:

1. Lot widths for townhouses and rowhouses may be less than the 50' minimum required for new parcels.
2. Site access to garages and parking area shall be consolidated with the use of alleys or a common driveway to shared parking areas.
3. Individual driveways are prohibited from any street with a street type designation and all on-site parking shall be located in the rear or in garages.

Chapter 18C.500

LANDSCAPE, OPEN SPACE, AND GREEN INFRASTRUCTURE

Sections:

- 18C.500.510** **Basic Standards.**
- 18C.500.520** **Special Landscape Treatments.**
- 18C.500.530** **Common and Open Space Standards.**
- 18C.500.540** **Green Infrastructure.**

18C.500.510 Basic Standards.

Landscaping shall be provided consistent with Chapter 18C.70 LMC, Article II, Landscaping, except as supplemented or superseded by this title.

18C.500.520 Special Landscape Treatments.

The following special landscape treatments are required or permitted along street types within the Station District:

18C.520-1. Special Landscape Treatments

Street Type	Foundations	Plazas/Courtyards	Pedestrian Walkways
Mixed-Used Street	R	P	R
Arterial Mixed-Used Street	P	P	R
Residential Street/Green Street	R	P	P

P = permitted, R= required

1 Plaza landscaping is required when the plaza frontage is selected on a street.

A. *Foundations.* Buildings shall be accented by foundation plantings. Foundation plantings may frame doors, anchor corners, or screen undesirable areas. Foundation plantings shall allow for five feet of unobstructed movement on the sidewalk. Examples of foundation plantings include raised planter boxes, containers, and similar.

B. *Plazas and Courtyards.* Where provided, consistent with frontage standards, plazas or courtyards shall meet the following standards:

1. Plazas and courtyards shall be visible and accessible to the public.
2. Minimum dimensions of plazas and courtyards are 20 feet. Courtyards may qualify as common open space per LMC 18C.500.530, Common and Open Space Standards.

3. Plazas or courtyards shall include one or more element from each category below.

- a. *Special Paving.* Scored concrete, stained/colored concrete, concrete pavers, paving inlays, mosaics, or other special paving material.
- b. *Landscaping.* Trees, shrubs, trellises, flowers, or container plants. Landscaping shall be consistent with this chapter and Chapter 18A.70 LMC, Article II, Landscaping.
- c. *Seating Area.* Benches or low seating walls. Walls or benches shall be a minimum of four feet long. Seating shall be provided at a ratio of at least one seat per 60 square feet of plaza area or open space.

C. *Pedestrian Walkways.* Pedestrian walkways are required as follows:

1. *Street to Building.* Required between a public right-of-way and building entrances where the building is set back from the street, sidewalk, or parking area.
2. *Parking.* Required between parking lots and building entrances.
3. *Mid-Block Connections.* One pedestrian walkway shall be provided between the street and the rear property line, spaced an average of every 200 feet or less of street frontage.

D. *Dimensional Standards.*

1. *Pedestrian Walkways.*

- a. *Width.* Six feet minimum, exclusive of landscaping or site furnishings, except that mid-block crossings shall have minimum walkway widths of 12 feet.
- b. *Lines of Sight.* Walkways shall be located and designed with clear sight lines for safety.
- c. *Definition.* Pedestrian walkways shall be defined through paving, landscaping, furnishings, and lighting.

2. *Design.* Pedestrian walkways shall be defined using a combination of one or more of the following techniques:

- a. *Special Paving.* Scored concrete, stained/colored concrete, concrete pavers, paving inlays, mosaics, or other special paving material. Not all portions of the pedestrian walkway are

required to be paved; however, a minimum of four feet in width of the pedestrian walkway shall provide an all-weather walking surface.

b. *Architectural Features.* Trellises, railing, low seat walls, weather protection, bollards, or other architectural features. Chain link fences are not allowed.

c. *Landscaped Edges.* A continuous, landscaped area consistent with Type II, Streetscapes, where the pedestrian walkway is from the street to building or mid-block connection, otherwise Type III, Open Space, is required, per Chapter 18A.70 LMC, Article II, Landscaping. If the walkway abuts a driveway or driving aisle on both sides, the landscaped edge shall apply to both sides.

d. *Lighting.* Pedestrian walkways shall include lighting such as pedestrian lights, bollards, and accent lighting to assist pedestrian navigation and promote a safe and comfortable walking space.

18C.500.530 Common and Open Space Standards.

The purpose of this section is to provide residents with access to usable, privately maintained and owned open space to create opportunities for active living and respite for on-site residents.

A. Common Open Space – Private.

1. Each mixed-use or residential development shall provide a common open space per dwelling unit of 100 square feet per dwelling unit.

2. For the purposes of this section, common open space means an open-air area intended for use by all residents, guests, employees or patrons of a site and may include lawns, gardens, squares, plazas, courtyards, terraces, barbecue and picnic areas, games court or multi-use recreational areas, and other types of built space.

3. Design Standards.

a. Required setback areas shall not count toward the open space requirement unless they are part of an open space that meets the other requirements of common open space.

b. Space shall have a minimum dimension of 20 feet in any direction to provide functional leisure or recreational activity. This dimension can be adjusted by the Community

Development Director based on site conditions such as topography or irregular lot geometry.

c. Space shall feature paths or walkable areas, landscaping, seating, lighting, play structures, sports courts, or other pedestrian amenities to make the area more functional and enjoyable for a range of users.

4. Common space shall be separated from ground level windows, streets, service areas and parking lots with landscaping, low-level decorative fencing (no chain link), or other treatments as approved by the Community Development Director that enhance safety and privacy for both the common open space and dwelling units.

5. The space shall be oriented to receive sunlight and = face south if possible. Open space may also face east or west, but not north, unless the Community Development Director determines that site conditions such as topography or irregular lot geometry warrant waiving this requirement. The common open space shall be designed to provide landscaping that defines the open space but permits surveillance from units and roads.

6. The space must be accessible from the dwelling units. The space must be oriented to encourage activity from local residents.

7. No more than 30 percent of the area may be covered by a structure.

8. The common open space may include multi-use storm water detention facilities if the Community Development Director determines that the facilities are designed to function as common open space by providing an enhanced natural or visually aesthetic design.

9. The common space shall be designed to ensure that the open space network addresses Crime Prevention Through Environmental Design (CPTED) principles such as security and surveillance from residential units. Common recreational spaces shall be located and arranged to allow windows to overlook them.

10. The common space shall provide adequate lighting in the open space network, but place and shield lighting so that it does not glare into housing units.

B. Private Open Space.

1. Each dwelling unit shall have a private open space, at a minimum of 48 square feet with a minimum width or depth of six feet.
2. For the purposes of this section, private open space includes individual decks, balconies, or patios.

C. *Fee in Lieu for Portion of Open Space.*

1. Within the station district planned action area, a development may pay a fee in lieu of providing common open space or a portion of the private open space to support park land purchase and improvements within the station district planned action area for urban nodal or urban linear parks identified in the station district subarea plan or the legacy plan for parks, recreation & open space master plan.
2. The fee in lieu is allowed for each 100 square feet of common open space not provided. No less than 50 square feet of common open space per unit shall be provided on site.
3. The fee in lieu is allowed for balconies or patios not provided on street-front facade. No less than 50 percent of the units shall include private open spaces.
4. The fee shall be equal to the average fair market value of the land otherwise required to be provided in on-site common or private open space.
5. Fee Payment and Use.
 - a. The payment shall be held in a reserve account by the City and may only be expended to fund a capital improvement for parks and recreation facility identified in the station district subarea plan or the legacy plan for parks, recreation & open space master plan;
 - b. The payment shall be expended in all cases within five years of collection; and
 - c. Any payment not so expended shall be refunded with interest to be calculated from the original date the deposit was received by the City and at the same rate applied to tax refunds pursuant to RCW 84.69.100; however, if the payment is not expended within five years due to delay attributable to the developer, the payment shall be refunded without interest.

18C.500.540 Green Infrastructure.

- A. Development shall implement storm water standards that incorporate low impact development (LID) principles and standards consistent with City standards in Chapter 12.11 LMC.
- B. Where on-site filtration is feasible, it shall be provided.

- C. Permeable surfaces shall be incorporated into plazas, courtyards, and pedestrian walkways, unless demonstrated infeasible to the satisfaction of the City Engineer, and shall meet City standards per Chapter 12.11 LMC.
- D. Open ponds shall be designed with both natural landscape and urban access and edge treatments consistent with Chapter 18A.70 LMC, Article II.
- E. Native and/or drought tolerant landscaping shall be incorporated into required landscape plans.

Chapter 18C.600

PARKING

Sections:

18C.600.610 **Parking.**

18C.600.610 Parking.

A. *Off-Street Parking Requirements.* The following off-street parking requirements supersede the requirements in Chapter 18A.80 LMC. Uses not listed below must comply with the requirements in Chapter 18A.80 LMC.

B.

18C-600-1. Off-Street Parking Requirements

Land Use	Vehicular Parking Requirement	Bicycle Parking Requirement
Residential	Single-family: 2 per dwelling unit Accessory dwelling: 1 per dwelling unit, provided that no additional parking is required when located within one-quarter mile of the Sounder Station. (RCW 36.70A.698) Senior citizen apartments: No minimum Multifamily housing: No minimum	Meet rates and standards of: Chapter 18A.80
Retail. Services, Restaurants	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Office	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Street level retail 3,000sq.ft. or less per business	None where there is available public parking within 500' or abutting on-street parking designed to serve street level retail	Meet rates and standards of: Chapter 18A.80

B. *Parking Reductions or Increases.* The amount of required parking may be reduced or eliminated, or increased above the maximum, based on a site-specific parking study that demonstrates one or more of the following:

1. *Reduction Due to Shared Parking at Mixed-Use Sites and Buildings.* A shared use parking analysis for mixed-use buildings and sites that demonstrates that the anticipated peak parking demand will be less than the sum of the off-street parking requirements for specific land uses.

2. *Reduction Due to Public Parking Availability.* The availability of public parking to accommodate the parking demand generated by the site or building. The City may approve a reduction in the amount of required parking by up to 50 percent for any parking stalls that will be open and available to the public. On-street parking may be considered for the reduction; any new on-street parking provided will be counted toward the required parking availability.

3. *Reduction Due to Lower Parking Demand or Increase Based on Greater Parking Demand.* Demonstrating that anticipated parking demand will be less than the minimum parking required, or greater than the maximum allowed, shall be based on collecting local parking data for similar land uses on a typical day for a minimum of eight hours.

4. *Reduction for Housing in Proximity to Sounder Station (RCW 36.70A.620).* When located within one-quarter mile of the Sounder Station, an applicant may apply for an exception allowing minimum parking requirements to be reduced at least to one parking space per bedroom or 0.75 space per unit, as justified through a parking study prepared to the satisfaction of the Community Development Director or their designee:

- a. housing units that are affordable to very low-income or extremely low-income individuals.
- b. housing units that are specifically for people with disabilities.

In determining whether to grant a parking reduction, the Community Development Director may also consider if the project is proposed in an area with a lack of access to street parking capacity, physical space impediments, or other reasons supported by evidence that would make on-street parking infeasible for the unit.

C. *Parking Location and Design.* Parking shall be located behind the [building](#) or in a [structure](#) except in locations where the parking frontage type is [permitted](#).

D. *Shared Parking.* Shared parking is encouraged to support a walkable and pedestrian-oriented Station District where people can park once and visit multiple destinations. Off-site shared parking may be authorized per the standards in Chapter [18A.80](#) LMC.

E. *Public Parking.* Public parking is permitted as a principal or accessory use in the Station District subject to the frontage and design standards.

F. *Dimensional Standards.* Parking stall and circulation design shall meet the standards of Chapter [18A.80](#) LMC.

Chapter 18C.700 ADMINISTRATION

18C.700.710. Form-Based Code Review

- A. All development in the Station Area District requiring a land use or building permit shall be subject to form-based code review consistent with the procedures in LMC 18.B.700.710 provided that all references to Downtown District shall be interpreted as the Station Area District.

18C.700.720. Optional Master Planned Development

- A. To establish specified uses in LMC 18.C.200.220 Development within the Station Area District may apply for a master plan for the development of (2) acres or greater.
- B. The master plan proposal shall be considered a Process II application for the purposes of Title 18A.20 and consolidated with any other relevant permit applications.
- C. Applications for a master planned development shall be filed with the Community and Economic Development Department on forms provided by the Department along with established fees. An applicant for a master planned development shall submit the following items to the City, unless the Director finds in writing that one or more submittals are not required due to unique circumstances related to a specific development proposal:
 - 1. A detailed narrative identifying how the master plan meets each criteria in subsection D.
 - 2. A site plan with the heading "Master Planned Development Site Plan" that includes information including street frontage types, building footprints, proposed landscaping, open space and parks and/or recreational areas including trails, public spaces, pedestrian walkways, parking locations, and proposed setbacks;
 - 3. Conceptual elevation drawings illustrating facade and building design elements, including height, overall bulk/mass and density and proposed residential design features that will provide for a superior development;
 - 4. A conceptual landscape plan/map showing the proposed location and types of vegetation and landscaping. The landscape plan may also be incorporated into the master planned development site plan and narrative;
 - 5. A phasing plan, if the development will occur in distinct phases, with a written schedule detailing the timing of improvements;
 - 6. A draft development agreement, if proposed by the applicant, or as required by the City; and

7. All of the submittal requirements in Chapter 18A.20 LMC, Article I.
- D. A master planned development shall only be granted after the Director has reviewed the proposed use and has made written findings that all of the standards and criteria set forth below have been met or can be met subject to conditions of approval:
 1. The master planned development is consistent with the comprehensive plan; and
 2. The master planned development is consistent with the vision and objectives of the station area plan in LMC 18C.100.110; and
 3. The master planned development proposes a mixed income development that includes:
 - a. Housing only includes units for permanent residential occupancy, as defined in LMC 3.64.010.
 - b. At least 10 percent of the units are provided as one or more of the following unit types:
 - i. Townhouses or Rowhouses ; and/or
 - ii. Live/work units; and/or
 - iii. Family-sized housing, where units contain two or more bedrooms and the design includes access to outdoor recreation space suitable for children where adults can appropriately supervise and easily view children.
 - c. The master plan includes one or more of the following:
 - i. Development demonstrates variety in unit types, tenure, or affordability that fits a need demonstrated in the Comprehensive Plan Housing Element or Consolidated Plan; and/or
 - ii. On-site units dedicated to serving persons displaced due to redevelopment within one-half mile of the Sounder Station as documented in a relocation assistance plan and affidavits; and/or
 - iii. A park open to the public due to an easement, dedication, or contract that meets system needs identified in the legacy plan for parks, recreation & open space master plan and meets relevant city design standards.
 4. The design is compatible with and responds to the existing or intended character, appearance, quality of development and physical characteristics of the subject property and immediate vicinity; and

5. Roads and streets, whether public or private, within and contiguous to the site comply with guidelines for construction of streets and the street frontage standards; and
6. Each phase of the proposed development, as it is planned to be completed, contains the parking spaces, open space, recreation space, landscaping and utility area necessary for creating and sustaining a desirable and stable environment; and
7. The master planned development is consistent with the planned action ordinance (Ordinance XXX).

18C.700.730. Update of Nonconforming Lots

All nonconforming lots in the Station Area District are subject to full compliance and proportional compliance regulations pursuant to LMC 18B.700.730 Update of Nonconforming Lots provided that all references to Downtown District shall be interpreted as the Station Area District.

EXHIBIT C

Amendments to LMC Title 18A

18A.10.140 Establishment of subareas.

Per RCW 36.70A.080(2), in order to plan for and regulate the use of land and structures in a manner which recognizes that residential neighborhoods and business areas within Lakewood vary one from another in desired character, subareas may be established as optional elements of the comprehensive plan and implementing zoning regulations may be adopted as a title of the Lakewood Municipal Code (LMC).

Subarea plans are implemented in part through the adoption of use, development, performance, or procedural regulations specific to the subarea or to a portion or portions of the subarea. Regulations which are specific to a subarea or portions of a subarea are located in the title of the LMC concerning the subarea.

The following subareas and subarea plans are established:

Name	Symbol	Code Title
Downtown Subarea Plan	DSAP	18B
This section is reserved Lakewood Station District Subarea Plan	Reserved LSDSP	Reserved 18C

18A.10.180 Definitions

“Rowhouse” means any of two or more identical or nearly identical attached dwelling units that share a common wall on one or both sides of the unit.

“Townhouse” means a type of attached multifamily dwelling in a row of at least two (2) such units in which each unit has its own front and rear access to the outside, no unit is located over another unit, and each unit is separated from any other unit by one (1) or more vertical common fire-resistant walls. Town houses may be situated or grouped in different layouts within a development.

18A.20.208 Applicability – Nonconformities.

A. This article shall apply to legally existing nonconformities, except the following items, which shall be governed by the standards set forth in the chapters identified below:

1. Nonconforming uses and structures within the Downtown Subarea Plan, as governed in LMC Title 18B.
2. Nonconforming uses and structures within the Lakewood Station District Subarea Plan, governed in LMC Title 18C.
23. Nonconforming signs as defined in Chapter 18A.100 LMC, Signs.
34. Nonconforming wireless service facilities as defined in Chapter 18A.95 LMC, Wireless Service Facilities.
45. Nonconforming mobile home parks as defined in LMC 18A.40.110(C), Manufactured Home Parks. Manufactured and mobile home parks which were legally approved prior to the effective date of this title may continue to exist; provided, that the density of the park does not increase over the number of dwelling units legally existing on the effective date of this

title. Manufactured home sites within legally nonconforming manufactured home parks may continue to be used; provided, that the placement of newer manufactured homes does not result in encroachment of the dwelling beyond the lot space boundaries or into the right-of-way and fire code requirements for structure spacing are met.

~~56~~. Nonconforming sexually oriented businesses as defined in the Chapter 18A.50 LMC, Article III, Sexually Oriented Businesses Overlay.

~~67~~. Permit applications at the time of this title's passage that constitute vested development.

i. Future plans to further develop property shall not constitute a basis for nonconformity status, whether or not documented in the public record, except when they constitute a vesting.

ii. Nothing in this article shall be construed to require a change in plans, construction, or intended use related to vested development, though it may thereafter be regulated as a nonconformity. [Ord. 726 § 2 (Exh. B), 2019.]

ATTACHMENT B

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: April 19, 2021	TITLE: Ordinance adopting a SEPA Planned Action related to the Lakewood Station District Subarea (LSDS) Plan.	TYPE OF ACTION: <div style="display: flex; justify-content: space-between;"> <u> X </u> ORDINANCE NO. 752 </div> <div style="display: flex; justify-content: space-between;"> <u> </u> RESOLUTION NO. </div> <div style="display: flex; justify-content: space-between;"> <u> </u> MOTION NO. </div> <div style="display: flex; justify-content: space-between;"> <u> </u> OTHER </div>
REVIEW: March 8, 2021 Study Session; April 5 Public Hearing	ATTACHMENTS: Exhibit A (LSDS Planned Action Ordinance and Exhibits); Exhibit B (SEPA Environmental Checklist and Exhibits)	

SUBMITTED BY: David Bugher, Assistant City Manager for Development Services
 Tiffany Speir, Long Range & Strategic Planning Manager

RECOMMENDATION: It is recommended that the City Council approve the attached Ordinance 752 adopting a SEPA Planned Action related to the Lakewood Station District Subarea (LSDS) Plan.

DISCUSSION: Lakewood has recognized the need for, and memorialized initial design for, a subarea plan in the Lakeview Neighborhood/Lakewood Station District since adopting its first Comprehensive Plan in 2000. In November 2019, the State Department of Commerce awarded Lakewood a grant under E2SHB 1923 to prepare a Lakewood Station District Subarea (LSDS) Plan, an implementing Hybrid Form-Based Code and a SEPA-based Planned Action. In-person and virtual stakeholder outreach was conducted between December 2019 and Spring 2021 to help develop the proposed package.

ALTERNATIVE(S): The Council could amend the proposed Ordinance; the Council could also not adopt the Ordinance.

FISCAL IMPACT: N/A

Prepared by
Tiffany Speir, Long Range & Strategic Planning
Manager

Department Director
Dave Bugher, Assistant City Manager for
Development Services



 City Manager Review

EXHIBIT A

ORDINANCE NO. 752

AN ORDINANCE of the City Council of the City of Lakewood, Washington, adopting a SEPA Planned Action related to the Lakewood Station District Subarea.

I. RECITALS

WHEREAS, the State Environmental Policy Act (SEPA) and its implementing regulations provide for the integration of environmental review with land use planning and project review through the designation of planned actions by jurisdictions planning under the Growth Management Act (GMA), such as the City of Lakewood (“City”); and

WHEREAS, Section 43.21C.440 of the Revised Code of Washington (RCW), Sections 197-11-164 through 172 of the Washington Administrative Code (WAC) allow for and govern the adoption and application of a planned action designation under SEPA, and Section 14.02.030 of the Lakewood Municipal Code (LMC) adopts Chapter 197-11 WAC by reference as amended; and

WHEREAS, the designation of a planned action expedites the permitting process for projects in proximity to major transit facilities when the impacts have been previously addressed in a Threshold Determination; and

WHEREAS, a subarea of the City commonly referred to as the “Lakewood Station District Subarea” (“LSDS”), as depicted on the map attached hereto as **Exhibit A** and incorporated herein by this reference, has been identified as a planned action area for future redevelopment to a mixed-use center (“Planned Action Area”) within a half-mile of a major transit stop under RCW 43.21C.440(1)(b)(ii) consisting of the existing Sounder Station and associated properties and the planned bus rapid transit (BRT) Route 2 in the Pierce Transit Destination 2040 anticipated to be operational in 2026; and

WHEREAS, the City has developed and adopted a subarea plan complying with the GMA (RCW 36.70A), dated April 19, 2021, to guide the redevelopment of the Planned Action Area (“Lakewood Station District Subarea Plan” or “LSDS Plan”); and

WHEREAS, after extensive public participation and coordination with all affected parties, the City, as lead SEPA agency, issued the LSDS Planned Action Threshold Determination of Non-Significance (“Threshold Determination”) dated November 12, 2020 and revised on March 30, 2021 as appropriate, which identifies the impacts and mitigation measures associated with planned development in the Planned Action Area as identified in the LSDS Plan; and

WHEREAS, the City desires to designate a planned action under SEPA for the LSDS (“Planned Action”); and

WHEREAS, adopting a Planned Action for the LSDS with appropriate standards and procedures will help achieve efficient permit processing and promote environmental quality protection; and

WHEREAS, the City is amending the Lakewood Comprehensive Plan to incorporate maps, text, and policies specific to the LSDS during the 2021 Comprehensive Plan amendment cycle; and

WHEREAS, the City is adopting zoning and development regulations concurrent with the LSDS Plan to implement said Plan, including this Ordinance; and

WHEREAS, the City Council finds that adopting this Ordinance and its Exhibits is in the public interest and will advance the public health, safety, and welfare.

II. FINDINGS

The procedural and substantive requirements of the State Environmental Policy Act (RCW 43.21C) have been complied with.

The procedural requirements of the Growth Management Act (RCW 36.70A) have been complied with.

The proposed action is consistent with the requirements of Revised Code of Washington, and the Washington Administrative Code.

The proposed action is consistent with the City of Lakewood Comprehensive Plan.

The proposed amendments have been reviewed and processed in accordance with the requirements of Title 14 Environmental Protection, Title 14A Critical Areas, and Title 18A Land Use and Development of the City of Lakewood Municipal Code.

All of the facts set forth in the Recitals are true and correct, and are incorporated herein by reference.

All necessary public meetings and opportunities for public testimony and comment have been conducted in compliance with State law and the City's municipal code.

The Lakewood City Council finds and determines that the regulation of development and land use within the LSDS is within the City's regulatory authority.

The Lakewood City Council finds and determines that approval of such amendments to the Land Use and Development Code is in the best interests of the residents of Lakewood, and will promote the general health, safety, and welfare.

The Lakewood City Council finds and determines that regulation of land use and development is subject to the authority and general police power of the City, and the City reserves its powers and authority to appropriately amend, modify, and revise such land use controls in accordance with applicable law.

The Planned Action Threshold Determination adequately identifies and addresses the probable significant environmental impacts associated with the type and amount of development planned to occur in the designated Planned Action Area.

The mitigation measures identified in the Planned Action Threshold Determination, attached to this Ordinance as **Exhibit B and Exhibit C** and incorporated herein by reference, together with adopted City development regulations are adequate to mitigate significant adverse impacts from development within the Planned Action Area.

The LSDS Plan and Planned Action Threshold Determination identify the location, type, and amount of development that is contemplated by the Planned Action.

Future projects that are implemented consistent with the Planned Action will protect the environment, benefit the public, and enhance economic development.

The City provided several duly noticed opportunities for meaningful public involvement and review in the LSDS Plan and Planned Action Threshold Determination processes, including a community meeting consistent with RCW 43.21C.440; has considered all comments received; and, as appropriate, has modified the proposal or mitigation measures in response to comments.

Essential public facilities as defined in RCW 36.70A.200 are excluded from the Planned Action as designated herein and are not eligible for review or permitting as Planned Action Projects unless they are accessory to or part of a project that otherwise qualifies as a Planned Action Project.

The designated Planned Action Area is located entirely within an Urban Growth Area.

Implementation of the mitigation measures identified in the Planned Action Threshold Determination will provide for adequate public services and facilities to serve the proposed Planned Action Area.

The documents and other materials that constitute the record of the proceedings upon which the Planning Commission's recommendations are based, including, but not limited to, the staff reports for the Project and all of the materials that support the staff reports for the Project, are located in the City of Lakewood, Community and Economic Development Department at 6000 Main Street SW, Lakewood, Washington, 98499-5027. The custodian of these documents is the Assistant City Manager for Development Services of the City of Lakewood.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. Adoption of City Council Findings. The Findings of the City Council are adopted as part of this Ordinance.

Section 2. Purpose. The purpose of this Ordinance is to:

A. Combine environmental analysis, land use plans, development regulations, and City codes and ordinances together with the mitigation measures in the Planned Action Threshold Determination to mitigate environmental impacts and process Planned Action development applications in the Planned Action Area;

B. Designate the LSDS shown in Exhibit A as a Planned Action Area for purposes of environmental review and permitting of designated Planned Action Projects pursuant RCW 43.21C.440;

C. Determine that the Planned Action Threshold Determination meets the requirements of a planned action Threshold Determination pursuant to the State Environmental Policy Act (SEPA);

D. Establish criteria and procedures for the designation of certain projects within the Planned Action Area as “Planned Action Projects” consistent with RCW 43.21C.440;

E. Provide clear definition as to what constitutes a Planned Action Project within the Planned Action Area, the criteria for Planned Action Project approval, and how development project applications that qualify as Planned Action Projects will be processed by the City;

F. Streamline and expedite the land use permit review process by relying on the Planned Action Threshold Determination; and

G. Apply applicable regulations within the City’s development regulations and the mitigation framework contained in this Ordinance for the processing of Planned Action Project applications and to incorporate the applicable mitigation measures into the underlying project permit conditions in order to address the impacts of future development contemplated by this Ordinance.

Section 3. Procedures and Criteria for Evaluating and Determining Planned Action Projects within the Planned Action Area.

A. Planned Action Area. This “Planned Action” designation shall apply to the area shown in Exhibit A of this Ordinance.

B. Environmental Document. A Planned Action Project determination for a site-specific project application within the Planned Action Area shall be based on the environmental analysis contained in the Planned Action Threshold Determination. The mitigation measures contained in Exhibit B and Exhibit C of this Ordinance are based upon the findings of the Planned Action Threshold Determination and shall, along with adopted City regulations, provide the framework the City will use to apply appropriate conditions on qualifying Planned Action Projects within the Planned Action Area.

C. Planned Action Project Designated. Land uses and activities described in the Planned Action Threshold Determination, subject to the thresholds described in Subsection 3.D of this Ordinance and the mitigation measures contained in Exhibit B and Exhibit C of this Ordinance, are designated “Planned Action Projects” pursuant to RCW 43.21C.440. A development application for a site-specific project located within the Planned Action Area shall be designated a Planned Action Project if it meets the criteria set forth in Subsection 3.D of this Ordinance and all other applicable laws, codes, development regulations, and standards of the City, including this Ordinance, are met.

D. Planned Action Qualifications. The following thresholds shall be used to determine if a site-specific development proposed within the Planned Action Area was contemplated as a Planned Action Project and has had its environmental impacts evaluated in the Planned Action Threshold Determination:

(1) Qualifying Land Uses.

(a) Planned Action Categories: The following general categories/types of land uses are defined in the LSDS Plan and can qualify as Planned Actions:

- i. Townhome or cottage dwelling units
- ii. Multi-family dwelling units
- iii. Commercial Office

- iv. Services,
- v. Medical
- vi. Hotel and Lodging
- vii. Retail and Eating and Drinking Establishments
- viii. Open Space, Parks, Plazas, Trails, Gathering Spaces, Recreation
- ix. Civic and Cultural Facilities
- x. Governmental and Utility Facilities
- xi. Other uses allowed in the LSDS Development Code, LMC Title 18C.

(b) Planned Action Project Land Uses: A primary land use can qualify as a Planned Action

Project land use when:

- i. it is within the Planned Action Area as shown in Exhibit A of this Ordinance;
- ii. it is within one or more of the land use categories described in Subsection 3.D(1)(a) above; and
- iii. it is listed in development regulations applicable to the zoning classifications applied to properties within the Planned Action Area.

A Planned Action Project may be a single Planned Action land use or a combination of Planned Action land uses together in a mixed-use development. Planned Action land uses may include accessory uses.

(c) Public Services: The following public services, infrastructure, and utilities can also qualify as Planned Actions: onsite roads, utilities, parks, trails, and similar facilities developed consistent with the Planned Action Threshold Determination mitigation measures, City and special district design standards, critical area regulations, and the Lakewood Municipal Code.

(2) Development Thresholds:

(a) Land Use: The following thresholds of new land uses are contemplated by the Planned Action:

FEATURE	PLANED ACTION
Residential Dwellings (units): Net 2020-2035	1,722
Jobs: Net 2020-2035	1,276

(b) Shifting development amounts between land uses in identified in Subsection 3.D(2)(a) may be permitted when the total build-out is less than the aggregate amount of development reviewed in the Planned Action Threshold Determination; the traffic trips for the Planned Action are not exceeded; and, the development impacts identified in the Planned Action Threshold Determination are mitigated consistent with Exhibit B and Exhibit C of this Ordinance.

(c) Further environmental review may be required pursuant to WAC 197-11-172, if any individual Planned Action Project or combination of Planned Action Projects exceeds the development thresholds specified in this Ordinance and/or alter the assumptions and analysis in the Planned Action Threshold Determination.

(3) Transportation Thresholds:

(a) Trip Ranges & Thresholds. The number of new PM peak hour trips anticipated in the Planned Action Area and reviewed in the Planned Action Threshold Determination for 2035 is as follows:

The net increase between Planned Action and Existing Conditions is 1,950 vehicle trips. Total rounded vehicle trips by scenario tested include:

i. Existing – 2,530

ii. No Action – 3,810

iii. Planned Action – 4,480

(b) Concurrency. All Planned Action Projects shall meet the transportation concurrency requirements and the Level of Service (LOS) thresholds established in LMC 18A.50.195.

(c) Traffic Impact Mitigation. Transportation mitigation shall be provided consistent with mitigation measures in Exhibit B and Exhibit C of this Ordinance, attached hereto and incorporated by this reference.

(d) The responsible City official shall require documentation by Planned Action Project applicants demonstrating that the total trips identified in Subsection 3.D(3)(a) are not exceeded, that the project meets the concurrency and intersection standards of Subsection 3.D(3)(b), and that the project has mitigated impacts consistent with Subsection 3.D (3)(c).

(e) Discretion.

i. The responsible City official shall have discretion to determine incremental and total trip generation, consistent with the Institute of Traffic Engineers (ITE) Trip Generation Manual (latest edition) or an alternative manual accepted by the City's Public Works Director at his or her sole discretion, for each project permit application proposed under this Planned Action.

ii. The responsible City official shall have discretion to condition Planned Action Project applications to meet the provisions of this Planned Action Ordinance and the Lakewood Municipal Code.

(4) Elements of the Environment and Degree of Impacts. A proposed project that would result in a significant change in the type or degree of adverse impacts to any element(s) of the environment analyzed in the Planned Action Threshold Determination would not qualify as a Planned Action Project.

(5) Changed Conditions. Should environmental conditions change significantly from those analyzed in the Planned Action Threshold Determination, the City's SEPA Responsible Official may determine that the Planned Action Project designation is no longer applicable until supplemental environmental review is conducted.

E. Planned Action Project Review Criteria.

- (1) The City's SEPA Responsible Official, or authorized representative, may designate as a Planned Action Project, pursuant to RCW 43.21C.440, a project application that meets all of the following conditions:
- (a) the project is located within the Planned Action Area identified in Exhibit A of this Ordinance;
 - (b) the proposed uses and activities are consistent with those described in the Planned Action Threshold Determination and Subsection 3.D of this Ordinance;
 - (c) the project is within the Planned Action thresholds and other criteria of Subsection 3.D of this Ordinance;
 - (d) the project is consistent with the Lakewood Comprehensive Plan including the policies of the LSDS Plan incorporated into the Comprehensive Plan and the regulations of the LSDS Plan integrated into the Lakewood Municipal Code;
 - (e) the project's significant adverse environmental impacts have been identified in the Planned Action Threshold Determination;
 - (f) the project's significant impacts have been mitigated by application of the measures identified in Exhibit B and Exhibit C of this Ordinance and other applicable City regulations, together with any conditions, modifications, variances, or special permits that may be required;
 - (g) the project complies with all applicable local, state and/or federal laws and regulations and the SEPA Responsible Official determines that these constitute adequate mitigation; and
 - (h) the project is not an essential public facility as defined by RCW 36.70A.200, unless the essential public facility is accessory to or part of a development that is designated as a Planned Action Project under this Ordinance.

- (2) The City shall base its decision to qualify a project as a Planned Action Project on review of the SEPA Checklist form in WAC 197-11 and review of the Planned Action Project submittal and supporting documentation, provided on City required forms.

F. Effect of Planned Action Designation.

- (1) Designation as a Planned Action Project by the City's SEPA Responsible Official means that a qualifying project application has been reviewed in accordance with this Ordinance and found to be consistent with the development parameters and thresholds established herein and with the environmental analysis contained in the Planned Action Threshold Determination.
- (2) Upon determination by the City's SEPA Responsible Official that the project application meets the criteria of Subsection 3.D and qualifies as a Planned Action Project, the project shall not require a SEPA threshold determination, preparation of an Threshold Determination, or be subject to further review pursuant to SEPA. Planned Action Projects will still be subject to all other applicable City, state, and federal regulatory requirements. The Planned Action Project designation shall not excuse a project from meeting the City's code and ordinance requirements apart from the SEPA process.

G. Planned Action Project Permit Process. Applications submitted for qualification as a Planned Action Project shall be reviewed pursuant to the following process:

- (1) Development applications shall meet all applicable requirements of the Lakewood Municipal Code and this Ordinance in place at the time of the Planned Action Project application. Planned Action Projects shall not vest to regulations required to protect public health and safety.
- (2) Applications for Planned Action Projects shall:
 - (a) be made on forms provided by the City;
 - (b) include the SEPA checklist in WAC 197-11;
 - (c) meet all applicable requirements of the Lakewood Municipal Code and this Ordinance.

- (3) The City's SEPA Responsible Official shall determine whether the application is complete and shall review the application to determine if it is consistent with and meets all of the criteria for qualification as a Planned Action Project as set forth in this Ordinance.
- (4) (a) If the City's SEPA Responsible Official determines that a proposed project qualifies as a Planned Action Project, he/she shall issue a "Determination of Consistency" and shall mail or otherwise verifiably deliver said Determination to the applicant; the owner of the property as listed on the application; and federally recognized tribal governments and agencies with jurisdiction over the Planned Action Project, pursuant to RCW 43.21C.440(3)(b).
- (b) Upon issuance of the Determination of Consistency, the review of the underlying project permit(s) shall proceed in accordance with the applicable permit review procedures specified in Chapter 18A.02 LMC, except that no SEPA threshold determination, Threshold Determination, or additional SEPA review shall be required.
- (c) The Determination of Consistency shall remain valid and in effect as long as the underlying project application approval is also in effect.
- (d) Public notice and review for qualified Planned Action Projects shall be tied to the underlying project permit(s). If notice is otherwise required for the underlying permit(s), the notice shall state that the project qualifies as a Planned Action Project. If notice is not otherwise required for the underlying project permit(s), no special notice is required by this Ordinance.
- (5) (a) If the City's SEPA Responsible Official determines that a proposed project does not qualify as a Planned Action Project, he/she shall issue a "Determination of Inconsistency" and shall mail or otherwise verifiably deliver said Determination to the applicant; the owner of the property as listed on the application; and federally recognized tribal governments and agencies with jurisdiction over the Planned Action Project, pursuant to RCW 43.21C.440.
- (b) The Determination of Inconsistency shall describe the elements of the Planned Action Project application that result in failure to qualify as a Planned Action Project.

(c) Upon issuance of the Determination of Inconsistency, the City's SEPA Responsible Official shall prescribe a SEPA review procedure for the non-qualifying project that is consistent with the City's SEPA regulations and the requirements of state law.

(d) A project that fails to qualify as a Planned Action Project may incorporate or otherwise use relevant elements of the Planned Action Threshold Determination, as well as other relevant SEPA documents, to meet the non-qualifying project's SEPA requirements. The City's SEPA Responsible Official may limit the scope of SEPA review for the non-qualifying project to those issues and environmental impacts not previously addressed in the Planned Action Threshold Determination.

(6) To provide additional certainty about applicable requirements, the City or applicant may request consideration and execution of a development agreement for a Planned Action Project, consistent with RCW 36.70B.170 et seq.

(7) A Determination of Consistency or Inconsistency is a Type II land use decision and may be appealed pursuant to the procedures established in Chapter 18A.20.080 LMC. An appeal of a Determination of Consistency shall be consolidated with any pre-decision or appeal hearing on the underlying project application.

Section 4. Monitoring and Review.

A. The City should monitor the progress of development in the designated Planned Action area biennially to ensure that it is consistent with the assumptions of this Ordinance and the Planned Action Threshold Determination regarding the type and amount of development and associated impacts and with the mitigation measures and improvements planned for the Planned Action Area.

B. This Planned Action Ordinance shall be reviewed by the SEPA Responsible Official every two (2) years from its effective date in conjunction with the City's regular Comprehensive Plan review or docket cycle, as applicable. The review shall determine the continuing relevance of the Planned Action assumptions and findings with respect to environmental conditions in the Planned Action Area,

the impacts of development, and required mitigation measures (Exhibit B and Exhibit C). Based upon this review, the City may propose amendments to this Ordinance or may supplement or revise the Planned Action Threshold Determination.

Section 5. Severability. If any portion of this Ordinance or its application to any person or circumstances is held invalid, the remainder of the Ordinance or the application of the provision to other persons or circumstances shall not be affected.

Section 6. Effective Date. That this Ordinance shall be in full force and effect thirty (30) days after publication of the Ordinance Summary.

ADOPTED by the City Council this 19th day of April, 2021.

CITY OF LAKEWOOD

Attest:

Don Anderson, Mayor

Briana Schumacher, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney

EXHIBIT A. PLANNED ACTION AREA



Map Note: The right of way for Bridgeport Way between the East Commercial Area and Colonial District is included for consistent landscaping. Abutting land use is not included in that segment.

EXHIBIT B. MITIGATION DOCUMENT

The Planned Action Threshold Determination has identified significant beneficial and adverse impacts that are anticipated to occur with the future development of the Planned Action Area, together with a number of possible measures to mitigate those significant adverse impacts.

A Mitigation Document is provided in this Exhibit B-1 to establish specific mitigation measures based upon significant adverse impacts identified in the Planned Action Threshold Determination. The mitigation measures in this Exhibit B-1 shall apply to Planned Action Project applications that are consistent with the Preferred Alternative range reviewed in the Planned Action Threshold Determination and which are located within the Planned Action Area (see Exhibit A).

Where a mitigation measure includes the words “shall” or “will,” inclusion of that measure in Planned Action Project application plans is mandatory in order to qualify as a Planned Action Project. Where “should” or “would” appear, the mitigation measure may be considered by the project applicant as a source of additional mitigation, as feasible or necessary, to ensure that a project qualifies as a Planned Action Project. Unless stated specifically otherwise, the mitigation measures that require preparation of plans, conduct of studies, construction of improvements, conduct of maintenance activities, etc., are the responsibility of the applicant or designee to fund and/or perform to the satisfaction of the City’s SEPA Responsible Official or authorized designee.

Any and all references to decisions to be made or actions to be taken by the City’s SEPA Responsible Official may also be performed by the City’s SEPA Responsible Official’s authorized designee.

SECTION B-1. MITIGATION REQUIRED FOR DEVELOPMENT APPLICATIONS

Transportation

1. Applicants for development shall provide traffic assessments to the satisfaction of the SEPA Responsible Official or their designee to determine transportation signal improvements phasing consistent with the Threshold Determination Transportation Analysis.

Environmental Health

2. Planned actions shall orient residential uses to be located a minimum of 500 feet from the centerline of I-5.
3. Applicants for development shall conduct a site assessment to determine if contamination is present from past use.

Cultural Resources

4. Applicants for development shall include Inadvertent Human Remains Discovery Language recommended by DAHP as a condition of project approval.

SECTION B-2. ADVISORY NOTES TO APPLICANTS: APPLICABLE REGULATIONS

Additionally, the Planned Action Threshold Determination identifies specific regulations that act as mitigation measures. These are summarized in this section by Threshold Determination topic. All applicable federal, state, and local regulations shall apply to Planned Action Projects. Planned Action Project applicants shall comply with all adopted regulations where applicable including those listed in the Planned Action Threshold Determination and those not included in the Planned Action Threshold Determination. In addition, this section identifies voluntary water and energy conservation measures that may be implemented by new development.

Earth

LMC Chapter 12.10 provides regulations addressing: storm drainage, earthwork (excavation and fill), soil reports, and erosion controls.

Air Quality

Application of the State Energy Code can improve energy efficiency and reduce greenhouse gas emissions. (LMC Chapter 15.05) Over time new standards will also be implemented: The 2019 Legislature adopted new standards that will increase the efficiency of these buildings and reduce emissions. The Legislature also increased efficiency standards for appliances. (Washington State Department of Ecology, 2019)

Water Resources

Application of Washington State Department of Ecology Stormwater Manual and its Engineering Standards Manual at LMC Chapter 12.11.

Stormwater management will be required of all new development whether under the No Action or Planned Action Alternatives. (LMC Chapter 12.11)

Plants and Animals

Planned Actions would implement LMC Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. When redevelopment would occur, perimeter landscaping and parking area landscaping would be required. Additionally significant trees would be retained where they exist.

The City maintains critical area regulations protecting fish and wildlife habitat conservation areas. Though none are mapped in the study area, if some were identified they would be protected under either the No Action or Planned Action Alternatives. (LMC Chapter 14.154)

Energy

All future development would be subject to the State Energy Code. (LMC Chapter 15.05)

Environmental Health

The State Model Toxics Control Act (MTCA) sets standards for cleanup of lower levels of contaminants that are incorporated into new development and redevelopment parcels noted to have contamination potential.

The City applies relevant standards regarding hazardous materials handling in the International Fire Code and Zoning Codes.

Land Use, Aesthetics, Recreation

Planned Actions shall comply with the LSDS Subarea Plan and LMC Title 18C, LSDS Development Code.

Housing

The City does provide inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing in Chapter 18A.90 Housing Incentives Program.

Light and Glare

LMC 18A.60.095 Outdoor Lighting Code provides lighting standards to reduce light and glare and to avoid interference with JBLM operations.

Historic and Cultural Preservation

Washington State has a number of laws that oversee the protection and proper excavation of archaeological sites (RCW 27.53, WAC 25-48), human remains (RCW 27.44), and historic cemeteries or graves (RCW 68.60). The Governor's Executive Order 05-05 requires state agencies to integrate DAHP, the Governor's Office of Indian Affairs, and concerned tribes into their capital project planning process. This executive order affects any capital construction projects and any land acquisitions for purposes of capital construction not undergoing Section 106 review under the National Historic Preservation Act of 1966.

Under RCW 27.53, DAHP regulates the treatment of archaeological sites on both public and private lands and has the authority to require specific treatment of archaeological resources. All precontact resources or sites are protected, regardless of their significance or eligibility for local, state, or national registers. Historic archaeological resources or sites are protected unless DAHP has made a determination of "not-eligible" for listing on the state and national registers.

The City applies Lakewood Municipal Code Chapter 2.48 Protection and Preservation of Landmarks.

Transportation, Public Services, and Utilities

All development is required to be served by primary infrastructure including sewer, water, stormwater, and transportation per LMC 18A.60.140 Concurrency.

Street frontage standards apply per the LSDS Subarea Plan and LMC Title 18C LSDS Development Code and LMC 12.09.031.

Parking standards apply per Title 18C LSDS Development Code and LMC 18A.80.030. Commercial and Multifamily Uses.

SECTION B-3. PUBLIC AGENCY STRATEGIES

The City will implement strategies in the Subarea Plan largely through the Form-Based Code, but will also determine implementation through its code docketing process.

Land Use

The City would propose to add the subarea to its multi-family tax exemption for affordable housing in LMC Chapter 3.64 Tax Incentive Urban Use Center Development.

Housing

Based on recent state laws, tiny homes and recreational vehicles must be allowed as a primary residence. (See ESSB 5183 (2019)). RCW 35.21.684, allows for tiny homes in manufactured home parks, and allows local jurisdictions more flexibility to authorize them in other zones). The City would propose amendments to address these recent state laws applicable across the city as well as in the subarea.

Transportation, Public Services, and Utilities

The City addresses public service levels of service in its Capital Facilities Plan Element. The element is updated periodically to ensure that proposed growth and change can be served.

EXHIBIT C TRANSPORTATION IMPROVEMENTS

C-1 LAKEWOOD TRANSPORTATION IMPROVEMENT PROGRAM IMPROVEMENTS

The Six-Year Comprehensive Transportation Improvement Program (TIP) for 2021-2026 was approved by the Lakewood City Council in June 2020, and is expected to be updated annually. This document outlines short and long term road projects, including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays, throughout the city. The following projects within the planned action area are identified in the TIP as long term improvements:

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study.
- Kendrick Street SW from 111th Street SW to 108th Street SW – sidewalks, street lighting, bicycle facilities.
- 112th/111th – Bridgeport to Kendrick – sidewalks, bicycle facilities, street lighting.
- 108th – Bridgeport Way to Pacific Highway – roadway patching and overlay.

C-2 PLANNED ACTION MITIGATION

Exhibit 1 hereto describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right-of-way acquisition or widening, rather only changes to signal timing to increase the efficiency of impacted intersections and roadways in the subarea. The exception is intersection of Pacific Highway and Sharondale Road, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better.

Per Planned Action requirements in Section B-1 planned action development in the study area will provide information about expected trips and will provide a traffic study where applicable per LMC 12.09.028. This analysis will affirm the mitigation approach and phasing, such as signal timing at Bridgeport Way/I-5 on and off ramps. It will also affirm the City' concurrency standards are met in LMC 18A.60.140.

Exhibit 1 to Exhibit C. Proposed Mitigation Measures

ID	INTERSECTION	NO ACTION	ALT 1	ALT 1 MITIGATED
4	Pacific Highway / Sharondale Road			
	Install signal using existing channelization and signal length consistent with adjacent intersections	F>100	F>100	A/4
8	Pacific Highway / Bridgeport Way			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.	D/54	E/78	D/49
11	Bridgeport Way / SB I-5 Ramp			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.	D/41	E/60	D/52

Source: Fehr & Peers, 2020.

SEPA ENVIRONMENTAL CHECKLIST

Lakewood Station District Subarea Planned Action

WAC 197-11-960

Purpose of checklist:

The State Environmental Policy Act (SEPA), chapter [43.21C](#) RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An environmental impact statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

Instructions for applicants:

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

Use of checklist for nonproject proposals:

For nonproject proposals complete this checklist and the supplemental sheet for nonproject actions (Part D). The lead agency may exclude any question for the environmental elements (Part B) which they determine do not contribute meaningfully to the analysis of the proposal.

For nonproject actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.

A. Background

1. Name of proposed project, if applicable:

Lakewood Station District Subarea Plan, Form-Based Code, and Planned Action

2. Name of applicant:

City of Lakewood

3. Address and phone number of applicant and contact person:

Tiffany Speir, Long Range & Strategic Planning Manager
Community Development Department
City of Lakewood
City Hall, 6000 Main St. SW
Lakewood, WA 98499
tspeir@cityoflakewood.us
253-983-7702

4. Date checklist prepared:

July 27, 2020; updated March 29, 2021

5. Agency requesting checklist:

City of Lakewood

6. Proposed timing or schedule (including phasing, if applicable):

Adoption Spring 2021

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

Future site specific development is anticipated under the nonproject subarea plan and development regulations.

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

Comprehensive Plan EIS, June 2000. The EIS contained a preferred alternative and two other alternatives, including a no action alternative and mixed-use alternative. The principal strategy of the preferred alternative was to: Protect established neighborhoods; Develop intensification within the city's spine, which stretched north along Bridgeport Way from the Lakewood Station, past the Town Center and the Colonial Center, through to the Custer Road neighborhood; Focused residential density in several neighborhoods, notably Springbrook, Tillicum, and Custer; and Increasing the employment base by converting parts of the Woodbrook Neighborhood into an industrial center. The preferred alternative provided 'development capacity' from an estimated 17,500 new residents and 12,275 new jobs.

Comprehensive Plan Supplemental EIS, 2003. There were 10 comprehensive plan amendments proposed in 2003 that would collectively redesignate numerous sections of the City of Lakewood from their existing land use and zoning designations to new designations. The majority of these amendments were relatively minor, parcel-specific

inconsistencies between the adopted future land use plan and existing or intended land uses. However, one amendment along Bridgeport Way, north of 75th Street SW, reduced high-density residential development in favor of commercial development (Wal-Mart).

Final Lakewood Downtown Subarea Plan and Draft Planned Action EIS, July 2018. The following proposals were evaluated for the Downtown to help support desired change and development: Subarea Plan, Comprehensive Plan land use, policy, and capital facility plan amendments, new hybrid form-based zoning standards, and upfront environmental review through a Planned Action consistent with RCW 43.21 c.440 and SEPA rules in WAC 197-11.

Lakewood Station District Subarea Situation Assessment – April 2020. Provides current conditions and trends across natural and built environment topics.¹

Lakewood Station District Subarea Transportation Technical Memo – October 2020. Provides transportation evaluation of the No Action and Planned Action Alternatives. See Appendix.

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.

A portion of the study area is under consideration for a master planned mixed use development called the Lakewood Landing.

10. List any government approvals or permits that will be needed for your proposal, if known.

The non-project action requires review and recommendations by the Lakewood Planning Commission and review and approval by the Lakewood City Council. It has undergone a 60-day review by state agencies led by the Washington Department of Commerce. It has been shared with regional agencies like Puget Sound Regional Council and Pierce County as well.

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.)

Comparison of Alternatives

Two alternatives are considered in this expanded checklist: the No Action Alternative that maintains current plans and regulations and a Planned Action. The Planned Action would expand housing and job opportunities.

The study area is an opportunity for future growth and development within Lakewood. First, there are several neighborhoods within a half mile of the Lakewood Station with a mix of multi-family and mixed residential zones that are developed at densities lower than planned or zoned. Second, with its proximity to I-5 and the Sounder regional commuter rail, the study area is a good location for employment. Third, since there are few environmental constraints, this is an ideal place to explore housing types and densities that take advantage of the proximity to high capacity transit. Fourth, the Town Center District is also only about a mile away (Exhibit 4) so there is also an opportunity to deepen connections between the two subareas.

Under the Planned Action, the City of Lakewood proposes to adopt a subarea plan for the Lakewood Station District, as well as a form-based code. As allowed under RCW 43.21 c.440, the City is developing a planned

¹ This report is available in City Council packet: <https://cityoflakewood.us/wp-content/uploads/2020/08/2020-08-10-Council-Agenda.pdf>.

action using a threshold determination process.

Following are key elements of the current plan (No Action proposal) and the Planned Action proposal.

Exhibit 1. No Action and Proposed Action Comparison

Element	No Action	Proposed Action
Land Use/Zoning	<ul style="list-style-type: none"> ▪ Existing Comprehensive Plan <ul style="list-style-type: none"> ○ Corridor Commercial ○ High Density Multi-Family ○ Neighborhood Business District ○ Open Space & Recreation ○ Public Semi-Public Institutional ▪ Existing Zoning <ul style="list-style-type: none"> ○ Commercial: C1, C2, NC2, TOC ○ MF3 ○ OSR2 ○ PI ▪ Density Range: 35 du/ac NC2 and 54 du/ac MF3 and TOC 	<ul style="list-style-type: none"> ▪ Retain Comprehensive Plan and Zoning designations, except: <ul style="list-style-type: none"> ○ Update Open Space & Recreation designation/zone. ▪ Zoning allowances: <ul style="list-style-type: none"> ○ Create new chapter in Title 18 for hybrid form-based code. Will serve functionally as an overlay zone. Address building and street typologies similar to Downtown, adapted for study area. ○ Update MF3 zone regarding incentives and standards for rowhouses/townhouses. ○ Consider Live/Work opportunities within areas where mixed use and townhomes are desired. Consider implications for home occupation types. ○ Promote Townhouses and Rowhouses at 30-40 du/ac. In form-based code, address 30 foot height and appropriate setbacks. Address minimum lot width and size allowing for townhomes and rowhouses. ▪ Land Use Assumptions: <ul style="list-style-type: none"> ○ Integrate Lakewood Landing. ○ Density Range: 35 du/ac NC2; 30-40 du/ac MF3 in study area; 54 du/ac TOC. Mixed use in proximity to station in MF3 as allowed currently.
Growth Capacity	<ul style="list-style-type: none"> ▪ The 2014 Buildable Lands Report indicated a low capacity of 258 new dwelling units and 1,186 jobs. <ul style="list-style-type: none"> ○ Application of 2014 Buildable Lands Report zone-based assumptions to underutilized and 	<ul style="list-style-type: none"> ▪ If achieving up to 40 units per acre in the MF3 zoned area and 54 units per acre in mixed use zones, capacity for residential would be about 1,019 new units. Lakewood Landing would result in another 760 planned units or a total of 1,779 dwellings.

Element	No Action	Proposed Action
	vacant lands shows a potential for up to 1,343 new dwelling units.	<ul style="list-style-type: none"> Job capacity would equal 1,186 jobs, plus Lakewood Landing jobs. Early phase includes retail jobs. Later phases include office jobs.
Growth Assumptions	<ul style="list-style-type: none"> Transportation Model 2010-2035: <ul style="list-style-type: none"> Households Distributed per Traffic Model: 962 Jobs Distributed per Traffic Model: 1,193 	<ul style="list-style-type: none"> Transportation Model 2010-2035: <ul style="list-style-type: none"> Households Distributed per Buildable Land Capacity Shares illustrating middle density distribution across MF3 zone (35 units per acre average): $962 + 760 = 1,722$ Jobs: $1,105 + 171$ Lakewood Landing retail jobs = 1,276
Housing	Zoning allows a variety of attached housing types up to 54 du/ac in TOC and MF 3 and 35 du/ac in NC 2.	<ul style="list-style-type: none"> Observed density in MF3 is nearly 12 units per acre. If redevelopable sites achieved 35 units per acre, the density would increase to about 26 units per acre zone wide or 65% of a maximum 40 units per acre. Focus on missing middle ownership: 65%-110% of area median income. Promote Townhouses and Rowhouses at 30-40 du/ac. Allow for/attract small builders as well as larger builders. Consider property assembly in area (10 acres brick & masonry plant). Opportunities for mixed use and open space.
Jobs	Service and retail are dominant sectors.	<ul style="list-style-type: none"> Encourage updated retail. Such as through façade improvement programs with existing properties and allowances for new commercial and mixed uses.
Parks and Open Space	Green streets, plazas and pocket parks, and oak tree preservation are highlights in the current Comprehensive Plan.	<ul style="list-style-type: none"> Address Open Space & Recreation designation/zone. Integrate Open Space into Larger Mixed Use Developments. Lakewood Landing: Courtyard Green streets and Alley Plaza near Occidental, within current rights of way

Element	No Action	Proposed Action
Transportation	<ul style="list-style-type: none"> ▪ Served by Pierce Transit, Intercity Transit, and Sound Transit <ul style="list-style-type: none"> ○ Sounder South Capacity Expansion ○ Sound Transit Access Improvements ○ Pierce Transit Long Range Plan ▪ Limited pedestrian and bike facilities to be improved – consistent with Comp Plan Transportation Element <ul style="list-style-type: none"> ○ Improvements Sidewalk Improvements – Kendrick Drive from 111th St to 108th St SW ○ Non-Motorized Access Improvements from 2009 NMTP and ST Access Improvement Study ○ Bridgeport Way Sidewalks ○ 111th/112th St SW Sidewalk Gap ○ 111th St SW Bike Lanes ○ Real-time parking availability signage ○ Bike Parking (short and long-term)consistent with TIP 	<p>Similar improvements as No Action plus:</p> <p>Streetscape Typologies</p> <ul style="list-style-type: none"> ▪ Allocate excess right-of-way for non-motorized uses ▪ Prioritize space for active modes over on-street parking ▪ Enhanced crossings <p>Connections</p> <ul style="list-style-type: none"> ▪ Bridge across I-5 (47th Avenue SW) <p>Other</p> <ul style="list-style-type: none"> ▪ Consider street vacation (depending on citywide policy review)

Lakewood Landing

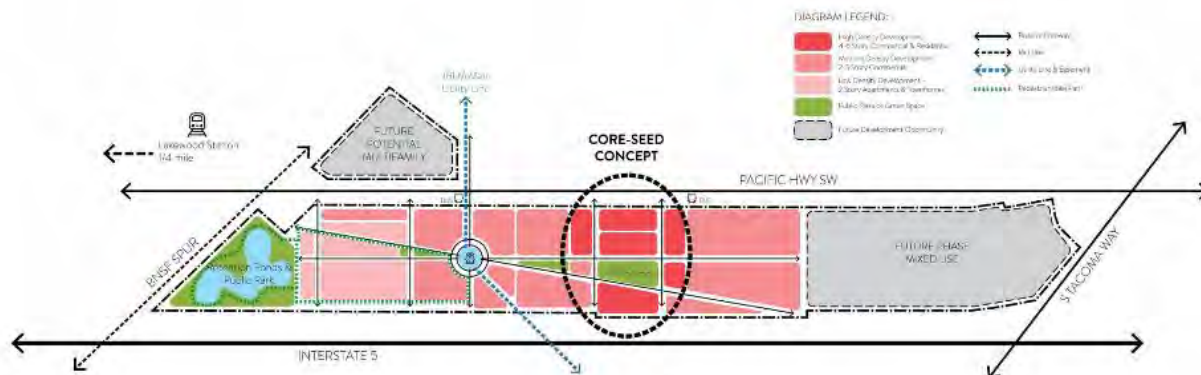
Lakewood Landing is a 30-acre site poised for mixed use and employment development. The site is adjacent to Sound Transit commuter rail station and between Pacific Highway and I-5. An initial mixed use plan with 760 units and 87,000 square feet of retail are part of Phase 1 in the Central Core. A Northwest concept, an aviation themed restaurant overlooking the McChord runway, coffee shops, a craft beer tap room, boutique shops, restaurants, and an interpretive educational park are all part of the long-term vision. Water features, carefully placed structures, and gabion walls will help to mitigate noise.

Phase I, Central Core

- 11.5 acres
- 5 over 2 construction and one level of underground parking
- 760 residential units
- 87,000 square feet of retail (about 171 employees)
- 1,474 parking spaces, including a parking field

Future phases are under consideration.

Exhibit 2. Lakewood Landing Location and Concept



Source: City of Lakewood 2020.

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist.

The proposed LSDS subarea boundary encompasses 231.1 parcel acres, and 329.5 gross acres; see Exhibit 3. Generally, the boundary incorporates the area within a half mile of the Sounder station and associated Sound

The subarea spans the area from the interchange with Bridgeport Way to the interchange with SR-512, to capture the full transportation corridor to the north and south of the station. In order to analyze transportation connections to Lakewood's Downtown, the transportation evaluation reviews an extended area north and west of the primary subarea (Exhibit 4). The extended study area looks at ways to enhance multi-modal transportation linkages. The extended study area does not address land use and housing beyond the LSDS.

Exhibit 3. Lakewood Station District Study Area



Source: BERK, 2020; Pierce County Assessor, 2020.

Exhibit 4. Lakewood Station in Proximity to Downtown



Source: BERK, 2020; Pierce County Assessor, 2020.

B. Environmental Elements

1. EARTH

a. General description of the site:

(circle one): Flat, rolling, hilly, steep slopes, mountainous, other _____

The study area is generally flat.

b. What is the steepest slope on the site (approximate percent slope)?

Slopes are 0 to 6 percent, with a representative slope of 3%.

c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils.

The study area is entirely mapped as 41A—Spanaway gravelly sandy loam. Properties include:

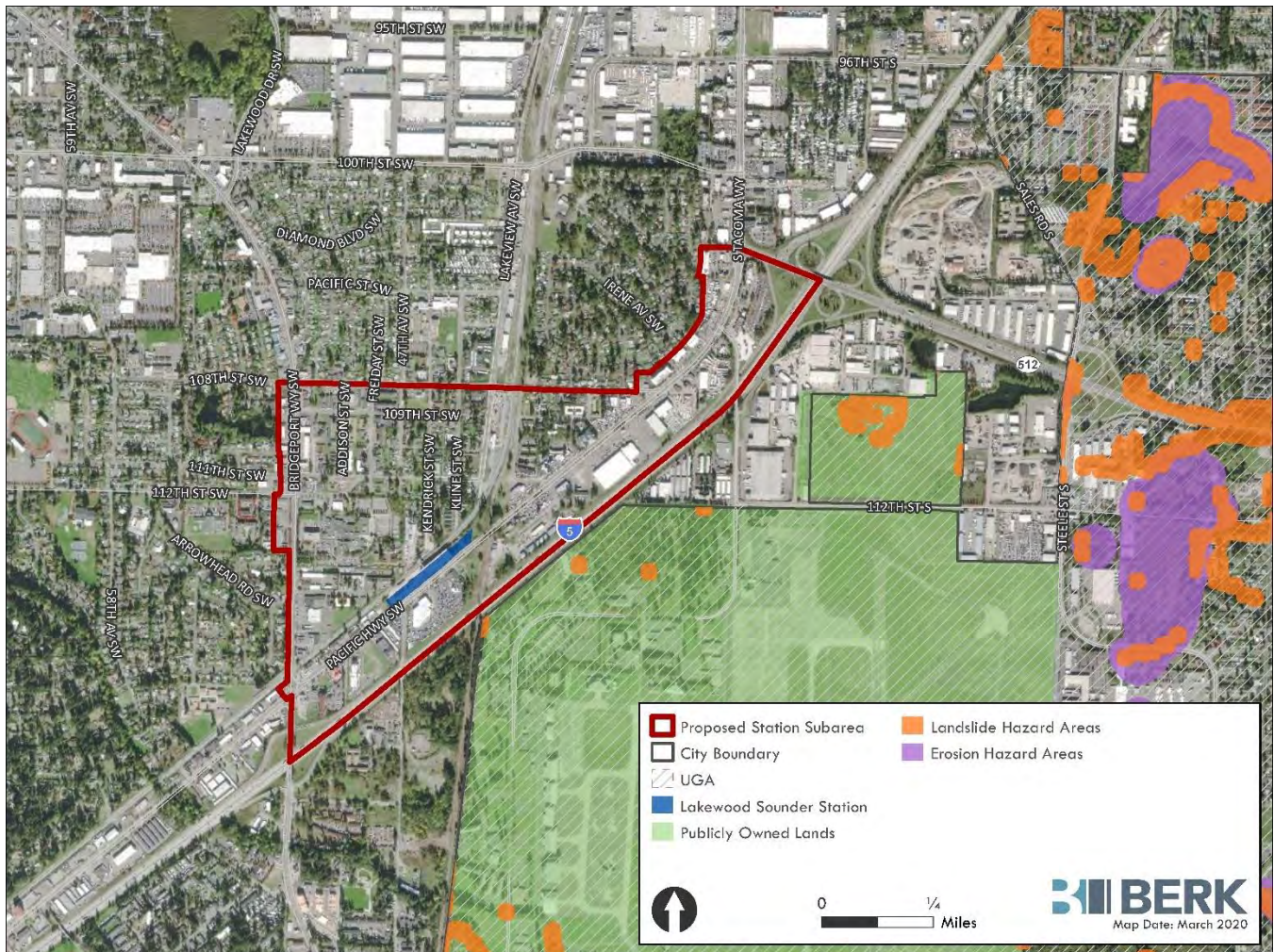
- Landform: Outwash plains
- Slope: 0 to 6%
- Depth to water table: More than 80 inches
- Hydric soil rating: No

Soil Survey Area: Pierce County Area, Washington, Survey Area Data: Version 16, Jun 4, 2020.

d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.

No. The City map of geologic hazards do not show liquefaction, erosion, or landslide hazard areas. See Exhibit 5.

Exhibit 5. Geologic Hazards



Source: City of Lakewood, BERK 2020.

e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites and require fill, excavation, or grading.

f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites and during construction expose soils to erosion.

g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

The applicable zones allow for about 70% impervious surfaces (MF3) to 100% impervious surfaces (NC2, TOC, C1 and C2). (LMC 18A.60.030 and 18A.60.040) Future site-specific development under either the No Action or Planned Action Alternatives may replace or increase impervious surfaces consistent with zoning.

h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

LMC Chapter 12.10 provides regulations addressing: storm drainage, earthwork (excavation and fill), soil reports, and erosion controls.

2. AIR

a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites. During construction there is a potential for dust. During operation there is potential for added emissions due to vehicular travel generated by the new growth. The Planned Action would increase dwellings and jobs in the study area and have a greater local potential for air emissions. Regionally, focusing growth in and near multimodal transportation opportunities could reduce vehicle miles travelled and associated air emissions compared to development on the fringe of the urban area. (Urban Land Institute, 2010)

Puget Sound Regional Council (PSRC) has identified that its 2019-2022 transportation improvement program. The Regional TIP includes continued implementation and expansion of commuter rail service between Seattle and Lakewood, and resurfacing of Pacific Hwy and South Tacoma Way between 108th ST SW and SR 512. The Air Quality Conformity Analysis indicates that the emissions from the projects and programs in the plan are below the established daily motor vehicle emissions budgets for PM2.5 and NOX. (Puget Sound Regional Council, 2018)

b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

The study area is located within a PM2.5 Maintenance Area, and in a former CO and Ozone maintenance area. I-5 forms the eastern border of the study area and results in air emissions to existing and future development.

c. Proposed measures to reduce or control emissions or other impacts to air, if any:

Providing for more mixed use, residential, and job opportunities in proximity to the in proximity to the Sounder Station and travel by transit is likely by a portion of residents and employees.

Application of the State Energy Code can improve energy efficiency and reduce greenhouse gas emissions. (LMC Chapter 15.05) Over time new standards will also be implemented: The 2019 Legislature adopted new standards that will increase the efficiency of these buildings and reduce emissions. The Legislature also increased efficiency standards for appliances. (Washington State Department of Ecology, 2019)

Dense landscaping along roadways can reduce air pollutants by up to 50% (Deshmukh, 2019) Green infrastructure is another source of potential air emission mitigation at a microscale (Tiwari, 2019). As part of the Subarea Plan update associated with the Planned Action, the City can promote landscaping and green infrastructure. It can also address orientation and location of residential uses in mixed use developments to reduce the potential for localized air quality effects and improve compatibility; for example residential development can be focused away from I-5 and business uses can buffer residential uses near the freeway.

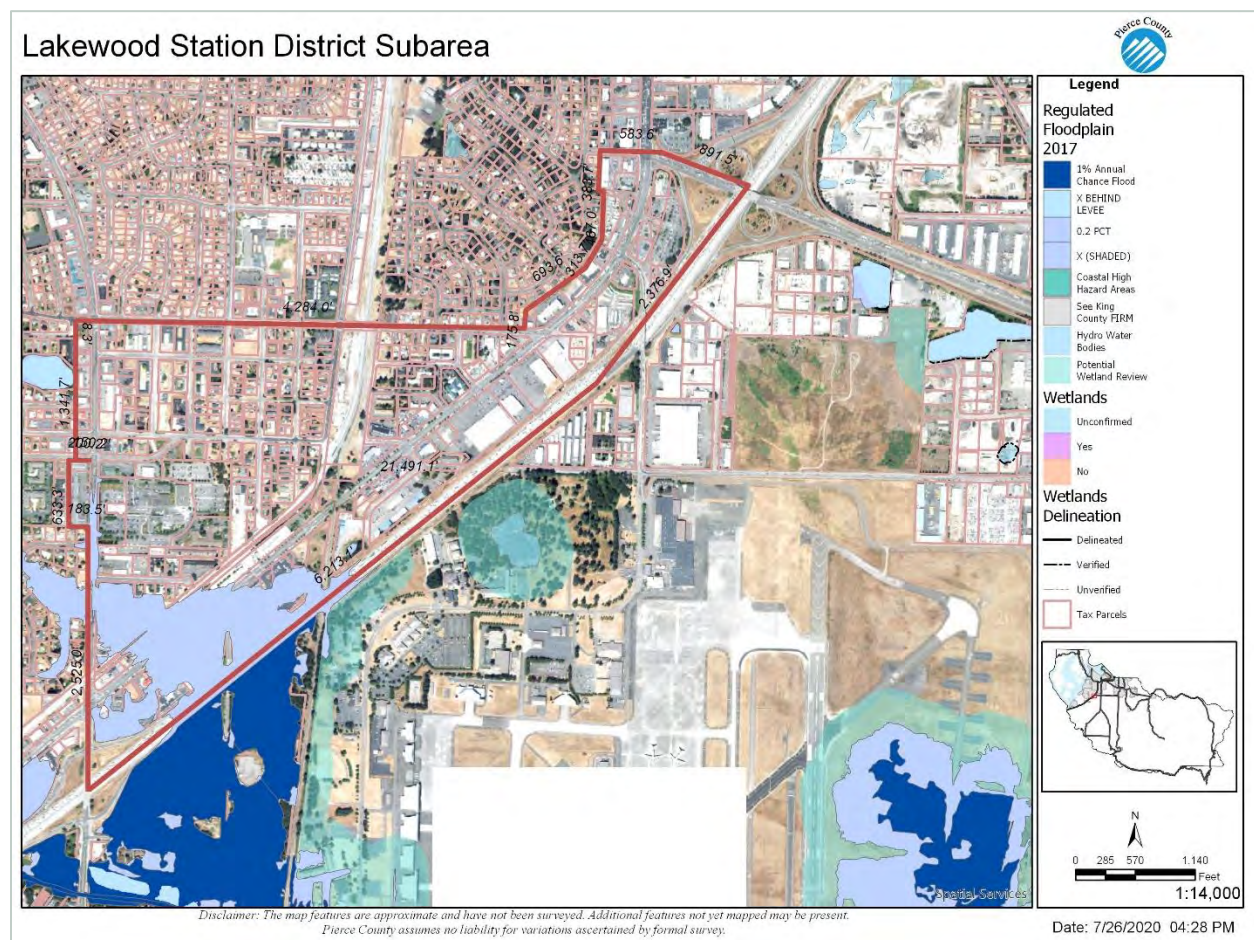
3. WATER

a. Surface Water:

1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

Exhibit 6 shows the hydrology of the area. There are no significant hydrological features within the subarea. Outside and abutting the northwest corner of the subarea is Hidden Lake, which is a delineated wetland just west of Bridgeport Way. Critical area buffer requirements may affect a limited number of parcels in the subarea west of Bridgeport. Along small lakes, regulated activities are subject to a 35-foot buffer requirement. (LMC 14.154.060) Wetland protection requirements depend on habitat quality and wetland categories, and buffers could range from 40 to 225 feet.

Exhibit 6. Hydrology in the LDS and Surrounding Area



Source: Pierce County PublicGIS, 2020.

2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

Redevelopment may occur on sites near Hidden Lake but would be subject to critical area buffers.

3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites and require fill, excavation, or grading though none of these activities are anticipated in a wetland given lack of presence. Fill of wetlands is not proposed as part of the legislative actions associated with the subarea plan and form-based code. Any activities affecting wetland buffers would be subject to Lakewood critical area regulations.

4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

No surface water withdrawals or diversions are anticipated. A public water system supplies the areas. Creeks are not located in the study area.

5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

See Exhibit 6. Zone X (shaded) is considered a moderate flood hazard area and is the area between the limits of the base flood and the 0.2-percent-annual-chance (or 500-year) flood. It is mapped along Bridgeport Way and between Pacific Highway and I-5 west of Lakeview Avenue SW. These are not regulated by the City's flood hazard regulations, which apply to special flood hazard areas, defined as the area that will be inundated by the flood event having a 1-percent chance of being equaled or exceeded in any given year.

6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

No point discharges of waste material to surface waters are anticipated. All development will be subject to the City's storm water management regulations that address nonpoint runoff. (LMC Chapter 12.11)

b. Ground Water:

1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

No wells are proposed. The area is served by the Lakewood Water District.

2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

No septic systems are proposed. The area is served by a public sewer system operated by Pierce County Planning and Public Works Sewer Utility.

c. Water runoff (including stormwater):

1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

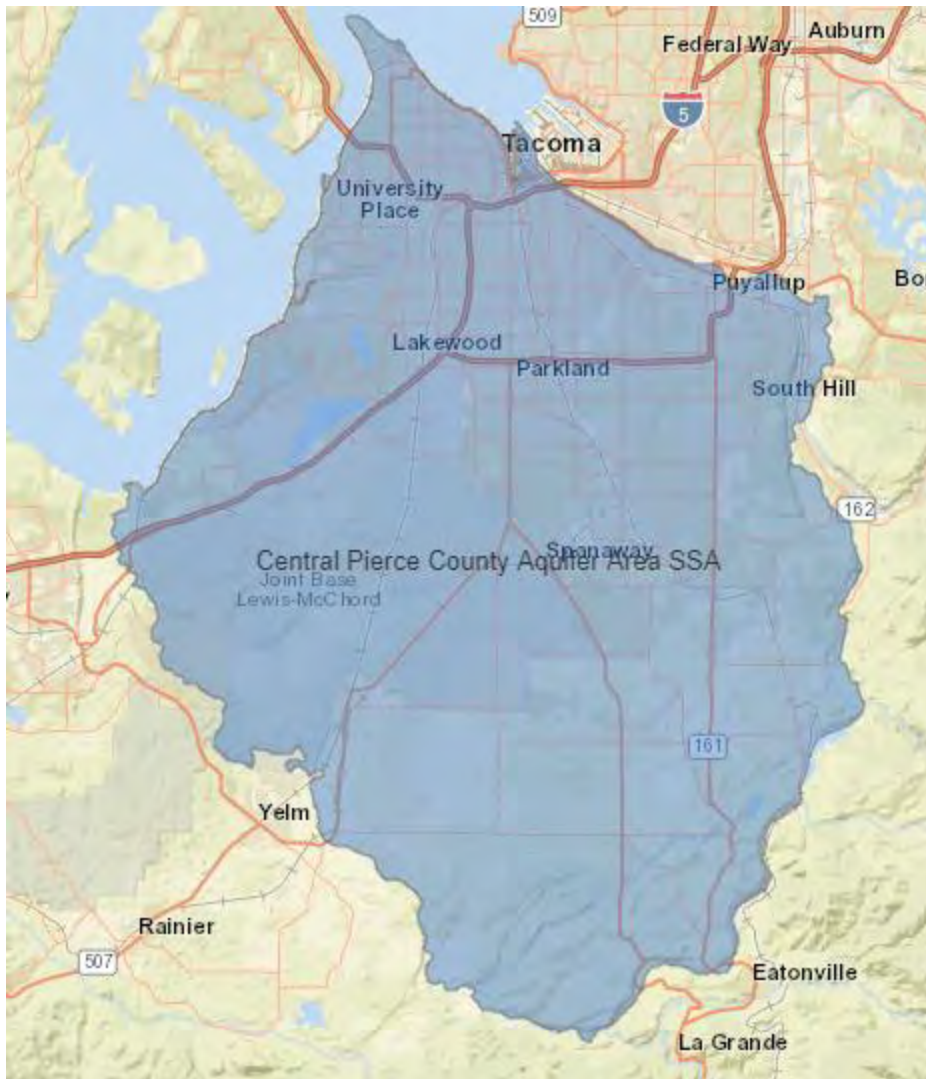
Stormwater runoff may occur though water quality and quantity is regulated, and should improve as sites developed under older regulations redevelop with newer regulations. Stormwater management will be required of all new development whether under the No Action or Planned Action Alternatives. (LMC Chapter 12.11) The City

applies the Washington State Department of Ecology Stormwater Manual and its Engineering Standards Manual.

2) Could waste materials enter ground or surface waters? If so, generally describe.

The City limits land uses that could result in groundwater contamination such as storage tanks, auto repair and auto washing facilities, dry cleaners, and others within the Clover/Chambers Creek Aquifer Basin boundary that encompasses the entire study area and beyond throughout Lakewood, Tacoma, and Central Pierce County. (LMC Chapter 14.150) See Exhibit 7. See C.1 regarding stormwater management.

Exhibit 7. Sole Source Aquifer in Central Pierce County



Source: USEPA 2020.

3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe.

The applicable zones allow for about 70% impervious surfaces (MF3) to 100% impervious surfaces (NC2, TOC, C1 and C2). (LMC 18A.60.030 and 18A.60.040) Future site-specific development under either the No Action or Planned Action Alternatives may replace or increase impervious surfaces consistent with zoning.

However, the Washington State Department of Ecology Stormwater Manual requires water quality treatment, including use of low impact development methods wherever feasible. (LMC Chapter 12.11)

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any:

Application of Washington State Department of Ecology Stormwater Manual and its Engineering Standards Manual at LMC Chapter 12.11.

Stormwater management will be required of all new development whether under the No Action or Planned Action Alternatives. (LMC Chapter 12.11)

The Subarea Plan and Form-Based Code proposed by the Planned Action includes alternative street cross sections and frontage improvements that would create an opportunity for added green infrastructure and stormwater management.

4. PLANTS

a. Check the types of vegetation found on the site:

The area is developed and has ornamental landscaping.

☒deciduous tree: alder, maple, aspen, other

☒evergreen tree: fir, cedar, pine, other

☒shrubs

☒grass

☐pasture

☐crop or grain

☐Orchards, vineyards or other permanent crops.

☐wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other

☐water plants: water lily, eelgrass, milfoil, other

☐other types of vegetation

b. What kind and amount of vegetation will be removed or altered?

Ornamental vegetation could be removed. However, landscaping will be required on sites.

c. List threatened and endangered species known to be on or near the site.

No threatened and endangered species are mapped in the study area. Hidden Lake abutting the study area to the west is a mapped aquatic habitat/wetlands under the State's Priority Habitats and Species program. (Washington State Department of Fish and Wildlife, 2020) It is protected by buffers. See Section B.3 above.

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

Future development would implement LMC Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. When redevelopment would occur, perimeter landscaping and parking area landscaping would be

required. Additionally significant trees would be retained where they exist.

The Planned Action Alternative would improve design standards applicable to the study area and provide standards for the public realm including street trees and green infrastructure.

e. List all noxious weeds and invasive species known to be on or near the site.

Noxious weeds are not mapped in detail, but along I-5 generally it is identified as an area with Spotted Knapweed. (Pierce County Weed Board, 2019) Noxious weeds are regulated by: RCW Chapter 17.10, and WAC Chapter 16-750.

5. ANIMALS

a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site.

Examples include:

✓birds: hawk, heron, eagle, songbirds, other:

✓mammals: deer, bear, elk, beaver, other: urban adapted wildlife

___fish: bass, salmon, trout, herring, shellfish, other _____

b. List any threatened and endangered species known to be on or near the site.

No threatened and endangered species are mapped near the study area. (Washington State Department of Fish and Wildlife, 2020)

c. Is the site part of a migration route? If so, explain.

All of Washington State is included in the Pacific Flyway that is traversed by migratory birds. (US Fish and Wildlife Service, 2020)

d. Proposed measures to preserve or enhance wildlife, if any:

The City maintains critical area regulations protecting fish and wildlife habitat conservation areas. Though none are mapped in the study area, if some were identified they would be protected under either the No Action or Planned Action Alternatives. (LMC Chapter 14.154)

e. List any invasive animal species known to be on or near the site.

No known invasive animal species have been identified in the study area.

6. ENERGY AND NATURAL RESOURCES

a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.

Lakeview Light and Power provides electrical service to the subarea and Puget Sound Energy provides natural gas service. Power and natural gas are available and are anticipated to serve future growth under either the No Action or Planned Action Alternatives.

b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe.

Heights range by zone from 60 to 90 feet but are lower at boundaries with lower intensity residential zones and should allow for solar energy use by adjacent uses (see LMC 18A.60.100). With the Form Based Code, heights would vary based on building type, and no building typologies would exceed current zoning and may be lower. For example, a townhouse/rowhouse style building would have a height more like three stories. Pedestrian areas in commercial zones must be designed with solar exposure in mind. (LMC 18A.70.040)

c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:

All future development would be subject to the State Energy Code. (LMC Chapter 15.05)

7. ENVIRONMENTAL HEALTH

a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe.

1) Describe any known or possible contamination at the site from present or past uses.

Exhibit 8 shows sites of known contamination in the LSDS. Apart from a dry-cleaning site located north of Pacific Highway and south of St. Clare Hospital, the sites are located between Pacific Highway and I-5. Contamination from petroleum products, metals, solvents, and lead is the result of current or past activities on these sites.² Clean-up is started on all the sites except the Flying B #18 site. Sites are in various stages of the clean-up process and subject to Washington State's Model Toxics Control Act.³

² Specific information on each site and the clean-up efforts that are underway can be found at: <https://ecology.wa.gov/Spills-Cleanup/Contamination-cleanup/Cleanup-sites/Locate-contaminated-sites>.

³ RCW 70.105D.

Exhibit 8. Environmental Clean-up Sites in the LDS and Surrounding Area



Source: BERK, 2020; Pierce County Assessor, 2020.

2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity.

The presence of I-5 is also a potential environmental health risk for those living in the subarea according to the Centers for Disease Control and Prevention.⁴ Those living within about 500 feet of a major highway have increased risk for exposure to carbon monoxide, nitrogen dioxide, ozone, and particulates. There are a variety of potential health impacts including increased incidences of asthma and lung disease, cardiovascular disease, cancer, and adverse birth outcomes. Children, older adults, households with low incomes, and those with underlying health conditions may be at higher risk for impacts.

⁴ <https://ephtracking.cdc.gov/showProximityToHighways.action>

3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project.

Local businesses such as auto sales or service operations may store toxic or hazardous chemicals but are regulated by local development regulations and state laws.

4) Describe special emergency services that might be required.

No special emergency services are anticipated. Commercial businesses would require City review of building and fire codes, and inspections by West Pierce Fire and Rescue.

5) Proposed measures to reduce or control environmental health hazards, if any:

The State Model Toxics Control Act (MTCA) sets standards for cleanup of lower levels of contaminants that are incorporated into new development and redevelopment parcels noted to have contamination potential. The City applies relevant standards regarding hazardous materials handling in the International Fire Code and Zoning Codes. Compliance with the Lakewood Municipal Code including aquifer protection regulations also addresses hazardous materials storage and handling.

It is recommended that the Planned Action Ordinance incorporate the following mitigation measure:

- The potential for airborne toxics from high volume roads like I-5 can be mitigated through site and building design and by increasing access to transit and non-motorized transportation options. Residential uses could be located 500 feet from the centerline of I-5 at a minimum.
- Applicants for development shall conduct a site assessment to determine if contamination is present from past use.

b. Noise

1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

I-5 is a primary source of traffic noise, and to a lesser extent traffic on Pacific Highway. Sounder Station operations also contribute to transportation noise sources. JBLM military operations result in aircraft noise.

2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

New residential and employment growth could contribute traffic trips and add cumulatively to noise. However, locating new housing and jobs in proximity to the Sounder Station could allow for more transit usage.

Land use could locate in proximity to noise sources, including residential uses, exposing population to noise.

3) Proposed measures to reduce or control noise impacts, if any:

LMC Chapter 8.36 Noise Control applies to nuisances under both studied alternatives.

Under the Planned Action, the following are proposed:

- Limiting residential uses within 500 feet of I-5 centerline to address air quality concerns could also be beneficial for noise reduction.

- Within Lakewood Landing, measures to reduce noise include: water features, carefully placed structures, and gabion walls.

8. LAND AND SHORELINE USE

a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe.

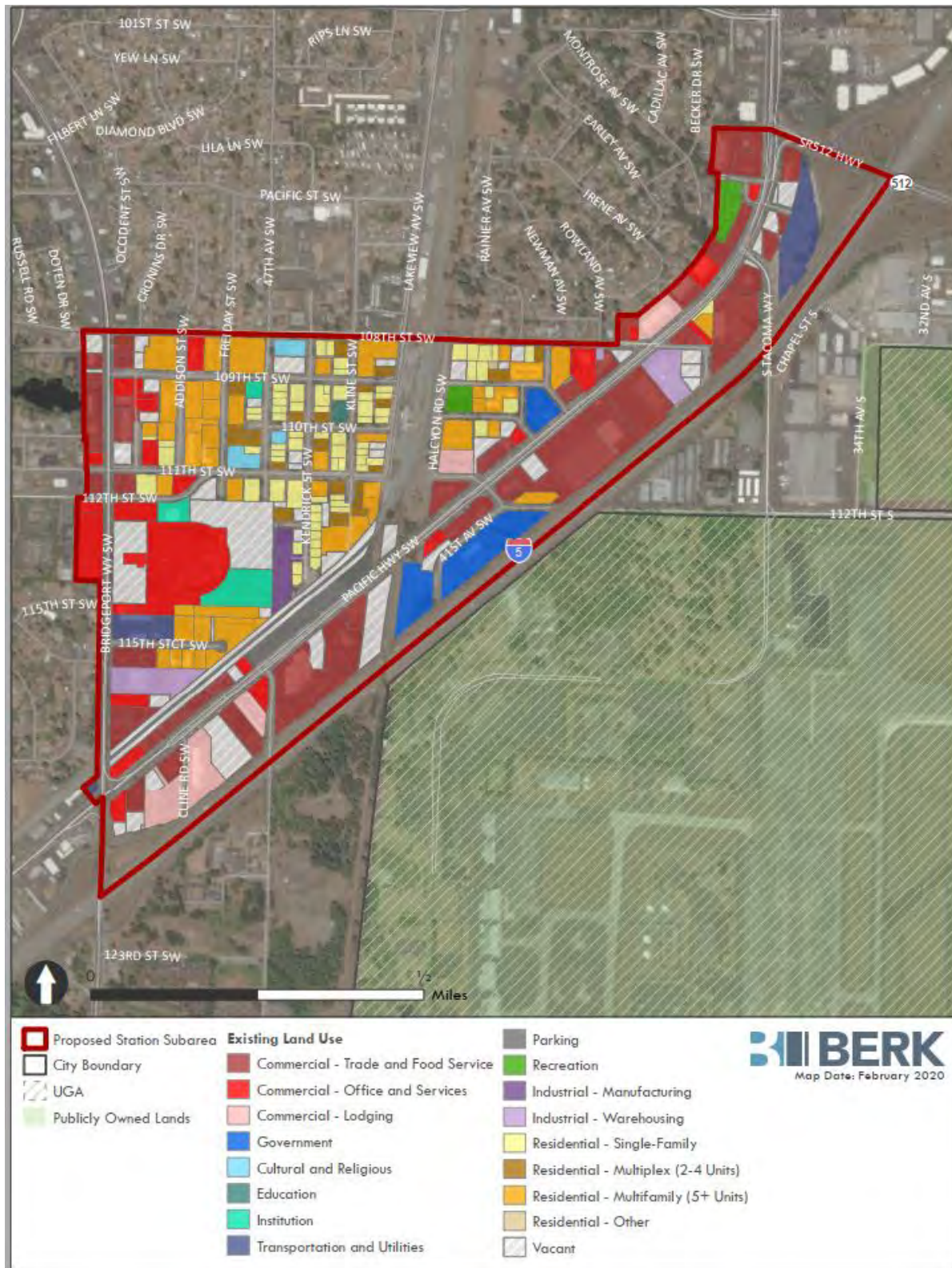
Most of the study area is in commercial use, followed by multifamily and single family residential combined, and vacant lands. See Exhibit 9 and Exhibit 10.

Exhibit 9. Current Land Use and Acres

Category	Acres	Share
Commercial - Food Service	8.7	4%
Commercial - Lodging	9.0	4%
Commercial - Office	4.5	2%
Commercial - Retail/Wholesale Trade	45.5	22%
Commercial - Service	10.5	5%
Cultural and Religious	1.9	1%
Education	0.5	0%
Government	9.0	4%
Industrial - Manufacturing	2.5	1%
Industrial - Warehousing	5.7	3%
Institution	13.9	7%
Parking	3.5	2%
Recreation*	2.7	1%
Residential - Multifamily (5+ Units)	29.4	14%
Residential - Multiplex (2-4 Units)	5.3	3%
Residential - Other	0.1	<1%
Residential - Single-Family	15.6	7%
Transportation and Utilities*	6.9	3%
Vacant	35.7	17%
Grand Total	211.0	100%

This includes a RV park at Halcyon that could be reclassified as multifamily. At 109th is Water District Well and should be Utility though it provides open space values.

Exhibit 10. Existing Land Use



Note: Recreation includes RV park and recreation activities.

Source: BERK, 2020; Pierce County Assessor, 2020.

b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use?

The site is urban and not used for agriculture. The area is not designated as a resource land of long-term significance.

1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how:

Not applicable. The study area is in the city limits and contains and is surrounded by urban lands.

c. Describe any structures on the site.

The sites contain residential and commercial structures. Commercial structures apart from the hospital and lodging are generally one or two stories in height. The hospital is 2-3 stories in height. The Candlewood Hotel is 3 stories. The Sounder Station Garage extends 4 stories. Residential structures vary in height but are generally less than three stories. See Exhibit 11.

Exhibit 11. Description of Residential Structures in Study Area

Type of Residential Structures	Sum of Units
1 1/2 Story Fin	14
1 Story	68
2 Story	4
Addon Only Res	0
Apartment <= 3 Stories	20
Detached Garage	0
Duplex 1 1/2 Story	2
Duplex One Story	20
Duplex Two Story	10
Home For the Elderly	5
Mixed Retail w/ Res Units	15
Multiple - Residential	662
Triplex One Story	3
Triplex Two Story	3
RV Park: 1 Apartment, 11 Mobile Homes & 26 RV Spaces	38
Grand Total	864

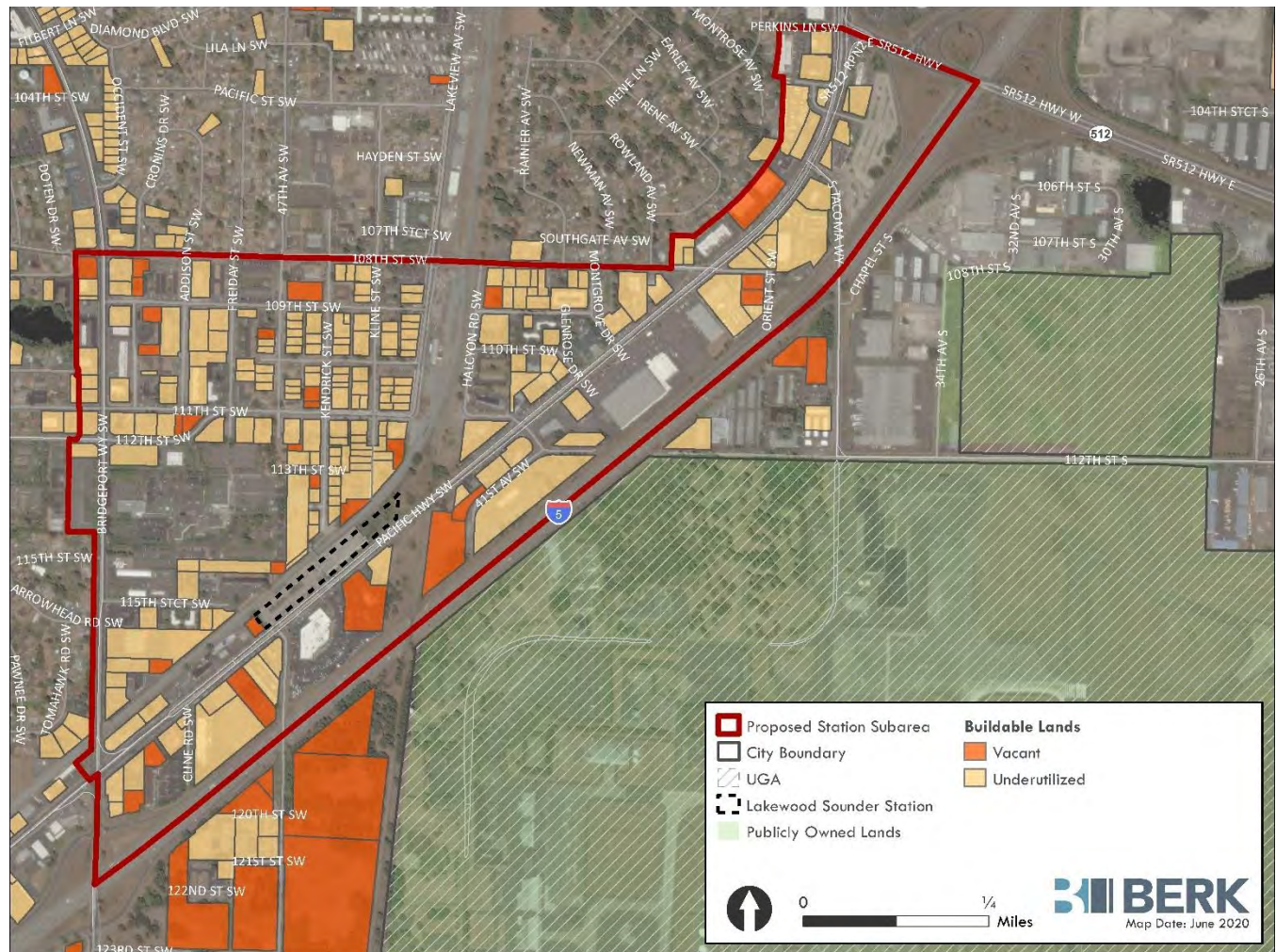
Source: Pierce County Assessor, BERK 2020.

d. Will any structures be demolished? If so, what?

Structures may be demolished and replaced by new residential and commercial development.

Sites most likely to change were identified by a 2014 Buildable Lands Report prepare by Pierce County in conjunction with cities. See Exhibit 12.

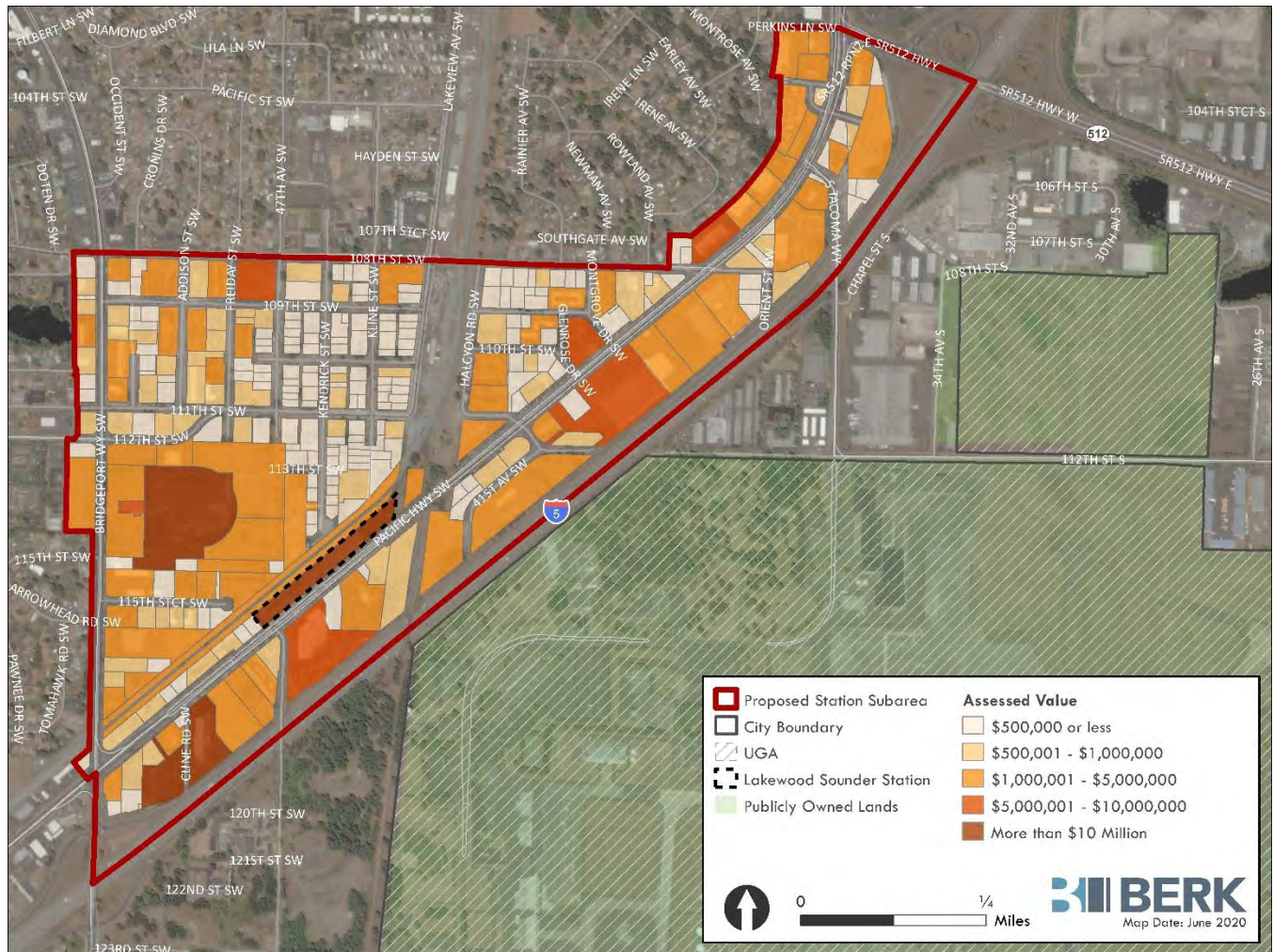
Exhibit 12. Buildable Lands: Vacant and Underutilized Properties



Source: Pierce County 2014, BERK 2020.

While the buildable lands data relies on older 2010 information published in 2014, the information is still relatively intact reviewing building to land values. Where land values are relatively equal or higher than building values, changes to land use are more likely. See Exhibit 13.

Exhibit 13. Building to Land Values



Source: Pierce County 2014, BERK 2020.

e. What is the current zoning classification of the site?

Most of the study area is zoned Transit-Oriented Commercial (TOC), Public Institutional, Multifamily 3 (MF3) and Commercial 2 (C2). See Exhibit 14. See also 12.a below.

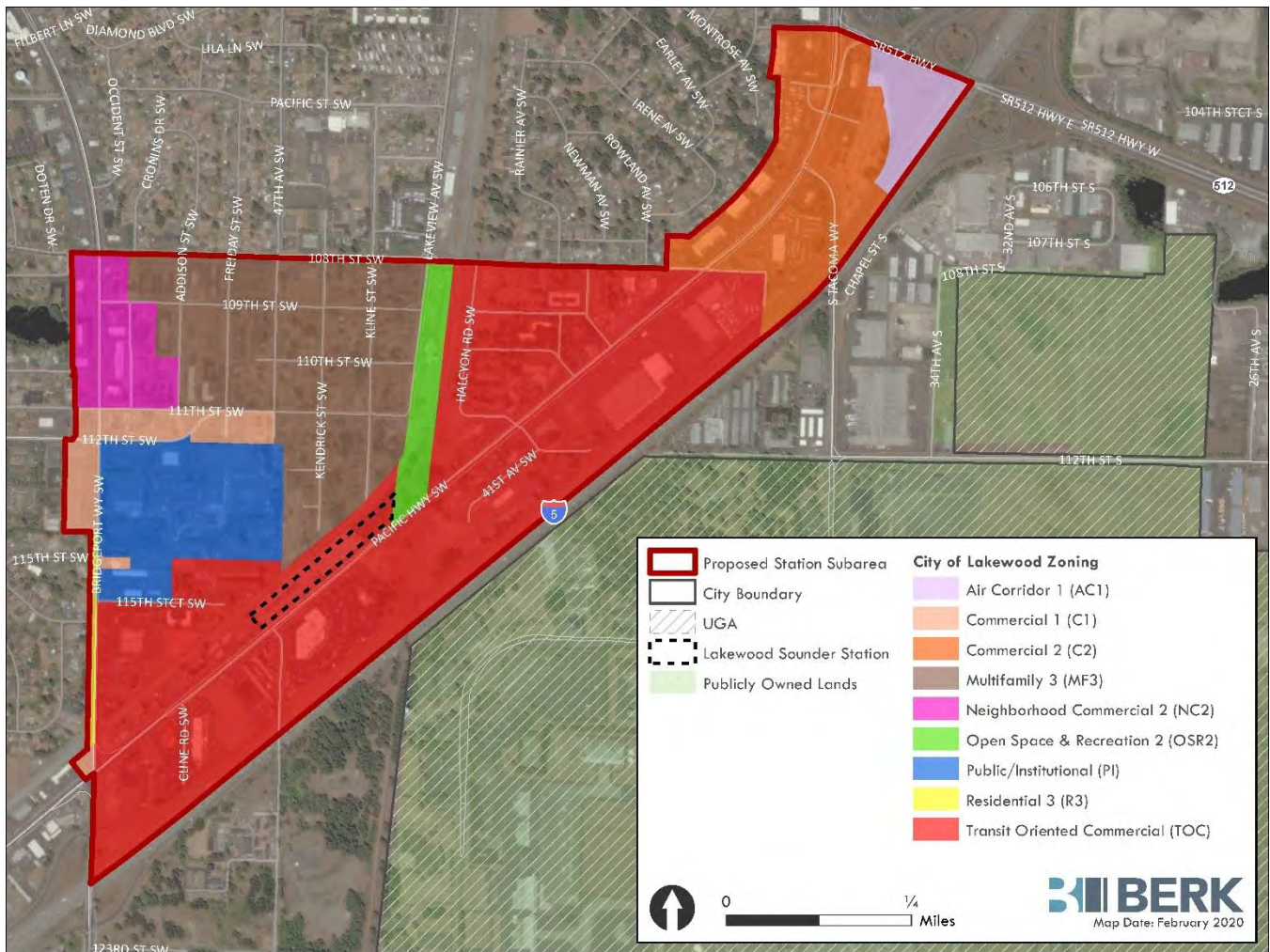
Exhibit 14. Current Zoning Acres

Zone District	Acres
Air Corridor 1	0.3
Commercial 1	7.9
Commercial 2	25.1
Multifamily 3	41.1
Neighborhood Commercial 2	10.6
Open Space & Recreation 2	0.6

Zone District	Acres
Public Institutional	46.0
Transit Oriented Commercial	99.5
Grand Total	231.1

Source: City of Lakewood, BERK 2020.

Exhibit 15. Current Zoning Map



Source: City of Lakewood, BERK 2020.

Under the Planned Action, typologies for buildings would be allowed based on a regulating plan that varies street typologies. See Exhibit 16 for the Station District Regulating Plan. With the Station District Form Based Code, building typologies would be defined; see Exhibit 17. A townhouse/rowhouse style building typology is added compared to Downtown.

Exhibit 16. Draft Planned Action Regulating Plan – Station District



Source: Framework 2021.

Exhibit 17. Form-Based Code Frontage Types – Station District

Street type	Sidewalk width	Linear	Forecourt	Plaza	Landscape	Porch /Stoop /Terrace	Parking
Mixed-Use Street	10' minimum	P	P	P/R1	P	P	X
Arterial Mixed-Use Street	As determined by Public Works	P	P	P	P	P	P
Residential Street	8' minimum	X	P	X	P	P	X
Green Street	8' minimum	X	P	X	P	P	X
Plaza Street	12' minimum or shared street	P	P	P	P	X	X

P=permitted, X=prohibited, R=required

1 Required when on a corner lot.

Description

Linear. The linear building frontage has zero setback from the street edge and is the primary frontage type on retail streets and is also appropriate for land uses such as townhouses and row houses.

Forecourt. The forecourt building frontage type has an open area at the entrance along the street edge. This building type is applicable to a wide range of land use types and mixed-use development.

Plaza. The plaza frontage type includes a pedestrian-oriented plaza between the building and the street edge. The plaza frontage type is applicable to retail and dining uses and can support activities such as outdoor dining, public art displays, seating, entertainment, and events.

Landscape. The landscape frontage has landscaping between the building and street edge. The landscape frontage is not permitted on retail streets and is appropriate for office and residential uses particularly when on the ground floor.

Porch/Stoop/Terrace. This frontage type has the building set back from the street edge to accommodate a porch, stoop, or terrace to serve as the primary access to the building. This frontage type is applicable primarily to residential and nonretail commercial uses and is not permitted on retail streets.

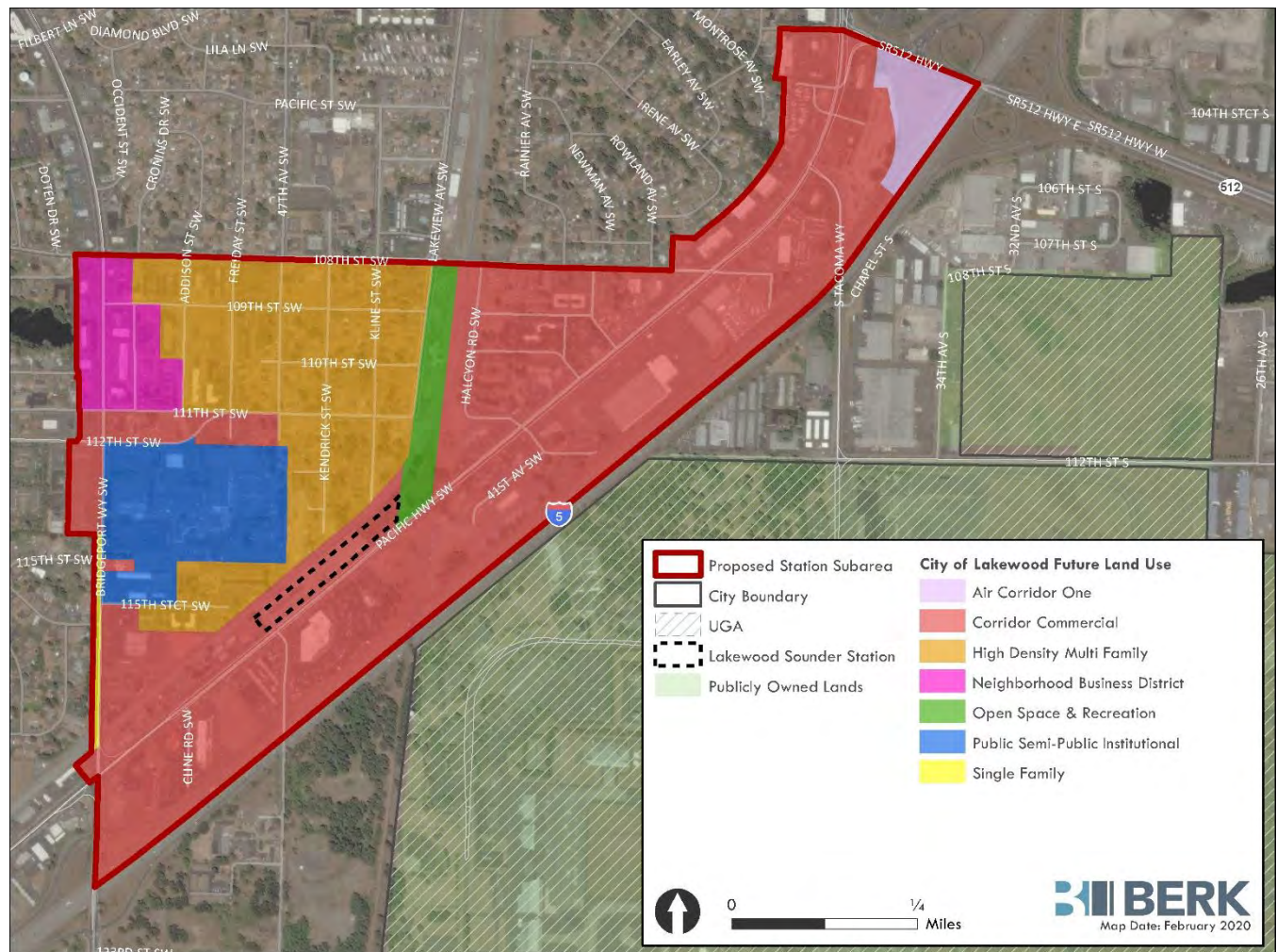
Parking. The parking frontage type is only permitted on arterial mixed-use streets and is the only frontage type where parking is allowed between the building and the street edge. Landscape and trees are required in the landscaped area between the street edge and the parking area.

The proposed code encourages the use of alleys to provide secondary access to properties, de-emphasize parking lots, and to promote continuous building frontages. Alleys shall meet Public Works engineering standards. Along Occidental, a special designation for Plaza Street identifies acceptable development frontages similar to Mixed Use Streets.

f. What is the current comprehensive plan designation of the site?

Most of the study area is designated as Corridor Commercial, or High Density Multi Family. There are smaller areas of Neighborhood Business District and Public Semi-Public Institutional. See Exhibit 18. See also 12.a below.

Exhibit 18. Current Comprehensive Plan



Source: City of Lakewood, BERK 2020.

g. If applicable, what is the current shoreline master program designation of the site?

There are no waterbodies or designated shorelines in the study area.

h. Has any part of the site been classified as a critical area by the city or county? If so, specify.

The area is considered an aquifer recharge area and contains a 500-year floodplain. There are no mapped wetlands or streams or geologic hazards. See Sections B1 and B3. There are no mapped threatened and endangered plants or animals; see Sections B.4 and B.5.

i. Approximately how many people would reside or work in the completed project?

Growth capacity ranges from 1,345 to 1,780 dwellings. There would be at least 1,190 to 1,280 jobs rounded, with a few thousand commercial jobs likely in later phases of the Lakewood Landing site. The lower range

represents the No Action Alternative and the Planned Action represents the higher range. Growth assumptions are similar to growth capacity though slightly lower to test a little less than maximum capacity (e.g. midrange density in amended MF3 zone). See Exhibit 19.

Population associated with 1,722 households under the Planned Action would equal about 4,047 persons assume 2.35 persons per household. Under the No Action, the expected population would equal 2,261.

Exhibit 19. Study Area Growth Capacity and Assumptions

	No Action Alternative	Planned Action Alternative
Growth Capacity	<ul style="list-style-type: none"> ■ The 2014 Buildable Lands Report indicated a low capacity of 258 new dwelling units and 1,186 jobs. <ul style="list-style-type: none"> ○ Application of 2014 Buildable Lands Report zone-based assumptions to underutilized and vacant lands shows a potential for up to 1,343 new dwelling units. 	<ul style="list-style-type: none"> ■ If achieving up to 40 units per acre in the MF3 zoned area and 54 units per acre in mixed use zones, capacity for residential would be about 1,019 new units. Lakewood Landing would result in another 760 planned units or a total of 1,779 dwellings. ■ Job capacity would equal 1,186 jobs, plus Lakewood Landing jobs. Early phase includes retail jobs (approximately 171). Later phases will be determined but could include several thousand office jobs.
Growth Assumptions	<ul style="list-style-type: none"> ■ Transportation Model 2010-2035: <ul style="list-style-type: none"> ○ Households Distributed per Traffic Model: 962 ○ Jobs Distributed per Traffic Model: 1,193 	<ul style="list-style-type: none"> ■ Transportation Model 2010-2035: <ul style="list-style-type: none"> ○ Households Distributed per Buildable Land Capacity Shares illustrating middle density distribution across MF3 zone (35 units per acre average): $962 + 760 = 1,722$ ○ Jobs: 1,105 plus 171 Lakewood Landing = 1,276

j. Approximately how many people would the completed project displace?

The study area has portions considered at moderate and high displacement risk. (Puget Sound Regional Council, 2019)⁵ Residents and businesses may be priced out of the area as new development occurs.

k. Proposed measures to avoid or reduce displacement impacts, if any:

There is sufficient growth capacity to replace existing job space as sites redevelop. The City can promote development standards that allow for commercial space in mixed use areas to maintain affordable rents (e.g. adjust parking ratios for businesses less than a certain size similar to Downtown).

There is sufficient capacity to replace existing housing as the sites redevelop. The City does provide inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing in Chapter 18A.90

⁵ According to PSRC, displacement risk is a composite of indicators representing five elements of neighborhood displacement risks: socio-demographics, transportation qualities, neighborhood characteristics, housing, and civic engagement.

Housing Incentives Program.

The intent of the Planned Action is to create more incentives for missing middle ownership housing such as townhomes/rowhouses. The City could add the area to its multi-family tax exemption for affordable housing in LMC Chapter 3.64 Tax Incentive Urban Use Center Development.

l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

The No Action Alternative would implement Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. The Planned Action would implement similar landscaping and tree presentation standards and custom design standards for the buildings and public realm in a hybrid form-based code.

Under the Planned Action, rowhouses/townhouses (e.g. 35 feet focusing on building types such as duplexes, triplexes or rowhouses in the MF3 zone would be less disruptive for existing single family residents that are located in pockets of the zone.

m. Proposed measures to reduce or control impacts to agricultural and forest lands of long-term commercial significance, if any:

None required.

9. HOUSING

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.

Currently there are 864 dwellings; see Exhibit 11. Growth capacity ranges from 1,345 to 1,780 dwellings. See Exhibit 19.

b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

About 159 of the total permanent dwellings and RVs could be displaced due to redevelopment on underutilized lands. The Oakknoll Trailer Park is on 1.7 acres and would not meet all the criteria of LMC 18A.40.110 due to its acreage. It may not meet space per unit, open space, and other standards.

Exhibit 20. Units and Potential for Displacement

Unit Type	Units on Vacant and Underutilized Land
Comm Land With SFR	1
Duplex 2 Units	14
Fourplex 4 Units	3
Multi Fam Apts 5 Units Or More	26
Other Residential	1
Single Family Dwelling	71
Triplex 3 Units	5

Grand Total	121
Grand Total With RV and Mobile Homes	159

Source: Pierce County Assessor, BERK 2020.

c. Proposed measures to reduce or control housing impacts, if any:

The draft subarea plan includes strategies to reduce displacement effects. There is sufficient capacity to replace existing housing as the sites redevelop. The City does provide inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing in Chapter 18A.90 Housing Incentives Program. The City could also implement RCW 36.70A.545 which allows for an increased density bonus for affordable housing located on property owned by a religious organization. The City's program in 18A.90 could be adapted to this recent state law, which requires that 100% of the units be affordable.

The intent of the Planned Action is to create more incentives for missing middle ownership housing such as townhomes/rowhouses. The City could add the study area to its multi-family tax exemption for affordable housing in LMC Chapter 3.64 Tax Incentive Urban Use Center Development.

Based on recent state laws, tiny homes and recreational vehicles must be allowed as a primary residence. (See ESSB 5183 (2019)). RCW 35.21.684, allows for tiny homes in manufactured home parks, and allows local jurisdictions more flexibility to authorize them in other zones). The City will implement strategies in the Subarea Plan largely through the Form-Based Code, but will also determine implementation through its code docketing process.

10. AESTHETICS

a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

Heights range by zone from 60 to 90 feet but are lower at boundaries with lower intensity residential zones (see LMC 18A.60.100). With the Form Based Code, building typologies would be similar to those applied in Downtown. Heights would vary based on building type, and no building typologies would exceed current zoning standards and may be lower. For example, a townhouse/rowhouse style building would have a height more like three stories.

Exhibit 21. Current Height by Zone

	MF3	NC2	TOC	CBD	C1	C2
Building height	80 feet	60 feet	90 feet	90 feet	60 feet	60 feet

Source: Lakewood Municipal Code, Title 18, BERK 2020.

b. What views in the immediate vicinity would be altered or obstructed?

There are no protected views in the study area. Greater building heights could result in view of Mount Rainier.

c. Proposed measures to reduce or control aesthetic impacts, if any:

The No Action Alternative would implement Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. The Planned Action would implement similar landscaping and tree presentation standards and custom design standards for the buildings and public realm in a hybrid form-based code.

Under the Planned Action, promoting MF3 building heights around three stories/35 feet, and focusing on building types such as duplexes, triplexes or rowhouses is less disruptive for existing single family residents.

11. LIGHT AND GLARE

a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

Future development allowed in the study area would add interior and exterior lights to buildings and parking lots to address night time visibility.

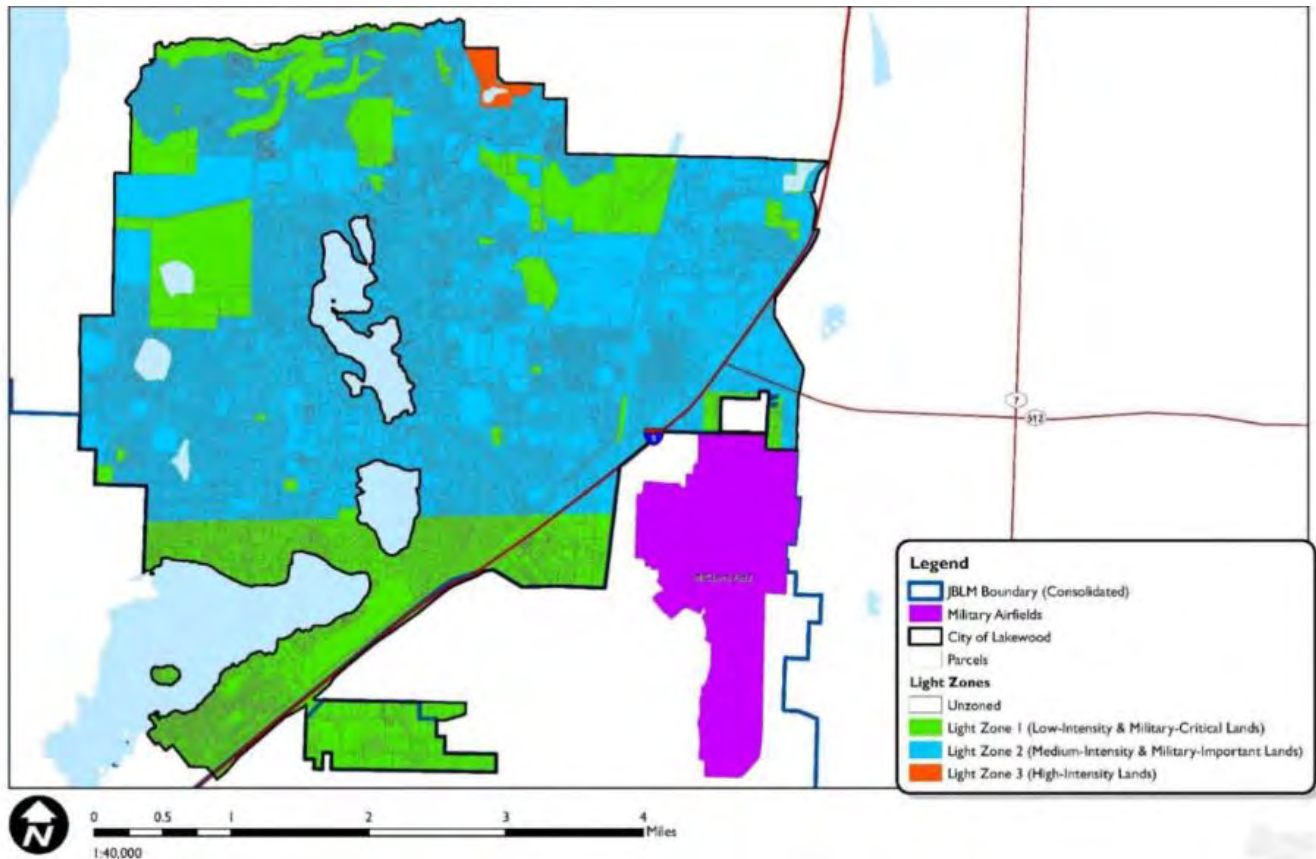
b. Could light or glare from the finished project be a safety hazard or interfere with views?

Impacts are not anticipated from typical residential and commercial uses, which are regulated by City lighting standards.

The subarea is subject to lighting standards and requirements to prevent interference with aircraft operation at JBLM.⁶ The northern portion of the subarea, near the interchange with SR-512 is part of Light Zone 2 and the rest of the subarea is in Light Zone 1 (Exhibit 22). Light Zone 1 is applicable to low-intensity land uses with low levels of exterior lighting at night. Light Zone 2 applies to medium intensity uses with model levels of exterior lighting such as residential, mixed-use, and commercial areas. Lighting standards are also intended to reduce light pollution, conserve energy, and provide safety and security. Generally, the code requires lighting to be shielded to prevent light shining above the luminary and to prevent light spill over on to adjacent properties.

⁶ 18A.60.095

Exhibit 22. Lakewood Light Zones



Source: Lakewood Municipal Code, 18A.60.095

c. What existing off-site sources of light or glare may affect your proposal?

Highways and streets abutting the study area like I-5 produce light.

d. Proposed measures to reduce or control light and glare impacts, if any:

LMC 18A.60.095 Outdoor Lighting Code provides lighting standards to reduce light and glare and to avoid interference with JBLM operations.

12. RECREATION

a. What designated and informal recreational opportunities are in the immediate vicinity?

Early conceptions of the subarea included the development of a linear park along the railway tracks on land now owned by Sound Transit. See Exhibit 23.

The Open Space designation/zone may be modified to be in a Public/Institutional designation and zone as the railway tracks where the land is owned by Sound Transit and not likely to become a dedicated off-street open space. However, there are opportunities for linear parks/trails.

Exhibit 23. Lakewood Station District Subarea Concept

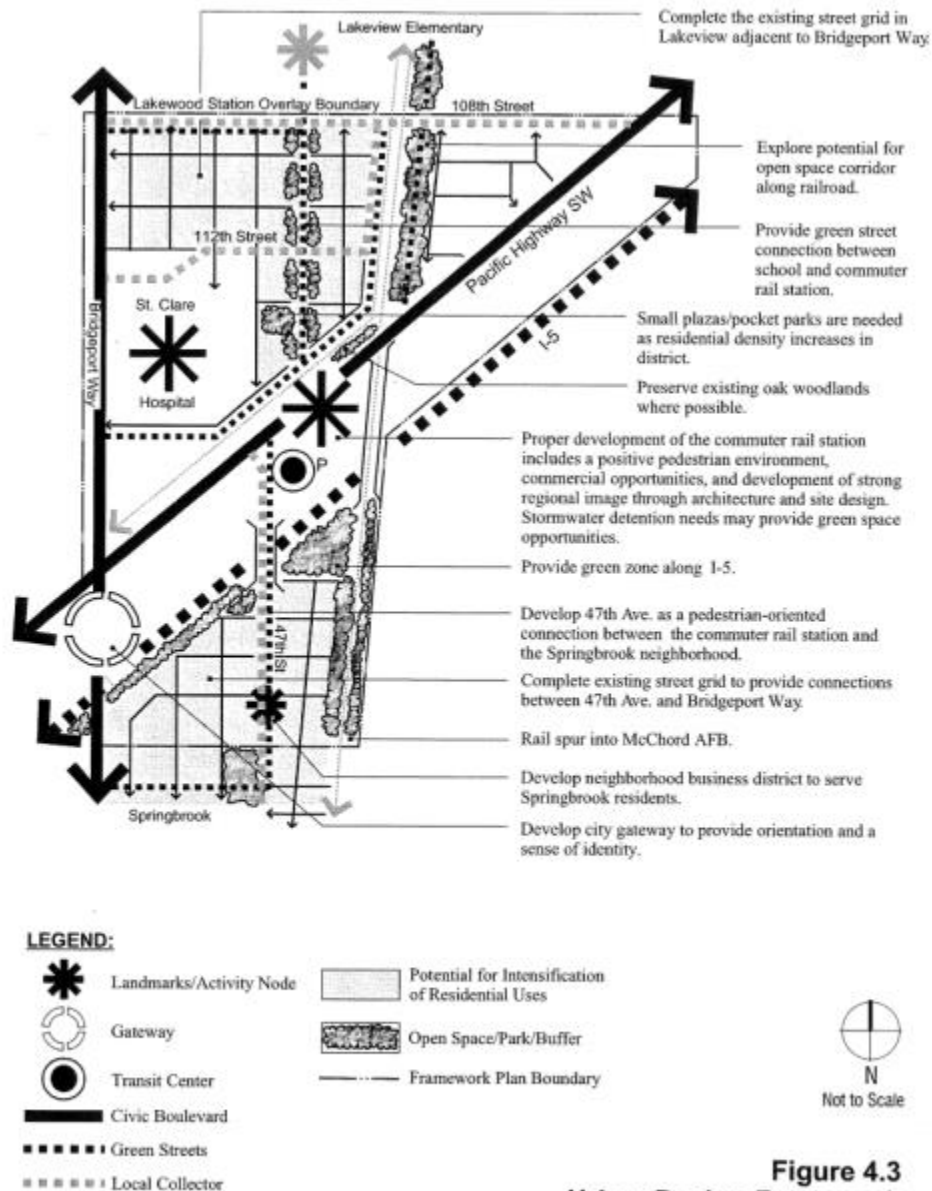


Figure 4.3
Urban Design Framework
for Lakewood Station District

SOURCE: EDAW, Inc. 1999

Source: Lakewood Comprehensive Plan, 2014.

b. Would the proposed project displace any existing recreational uses? If so, describe.

No parks exist or would be displaced. The area is beyond a 10-minute walk of parks per the City's Legacy Parks Plan 2020 (Figure 3-14).⁷ The City's plan calls for a park in or near the Lakewood Station District. With planned sidewalk improvements (see B.14) and street frontage improvements there is an opportunity to provide for linear parks.

The Planned Action proposes onsite open space standards for multifamily and commercial uses similar to the

⁷ See: <https://cityoflakewood.us/wp-content/uploads/2020/05/FINAL-Legacy-Plan-2020-w-pg-numbers-flattened-1.pdf>.

Downtown. The Subarea Plan includes the following public space concepts:

- Lakewood Landing will include a landscaped public plaza in its first phase to support community gathering and provide urban open space.
- A linear park along Kendrick Street south of 111th Avenue SW that would bring green space to the heart of the district's residential area. This would better define the pedestrian connection between Lakewood Station and the Lakeview Hope Academy and facilitate the use of the playground there as a neighborhood park as envisioned in the *Legacy Plan 2020*.
- Green corridors with enhanced landscaping in areas with planned pedestrian and bicycle improvements such as 111th /112th Avenue SW.
- A pocket of open space in the alley near Occidental Street.
- A linear park connecting Lakewood Station to the Springbrook Area through improvements along 47th Avenue as envisioned in the *Legacy Plan 2020*.

Public and semi-private open spaces will help to balance the intensity of land use in the LSDS. This could include the development of plazas, pocket parks, courtyards, or other landscaped open spaces that would provide opportunities for gathering and passive recreation.

c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

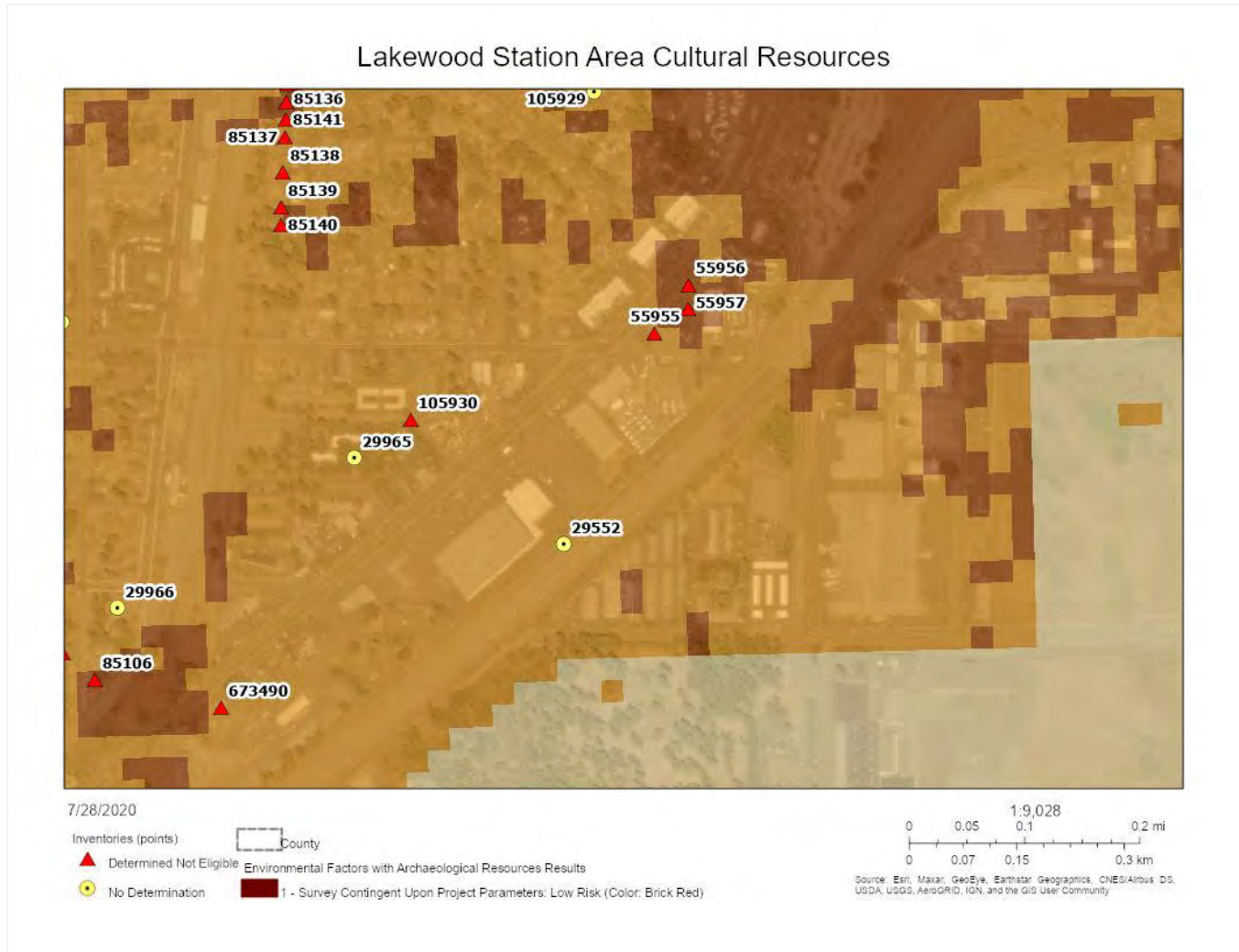
Under the Planned Action there would be application of common and private open space standards. Green Streets and other public spaces listed in B.12.b above are identified for enhanced pedestrian and landscaping experience. The form-based code and subarea plan include cross sections for streets that fit into current rights of way.

13. HISTORIC AND CULTURAL PRESERVATION

a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers? If so, specifically describe.

No structures are determined as eligible, and some are undetermined according to the Department of Archaeology and Historic Preservation (DAHP). See Exhibit 24. There are several properties that have structures that are over 45 years old. See Exhibit 25.

Exhibit 24. Department of Archaeology and Historic Preservation Historic and Cultural Resources

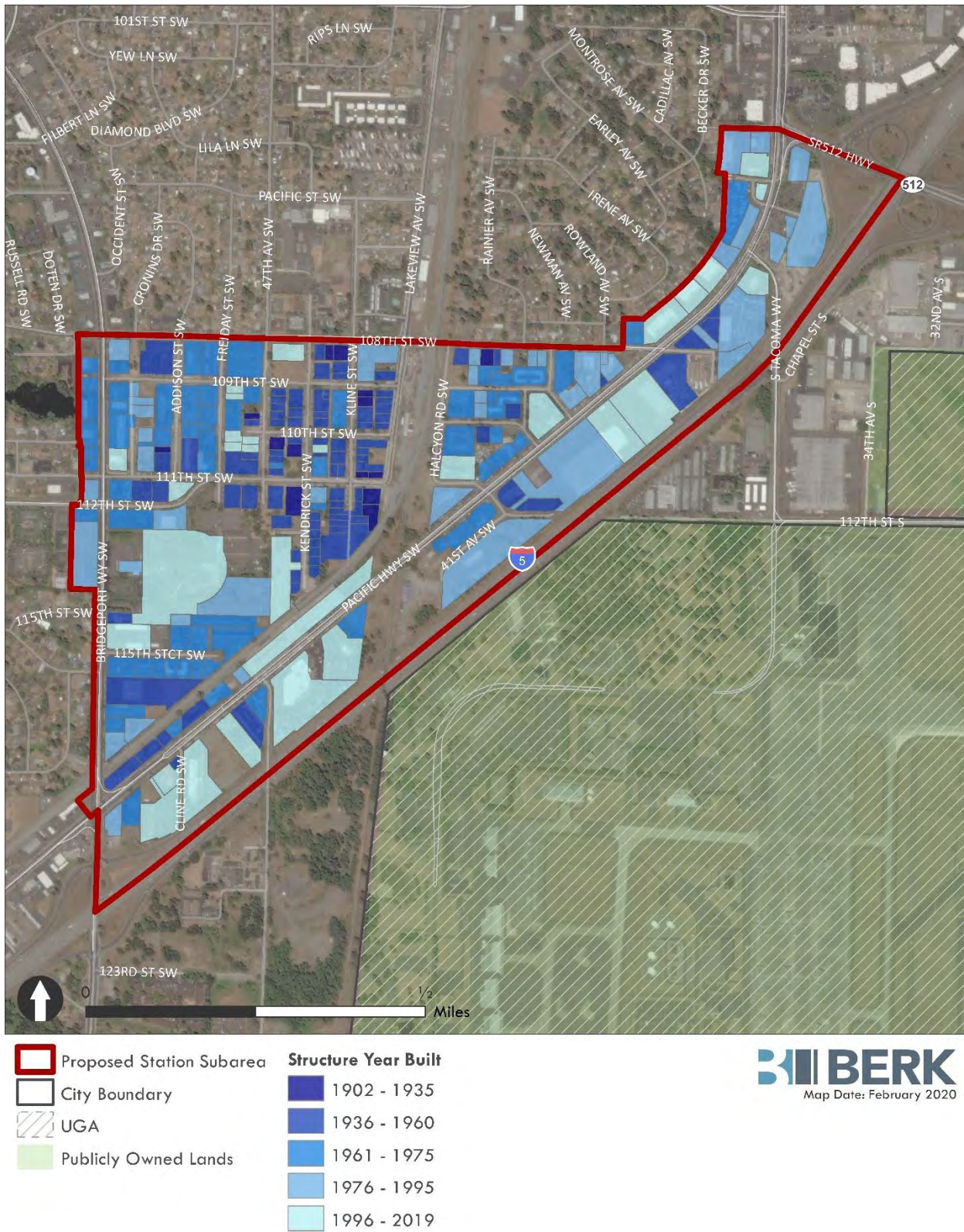


Source: Department of Archaeology and Historic Preservation, WISAARD, 2020.

Additional Legend:

- 1 - Survey Contingent Upon Project Parameters: Low Risk (Color: Brick Red)
- 2 - Survey Contingent Upon Project Parameters: Moderately Low Risk (Color: Burnt Orange)
- 3 - Survey Recommended: Moderate Risk (Color: Orange)
- 4 - Survey Highly Advised: High Risk (Color: Pale Yellow)
- 5 - Survey Highly Advised: Very High Risk (Color: Brightest Yellow/Canary Yellow)

Exhibit 25. Age of Buildings



Source: Pierce County Assessor, BERK 2020.

b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources.

There are areas of low to moderate risk of finding cultural resources per Exhibit 24.

c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc.

Methods include review of Pierce County Assessor Records, DAHP WISAARD program, and Lakewood's local register review.

d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required.

Washington State has a number of laws that oversee the protection and proper excavation of archaeological sites (RCW 27.53, WAC 25-48), human remains (RCW 27.44), and historic cemeteries or graves (RCW 68.60). The Governor's Executive Order 05-05 requires state agencies to integrate DAHP, the Governor's Office of Indian Affairs, and concerned tribes into their capital project planning process. This executive order affects any capital construction projects and any land acquisitions for purposes of capital construction not undergoing Section 106 review under the National Historic Preservation Act of 1966.

Under RCW 27.53, DAHP regulates the treatment of archaeological sites on both public and private lands and has the authority to require specific treatment of archaeological resources. All precontact resources or sites are protected, regardless of their significance or eligibility for local, state, or national registers. Historic archaeological resources or sites are protected unless DAHP has made a determination of "not-eligible" for listing on the state and national registers.

The City applies Lakewood Municipal Code Chapter 2.48 Protection and Preservation of Landmarks.

It is proposed that the following mitigation measures be made part of the Planned Action Ordinance in order to be considered Planned Action. This would include:

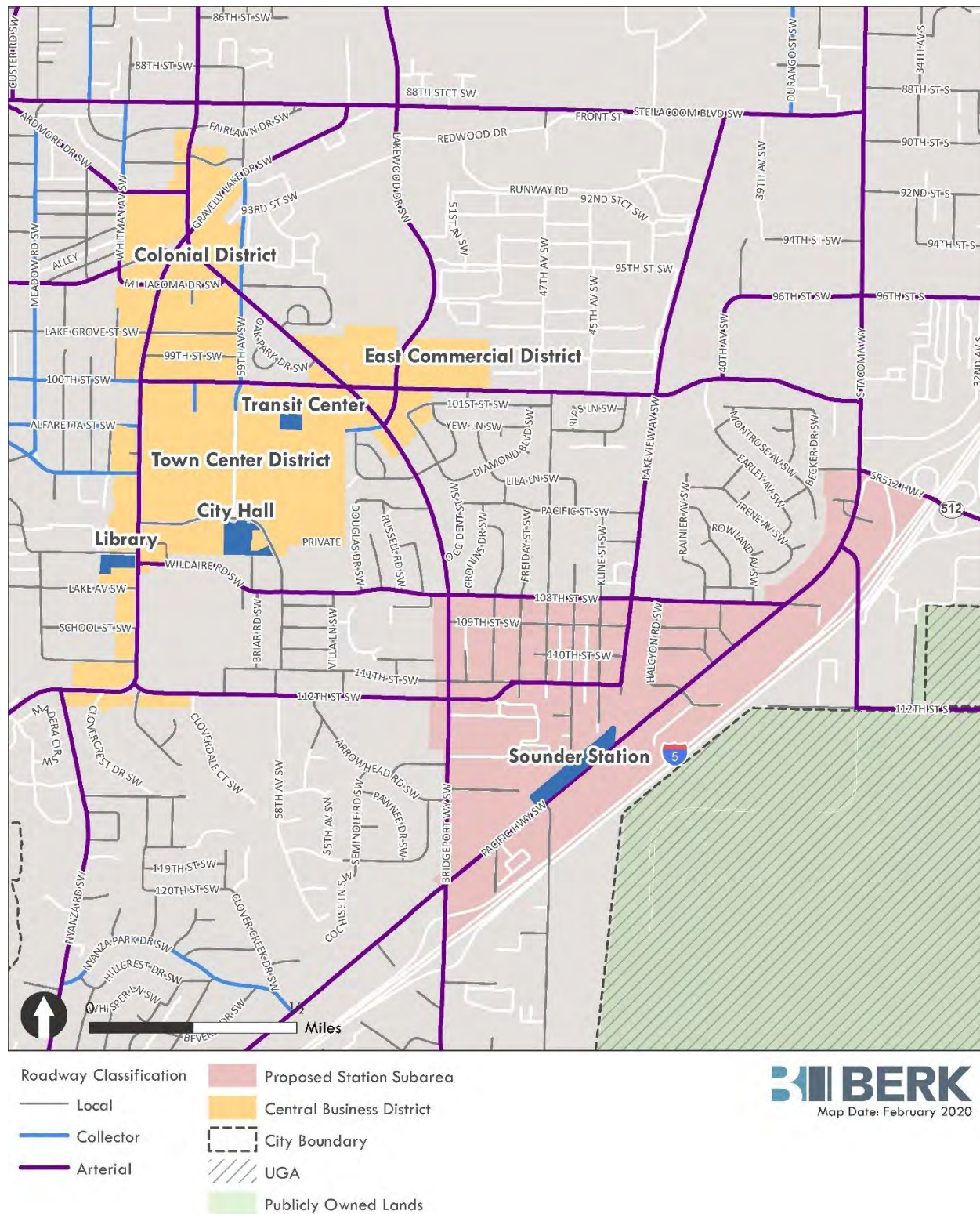
- Include Inadvertent Human Remains Discovery Language recommended by DAHP as a condition of project approval.

14. TRANSPORTATION

a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any.

Major roadways in the subarea include Pacific Highway, 108th Street SW, and Bridgeport Way. These are classified as either principal or minor arterials with 35 mph posted speed limits. Pacific Highway provides a north-south connection between Tacoma and Lakewood, with access to 1-5 ramps and the Lakewood Station within the subarea. See Exhibit 26.

Exhibit 26. Road Classifications in Study Area and Vicinity

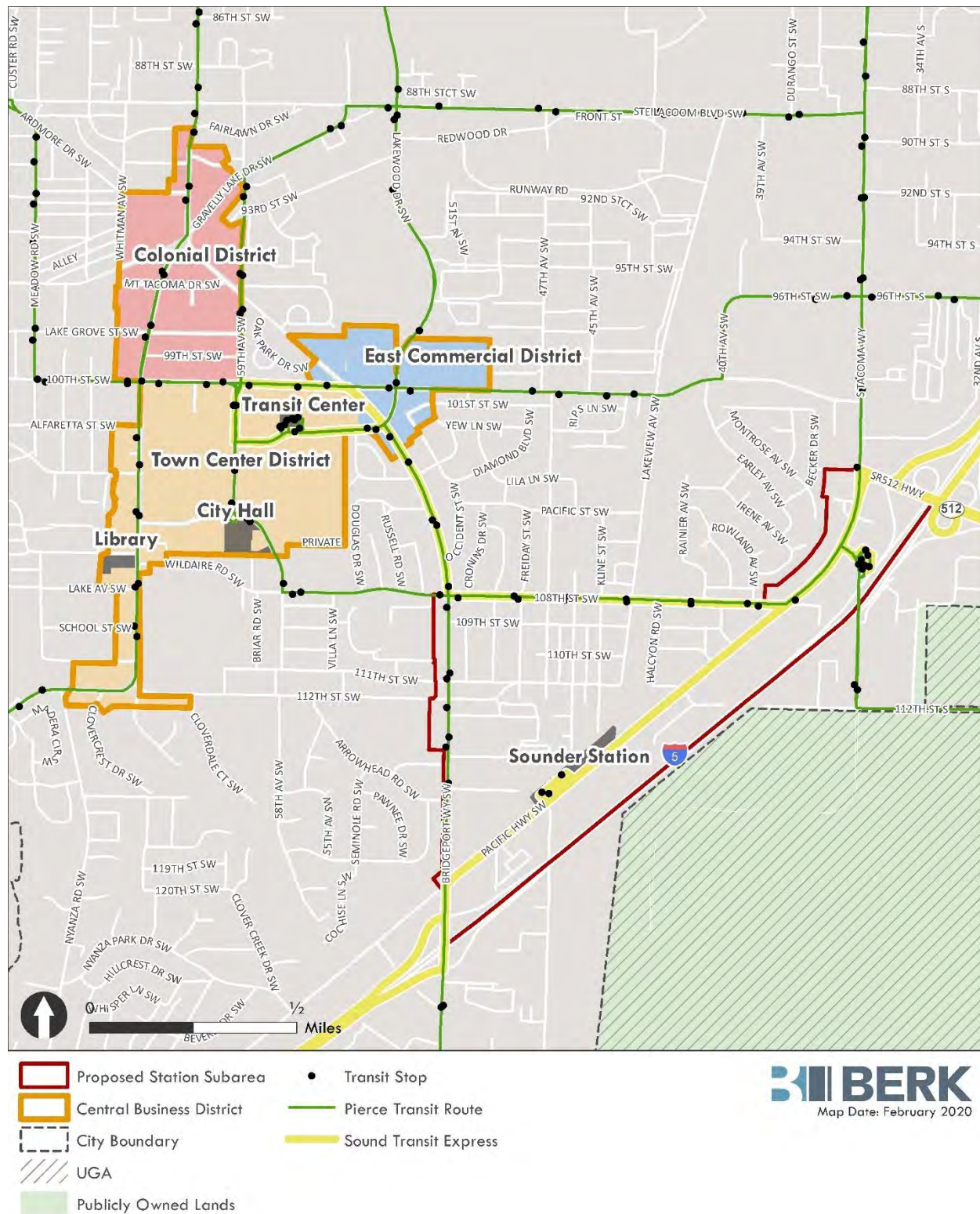


Source: City of Lakewood, Fehr & Peers 2020.

b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop?

Lakewood Station is a focal point of many regional commuter trips including service to downtown Seattle via the Sounder train in addition to bus service to local and regional cities operated by Sound Transit and Intercity Transit. The SR 512 Park & Ride provides bus connections for local and regional commuters to Lakewood Town Center, Tacoma, Puyallup, SeaTac, Dupont, and Olympia and communities throughout the South Sound. Pierce Transit, Sound Transit, and Intercity Transit provide bus service to the SR 512 Park & Ride via Pacific Highway and I-5, Bridgeport Way, and 108th Street SW. Both Lakewood Station and SR 512 Park & Ride provide vehicle parking for transit users.

Exhibit 27. Transit Service in Study Area and Vicinity



Source: Pierce Transit, City of Lakewood, Fehr & Peers 2020.

c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate?

There is very little on-street parking on arterials in the study area, but on-street parking exists on several local streets. Commercial uses tend to have dedicated parking lots adjacent to their buildings, frequently buffering the building from the street. St Clare Hospital provides off-street parking at multiple lots on the hospital's campus. Parking intended for transit users is provided at the Lakewood Station garage (600 spaces) and at the SR 512 Park & Ride (493 stalls). During the daytime, the SR 512 Park & Ride is at about 90 percent capacity. Bicycle parking is provided at Lakewood Station garage via bicycle racks and lockers.

Under the No Action Alternative, future uses must comply with the requirements in Chapter 18A.80 LMC. The Planned Action would follow parking standards for residential, retail, and office uses, and otherwise follow LMC Chapter 18A.80 for others.

Exhibit 28. Station District Proposed Specific Parking Standards

LAND USE	PARKING REQUIREMENT: STAFF RECOMMENDATIONS	BICYCLE PARKING
Residential	<p>Single-family: 2 per dwelling unit</p> <p>Accessory dwelling: 1 per dwelling unit, provided that no additional parking is required when located within one-quarter mile of the Sounder Station. (RCW 36.70A.698)</p> <p>Senior citizen apartments 1 per 3 dwelling units*</p> <p>Multifamily housing: 1.25 spaces per dwelling unit*</p> <p>*A process is included to prepare parking study to reduce further near station.</p>	Meet rates and standards of: Chapter 18A.80
Retail. Services, Restaurants	<p>2 per 1,000 GSF minimum;</p> <p>3 per 1,000 GSF maximum</p>	Meet rates and standards of: Chapter 18A.80
Office	<p>2 per 1,000 GSF minimum;</p> <p>3 per 1,000 GSF maximum</p>	Meet rates and standards of: Chapter 18A.80
Street level retail 3,000sq.ft. or less per business	None where there is available public parking within 500' or abutting on-street parking designed to serve street level retail	Meet rates and standards of: Chapter 18A.80

Source: Lakewood Municipal Code, draft 18C Form Based Code.

City staff recommendations are included in the table above. The Planning Commission has recommended no minimum parking for senior citizen or multifamily housing, and proponents would consider parking demand and market forces to determine spaces.

d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private).

Planned Improvements

The Six-Year Comprehensive Transportation Improvement Program (TIP) for 2021-2026 was approved by the Lakewood City Council in June 2020. This document outlines short and long term road projects, including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays, throughout the city. The following projects within the LSDS are identified in the TIP as long term improvements:

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study.
- Kendrick Street SW from 111th Street SW to 108th Street SW – sidewalks, street lighting, bicycle facilities.
- 112th/111th – Bridgeport to Kendrick – sidewalks, bicycle facilities, street lighting.
- 108th – Bridgeport Way to Pacific Highway – roadway patching and overlay.

Sound Transit and Pierce Transit have also identified improvements for the LSDS in their plans. The Sound Transit System Access Fund awarded funds in 2019 to support non-motorized access to Lakewood Station. These include a variety of improvements to support pedestrian and bicycle access on 11th Street SW and 112th Street SW. Pierce Transit's *Destination 2040* plan identifies two new Bus Rapid Transit (BRT) routes that will bring additional transit service to the subarea. A new line in 2026 would connect to Downtown Tacoma. In 2030 a BRT line would replace an existing transit line connecting Lakewood to Puyallup's South Hill.

Specific Mitigation Measures

The City could make signal timing improvements to increase the efficiency of impacted intersections and roadways in the subarea. Exhibit 29 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right-of-way acquisition or widening, rather only changes to signal timing. The exception is intersection of Pacific Highway and Sharondale Street, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better.

Exhibit 29. Proposed Mitigation Measures

ID	Intersection	No Action	Alt 1	Alt 1 Mitigated
4	Pacific Highway / Sharondale Street			
	Install signal using existing channelization and signal length consistent with adjacent intersections	F>100	F>100	A/4
8	Pacific Highway / Bridgeport Way			

ID	Intersection	No Action	Alt 1	Alt 1 Mitigated
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.	D/54	E/78	D/49
11	Bridgeport Way / SB I-5 Ramp			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.	D/41	E/60	D/52

Source: Fehr & Peers, 2020.

With implementation of the potential signal timing mitigation measures outlined in the previous section and future improvements at the Bridgeport Way/I-5 ramps, the magnitude of the impacts could be mitigated such that auto, freight, and transit are not significantly impacted.

Development in the study area would provide information about its expected trips and will provide a traffic study where applicable per LMC 12.09.028. This analysis can affirm the mitigation approach and phasing, such as signal timing at Bridgeport Way/I-5 on and off ramps.

Encouraging bike usage would reduce transportation demand on local roads and state highways. The City could continue applying its required bicycle parking standards by use in 18A.80.030.

Street Standards

Street frontage standards are part of the proposed Title 18C Station District Development Code.

e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

All of Lakewood, including the subarea, is within the Lakewood Military Coordination & Notice Area (MCNA).⁸ Jurisdictions within the MCNA coordinate with JBLM prior to the approval of zoning and Comprehensive Plan amendments. Current zoning has already been subject to MCNA review, but the City notifies JBLM of all land use and building permits, subdivisions, and site plans to provide opportunity for comment.

Parts of Lakewood are also subject to airport compatible land use restrictions. The subarea is not within the most restrictive aircraft safety zones, but is within the Inner Horizontal Surface imaginary surface area for the safe operation of aircraft around JBLM.⁹ JBLM reviews proposed development to determine if the use is prohibited or could interfere with pilot vision, communication, radar, or other elements of safe operation. Typically, uses that produce steam, dust, glare that may impair visual operation, or those that attract birds, are prohibited.

The subarea is bisected by an existing rail line on which Sounder and freight operate. While the rail line is an important regional transit and freight corridor, it has also created an environment with few roadway crossing opportunities which can affect vehicle operations and bicycle and pedestrian mobility.

The City's 2016 Comprehensive Plan identifies designated truck routes for freight as a transportation goal. Designated major truck streets are primary routes for goods movement throughout the City. Designation as a major truck street helps Lakewood's Public Works Transportation division determine street design, traffic management plans, and pavement improvement projects that allow and facilitate the movement of larger vehicles along the designated Way. Bridgeport Way and South Tacoma Way are designated as truck routes in WSDOT's Freight and Good Transportation System (FGTS) 2019 update, as shown in Exhibit 30.¹⁰ In addition to the designated routes, 108th Street SW serves as a de facto freight route for trucks heading northbound on I-5 due to the limited height of the railroad bridge crossing I-5 at JBLM. These trucks exit the freeway at Bridgeport, take 108th to Pacific Highway, and then reenter I-5 at SR-512.

⁸ 18A.10.135.6

⁹ 18A.10.135.10B

¹⁰ <https://www.wsdot.wa.gov/sites/default/files/2006/02/13/washington-freight-and-goods-transportation-system-2019.pdf>

Exhibit 30. Freight and Good Transportation System Routes



Source: WSDOT, 2019; Fehr & Peers, 2020; City of Lakewood, 2020.

f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates?

The net increase between Planned Action and Existing Conditions is 1,950 vehicle trips. Total rounded vehicle trips by scenario tested include:

- Existing – 2,530
- No Action – 3,810
- Planned Action – 4,480

g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe.

This is not applicable. There are no areas in use for agricultural and forest products.

h. Proposed measures to reduce or control transportation impacts, if any:

See “d” above for planned improvements.

Other mitigation includes:

- Washington state Commute Trip Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip reduction plans and work toward meeting their mode share targets through internal programs and monitoring.
- The City of Lakewood included policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote transportation demand management (TDM) strategies.

15. PUBLIC SERVICES

a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe.

Emergency Services

Emergency response services in Lakewood are provided by West Pierce Fire and Rescue and the Lakewood Police Department. St. Clare hospital also supports a range of medical services including a 24 hour emergency room.

Future development would add new residents and employees that could increase the demand for emergency services.

Comprehensive Plan Policy P-5.1 establishes police response time objectives:

PS-5.1: Provide police protection with a three-minute response time for life-threatening emergencies (Priority 1), a six-minute response time for crimes in progress or just completed (Priority 2), and a routine/non-emergency response time of 20 minutes (Priority 3).

The status of response times is unknown.

The department 101 commissioned officers that service the City, and in 2016 responded to 49,569 calls for service. (City of Lakewood, 2020) With this information, an effective level of service can be calculated, resulting in about 1.72 officers per 1,000 residents.

Exhibit 31. Police Services Effective Level of Services Standards

Year	Population	Officers	Officers Per 1,000 Residents
2020	60,030	101	1.68

Source: (City of Lakewood, 2020)

The demand for the number of police officers under the Planned Action is anticipated to be 6.8, and under the No Action Alternative 3.8 officers.

Lakewood has adopted policies setting level of service standards for West Pierce Fire and Rescue (WPFR):

- PS-1.1: Maintain a Washington Surveying and Rating Bureau (or successor agency) rating of ISO Class 3 or better; and
- PS-4.2: Provide a four-minute initial time standard for EMS calls.

WPFR has met the PS-1.1 LOS standard with a class 3 WSRB through 2020.¹¹

WPFR has a turnout time for all incidents of 110 seconds (less than 2 minutes) and a response time of 6 minutes for a first arriving unit with a “first responder” or higher. In 2017, the goal of a 110 second response time was met 70% of the time. The goal of arrival of a unit with first responder or higher level capability at an emergency medical incident was met 83% of the time.¹²

A common effective level of service standard is to look at fire response personnel per 1,000 capita. This helps compare service capabilities over-time and across jurisdictions. Fire suppression personnel are often trained in emergency medical services, and there is overlap in the number of full time equivalents (FTEs) for each activity.

Based on the expected population of the No Action Alternative, there would be a demand for 3.44 firefighters, and 6.15 under the Planned Action.

Exhibit 32. Fire Services Effective Level of Services Standards

Year	District Population	Firefighters / EMT/Medics	Firefighters Per 1,000 Residents
2020	100,000	152 Firefighters/EMTs	1.52

Source: [WPFR Budget 2020](#).

There is no level of service for the St. Clare Hospital. Relevant Comprehensive Plan policies include:

¹¹ See: <https://www.westpierce.org/west-pierce-fire-rescue-maintains-class-3-rating/>.

¹² See: <https://www.westpierce.org/wp-content/uploads/Annual-Report-2017-with-52.33-report.pdf>.

PS-14.7 Encourage the location of medical clinics and services near transit facilities.

Public Schools

Future residents would increase the demand for public schools. Public school services are provided by the Clover Park School District (CPSD). The study area is primarily served by Lakeview Hope Academy Elementary School and to a small degree by Four Heroes Elementary School. Lochburn and Hudtloff Middle Schools also serve the study area. Clover Park High School serves the entire study area.¹³

To estimate student generation, it is also possible to consider the number of households in the district in relation to the number of students. The number of households in the Clover Park School district is 29,700 as of 2019 based on State of Washington Office of Financial Management (OFM) small area estimates. There are 13,789 students in the district as of 2019. Thus, the effective student per household ratio is 0.46.

Under the Planned Action, there would be 792 students in new households, and under the No Action Alternative there would be a potential for 447 new students.

b. Proposed measures to reduce or control direct impacts on public services, if any.

The growth will occur through 2035 and beyond and would contribute taxes to help support services; growth will be incrementally absorbed.

The City addresses public service levels of service in its Capital Facilities Plan Element. The element is updated periodically to ensure that proposed growth and change can be served.

16. UTILITIES

a. Circle utilities currently available at the site:

electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other ____

All power, water, and sewer services are available. Lakeview Light and Power provides electrical service to the subarea and Puget Sound Energy provides natural gas service.

Water service is provided by Lakewood Water District. District policy requires the developer to pay for system improvements related to new development. Depending on the location and intensity of new development in the subarea this may include water main upgrades or line extensions to provide additional capacity or fire flow. Coordination of water line upgrades with the District's 50-year repair and replacement program could provide cost and timing efficiency for redevelopment projects in the subarea. Fire flow requirements would be applied at the time of building and construction permits. See Appendix A.

Wastewater service in Lakewood is provided by Pierce County Public Works. See Appendix A. Most of the sewer pipes were installed in the 1980's (some later) and are primarily 30 inch diameter PVC pipes, which have a 100-year lifetime.

Pierce County coordinates quarterly with the City of Lakewood to discuss upcoming and future projects. A Sewer Improvement Plan was adopted in September 2019, addressing capital facility planning from 2020-2040 and identifying funding for the next six years of capital facility improvements. There are no listed projects within or near the subarea identified in this plan.

¹³ See: <http://apps.schoolsitedlocator.com/index.html?districtCode=49275>.

The most recent system plan is the 2010 Unified Sewer Plan, adopted in 2012. Pierce County is scheduled to begin an update to this plan in 2020. The plan accounted for zoning densities in place today, e.g. up to 54 units per acre. Development that occurs at a higher intensity than the existing conditions may require wastewater system capacity upgrades. New development or redevelopment at the levels envisioned by the future land use plan will require larger collection lines. Update of the Unified Sewer Plan provides an opportunity to plan for future development in the LSDS.

The Lakewood Landing Project would have densities of 66 units per acre if considering the Phase 1 site area of 11.5 acres, but would have densities at 25 units per acre across the entire 30 acre site. Based on the whole site area, densities would be consistent with the TOC zone that has a maximum of 54 units per acre. Coordination regarding the phasing of development and needs for sewer service improvements would assist in development implementation.


b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

Potential strategies for water and sewer include coordination with service providers by each Planned Action Applicant consistent with LMC 18A.60.140 Concurrency:

- Water mains may need to be upgraded for capacity or fire flow to support redevelopment. This may be done efficiently and at lower cost if strategic upgrades are coordinated with the Lakewood Water District's 50-year repair and replacement plan.
- Participation in the update of Pierce County's Unified Sewer Plan will help to identify future capacity challenges, needed upgrades to collection lines, and funding options for replacement.

C. Signature

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature 

Name of signee: Lisa Grueter, AICP

Position and Agency/Organization: Principal, BERK Consulting

Date Submitted: July 27, 2020; updated March 29, 2021.

D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS

(do not use this sheet for project actions)

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise?

See B.2, B.3, and B.7.

Proposed measures to avoid or reduce such increases are:

See B.2, B.3, and B.7.

2. How would the proposal be likely to affect plants, animals, fish, or marine life?

See B.4 and B.5.

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

See B.4 and B.5.

3. How would the proposal be likely to deplete energy or natural resources?

See B.7.

Proposed measures to protect or conserve energy and natural resources are:

See B.7.

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains, or prime farmlands?

See B.3, B.4, B.5, B.12, and B.13.

Proposed measures to protect such resources or to avoid or reduce impacts are:

See B.3, B.4, B.5, B.12, and B.13.

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

See B.8 regarding land use.

Proposed measures to avoid or reduce shoreline and land use impacts are:

The integration of the LSDS Subarea Plan may require minor consistency edits to the Comprehensive Plan. For example, replacement of the urban design diagram shown in Exhibit 23 and references from the Comprehensive Plan to the Subarea Plan in the Land Use Element, Transportation Element, and Capital Facilities Element as appropriate.

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

See B.14 and B.15.

Proposed measures to reduce or respond to such demand(s) are:

See B.14 and B.15.

7. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

The area has limited natural environment features, but all development will be required to address critical area and water quality regulations.

Transportation

EXISTING FEATURES

This section presents a multimodal transportation evaluation of current conditions in the Lakewood Station District Subarea (LSDS). Existing transportation conditions are documented for pedestrians, bicycles, automobiles, freight, transit, and parking.

The study area is a predominantly auto-oriented environment, however the area also includes two important regional transit facilities: Lakewood Station and the SR-512 Park & Ride. Major roadways in the study area include Pacific Highway, 108th Street SW, and Bridgeport Way SW. These are classified as either principal or minor arterials with 35 mph posted speed limits. Pacific Highway provides a north-south connection between Tacoma and Lakewood, with access to I-5 ramps and Lakewood Station within the study area. **Exhibit 1** shows transportation connections in and near the LSDS.

The study area contains two key regional transportation facilities: Lakewood Station and SR 512 Park & Ride. Lakewood Station is a focal point of many regional commuter trips including service to downtown Seattle via the Sounder train in addition to bus service to local and regional cities operated by Sound Transit and Intercity Transit. The SR 512 Park & Ride provides bus connections for local and regional commuters to Lakewood Town Center, Tacoma, Puyallup, SeaTac, Dupont, and Olympia and communities throughout the South Sound. Pierce Transit, Sound Transit, and Intercity Transit provide bus service to the SR 512 Park & Ride via Pacific Highway and I-5, Bridgeport Way, and 108th Street SW. Both Lakewood Station and SR 512 Park & Ride provide vehicle parking for transit users.

The study area is bisected by an existing rail line on which Sounder and freight operate. While the rail line is an important regional transit and freight corridor, it has also created an environment with few roadway crossing opportunities which can affect vehicle operations and bicycle and pedestrian mobility.

Roadway Network

Exhibit 1. Transportation Features in the Extended Study Area



Source: Fehr & Peers, 2020; Pierce County, 2020.

Parking

There is very little on-street parking on arterials in the study area, but on-street parking exists on several local streets. Commercial uses tend to have dedicated parking lots adjacent to their buildings, frequently buffering the building from the street. St Clare Hospital provides off-street parking at multiple lots on the hospital's campus. Parking intended for transit users is provided at the Lakewood Station garage (600 spaces) and at the SR 512 Park & Ride (493 stalls). During the daytime, the SR 512 Park & Ride is at about 90 percent capacity. Bicycle parking is provided at Lakewood Station garage via bicycle racks and lockers.

Transit Network

The Lakewood Station District Subarea is served by Pierce Transit, Intercity Transit, and Sound Transit, and includes Lakewood Station and the SR 512 Park & Ride. **Exhibit 2** shows routes serving the area and their associated headways, and Exhibit 3 shows transit pathways and bus stops. Lakewood Station serves six bus routes, providing connections to Tacoma, Puyallup, Olympia, Lacey, and Seattle. The SR 512 Park & Ride serves three bus routes providing connections to Tacoma, Puyallup, and Sea-Tac Airport. Bus stops are present along many of the study area's arterials including Bridgeport Way, 108th Street SW, and Pacific Highway SW. Bus routes run at 15 to 30 minute headways (frequencies) throughout the peak periods, with routes 592 (DuPont-Seattle) and 612 (Olympia-Tacoma Dome Station) running most frequently. Off-peak headways vary substantially, ranging between 12 and 120 minutes.

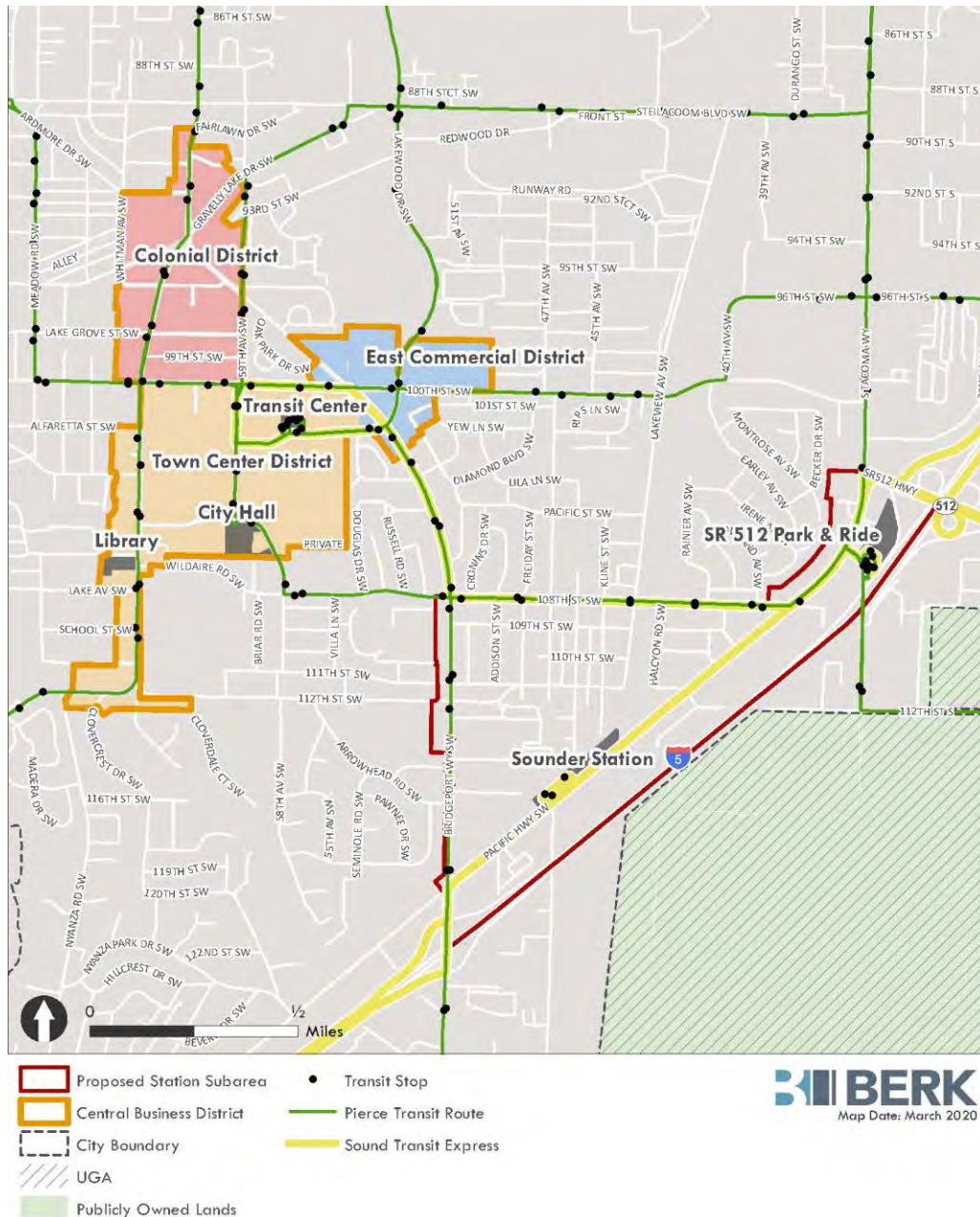
Sound Transit's commuter train, the Sounder, has a stop at Lakewood Station in the southeast portion of the study area. The Sounder provides a regional transit connection between Lakewood, Tacoma, and Seattle during AM and PM commute times.

Exhibit 2. Existing Bus Routes.

ROUTE	DESTINATIONS	PEAK HEADWAY (IN MINUTES)	OFF-PEAK HEADWAY (IN MINUTES)
Sounder	Lakewood - Seattle	20	20-45
3	Lakewood to Downtown Tacoma	30	30
4	Lakewood to South Hill Mall	30	30
574	Lakewood to Sea-Tac Airport	30	30
580	Lakewood to Puyallup Station/South Hill Park and Ride	20	20-40
592	Dupont to Seattle	15	12-15
594	Lakewood to Seattle	20	20-30
612	Olympia to Tacoma Dome Station	15	40-120
620	Olympia to Tacoma Mall	30	60-90

Source: Pierce Transit, 2020; Intercity Transit, 2020; and Sound Transit, 2020.

Exhibit 3. Transit in the Extended Study Area



Source: Fehr & Peers, 2020; Sound Transit; Pierce Transit; Intercity Transit

Non-Motorized Network

Marked bicycle lanes are located on Pacific Highway from Lakewood Station south to Gravelly Lake Drive SW and north from Sharondale Street SW to the South Tacoma Way/SR 512 interchange. Bicycle lanes are also located on 108th Street SW from Bridgeport Way to Pacific Highway. While sidewalks are generally located on all major streets in the project area, the residential neighborhood north of

Pacific Highway lacks sidewalks on most roadways. Due to the train tracks and lack of dedicated facilities, direct non-motorized connection from neighborhoods to Pacific Highway is limited. As a result, accessing Lakewood Station via bicycle or walking can also be challenging. Crossing opportunities are limited to the Lakewood Station pedestrian walkway, Bridgeport Way, and 108th Street SW. Although there is sidewalk infrastructure on Pacific Highway and Bridgeport Way, these roadways are high volume and high speed within the study area. This creates a more uncomfortable pedestrian experience that may discourage non-motorized use on these roadways.

Freight Network

The City's 2016 Comprehensive Plan identifies designated truck routes for freight as a transportation goal. Designated major truck streets are primary routes for goods movement throughout the city. Designation as a major truck street helps Lakewood's Public Works Transportation Division determine street design, traffic management plans, and pavement improvement projects that allow and facilitate the movement of larger vehicles along the designated street. Bridgeport Way and S Tacoma Way are designated as truck routes in WSDOT's Freight and Good Transportation System (FGTS) 2019 Update.¹ Exhibit 4 shows the FGTS routes that run through the study area.

¹ <https://www.wsdot.wa.gov/sites/default/files/2006/02/13/washington-freight-and-goods-transportation-system-2019.pdf>

Exhibit 4. FGTS Routes within the Study Area



Source: WSDOT, 2019, Fehr & Peers, 2020.

Traffic Operations

Study Intersections

Within the project study area, traffic operations at ten locations were analyzed, as shown in **Exhibit 5**. At Lakewood Station, the garage entrance and north and south bus driveways were also analyzed, bringing the total number of study intersections to 12. These intersections are located on key roadway connections, including Pacific Highway, Bridgeport Way SW, and 108th Street SW and are most likely to be affected by potential land use changes.

Exhibit 5. Study Area Intersections



Source: Fehr & Peers, 2020.

Intersection Level of Service

The City uses PM peak hour average delay to evaluate traffic operations level of service (LOS) at its intersections. Traffic operations were analyzed using the Synchro software package. The Synchro network reflects the study area's existing roadway network including segment and intersection geometry, signal timings, and recent traffic counts (2018-2020). For signalized, roundabout, and all-way stop controlled intersections, the LOS is based on the average delay for all approaches. For minor street stop controlled intersections, the LOS is based on the movement with the highest delay. **Exhibit 6** summarizes the LOS and delay thresholds specified in the Sixth Edition of the Highway Capacity Manual (HCM), which is a standard methodology for measuring intersection performance.

The Transportation Element designates level of service guidelines for the city's arterial streets and intersections. Within the study area, that City sets a standard of LOS D during the weekday PM peak hour at all arterial street intersections. However, according to Policy T-20.5, the City may allow minor street stop-controlled intersections to operate below that LOS standard if those instances are thoroughly analyzed from an operational and safety perspective.

Exhibit 6. LOS/Delay Thresholds for Signalized and Unsignalized Intersections.

LOS	SIGNALIZED INTERSECTIONS (DELAY IN SECONDS)	UNSIGNALIZED INTERSECTIONS (DELAY IN SECONDS)
A	≤ 10	≤ 10
B	> 10 to 20	> 10 to 15
C	> 20 to 35	> 15 to 25
D	> 35 to 55	> 25 to 35
E	> 55 to 80	> 35 to 50
F	> 80	> 50

Source: Highway Capacity Manual (Transportation Research Board, 2017).

This study considers 12 intersections, 11 of which are signalized. **Exhibit 7** summarizes the existing intersection LOS at the study intersections. The level of service analysis suggests that automobiles generally move through the study area with acceptable levels of delay during the PM peak period. All study intersections operate at the City's LOS D standard or better, although some approaches may operate with higher delay. Most intersections operate at LOS C or higher, which represents stable conditions with moderate congestion levels for an urban area. Three intersections - South Tacoma Way & SR 512, Pacific Highway & 108th Street, Pacific Highway & Bridgeport Way all operate at LOS D during the PM peak period, which indicates traffic conditions are approaching unstable flow.

As mentioned above, these are intersections that are affected by regional travel patterns, such as afternoon commute congestion stemming from I-5. According to local stakeholders, during certain congestion events on I-5 roadway users may be using Pacific Highway to bypass interstate traffic.

Exhibit 7. Existing PM Peak Hour Intersection Level of Service and Delay.

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
1	South Tacoma Way / SR 512 ¹	Signal	D/35
2	Pacific Highway / South Tacoma Way ¹	Signal	C/21
3	Pacific Highway / 108 th Street ¹	Signal	D/37
4	Pacific Highway / Sharondale Road	TWSC ²	C/23
5	Pacific Highway / Sounder Station Garage Entrance ¹	Signal	A/6
6	Pacific Highway / Sounder Station North Transit Access ¹	Signal	B/12
7	Pacific Highway / Sounder Station South Transit Access ¹	Signal	B/18
8	Pacific Highway / Bridgeport Way	Signal	D/41
9	108 th Street / Lakeview Drive	Signal	B/14
10	Bridgeport Way / 112 th Street	Signal	C/27
11	Bridgeport Way / SB I-5 Ramp ¹	Signal	B/16
12	Bridgeport Way / NB I-5 Ramp ¹	Signal	B/18

1. This intersection required the use of HCM 2000 methodology, due to non-standard traffic signal phasing.

2. TWSC = Two way stop control

Source: Fehr & Peers, 2020.

FUTURE PLANS

Transportation Improvements

This section describes existing local area plans and planned improvements to the transportation network. Some long-range plans identify strategies for the development of the subarea, and others provide specific roadway improvements such as bicycle and pedestrian facilities. Future transportation improvements will be incorporated into the No Action and Planned Action alternatives developed as part of this project.

Lakewood Comprehensive Plan (2019)

Last revised in 2019, the Comprehensive Plan articulates the City's vision for its future for the next 20 years. The City envisions the Lakewood Station District Subarea as a combination of transit oriented commercial, neighborhood commercial, public and institutional land, and a mix of multifamily land uses. LSDS is designated as a Regional Growth Center under Puget Sound Regional Council's VISION 2040.

Transportation Element (2019)

The transportation element of the Comprehensive Plan provides policy direction to address local and regional mobility. The transportation element acknowledges the increase of traffic congestion within Lakewood and seeks to mitigate it by developing a balanced multimodal system that effectively moves people, goods, and services without compromising community character. The Plan specifically strategizes for the incorporation of non-motorized facilities, enhanced illumination, and other pedestrian amenities into new development designs.

Six-Year Transportation Improvement Program: 2020-2025

The Six-Year comprehensive Transportation Improvement Program (TIP) for 2020-2025 was approved by the Lakewood City Council in June 2020. This document outlines short- and long-term road projects – including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays – throughout the city. The following projects are identified in the TIP.

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study
- Kendrick Street SW from 111th Street SW to 108th Street SW – sidewalks, street lighting, bicycle facilities
- 111th Street SW and 112th Street SW bicycle and pedestrian improvements
- 47th Avenue SW bridge improvements supporting non-motorized mobility

Additional projects included in the TIP are discussed in the No Action alternative section.

Sound Transit System Access Fund 2019

The Sound Transit Board of Directors awarded System Access Funds in September 2019 for various non-motorized improvements intended to facilitate connection to transit services. Some of these projects have also been included on the City's Transportation Improvement Program. Among these projects are proposed improvements on 111th Street SW and 112th Street SW in Lakewood. These improvements include curb, gutter, sidewalks, bicycle lanes, street lighting, pavement overlay, and associated storm drainage on both sides of these two streets between Bridgeport Way SW and Kendrick Street SW. Kendrick Street SW provides direct access to the Lakewood Sounder Station via a pedestrian bridge. Although this project will not have a direct impact on traffic operations at the study intersections, it represents a substantial improvement to the non-motorized network within the study area.

Destination 2040: Pierce Transit Long Range Plan Update

Destination 2040 is Pierce Transit's first Long Range Plan. This plan was originally adopted in 2016 and is currently being updated with the most recent available draft released in February 2020. Under this plan, two new Bus Rapid Transit (BRT) routes are being proposed that include services within the study area in 2026 and in 2030.

- **Downtown Tacoma to Lakewood BRT Line:** This BRT line would run from Downtown Tacoma, through the South Tacoma Sounder Station and along South Tacoma Way/Pacific Highway to the SR 512 Park & Ride. The line would then run along 108th Street SW toward the Lakewood Mall Transit Center. It is anticipated that this line would begin service in 2026.

- **Lakewood to South Hill BRT Line:** This BRT line would replace the existing Route 4, which currently runs along South Tacoma Way south of the SR 512 Park & Ride before running east/west along 112th Street SW towards the South Hill Mall Transit Center in Puyallup. It is anticipated that this line would begin service in 2030.

KEY FINDINGS AND PLANNING IMPLICATIONS

- All intersections operate at LOS D or better in current conditions.
- Planning for multimodal improvements connecting to Lakewood Station is programmed for implementation within the next 6 years.
- The area is well-served by transit with two existing hubs at Lakewood Station and the SR 512 Park & Ride. Both Sound Transit and Pierce Transit are enhancing transit connectivity and mobility at Lakewood Station over the next decade.

ALTERNATIVES EVALUATION

No Action Alternative

The No Action Alternative serves as the baseline for the impact analysis of the Planned Action Alternative. It represents the operation of the transportation system if no zoning or network changes were made in the Study Area. However, some growth would continue to occur under the No Action Alternative. The travel model used to forecast vehicle volumes for the No Action Alternative is consistent with the Planned Action model developed for the Lakewood Downtown Plan EIS.

This section summarizes analysis results and environmental impacts of the No Action Alternative. Specifically, the following definitions are used to identify impacts under the No Action Alternative:

- **Auto and Freight:** average vehicle delay below LOS D at a study intersection.
- **Transit:** average vehicle delay below LOS D at a study intersection through which transit routes travel.

Pedestrian, bicycle, and parking impacts are discussed qualitatively. As defined above, this EIS identifies impacts if future transportation operations under the No Action Alternative are not expected to meet the City's adopted level of service standards.

Intersection Level of Service

Exhibit 8 summarizes the average vehicle delay for each study intersection. By 2035, traffic volumes would increase due to the land use growth that would occur within the Study Area itself as well as citywide and regional growth not associated with the Study Area. Therefore, delay at individual intersections is also expected to increase although changes to the future land use are moderate. Of the 12 study intersections, six are expected to drop by at least one LOS grade compared to existing conditions, though only one intersection is expected to operate below the City's LOS D standard.

The Pacific Highway/Sharondale Road intersection is anticipated to operate at LOS F, which is below the City standard. It should be noted that the City has a policy stating that it may allow minor street stop-

controlled intersections (such as this intersection) to operate below the LOS D standard if they are thoroughly analyzed from an operational and safety perspective.

Exhibit 8. 2035 No Action PM Peak Hour Intersection Level of Service and Delay.

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
1	South Tacoma Way / SR 512 ¹	Signal	D/50
2	Pacific Highway / South Tacoma Way ¹	Signal	D/41
3	Pacific Highway / 108 th Street ¹	Signal	D/46
4	Pacific Highway / Sharondale Road	TWSC ²	F/>100
5	Pacific Highway / Sounder Station Garage Entrance ¹	Signal	A/7
6	Pacific Highway / Sounder Station North Transit Access ¹	Signal	B/14
7	Pacific Highway / Sounder Station South Transit Access ¹	Signal	B/19
8	Pacific Highway / Bridgeport Way	Signal	D/54
9	108 th Street / Lakeview Drive	Signal	C/24
10	Bridgeport Way / 112 th Street	Signal	D/35
11	Bridgeport Way / SB I-5 Ramp ¹	Signal	D/41
12	Bridgeport Way / NB I-5 Ramp ¹	Signal	C/29

1. This intersection required the use of HCM 2000 methodology, due to non-standard traffic signal phasing.

2. TWSC = Two way stop control

Source: Fehr & Peers, 2020.

Vehicle Queuing

Vehicle queuing analysis was completed at select locations to determine potential impacts to state highways. Although the overall intersection LOS meets the City standard, queuing at Bridgeport Way & the I-5 southbound off-ramp is expected to extend from the southbound off-ramp onto I-5, which could impact traffic flow, including buses, on I-5 near this interchange.

Transit

No transit impacts are expected under the No Action Alternative.

Pedestrian and Bicycle

Under the No Action Alternative, the pedestrian and bicycle network would remain similar to its current state. The City plans to implement sidewalk projects on 47th Avenue SW, 111th and 112th Streets SW, Kendrick Street SW, and 115th Street SW. The City also plans to implement bicycle facility projects on 47th Avenue SW, 108th Street SW, 111th and 112th Streets SW, and Kendrick Street SW. Many of these projects combine pedestrian and bicycle improvements as part of the same project and may also include improvements to roadway lighting. The 47th Avenue SW project enhances non-motorized connectivity over the existing I-5 bridge between Pacific Highway SW and Lakewood Station to the neighborhood on the opposite side of I-5.

Development is expected to meet the City design standards related to bicycle and pedestrian facility accommodations so no significant adverse impacts to pedestrians or bicycles are identified under the No Action Alternative. A full list of known planned pedestrian and bicycle projects is below.

Pedestrian Improvements

- 47th Avenue SW from 121st Street SW to Pacific Highway – New Sidewalks
- 111th & 112th Street SW - Bridgeport Way to Kendrick Street SW – Construct sidewalks to fill gaps in pedestrian network
- 112th Street SW from Gravelly Lake Drive SW to Bridgeport Way SW – New Sidewalks
- Kendrick Street SW from 111th Street SW to 108th Street SW – New Sidewalks
- 115th Court SW from Bridgeport Way SW to Kendrick Pedestrian Bridge – New Sidewalks and pedestrian lighting

Bike Improvements

- 47th Avenue SW from 121st Street SW to Pacific Highway – New Bike Lanes
- 108th Street SW from Bridgeport Way SW to Lakeview Avenue SW – New Bike Lanes
- 111th Street SW Bike Lanes – Bridgeport Way SW to 112th Street SW
- 112th Street from Gravelly Lake Drive SW to Bridgeport Way SW – New Bike Lanes
- Kendrick Street SW from 111th Street SW to 108th Street SW – New Bike Facilities

Parking

Off-street parking is expected to be provided for residential, retail and other uses throughout the Study Area. Residences in the study area generally have their own off-street parking, allowing the roadway right-of-way to be reallocated to other modes as needed. New developments in the Study Area would be required to meet the City's parking requirements, so no adverse impacts to parking are expected.

Safety

Traffic volumes in the Study Area are projected to increase by 2035. With higher volumes, there is potential for an increased number of collisions. However, there is no indication that collision rates at intersections or along segments would increase. Therefore, no safety impacts are identified under the No Action Alternative. In fact, some of the projects planned that improve or construct new pedestrian and bicycle facilities may result in a safety benefit.

Planned Action Alternative

One Planned Action Alternative was developed which incorporates an increase in residential and retail land use at the Lakewood Landing and within the study area as compared to the No Action Alternative. These land use changes would result in an increase in the number of trips made within the Study Area; vehicle trips are expected to increase by 700 compared to the No Action Alternative. This section summarizes analysis results and environmental impacts of the Planned Action Alternative. Specifically, the following definitions are used to identify intersection impacts:

- **Auto and Freight:** average vehicle delay below LOS D at a study intersection that operated acceptably under the No Action Alternative; or an increase in delay of at least 5 seconds at a study intersection already expected to operate below LOS D under the No Action Alternative.
- **Transit (applicable to study intersections through which transit routes travel):** average vehicle delay below LOS D at a study intersection that operated acceptably under the No Action Alternative; or an increase in delay of at least 5 seconds at a study intersection already expected to operate below LOS D under the No Action Alternative.

Pedestrian, bicycle, safety, and parking impacts are discussed qualitatively in comparison to the No Action Alternative. An impact is defined if a project would preclude or fail to implement a City-identified bicycle or pedestrian improvement.

Intersection Level of Service

Exhibit 9 summarizes level of service and average vehicle delay for each study intersection. The Planned Action Alternative's proposed land use growth would result in increased volumes compared to the No Action Alternative. There were no major changes to roadway or intersection capacity between the No Action and Planned Action scenarios. As defined above, impacts are evaluated in comparison to the No Action Alternative. The following significant auto and freight impacts are expected under the Planned Action Alternative.

- Intersection 4 – Pacific Highway / Sharondale Road SW – Increase in delay, LOS F to LOS F
- Intersection 8 – Pacific Highway / Bridgeport Way – from LOS D to LOS E
- Intersection 11 – Bridgeport Way / SB I-5 Ramp – from LOS D to LOS E

Because bus routes pass through all of the aforementioned intersections, these intersections are also considered as significant transit impacts. Potential measures to mitigate the impacts on these intersections are presented in the Mitigation Measures section.

Exhibit 9. 2035 Planned Action PM Peak Hour Intersection Level of Service and Delay.

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
1	South Tacoma Way / SR 512 ¹	Signal	D/52
2	Pacific Highway / South Tacoma Way ¹	Signal	D/42
3	Pacific Highway / 108 th Street ¹	Signal	D/48
4	Pacific Highway / Sharondale Road SW	TWSC	F/>100

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
5	Pacific Highway / Sounder Station Garage Entrance ¹	Signal	A/7
6	Pacific Highway / Sounder Station North Transit Access ¹	Signal	B/16
7	Pacific Highway / Sounder Station South Transit Access ¹	Signal	C/21
8	Pacific Highway / Bridgeport Way	Signal	E/78
9	108 th Street / Lakeview Drive	Signal	C/24
10	Bridgeport Way / 112 th Street	Signal	D/41
11	Bridgeport Way / SB I-5 Ramp ¹	Signal	E/60
12	Bridgeport Way / NB I-5 Ramp ¹	Signal	C/31

1. This intersection required the use of HCM 2000 methodology, due to non-standard traffic signal phasing.

2. TWSC = Two way stop control

Source: Fehr & Peers, 2020.

Vehicle Queuing

In the Planned Action Alternative, queuing is expected to extend onto I-5 at Bridgeport Way and the I-5 southbound off-ramp, and at Bridgeport Way and the I-5 northbound off-ramp. Queuing at the southbound off-ramp was identified as an impact under the No Action Alternative, with the 95th percentile queue length expected to increase substantially with the Planned Action Alternative. Queuing on the I-5 northbound off-ramp was not expected to be an issue under the No Action Alternative, but substantial queuing is expected under the Planned Action Alternative. Therefore, queueing impacts to I-5 are expected under the Planned Action Alternative.

Pedestrian and Bicycle

The pedestrian and bicycle environment will be similar to the No Action Alternative. However, several local roadways are expected to be redesigned to better accommodate walking and bicycling through the implementation of street typology design proposed as part of this project. These typologies will reallocate right of way on residential roadways to sidewalks, bicycle facilities, and other improvements that enhance the non-motorized experience in these areas. Because of these additional benefits of the Planned Action Alternative and because development is expected to meet the City design standards related to bicycle and pedestrian facility accommodations, no significant adverse impacts to pedestrians and bicycles are identified under the Planned Action Alternative.

Parking

Select residential streets in the study area have been identified to be redesigned to better accommodate non-motorized travel through application of street typologies. As part of this effort, on-street parking may be removed or changed to other modes and uses. Many households in this area currently have off-

street parking, and parking demand may be lower in the future given the study area’s proximity to transit facilities. New developments in the study area would be required to meet the City’s parking requirements. Therefore, no significant impacts to parking are identified under the Planned Action Alternative.

The City requires bicycle parking in LMC 18A.80.030 for commercial and multifamily uses.

Safety

Traffic volumes in the Study Area are projected to increase under the Planned Action Alternative compared to the No Action Alternative. With higher volumes, there is potential for an increased number of collisions. However, there is no indication that collision rates at intersections or along segments would increase meaningfully compared to the No Action Alternative. Bicycle and pedestrian improvements may improve roadway safety conditions for the Planned Action Alternative. Therefore, no significant adverse impacts to safety are identified under the Planned Action Alternative.

Summary of Impacts

Exhibit 10 summarizes the significant impacts for each alternative.

Exhibit 10. Summary of Transportation Impacts

TYPE OF IMPACT	NO ACTION	PLANNED ACTION
Auto and Freight	1 intersection and I-5 SB off-ramp queuing impact	3 intersections and I-5 NB and SB off-ramp queuing
Transit	1 intersection and I-5 SB off-ramp queuing impact	3 intersections and I-5 NB and SB off-ramp queuing
Pedestrian	None	None
Bicycle	None	None
Parking	None	None
Safety	None	None

Source: Fehr & Peers, 2020.

Mitigation Measures

This section identifies a range of potential mitigation strategies that could be implemented to help reduce the significance of the adverse impacts identified for four intersections in the Planned Action alternative.

Managing demand for auto travel is an important part of mitigating the traffic congestion impacts. Transportation demand management (TDM) strategies could include subsidies or discounts for non-auto travel, education and assistance to help travelers identify non-auto commute options, rideshare and ridematch promotion, and local incentive and reward programs.

Washington state Commute Trip Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip

reduction plans and work toward meeting their mode share targets through internal programs and monitoring.

The City of Lakewood included policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote transportation demand management (TDM) strategies.

The potential TDM strategies discussed here could be implemented regardless of which alternative is selected. With a robust TDM program in place, it is expected that actual trip generation in the Study Area could be lowered beyond the levels analyzed in the impacts section of this EIS.

Other Proposed Mitigation Measures

The City could make signal timing improvements to increase the efficiency of impacted intersections and roadways in the Study Area. Exhibit 11 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right of way acquisition or widening, rather only changes to signal timing. The exception is intersection 4, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better. The City is planning to include improvements at the intersections of Bridgeport Way with the I-5 NB/SB ramps in their upcoming Transportation Improvement Program. While queuing at these locations may still occur, the improvements are expected to mitigate the effects of the Planned Action Alternative.

Exhibit 11. Proposed Mitigation Measures

ID	INTERSECTION	NO ACTION	ALT 1	ALT 1 MITIGATED
4	Pacific Highway / Sharondale Road			
	Install signal using existing channelization and signal length consistent with adjacent intersections	F>100	F>100	A/4
8	Pacific Highway / Bridgeport Way			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way	D/54	E/78	D/49

ID	INTERSECTION	NO ACTION	ALT 1	ALT 1 MITIGATED
	from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.			
11	Bridgeport Way / SB I-5 Ramp			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.	D/41	E/60	D/52

Source: Fehr & Peers, 2020.

Therefore, with implementation of the potential signal timing mitigation measures outlined in the previous section and future improvements at the Bridgeport Way/I-5 ramps, the magnitude of the impacts could be mitigated such that auto, freight, and transit are not significantly impacted.

Development in the study area would provide information about its expected trips and will provide a traffic study where applicable per LMC 12.09.028. This analysis can affirm the mitigation approach and phasing, such as signal timing at Bridgeport Way/I-5 on and off ramps.

Encouraging bike usage would reduce transportation demand on local roads and state highways. The City could continue applying its required bicycle parking standards by use in 18A.80.030. The City could consider alternative standards or incentives in developments in the planned action or form-based code.

ATTACHMENT C

2021-06 Updates to reflect adoption of the Downtown Subarea Plan and the Lakewood Station District Subarea (LSDS) Plan.

- Adopt subarea plans by reference into the Comprehensive Plan
- Update relevant references from “town center” or “central business district” to Downtown
- Recognize development within the Downtown and Lakewood Station District Subareas (e.g., City Hall, Sounder Station, Colonial Plaza, etc.)
- Update population and employment growth anticipated within the subareas

Downtown Subarea (adopted October 2018):

[Update Comprehensive Plan maps and Sections 1.4.1, 1.4.3, and 2.4, Table 3.2, Section 3.3, Goals LU-43 and LU-49, Sections 4.1, 4.2, text above Table 4.4, Sections 4.5.1, Goal UD-8, Sections 5.2.1, 5.2.8, and 5.4, Goals ED-5, T-16, U-14, and S-5, Sections 11.3.3, 11.3.11, 11.3.14, and Transportation Implementation Strategies]

Lakewood Station District Subarea (adoption scheduled for April 2021):

[Update Comprehensive Plan maps and Sections 1.4.1, 1.4.3, 1.6.7, 2.3.5, 2.3.7, and 2.3.16, LU-17.3, Sections 3.3.3, 3.3.5, 4.1, 4.2, 4.4, 4.5, 4.5.2, 5.2.1, 5.2.7, 5.3.1, Goal UD-9, ED-5, and T-13, Sections 11.3.3 and 11.3.11, and Transportation Implementation Strategies. Rezone OSR parcels owned by Sound Transit within LSDS boundaries to Industrial.]

ATTACHMENT D

City Council April 5, 2021 Public Hearing Comments	City Response
<p>James Guerrero Mr. Guerrero presented information regarding the projected need for parking spaces in the LSDS and advocated for less required parking spaces.</p>	<p>During the development of the LSDS Plan, Code and Planned Action, some stakeholders were supportive of reducing parking to lower levels given proximity to the Sounder Station and the changing future of automobile usage. Other stakeholders indicated that it would be some time before cars were less prevalent in Lakewood in this area, and that the parking standards should not “under park” the area. A third group was interested in ensuring adequate parking for commercial development without pricing developers out of the market.</p> <p>After significant discussion among stakeholders and then Planning Commission members, the decision was made to dedicate the use of “excess” ROW in the LSDS for plazas and open space over on-site parking due to the lack of parks or other open space within the subarea. The decision was also made to enact on-site parking standards due to the subarea’s current population’s needs and car usage coupled with both the City’s foreseeable ability to maintain or regulate off-site parking usage.</p> <p>The LSDS Street Sections do not allow on-street parking in favor of pedestrians and bicyclists, the encouragement of which will also help reduce the share of cars over time. Developments need to provide adequate on-site parking.</p> <p>The LSDS Plan and Code allow for a transition over time to lesser parking needs. The residential parking standards require less parking than the citywide standards, and more than Downtown. The commercial parking standards are similar to the Downtown and lower than citywide.</p> <p>The originally proposed LSDS Form-Based Code at LMA 18C.600.610 reduces parking standards for multifamily and accessory dwelling units.</p> <p>There is a process to lower parking further with a parking study for Housing in Proximity to Sounder Station (RCW 36.70A.620):</p> <ul style="list-style-type: none"> - When located within one-quarter mile of the Sounder Station, an applicant may apply for an exception allowing minimum parking requirements to be reduced at least to one parking space per bedroom or 0.75 space per unit, as justified through a parking study.
<p>Seth Borman comments related to properties owned by Washington & Rice, LLC.</p>	<p>For reference, the following are attached as Exhibits A-C:</p> <ul style="list-style-type: none"> - a map of the properties owned by Washington & Rice, LLC within the LSDS subarea; - a map depicting existing various ROW widths within the LSDS MF3 area; and - renditions of attached single family developments at 34, 36

<p>1) Concerned that allowed densities within subarea plan will prevent apartment complexes, which provide the number of units to make projects financially workable.</p> <p>2) Concerned that on-street parking is not allowed since on-site acreage is needed for dwelling units to make projects work financially</p> <p>3) Concerned that open space requirements per unit will stop redevelopment</p>	<p>and 44 units per acre.</p> <p>1) The current zoning on the parcels owned by Washington & Rice, LLC is MF3; this zone will not change, and its maximum allowed density (up to 54 units per acre) will remain the same, under the LSDS Subarea Plan and Code.</p> <p>Allowed uses per proposed LMC 18C.200.220 (D) are row houses, townhouses, and 3 Family attached units. The 4/19/21 draft LSDS Code would also permit Multifamily uses of 4+ units under LMC 18C.200.220(E) via a Master Plan administrative approval as described in LMC 18C.700.720.</p> <p>A definition for “rowhouse” and amendments to the existing definition for “townhouse” is included in the 4/19/21 amendments to LMC 18A.10.180.</p> <p>2) Properties owned by Washington & Rice, LLC are predominantly on a “Green Street” with 60 foot ROW. The proposed design for LSDS Green Streets includes vehicle travel lanes, 8 foot minimum width sidewalks, bike lanes, and open space for stormwater, landscaping and gathering spaces. Street parking is not allowed.</p> <p>During the development of the LSDS Plan, Code and Planned Action, some stakeholders were supportive of reducing parking to lower levels given proximity to the Sounder Station and the changing future of automobile usage. Other stakeholders indicated that it would be some time before cars were less prevalent in Lakewood in this area, and that the parking standards should not “under park” the area.</p> <p>After significant discussion among stakeholders and then Planning Commission members, the decision was made to dedicate use of “excess” ROW in the LSDS for plazas and open space over on-site parking due to the lack of parks or other open space within the subarea.</p> <p>3) The 100-foot common open space requirement has been in the LMC since 2001. The LSDS Code includes a private open space requirement of 48 square feet per unit, the same as that required in the Downtown Subarea Code.</p> <p>The 4/29/21 proposed LSDS Code includes a park/open space fee-in-lieu program. Unlike the Downtown Subarea Code, the LSDS Code references the City’s Legacy Plan as well as the LSDS Plan, and the fee-in-lieu funds can be used on projects listed in either plan document.</p>
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Planning Commission November 18, 2020 Public Hearing Comments	City Response
Coral Clark @ 11/18/20 public hearing and written comments: Parents and family members live at/near intersection of 109 th and Kendrick St. Concern that ROW as depicted on LSDS maps runs through parent's property. Request that Kline St. or 47 th St. be converted to a "green street" instead of Kendrick St.	Comments acknowledged. Information regarding existing ROW widths near 109 th and Kendrick St. provided to commenters and Planning Commission.
Jeremy Long @ 11/18/20 public hearing and written comments: Concerned about effects of LSDS Plan implementation on 10917 Kendrick St. Concurs with Coral Clark's comments and recommendation that only sidewalks be added to green streets in order to reduce amount of ROW needed for improvements.	Comments acknowledged. Information regarding existing ROW widths near 109 th and Kendrick St. provided to commenters and Planning Commission.
Mike Brandstetter @ 11/18/20 public hearing: Questioning whether intersection improvements list correct streets since Halcyon does not intersect with Pacific Highway. Stated need for subsidized housing to avoid displacement of current area residents.	Halcyon reference corrected to Sharondale Rd.
Pierce County Planning and Public Works and Lakewood Water District Written Comments	
Draft LSDS Plan page 3: is the 54 units per acre identified for TOC zone referring just to residential development within the zone?	Comment acknowledged. 54 units per acre refers to residential and mixed use projects.
Draft LSDS Plan page 4: A 20% reduction factor was applied to gross parcel area to account for storm and parking. An additional 20% reduction factor was applied to account for critical areas in general. Based on County GIS there appears to be a flood plain in the South TOC zoned area, so these reductions percentages seem valid. Does the City agree with this? If not what does the City use? I know that the City is hoping that the parking reduction will be less over time. Does the City have an anticipated timeline on when the parking reduction will start taking place?	Comment acknowledged. Parking reduction will happen over time and market demand and technological changes affect vehicle usage.
Draft LSDS Plan page 5: Diagram at bottom of page 5 depicting sewer line as JBLM line incorrect – it is a PC Sewer line. What is image depicting over utility easement?	Comment acknowledged and image removed.
Draft LSDS Plan page 7: Area on 1999 Station District Map south of I-5 Owned by the County and managed by OCE department (former road shop and gravel pit). Currently being marketed for distribution center use.	Comment acknowledged. City working with the County on sale of property.
Draft LSDS Plan page 11: Utilities and Public Services phrase "Sponsoring the upfront costs of infrastructure investment may be a strategy to support new development in the LSDS." What does this mean? Lakewood would sponsor or is this utility sponsoring improvements? Pierce County currently does upgrades in a "just in time" model. Because of this we will need the City of Lakewood's help to determine when and where development will take place. Once we are able to determine when and where projects will be needed we will	Comment acknowledged. City works regularly with the County regarding utility capacity needs and improvements.

<p>have to evaluate our finances and see if we can build these projects, if there will have to be some partnering between the City and County, or if some developers will have to build projects (if timelines hold). Also see Page 57.</p>	
<p>Draft LSDS Plan page 54:</p> <p>Wastewater service in Lakewood is provided by Pierce County Planning and Public Works. Most of the sewer pipes shown in Exhibit 24 were installed in the 1980s (some later) and are primarily 308-inch diameter PVC pipes, which have a 100-year lifetime. Larger interceptors are typically made of concrete, which has a service life of 50 to 100 years. Generally, the sewer infrastructure is considered in good condition with plenty of remaining service life and no current need for large scale line replacements or upgrades.</p>	<p>Comment acknowledged and edits incorporated.</p>
<p>Draft LSDS Plan page 57 and 60:</p> <p>“Work with public service and utility providers to ensure that capital improvement plans include the projects necessary to support development in the LSDS subarea.”</p> <p>Does Lakewood believe that utilities should be upgrading their system ahead of developments? Can the City of Lakewood provide exhibits of where they believe that development will take place, in 5-6 year increments is preferred, so that utilities can evaluate what improvements are needed in this 20 year period being discussed. Once we are able to evaluate this the Sewer Division will be able to add it to our financial model and see if meeting the City's timeline is possible or if there needs to be some partnership between the City and County or Developer to build the project if the timelines do hold.</p>	<p>Comment acknowledged. City coordinating with County sewer utility regarding Unified Sewer Plan update and other sewer utility planning. City has notified County regarding anticipated capacity increase needs related to Lakewood Landing.</p>

Map Date:



April 06, 2021

47TH AVE SW

111TH ST SW

KLINE ST SW

LAKEVIEW AVE SW

KENDRICK ST SW

113TH ST SW

Tax Parcel

Washington & Rice, LLC Tax Parcel*

0 50 100 Feet

*Based on Tax Payer

\\Projects\\CD\\Maps\\W&R\\LLC_TPs.mxd

ID	TAX PARCEL	SITE ADDRESS	LAND ACRES	LAND SQ FT
1	5080000850	11014 KENDRICK ST SW	0.275	12,000
2	5080000870	11018 KENDRICK ST SW	0.275	12,000
3	5080000880	11028 KENDRICK ST SW OR 4601 111TH ST SW	0.138	6,000
4	5080000945	4528 111TH ST SW	0.686	29,900
5	5080001000	4500 TO 4504 111TH ST SW	0.367	16,000
6	5080001993	11101 & 11103 KLINE ST SW	0.145	6,300
7	5080001995	4400 111TH ST SW	0.399	17,375
8	5080000973	11110 KLINE ST SW	0.149	6,480
9	0219122154	11109 & 11111 KLINE ST SW	0.200	8,700
10	5080000990	11113 KENDRICK ST SW	0.164	7,124
11	0219122139	XXXX LAKEVIEW AVE SW	0.487	21,211
12	0219122146	11116 KLINE ST SW	0.170	7,405
13	0219122016	XXXX KENDRICK ST SW	0.031	1,350
14	0219122152	Undetermined Situs	0.270	11,761
15	4935000060	11201 KENDRICK ST SW	0.134	5,850
16	0219122147	11204 KLINE ST SW	0.160	6,970
17	4935000010	11204 KENDRICK ST SW	0.179	7,806
18	0219122148	11208 & 11210 KLINE ST SW	0.180	7,841
19	0219122167	4610 114TH ST SW & 4614 113TH ST SW	2.530	110,207
20	4935000020	11302 KENDRICK ST SW	0.179	7,806
21	4935000090	11307 KENDRICK ST SW	0.145	6,300
22	4935000100	11309 KENDRICK ST SW	0.145	6,300
23	4935000030	11304 KENDRICK ST SW	0.180	7,843
24	0219122153	XXXX KLINE ST SW	0.450	19,605
25	4935000120	11409 KENDRICK ST SW	0.188	8,195
26	4935000040	11402 KENDRICK ST SW	0.180	7,843
27	4935000052	11412 KENDRICK ST SW	0.137	5,980
TOTAL:			8.54	372,152

Lakewood Station District Subarea Plan, Borman Properties (Washington and Rice, LLC)

This product was prepared with care by City of Lakewood GIS. City of Lakewood expressly disclaims any liability for any inaccuracies which may yet be present. This is not a survey. Datasets were collected at different accuracy levels by various sources. Data on this map may be shown at scales larger than its original compilation. Call 253-589-2489 for further information.

Attachment D Exhibit C

Renditions of Attached Single Family Dwellings at 34, 36 and 44 Units per Net Acre

34 Dwelling Units Per Acre (Net)

Notable features: An urban, street-oriented townhouse development integrating corner retail spaces, live-work spaces, internal auto courts with private garages, and common open space.



Lionsgate Townhouses in Redmond.

36 Dwelling Units Per Acre (Net)

Notable features: A cluster of single-family homes integrated into surrounding neighborhood with alley and internal auto-court access, private garages, a large cedar tree, and a manmade stream running through the site.



Detached single-family homes in The Boulders at Green Lake development in Seattle. (Johnston Architects)

44 Dwelling Units Per Acre (Net)

Notable features: New urban townhouses and live-work units served by underground parking and containing private patios and a centralized, shared courtyard space.



Live-work and regular townhouse units in Seattle's Lower Queen Anne neighborhood.
(Landscape plan courtesy of David Vandervort Architects)

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED:	TITLE: Expressing a commitment to valuing diversity, equity and inclusion.	TYPE OF ACTION:
April 19, 2021	ATTACHMENTS:	<input type="checkbox"/> ORDINANCE
REVIEW:	Resolution	<input checked="" type="checkbox"/> RESOLUTION 2021-04
March 15, 2021		<input type="checkbox"/> MOTION
		<input type="checkbox"/> OTHER

SUBMITTED BY: Heidi Ann Wachter, City Attorney

RECOMMENDATION: It is recommended that the City Council adopt a joint Resolution with the Board of Directors of the Clover Park School District expressing a commitment to valuing diversity, equity and inclusion.

DISCUSSION: Lakewood City Council and Clover Park School District Board of Directors recognize the critical role that city and school leaders have in affirming differences, removing barriers to access and opportunity, and facilitating a culture of inclusion. The Lakewood City Council and Clover Park School District Board of Directors met in January 2021, and agreed to continue partnering in furthering these efforts and is committed to collaborate, develop and lead diversity, equity, and inclusion initiatives and share resources for the benefit of our respective organizations and our community.

ALTERNATIVE(S): None.

FISCAL IMPACT: None.

Heidi Ann Wachter, City Attorney
Prepared by

City Manager Review

Department Director

RESOLUTION NO. 2021-04

A RESOLUTION of the City Council of the City of Lakewood, Washington and the Board of Directors of the Clover Park School District expressing a commitment to valuing diversity, equity and inclusion.

WHEREAS, the people of the City of Lakewood and the Clover Park School District value and acknowledge that diversity, equity, and inclusion are essential to our community; and

WHEREAS, the Lakewood City Council and Clover Park School District Board of Directors recognize the critical role that city and school leaders have in affirming differences, removing barriers to access and opportunity, and facilitating a culture of inclusion; and

WHEREAS, the Lakewood City Council and Clover Park School District Board of Directors recognize systemic inequality and racism has been a barrier to assuring the basic principles of diversity, equity, inclusion for historically marginalized people groups; and

WHEREAS, the Lakewood City Council and Clover Park School District Board of Directors value the multifaceted way embracing diversity enables the community to be a welcoming and inclusive place to live, work and enjoy life; and

WHEREAS, the Lakewood City Council and Clover Park School District Board of Directors believes that the road to systemic equity and inclusive practices is a journey that can consistently be achieved through meaningful collaboration; and

WHEREAS, the Lakewood City Council and Clover Park School District Board of Directors celebrates our community's diversity and is committed to fostering a community that welcomes all people who live, work, and go to school here, regardless of their national origin, race, sexual orientation, gender identity, financial or socioeconomic status; and

WHEREAS, the Lakewood City Council and Clover Park School District Board of Directors met in January 2021, and have agreed to continue partnering in furthering these efforts and is committed to collaborate, develop and lead diversity, equity, and inclusion initiatives and share resources for the benefit of our respective organizations and our community; and

WHEREAS, the Lakewood City Council and Clover Park School District Board of Directors believes that by working together, sharing our perspectives and implementing targeted practices to improve the well-being of historically marginalized members in our community we can take steps closer to more justice and more fairness in the community of Lakewood.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON AND THE BOARD OF DIRECTORS OF CLOVER PARK SCHOOL DISTRICT, DO HEREBY RESOLVE, as follows:

Section 1. Proclaim that we reaffirm our community's shared values of diversity, equity, inclusion, compassion, respect, and dignity; and our commitment to building a community in which everyone and especially people of historically marginalized identities have the opportunity to thrive; and we encourage all citizens to likewise affirm these values.

Section 2. This Resolution shall be in full force and effect upon passage and signatures hereon.

PASSED by the City Council this 19th day of April, 2021.

CITY OF LAKEWOOD

CLOVER PARK SCHOOL DISTRICT

Don Anderson, Mayor

Marty Schafer, Board President

Attest:

Briana Schumacher, City Clerk

Approved as to form:

Heidi Wachter, City Attorney

REQUEST FOR COUNCIL ACTION

**DATE ACTION IS
REQUESTED:**

April 19, 2021

TITLE: Adopting a statement on
Equity.

TYPE OF ACTION:

— ORDINANCE NO.

X RESOLUTION NO. 2021-05

REVIEW:

April 12, 2021

ATTACHMENTS:
Resolution

— MOTION NO.

— OTHER

SUBMITTED BY: Heidi Ann Wachter, City Attorney

RECOMMENDATION: It is recommended that the City Council pass the Resolution adopting a Statement on Equity.

DISCUSSION: The Lakewood City Council acknowledges that equity is essential to a healthy community and is committed to identifying and eliminating systemic racism. This resolution affirms the City Council's commitment to the advancement of equity and the deliberate practice of inclusion.

ALTERNATIVE(S): The City Council is not required to take any position on the topic of equity. Adoption of the Resolution clearly states the Council's collective position on the issue.

FISCAL IMPACT: No impact.

Heidi Ann Wachter

Prepared by


City Manager Review

Department Director

RESOLUTION NO. 2021-05

A RESOLUTION of the City Council of the City of Lakewood,
Washington, adopting a statement on Equity.

WHEREAS, the Lakewood City Council acknowledges that equity is essential to a healthy community; and

WHEREAS, the Lakewood City Council is committed to identifying and eliminating systemic racism; and

WHEREAS, the Lakewood City Council intends to lead by example in the advancement of equity and the deliberate practice of inclusion.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD,
WASHINGTON, HEREBY RESOLVES, as follows:

Section 1. A Statement on Equity by the Lakewood City Council, is hereby adopted as set forth in Exhibit A, which is attached.

Section 2. This Resolution shall be in full force and effect upon passage and signatures hereon.

PASSED by the City Council this 19th day of April, 2021.

CITY OF LAKEWOOD

Don Anderson, Mayor

Attest:

Briana Schumacher, City Clerk

Approved as to form:

Heidi Ann Wachter, City Attorney



A Statement on Equity by the Lakewood City Council

The Lakewood City Council acknowledges that equity is essential to a healthy community.

We are committed to identifying and eliminating systemic racism. We intend to lead by example in the advancement of equity and the deliberate practice of inclusion.

The City Council commits to the following practices:

- Instilling equity as a priority of policy and the delivery of services.
- Enacting initiatives that support and celebrate the diversity of the community.
- Ensure equity in municipal planning.
- Identifying and dismantling preconceived prejudices.
- Increasing sensitivity to social norms and cultural expectations.
- Pursuing justice and equity for all residents.

We recognize the critical role that city leaders have in removing barriers to opportunity. We recognize that systemic inequality has endured, but commit that it shall not persist.

These intentional practices will inform our decision-making on policing, zoning, capital investment and all other matters of the City Council. Our objective is to create a more diverse, equitable and inclusive Lakewood community for all residents and invite all Lakewood organizations and residents to engage in the opportunities of our future.

This City Council offers one final commitment: The City Council will not tolerate *intolerance*.

It shall be the policy of the Lakewood Police Department to be vigilant in its investigation and prosecution crimes of hate.

It is unconscionable that some members of our community fall victim to acts of deliberate malignance. Hate based upon race, creed, ancestry, disability, sex, sexual orientation, or gender identity is unwelcome in Lakewood, Washington.

SIGNED BY:

Mayor Don Anderson

Councilmember Mary Moss

Councilmember Mike Brandstetter

Deputy Mayor Jason Whalen

Councilmember Patti Belle

Councilmember Linda Farmer

Councilmember Paul Bocchi

REQUEST FOR COUNCIL ACTION

**DATE ACTION IS
REQUESTED:**

April 19, 2021

REVIEW:

April 12, 2021

TITLE: Adopting the Region 5
All Hazard Mitigation Plan (2020-
2025) Edition and the 2021
Addendum to the Region 5
Hazard Mitigation Plan

ATTACHMENTS:
Resolution

TYPE OF ACTION:

— ORDINANCE NO.

X RESOLUTION NO. 2021-06

— MOTION NO.

— OTHER

SUBMITTED BY: John Unfred, Assistant Chief of Police

RECOMMENDATION: It is recommended that the City Council adopt the Region 5 All Hazard Mitigation Plan (2020-2025) Edition and the 2021 Addendum to the Region 5 Hazard Mitigation Plan.

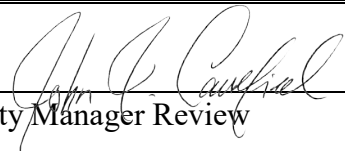
DISCUSSION: The Local Mitigation Plan is the representation of the City's commitment to reduce risks from natural hazards, serving as a guide for decision makers as they commit resources to reducing the effects of natural hazards. Pierce County DEM writes a multi-jurisdictional plan for all of Region 5 (Pierce County), with Addendums from cities within the region including Lakewood. The **Plan** and **Addendum** must be approved by resolution from the City Council and updated every 5 years.

ALTERNATIVE(S): Not adopt the recommended Resolution although the City could risk not receiving future disaster funding if the All Hazard Mitigation Plan Update is not adopted.

FISCAL IMPACT: No impact.

John Unfred

Prepared by


City Manager Review

Mike Zaro

Department Director

RESOLUTION NO. 2021-06

A RESOLUTION of the City Council of the City of Lakewood, Washington, adopting the Region 5 All Hazard Mitigation Plan (2020-2025) Edition and the 2021 Addendum to the Region 5 Hazard Mitigation Plan.

WHEREAS, the Federal Disaster Mitigation Act of 2000 requires that for all disasters declared on or after November 1, 2004, applicants for sub-grants following any disaster must have an approved Natural Hazard Mitigation Plan in accordance with 44CFR 201.6 prior to receipt of Hazard Mitigation Grant Program project funding; and

WHEREAS, the Federal Disaster Mitigation Act of 2000 requires that for Pre-Disaster Mitigation grant program project funding on or after November 1, 2003, applicants must have an approved Natural Hazard Mitigation Plan in accordance with 44CFR 201.6 prior to receipt of project funding; and

WHEREAS, the All Hazard Mitigation Plan Update represents the commitment of the City of Lakewood along with other surrounding government entities to reduce the risks from natural, man-made and technological hazards, serving as a guide for decision makers as they commit resources to reducing the effects of hazards, and it is in the public interest to proceed with the planning process in a timely manner; and

WHEREAS, the City of Lakewood has participated with the Pierce County Department of Emergency Management in the development of the City of Lakewood's All Hazard Mitigation Plan Update, and recognizes the economic loss, personal injury, and damage that can arise from these hazards; and

WHEREAS, reduction of these impacts can be achieved through a comprehensive coordinated planning process which includes an updated risk assessment that provides the factual basis for activities proposed in the mitigation strategies to reduce losses and vulnerabilities, a five-year cycle for plan maintenance, and documentation of formal adoption by the City of Lakewood; and

WHEREAS, the 2020-2025 Region 5 All Hazard Mitigation Plan Edition has been completed and approved by the State and the Federal Emergency Management Agency; and

WHEREAS, the City of Lakewood could risk not receiving future disaster funding if the All Hazard Mitigation Plan Update is not adopted; and

WHEREAS, the Lakewood City Council reviewed the All Hazard Mitigation Plan Update.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON, HEREBY RESOLVES, as follows:

Section 1. The Region 5 All Hazard Mitigation Plan, 2020-2025 Edition, is hereby adopted as set forth in Exhibit A, which is attached.

Section 2. The City of Lakewood Addendum to the Region 5 Hazard Mitigation Plan, an update to the City of Lakewood All Hazard Mitigation Plan is hereby adopted and shall be in full force and effect upon passage and signatures hereon.

PASSED by the City Council this 19th day of April, 2021.

CITY OF LAKEWOOD

Don Anderson, Mayor

Attest:

Briana Schumacher, City Clerk
Approved as to form:

Heidi Ann Wachter, City Attorney



CITY OF LAKEWOOD
ADDENDUM A-8
REGION 5 HAZARD MITIGATION PLAN
2020-2025 EDITION

Prepared for:

City of Lakewood
6000 Main Street SW
Lakewood, WA 98499

In Cooperation with:

Pierce County Department of Emergency Management
2501 S. 35th Street, Suite D
Tacoma, WA 98409

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REGION 5 ALL HAZARD MITIGATION PLAN
2020-2025 EDITION
CITY OF LAKEWOOD

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Section 1

Plan Process Requirements

Planning Process---Requirement §201.6(b):

An open public involvement process is essential to the development of an effective plan.

Documentation of the Planning Process---Requirement §201.6(b):

In order to develop a more comprehensive approach to reducing the effects of natural disasters, the planning process **shall** include:

- (1) An opportunity for the public to comment on the plan during the drafting stage and prior to plan approval;
- (2) An opportunity for neighboring communities, local and regional agencies involved in hazard mitigation activities, and agencies that have the authority to regulate development, as well as businesses, academia and other private and non-profit interests to be involved in the planning process; and
- (3) Review and incorporation, if appropriate, of existing plans, studies, reports, and technical information.

Documentation of the Planning Process---Requirement §201.6(c)(1):

[The plan **shall** document] the planning process used to develop the plan, including how it was prepared, who was involved in the process, and how the public was involved.

- Does the plan provide a narrative description of the process followed to prepare the new or updated plan?
- Does the new or updated plan indicate who was involved in the current planning process? (Who led the development at the staff level and were there any external contributors such as contractors? Who participated on the plan committee, provided information, reviewed drafts, etc.?)
- Does the new or updated plan indicate how the public was involved? (Was the public provided an opportunity to comment on the plan during the drafting stage and prior to the plan approval?)
- Does the new or updated plan discuss the opportunity for neighboring communities, agencies, businesses, academia, nonprofits, and other interested parties to be involved in the planning process?
- Does the planning process describe the review and incorporation, if appropriate, of existing plans, studies, reports, and technical information?
- Does the updated plan document how the planning team reviewed and analyzed each section of the plan and whether each section was revised as part of the update process?

SECTION 1

REGION 5 ALL HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD PROCESS SECTION

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Changes To Jurisdiction Plan in this Document

This Addendum to the Region 5 Hazard Mitigation Plan includes the following changes that are documented as a result of a complete review and update of the existing plan for the City of Lakewood. The purpose of the following change matrix is to advise the reader of these changes updating this plan from the original document approved in November 2008.

The purpose for the changes is three-fold: 1) the Federal Law (Code of Federal Regulations (CFR), Title 44, Part 201.4) pertaining to Mitigation Planning has changed since the original Plan was undertaken; 2) this Plan will be an Addendum to the 2020 comprehensive and FEMA approved Region 5 All Hazard Mitigation Plan and 3) the Local Mitigation Planning Requirements of the Disaster Mitigation Act of 2000 201.6 (d) (3) Plan Review states Plans **must** be reviewed, revised if appropriate, and resubmitted for approval within five years in order to continue to be eligible for HMGP project grant funding. This document when completed and approved will become an additional Addendum to the other jurisdictions encompassed in the 2020 approved Region 5 All Hazard Mitigation Plan.

Change Matrix

This Matrix of Changes documents the pertinent changes made from the July 2015 City of Lakewood Plan for the Region 5 All Hazard Mitigation Plan; 2020-2025 Update. Most of the changes are a matter of additional detail, more information provided, and in some cases a response to new requirements. This 2020-2025 version represents a complete review and update by City of Lakewood and Pierce County Emergency Management using a detailed process for development and following an established format. During this procedure, all web links have been verified and updated.

Table 1-1 Change Matrix – City of Lakewood Region 5 Hazard Mitigation Plan 2020-2025 Update

Section 1 – Plan Development, Process Section	
Section or Part of Plan	New in 2020 Plan
Section 1 – Process Section	Section 1 – Process Section
	The 2020 Process Section contains updated Planning Meeting overviews, Planning Team Members, Drop-in schedule, Public Comment dates, Elected Official Meetings and updated dates for Plans that collaborate with the mitigation plan.

Section 2 – Participating Jurisdiction Profiles		
Section or Part of Plan	Previous	2020 Plan
Section 2 – Profile	Information was current as of 2010 Census Data.	The 2020 version of the Profile has been reviewed and updated. The Infrastructure Summary section was updated

		showing a significant increase in tax parcel values. In addition, the Economic Summary was updated also showing an increase.
	Information was current as of 2010 Census Data.	The 2010 Census Data remained for population data and is the current GIS available information from Pierce County. Once the 2020 Census data becomes available in Pierce County GIS format, population data figures will be updated in the Profile Section 2 and the Risk Assessment Section 4.
		A new Demographic Analysis paragraph was added to the 2020 Mitigation Plan to elaborate on Lakewood's demographics in more detail and capturing some of the at-risk populations. This also allowed the city to provide an updated overview of its growing population beyond the 2010 census which is outdated.

Section 3 – Capability Identification		
Section or Part of Plan	Previous	2020 Plan
Section 3 – Capability	The Capability Tables shown in the previous plan are in a similar format.	The 2020 Capability Section has been improved and updated to show current information from the jurisdiction.

Section 4 – Vulnerability, Risk Analysis	
Section or Part of Plan	2020 Plan
Vulnerability and Hazard Impact Analysis	This section was added to provide a better understanding on how the identified hazards affect the City of Lakewood and its critical infrastructure.
Changes in Development	This required element was added to provide a clearer understanding and location within the

	plan of the changes in development that have occurred within the City of Lakewood over the past five years.
Disaster Declarations Charts.	The Geological, Meteorological and Technological Charts have been updated to reflect current changes in Pierce County's Hazard Identification Risk Assessment (HIRA). Major changes include updating the maps, figures and table column to align with the changes in the HIRA. Technological Hazards added "Active Threat" and "Cyber Attack" under the Terrorism category.
Hazard Maps - Overview of Data Source Descriptions	This section was added to provide the reader with a better understanding of the data source that was used to produce the hazard maps.
The previous version of the plan contained hazard maps.	The 2020 Risk Section includes updated maps and contains additional hazard maps such as deep/shallow landslides susceptibility.
The previous version included specific analysis showing vulnerability of population, land and infrastructure according to Census 2010 and 2013/2014 tax parcel data.	The 2020 Risk Section includes completely updated tables showing vulnerability of population, (where different hazard maps were used) land and infrastructure using Census 2010 data and 2019/2020 tax parcel data.

Section 5 – Mitigation Strategy	
Section or Part of Plan	2020 Plan
The previous document used the standard goals as outlined for the entire project.	The 2020 Mitigation Section was drafted using specific goals and objectives written by the jurisdictions to their specific hazards and concerns.
The previous document contained a Mitigation Measure Matrix chart followed by written descriptions of each individual measure.	The new document uses the same format as the original plan with the addition of a 'Status Update' table under each mitigation measure. This provides the opportunity to update each mitigation strategy and track the status. New measures have been added to both the Matrix and the individual measure descriptions. Measures completed in the past five years have been moved to a historical appendix in the plan to track projects completed by the jurisdiction.

Section 6 – Infrastructure	
Section or Part of Plan	2020 Plan
The previous plan used a full table with details on each piece of critical infrastructure. In addition, a matrix summary of hazards and dependencies affecting the critical infrastructure was completed.	The 2020 plan uses the same table. The tables have been reviewed and updated by the jurisdiction. This section is only available to the jurisdiction due to the sensitivity of information contained. A disclosure statement acts as a placeholder for Section 6.

Section 7 – Plan Maintenance	
Section or Part of Plan	2020 Plan
The previous Plan Maintenance for the jurisdiction was very similar in format to the newer version for 2020.	The 2020 version of the Plan Maintenance borrows from the format and content of the original; however, the entire document has been reviewed and updated to current information.

Section 8 – Other Changes	
Section or Part of Plan	2020 Plan
The previous document contained four Appendices.	The 2020 Plan contains six Appendices including: place for the final resolution and approval letter from FEMA, list of jurisdiction’s planning team, a chart for any changes, 2014 HAZUS analysis, documentation records for Public Outreach events and a historical appendix for completed projects. The Acronym list appears in the Base Plan for the entire project.

Plan Process

The Region 5 Hazard Mitigation Plan Process Section is a discussion of the planning process used to update the Region 5 Hazard Mitigation Plan (Pierce County is Region 5 for Homeland Security (HLS) in Washington State, including how the process was prepared, who aided in the process, and the public involvement.

The Plan update is developed around all major components identified in 44 CFR 201.6, including:

- **Public Involvement Process;**
- **Jurisdiction Profile;**
- **Capability Identification;**
- **Risk Assessment;**
- **Mitigation Strategy;**
- **Infrastructure Section; and,**
- **Plan Maintenance Procedure.**

Below is a summary of those elements and the processes involved in their development.

Public Involvement Process

Public participation is a key component to strategic planning processes. Citizen participation offers citizens the chance to voice their ideas, interests, and opinions.

“Involving stakeholders who are not part of the core team in all stages of the process will introduce the planning team to different points of view about the needs of the community. It will also provide opportunities to educate the public about hazard mitigation, the planning process, and findings, and could be used to generate support for the mitigation plan.”ⁱ

In order to accomplish this goal and to ensure that the updated Region 5 Hazard Mitigation Plan is comprehensive, the six planning groups in conjunction with Pierce County Emergency Management developed a public participation process of three components:

1. A Planning Team comprised of knowledgeable individual representatives of HLS Region 5 area and its hazards;
2. Hazard Meetings to target the specialized knowledge of individuals working with populations or areas at risk from all hazards; and
3. Public meetings to identify common concerns and ideas regarding hazard mitigation and to discuss specific goals, objectives and measures of the mitigation plan.

This section discusses each of these components in further detail below with public participation outlined in each. Integrating public participation into the development of the Region 5 Hazard

Mitigation Plan update has helped to ensure an accurate depiction of the Region’s risks, vulnerabilities, and mitigation priorities.

Planning Team

The Planning Team was organized early in 2019. The individual Region 5 Hazard Mitigation Planning Team members understand the portion of Pierce County containing their specific jurisdiction, including how residents, businesses, infrastructure, and the environment may be affected by all hazard events. The members are experienced in past and present mitigation activities and represent those entities through which many of the mitigation measures would be implemented. The Planning Team guided the update of the Plan, assisted in reviewing and updating goals and measures, identified stakeholders, and shared local expertise to create a more comprehensive plan. The Planning Team was comprised of:

Table 1-2 Planning Teams – Discipline Group

NAME	TITLE	JURISDICTION-DEPARTMENT
Woody Edvalson	Emergency Manager	City of Bonney Lake
Alan Predmore	Fire Chief/Emergency Manager	City of Buckley
Jeffrey Wilson	Director of Community Development	City of DuPont
Micah Lundborg	Chief of Police	City of Edgewood
Pete Fisher	Police Chief	City of Fife
Robert Eugley	Patrol Officer	City of Fife
John Cheesman	Chief of Police	City of Fircrest
Kelly Busey	Chief of Police	City of Gig Harbor
Carl Desimas	City Planner	City of Gig Harbor
John Unfred	Assistant Police Chief	City of Lakewood
Tony Hernandez	Police Chief	City of Milton
Mark Bethune	City Manager	City of Orting
Kirstin Hofmann	Emergency Manager	City of Puyallup
Chief Armitage	Police Chief	City of Roy
Officer Armitage	Police Officer	City of Roy
Ryan Windish	Community Development Director	City of Sumner
Ute Scofield	Emergency Manager	City of Tacoma
Jacob Rain	EM Program Coordinator	City of Tacoma
Lisa Petorak	Human Resources Manager	City of University Place
Jack Ecklund	Dir. of Engineering & Capital Projects	City of University Place
Dallene Argo	Clerk-Treasurer	Town of Carbonado
Abby Gribi	Town Administrator	Town of Eatonville
Glen Yates	Eatonville Police Department	Town of Eatonville
Emily Terrell	Consultant	Town of South Prairie
Paul Loveless	Town Administrator	Town of Steilacoom
Alan Predmore	Fire Chief	Town of Wilkeson

Table 1-3 Planning Teams – Regional Group

NAME	TITLE	JURISDICTION-DEPARTMENT
Jeffrey S. Wilson	Director Community Development	City of DuPont
John Unfred	Assistant Police Chief	City of Lakewood
Christine Badger	Emergency Manger	City of Lakewood
Paul Loveless	Town Administrator	Town of Steilacoom
John Ecklund	Director of Engineering	City of University Place
Lisa Petorak	HR Manager	City of University Place
Hallie McCurdy	Assistant Fire Chief	West Pierce Fire & Rescue #3
Christine Badger	Emergency Manger	West Pierce Fire & Rescue #3
Randy Granum	Risk Manager	Clover Park School District
Susanne Beauchaine	Executive Director for Human Resources	Steilacoom Historical School District
Torey Heidelberg	Preparedness/Safety Cor.	University Place School District
Don Stanley	Operations Department Head	Lakewood Water District
Linda Horey	Emergency Management Program Specialist	Western State Hospital
John Cammon	Maintenance Superintendent	Riviera Community Club
Sgt Jason Harms	Pierce Transit Police	Pierce Transit

Planning Team Meetings

The Planning Team held 7 Planning Team Meetings either in their Discipline Groups or Regional Planning Groups. Meeting in Regional Planning Groups supported a whole community planning approach which either developed new or stronger relationships amongst jurisdictions. This allowed for an integration of mitigation strategies for regions sharing the commonality in hazards. There was a total of 45 meetings from February 2019 to January 2020 between all Planning Groups. Additional working group drop-in workshops were provided for jurisdictions to continue to work on and update their plans. Two “drop-in” workshops were provided each month from January through June alternating between morning and afternoons to accommodate work schedules.

The Planning Teams Discipline Groups: City and Town Group, Fire Group, School Group, Special Purpose Group, Utility Group, Medical Group and Unincorporated Pierce County Group.

These discipline groups will continue to meet on an annual basis for the relationship building and sharing of mitigation strategies and ideas.

The Planning Team Regional Groups broken down into five geographical areas in Pierce County: West Group (all of Gig Harbor, Key Peninsula, Herron Island, Fox Island and Raft Island), SW Group (Lakewood, Anderson Island, Steilacoom), Central Group (Puyallup, Graham, Eatonville), NE Group (Buckley, Carbonado, Bonney Lake, Wilkeson), North Group (Tacoma, Fife, Edgewood, Sumner). The Regional Groups were developed based on geographic location and the commonality of hazards shared and was new with this update. This provided for better community planning, relationship building, and collaboration of mitigation strategies ultimately leading to community resiliency. These Regional groups will continue to meet on an

annual basis and as sub committees are developed to work on specific projects the frequency of meetings will potentially increase.

Table 1-4 Planning Team Meetings

Planning Team Meeting #1 – Cities & Towns: PCEM Puyallup Room – February 21, 2019
Planning Team members Debbie Bailey and Wyatt Godfrey conducted the meeting and the Planning Team discussed the following items: Introduction of Planning Team, Review of the history of the Grant Application, Defining the Planning Requirements, How We Establish the In-Kind Match, Benefits of Developing a Plan, Defining the Planning Process, Establishing the Planning Team Meetings, Elected Official Meetings and Public Comment Meetings, reviewing each jurisdiction’s profile information, and defining next steps.
Planning Team Meeting #2 – SW Regional Group: PCEM Nisqually Room – March 25, 2019
Planning Team members Debbie Bailey and Wyatt Godfrey conducted the meeting and the Planning Team discussed the following items: Introduction of Planning Team as this was our first Regional Planning meeting and there were new members present. We reviewed items presented at the previous meeting, Defining the Planning Requirements, Defining the Process, Establishing the Planning Team Meetings, Elected Official Meetings and Public Comment Meetings, and explaining the next steps. This meeting focused on continuing review of the Profile Section, an introduction to begin thinking about mitigation strategies to include a review of what measures from their original plan have already been completed and thinking about new measures they may like to add. In addition, this group discussed the Capability Section and how to recognize capabilities that already exist within the jurisdiction. Everyone was reminded to set up their Elected Official meetings. Everyone was given a copy of their original Section 3 – Capability Section.
There was not a Regional Planning Meeting in April of 2019
Planning Team Meeting #3 – SW Regional Group: PCEM Nisqually Room – May 28, 2019
Planning Team members Debbie Bailey and Wyatt Godfrey conducted the meeting with the majority of the regional jurisdictions present. We reviewed the Profile, Capabilities, and Mitigation Strategy Sections, along with introducing the Risk Assessment Section to the group. We also talked about progress made on the In-Kind Match sheets and pre-authorization approval from jurisdictions’ governing bodies. Finally, we gathered feedback about our Threat and Hazard Identification Workshop held on May 1-2, and everyone’s progress with outreach events for their mitigation plans, especially in relation to fire season starting and the opportunity for communities in this region to incorporate more fire protection and mitigation elements into their planning process.
There was not a Regional Planning Meeting in June of 2019
Planning Team Meeting #4 – SW Regional Group: PCEM Nisqually Room – July 18, 2019

Planning Team members Debbie Bailey and Wyatt Godfrey reviewed the Profile, Capabilities, Risk Assessment, and Mitigation Strategy Sections to see how everyone was coming along with their update process. A reminder was provided for those who had not turned in their in-kind match sheet, as well as for those who had not completed the governing body pre-approval requirement yet. Debbie offered to create jurisdictional maps for public outreach events to bring residents in to talk about hazards that can affect them and how the mitigation plan plays a role in community resilience. Lastly, Todd Kilpatrick, the former Mitigation Grant Program Manager with Washington State Emergency Management Division who now works at Pierce County Emergency Management, spoke to the group about the Hazard Mitigation Grant Program (HMGP), the Pre-Disaster Mitigation Grant (PDM), potential projects that are eligible for those grants, and the upcoming Mitigation Grant Workshop that'll be held on August 12th and 19th.

There was not a Regional Planning Meeting in August of 2019

Planning Team Meeting #5 – SW Regional Group: PCEM Nisqually Room – September 19, 2019

Planning Team members Debbie Bailey and Wyatt Godfrey reviewed the Profile, Capabilities, Risk Assessment, and Mitigation Strategy Sections to check on the jurisdictions' progress. More specifically, Debbie explained the process of developing new mitigation strategies to add to their plans. This discussion covered how to select a new mitigation strategy, the required components for their strategy development, and the format required to input the strategy into the plan. Feedback was gathered about the August Mitigation Grant Workshop – unanimous positive feedback with a few recommendations to improve for next time. A reminder for the In-Kind Match Sheet and pre-authorization documentation was provided. Finally, the meeting was closed out with a discussion on the progress of meeting the public outreach requirements and ideas for those who had not completed that component yet.

Planning Team Meeting #6 – SW Regional Group: PCEM Nisqually Room – October 31, 2019

Planning Team members Debbie Bailey and Wyatt Godfrey held the meeting with less participation than preferred but included a call-in option for those who couldn't attend in person. The usual review of previous sections occurred, with the introduction of the Infrastructure and Plan Maintenance Sections. Participants were taught how to fill out the potentially overwhelming tables in the Infrastructure Section and told to review the Plan Maintenance Section for any inaccurate statements or language. Like the previous meeting, a reminder for the In-Kind Match Sheet, pre-authorization documentation, and public outreach documentation was provided.

Planning Team Meeting #7 – SW Regional Group: PCEM Nisqually Room – December 3, 2019

The final planning meeting was conducted by Debbie Bailey and Wyatt Godfrey. All sections of the plan were discussed and reviewed to ensure participants' questions were answered. A detailed discussion of the Mitigation Strategy Section occurred, specifically looking at the integration of new strategies into the plan and how to reorder them by priority. Like the previous meeting, a reminder for the In-Kind Match Sheet, pre-authorization documentation, and public outreach documentation was provided. Participants were informed that in the new year, Pierce County DEM would be hosting two "workshops" a month where jurisdictions can walk in and get help with their plan on an individual basis, instead of only in the previously used group format. The goal is to refine the work that participants have done thus far and craft it into a well-rounded, comprehensive, and usable Hazard Mitigation Plan.

Drop – In Workshop

To provide further opportunity for participating jurisdictions to work on their plan updates Pierce County DEM hosted two additional "workshop" meetings per month starting in January 2020. These were not formal meetings but provided individual instruction or assistance to jurisdictions. They were scattered at two-week intervals during the month with alternating morning and afternoon times trying to accommodate busy schedules. Due to the COVID-19 virus pandemic our "drop-in" workshops were canceled for the remainder of the update cycle. We remained available through email and phone call conversations.

Date	Location
January 7, 2020 – 1:00-3:30	Pierce County - DEM
January 23, 2020 – 9:00-11:30	Pierce County - DEM
February 11, 2020 – 1:00-3:30	Pierce County - DEM
February 27, 2020 – 9:00-11:30	Pierce County - DEM

Public Comment and Process

Date	Location	Time
September 3, 2019	Lakewood Farmer's Market 6000 Main St SW, Lakewood, WA 98499	10:00 am – 3:00 pm
October 2, 2019	Lakewood Police Station 9401 Lakewood Dr. SW, Lakewood, WA 98499	5:15 pm – 6:15 pm
January 16, 2020 – February 15, 2020	Social Media Lakewood Police Department Facebook Page	On going

In updating our mitigation plan, we utilized an inclusive process to involve the whole community to develop the best plan that is reasonable to address the specific hazards within our jurisdiction. To accomplish this, we utilized a variety of tactics including an online survey, outreach booth at our local farmer's market, social media, and sought input from our Public Safety Advisory Committee and City Council at one of their public meetings.

City Council Meeting: During a regular scheduled Council meeting, the planning process was presented to our City Council. Input was sought from them as well. (agenda in Appendix E)

Outreach booth: During one of our local Farmer’s Markets, we manned a booth to engage with our community members in order to educate the public on specific hazards and to seek their input to our plan. Fliers with information explaining this process and contact information were distributed. (Pictures and flier in Appendix E)

Advisory Committee: Lakewood has a Public Safety Advisory Committee, which is a committee appointed by the City Council to advise them on all matters concerning public safety. These meetings are held monthly and are open to the public. At their regular meeting, their input, as well as any citizens attending, was sought into our mitigation plan. (agenda and minutes in Appendix E)

This was probably the most effective outreach we had because several committee members took the time to read the plan and provided us with valuable feedback. One caught that we missed the addition of two passenger rail incidents. We also appreciated the opportunity to explain how processes work and implement some expectation management around mitigation strategies. There was a theme of suggestions and inquires that required further explanation as to existing plans, why we identified a need and specific details of gaps. In summary, the projects discussed included a generator for our Vehicle and Facilities Maintenance Facility and the expansion of the Department Operations Center at Station 21.

Social Media: We used our Police Department’s social media platform on Facebook to publicize the online survey and solicit feedback or input into our plan. (screenshot in Appendix E)

Online Survey: In conjunction with our County Department of Emergency Management, we offered citizens the ability to submit feedback, suggestions, or concerns via an online survey. We promoted this survey using our Police Department’s Facebook account.

Elected Officials Meetings

On July 8, 2019, Debbie Bailey presented in front of the Lakewood City Council with the assistance of Assistant Chief of Police John Unfred. She first explained the overarching project of updating local jurisdictions’ mitigation plans and then went into detail about the purpose, components, and processes involved in this update. Debbie provided reading material to the Council – in particular, a list of all jurisdictions participating in this update, as well as a short brochure explaining the mitigation plan and how it benefits the whole community. Once she was finished presenting, the Council had the opportunity to ask questions and provide comments. Many Council members provided positive feedback regarding the City of Lakewood’s involvement in this planning process, while others asked questions about the project timeline, grant funding opportunities, and the applicability of specific mitigation projects for their jurisdiction. After the commentary period ended, the Council unanimously voted to approve the City of Lakewood’s participation in the update of their Hazard Mitigation Plan.

Joint Planning Requirement

The City of Lakewood has the following identified plan which must collaborate with the mitigation plan; these plans are identified in the table below and must be updated within the predetermined timeline.

Plan	Next Update
City of Lakewood Comprehensive Plan	June 2020

Endnote

¹State and Local Mitigation Planning How-to Guide Getting Started: building support for mitigation planning, FEMA 386-1, September 2002, p. 3-1.

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SECTION 2

REGION 5 ALL HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD PROFILE SECTION

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Mission Statement

The mission of the City of Lakewood is as follows:

The City of Lakewood enhances the quality of life of our residents by providing essential services in a fiscally responsible manner, ensuring public safety, promoting economic development and preserving our natural environment.

Services Summary

The City of Lakewood was incorporated in the year 1996.

The jurisdiction provides the following services through their own capabilities:

Table 2-1 City Services¹

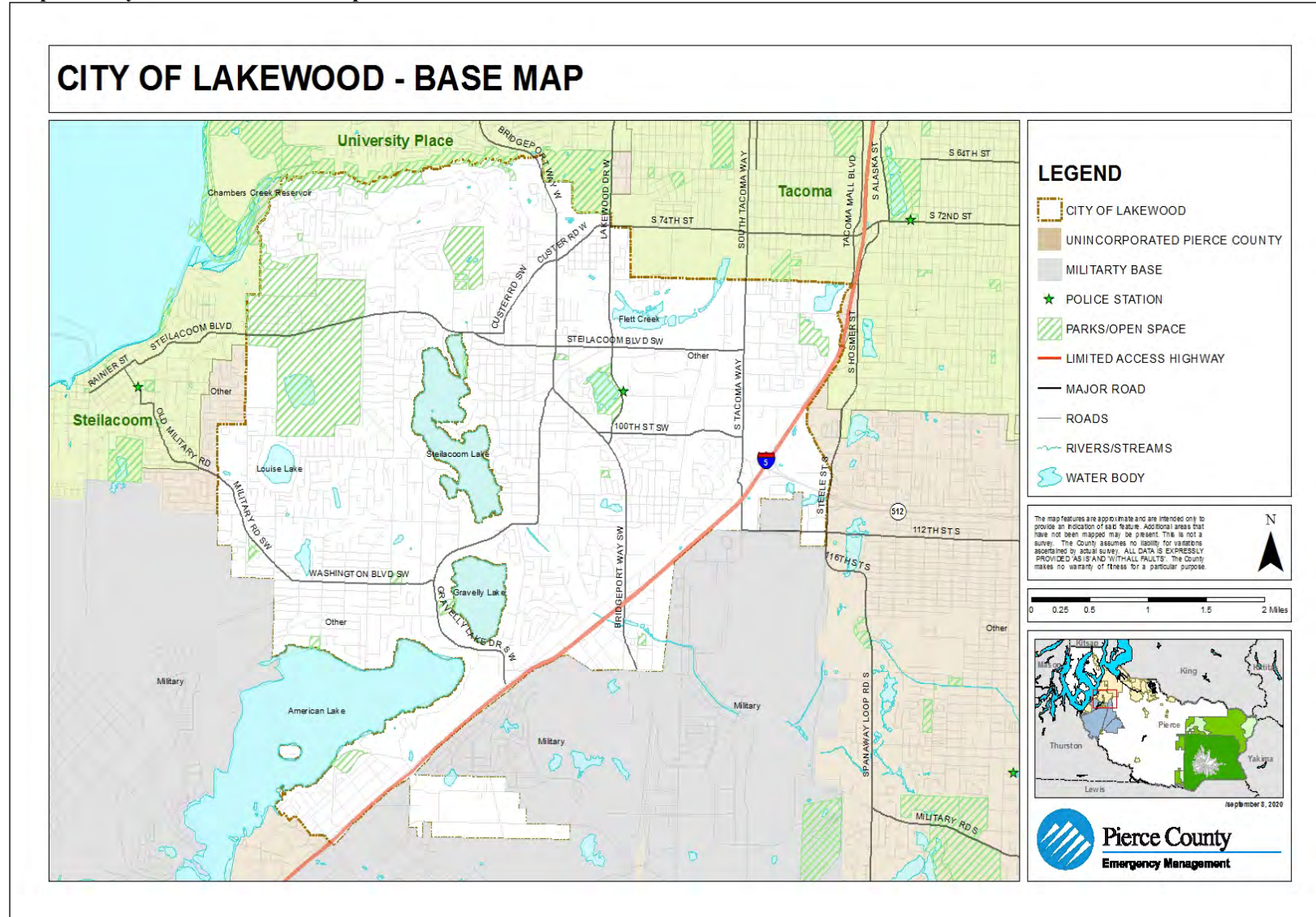
CITY SERVICES			
Service	Yes	Service	Yes
Mayor/City Manager	Yes	Municipal Airport	No
City Attorney	Yes	Municipal Court	Yes
City Clerk	Yes	Public Works/Improvements	Yes
City Treasurer	Yes	Comprehensive Planning	Yes
Sheriff or Police	Yes	Parking Meter Revenue	No
Parks Commissioners	No	Construction and Operation of Boat Harbors, Marinas, Docks, etc.	Yes
City Council	Yes	Issue Bonds and Levies of General Tax	Yes
License and Tax Fees	Yes	Fire Department/EMS	No
Non-Polluting Power Generation	No	Parking, Off-street Facilities	No
Hydroelectric Resources	No	Sanitary Landfill/Refuse Service	No
Radio Communications	Yes	Sidewalks	Yes
Streets	Yes	Storm Drains	Yes
Wastewater Treatment	No	Streets/Alleys	Yes
Water Utility	No	Parks and Parkways	Yes
Public Transportation Systems	No	Water Pollution Abatement	Yes
Residential Care Facilities	No	Local Improvement Districts	Yes
Child Care Facilities	No		

Geo-Political Summary

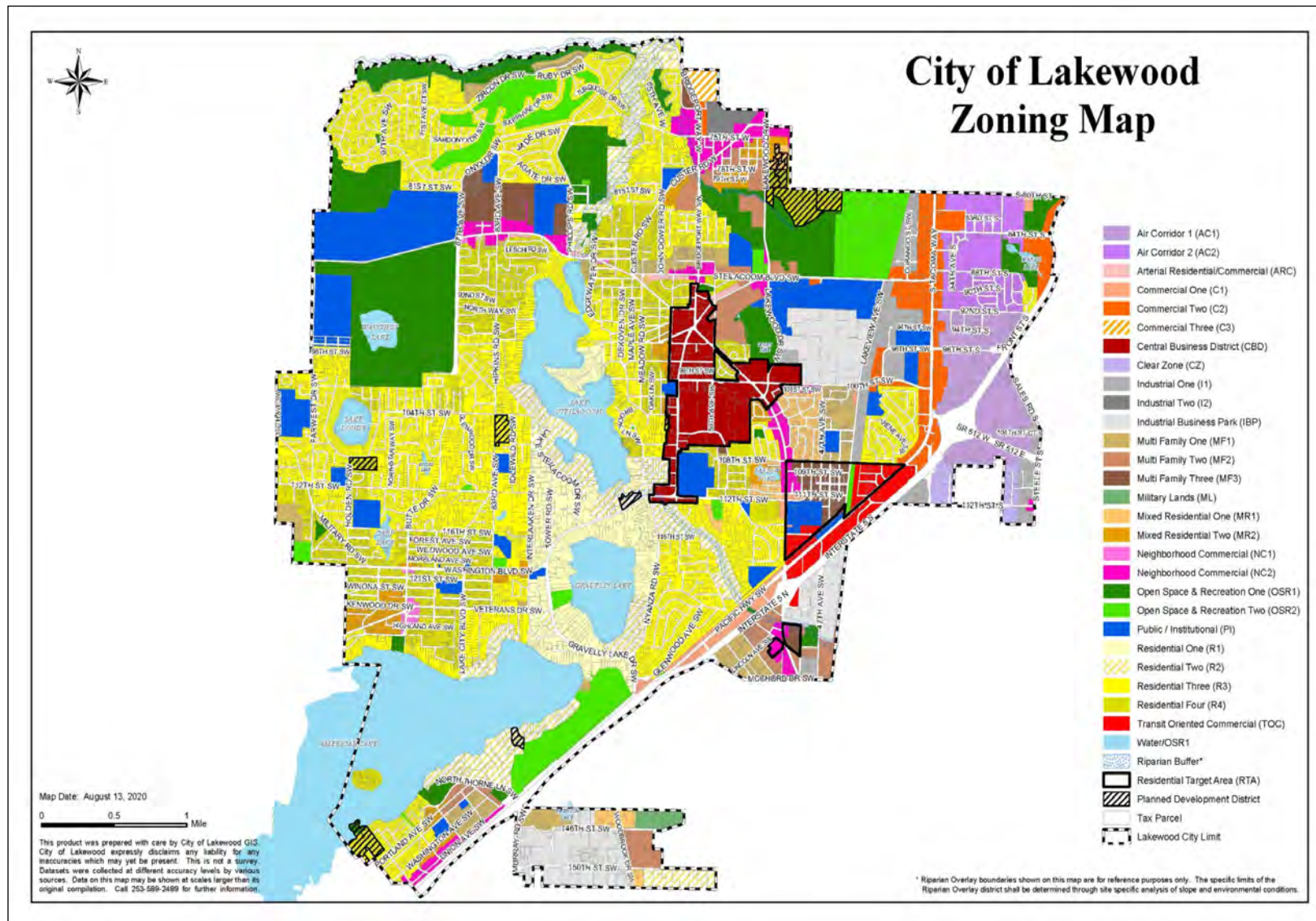
Table 2-2 Geo-Political Summary²

Jurisdiction	Area (sq mi)	Elevation Range (ft)	Major Water Features	Regional Partners	
				Shared Borders	Land Use Authorities
City of Lakewood	17.2405	60-380	<ul style="list-style-type: none"> • Chambers Clover Watershed • 2-Chambers Bay Basin • 6-Clover Creek/Steilacoom Basin • 9-American Lake Basin 	<ul style="list-style-type: none"> • University Place • Tacoma • Steilacoom • Unincorporated Pierce County • Western State Hospital • Camp Murray • WA—DNR • Joint Base Lewis-McChord 	<ul style="list-style-type: none"> • Lakewood • University Place • Tacoma • Steilacoom • Unincorporated Pierce County • WA—DSHS • WA—EMD • WA—DNR • WA—F &W • US DoD (Joint Base Lewis-McChord)

Map 2- 1 City of Lakewood - Basemap



Map 2-2 City of Lakewood Zoning Map



Population Summary

Demographics

Table 2-3 Population^{3, 4, 5, 6}

Jurisdiction	Population	Population Density (people/sq mi)	Population Served	Projected Year 2022 Population Change (%)	Projected Population Density	Projected 2022 Population Served
City of Lakewood	57,431	3,331	57,431	25.37%	4,176	72,000
Region 5	795,225	440	795,225	-18.39%	359	648,895

Population data in Tables 2-3 and 2-4 are outdated and based off the Washington State Office of Financial Management (OFM) 2010 Census data. Once the 2020 Census data becomes available these population numbers will be updated and replaced in the plan. The same situation occurs with all population figures in Section 4 Risk Assessment assessing the risk and vulnerability for all identified hazards in Unincorporated Pierce County.

Special Populations

Table 2-4 Special Populations⁷

Jurisdiction	Population	Population 65 Plus	% of Total	Population Under 20	% of Total
City of Lakewood	57,431	7,735	13%	14,646	26%
Region 5	795,225	87,770	11%	220,351	28%

Demographic Analysis

The City of Lakewood is located in southern Pierce County. According to the US Census 2017 Estimate, the city has a population of approximately 60,538, which is an increase from the 2015 estimate of 59,097. Lakewood has a population density of 3,387 people per square mile. This increase in population and density create a more vulnerable community. Nearly 22 percent of Lakewood residents are under the age of 18 and approximately 15 percent are over 65.

Lakewood is a very diverse community as shown by the following race demographics as estimated in 2017.

White – 58%
Black or African American - 12.1%
American Indian and Alaska Native – 2.1%
Asian – 8.9%
Native Hawaiian and Other Pacific Islander – 3.1%
Some other race – 5.5%
Two or more races 10.3%

Fifteen percent of Lakewood residents are foreign born and the Clover Park School District reported that 33.7% of students enrolled in school are of Hispanic or Latino descent. The City of Lakewood has a higher poverty rate than other nearby communities at 19.7%, with a staggering 72% of students in the Clover Park School District receiving free or reduced lunch. The vulnerability of this population is higher than that of the rest of the city.

Infrastructure Summary

General

Table 2-5 Parcel Summary⁸

Jurisdiction	# Parcels	Land Value	Average Land Value	Improved Value	Average Improved Value
City of Lakewood	17,583	\$373,441,800	\$218,492	\$5,299,939,100	\$301,424
Region 5	328,831	\$55,032,560,799	\$167,358	\$82,766,510,038	\$251,699

Jurisdiction	Total Assessed Value	Average Assessed Value
City of Lakewood	\$9,141,685,400	\$519,916
Region 5	\$137,799,070,837	\$419,057

Table 2-6 Housing Summary⁹

Jurisdiction	# Houses	Housing Density	Avg Year Built	Avg Year Built (%)
City of Lakewood	26,548	1,540	<ul style="list-style-type: none"> • <1939: 721 • 1940 – 1979: 17,065 • 1980 – 2004: 8,305 • 2005>: 505 	<ul style="list-style-type: none"> • <1939: 2.7% • 1940 – 1979: 64.1% • 1980 – 2004: 31.2% • 2005>: 3.9%
Region 5	291,983	162	<ul style="list-style-type: none"> • <1939: 34,368 • 1940 – 1979: 126,363 • 1980 – 2004: 139,894 • 2005>: 22,830 	<ul style="list-style-type: none"> • <1939: 10.6% • 1940 – 1979: 39% • 1980 – 2004: 43.2% • 2005>: 7.1%

Jurisdiction Infrastructure

The following table shows the overview of infrastructure owned by the City of Lakewood. The infrastructure is categorized according to the infrastructure sectors as designated by the Department of Homeland Security. This chart is intended as a summary only.

For further details on Department of Homeland Security infrastructure sectors, please see the Process Section 1.

Table 2-7 City: Owned Infrastructure¹⁰

Total Infrastructure	Emerg. Services	Tele-comm	Transportation	Water	Energy	Government	Commercial	Total Value (\$)
4	0	0	0	0	0	4	0	\$27,398,600

Economic Summary

Table 2-8 Fiscal Summary¹¹

Jurisdiction	Operating Costs (per month)	Operating Budgeted Revenues¹²	Operating Budgeted Expenditures¹³	Fund Balance as % of Operating Cost	Avg Fund Balance (5 yrs)
City of Lakewood	\$3,206,000	\$41,764,000	\$38,468,000	21.1%	\$7,274,000

Table 2-9 Employment Profile¹⁴

Employment Category (SIC)	City of Lakewood	Pierce County
Construction/Res	1,678	24,297
Finance, Insurance, and Real Estate (FIRE)	1,193	13,640
Manufacturing	1,067	17,239
Retail	3,067	34,957
Services	11,407	132,801
Warehousing, Transportation, and Utilities (WTU)	2,086	30,278
Government	4,870	34,915
Education	2,822	24,382

Table 2-10 Economic Summary¹⁵

Jurisdiction	Unemployment Rate
City of Lakewood	13.1%
Region 5	9.6%
WA State	8.4%

Table 6-11 North American Industry Classification System (NAICS)Table Key

2-DIGIT NAICS RATING	NAICS CATEGORIES
11	Agriculture, Forestry, Fishing and Hunting
21	Mining
22	Utilities
23	Construction
31-33	Manufacturing
42	Wholesale Trade
44-45	Retail Trade
48-49	Transportation and Warehousing
51	Information
52	Finance and Insurance
23	Real Estate and Rental and Leasing
54	Professional, Scientific and Technical Services
55	Management of Companies and Enterprises
56	Administrative and Support and Waste Management and Remediation Services
61	Educational Services
62	Health Care and Social Assistance
71	Arts, Entertainment and Recreation
72	Accommodation and Food Services
81	Other Services (except Public Administration)

Table 6-12 North American Industry Classification System (SIC)Table Key

Major sector categories combine NAICS categories as follows

SECTOR INDUSTRY COMBINED (SIC)
Construction and Resources (Const/Res): 11,21,23
Finance, Insurance and Real Estate (FIRE): 52,53
Manufacturing: 31-33
Wholesale Trade
Services: 51, 54-56, 61 (private-sector portion), 62,71,72,81
Wholesale Trade, Transportation, and Utilities (WTU): 22, 42, 48,
Government: Public-sector employment, excluding education
Education: 61 (public-sector portion)

Resource Directory

Regional

- City of Lakewood
<http://www.cityoflakewood.us/>
- Pierce County Government
<https://co.pierce.wa.us/>
- Pierce County DEM
<https://www.co.pierce.wa.us/104/Emergency-Management>
- Pierce County Planning & Public Works
<https://www.co.pierce.wa.us/4999/Planning-Public-Works>
- Municipal Research & Services Center of Washington (MRSC)
<http://www.mrsc.org/>

National

- US Census
www.census.gov/

Endnotes

¹ Information from a survey completed by the City.

² Information from Pierce County GIS application, CountyView Pro (2020).

³ “Population” from Census 2010, Office of Financial Management. It should be noted that current (as of April 2020) population of City of Lakewood is reported by the Office of Financial Management as 60,030. Once 2020 Census becomes available, PC DEM will rerun population data using Pierce County GIS application, County View Pro.

⁴ “Projected Population Change (%)” from Pierce County Buildable Lands Report, Dec. 2007.

⁵ “Projected Population Density” is based on an assumption of the jurisdiction maintaining the same geographic area and boundaries. It does not consider changes in annexation, district mergers, etc.

⁶ “Projected 2022 Population” from Pierce County Buildable Lands Report, Dec. 2007.

⁷ “Special Population” from Census 2010 block level, Office of Financial Management. Once 2020 Census becomes available PC DEM will rerun population data using Pierce County GIS application, County View Pro.

⁸ Information from Pierce County GIS application, CountyView Pro 2020.

⁹ Information from Office of Financial Management 2020.

¹⁰ Information provided by the City of Lakewood based from the Department of Homeland Security (DHS) Infrastructure sectors.

¹¹ Information provided by City of Lakewood.

¹² Information provided by City of Lakewood, non-capital.

¹³ Information provided by City of Lakewood, non-capital.

¹⁴ Information from Puget Sound Regional Council based on 2018 data. <https://www.psrc.org/covered-employment-estimates>.

¹⁵ Information from Census 2010, Office of Financial Management, 2020.

Section 3

Capability Identification Requirements

Planning Process---Requirement §201.6(b):

An open public involvement process is essential to the development of an effective plan.

Documentation of the Planning Process---Requirements §201.6(b):

In order to develop a more comprehensive approach to reducing the effects of natural disasters, the planning process **shall** include:

(3) Review and incorporation, if appropriate, of existing plans, studies, reports, and technical information.

- Does the planning process describe the review and incorporation, if appropriate, of existing plans, studies, reports, and technical information?

Assessing Vulnerability: Analyzing Development Trends---Requirement §201.6(c)(2) (ii)(C):

[The plan **should** describe vulnerability in terms of] providing a general description of land uses and development trends within the community so that mitigation options can be considered in future land use decisions.]

- Does the plan describe land uses and development trends?

Identification and Analysis of Mitigation Actions: National Flood Insurance Program (NFIP) Compliance--Requirement §201.6(c)(3)(ii):

[The mitigation strategy] must also address the jurisdiction's participation in the National Flood Insurance Program (NFIP), and continued compliance with NFIP requirements, as appropriate.

- Does the new or updated plan describe the jurisdiction(s) participation in the NFIP?

SECTION 3

REGION 5 HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD CAPABILITY IDENTIFICATION SECTION

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Legal and Regulatory

Table 3-1 Legal and Regulatory

Regulatory Tools (Ordinances and Codes)	Yes or No
<u>Jurisdiction Capabilities</u>	
Building Construction/Design Construction Codes	Yes
Flood Damage Prevention Ordinance	Yes
Growth Management Ordinance	Yes
Critical Area Ordinance	Yes
Hazard Setback Requirements	Yes
Hillside and Steep Slope Ordinance	Yes
Land Use and Regulatory Codes	Yes
Mechanical Codes	Yes
Plan Review Requirements	Yes
Plumbing Codes	Yes
Real Estate Disclosure Requirements	No
Storm Water Management	Yes
Subdivision Ordinance or Regulations	Yes
Tax and License Codes	Yes
Wildfire Ordinance	No
Zoning Ordinance	Yes

Administrative Capability

Table 3-2 Administrative Capability

Administrative Tools (Agency, Departments or Programs)	Yes or No
<u>Jurisdiction Capabilities</u>	
Architectural Review Board/Historic Review	Yes
Board of Adjustments/Hearing Examiner	Yes
Building Official	Yes
Chamber of Commerce	Yes
City/Town Council	Yes
City/Town Meetings	Yes
City/Town Planning Commission	Yes
City/Town Website	Yes
Commercial Fire Safety/Code Inspection Program	WPFR
Community CPR/First Aid Program	No
Community Emergency Response Teams (or PCNET)	Yes
Downtown Revitalization Committee	Yes
Economic Development Board	Yes
Emergency Manager	Yes
Engineers	Yes
Families First Coalition	No
Fire and Injury Prevention Program	WPFR
Fire Chief	WPFR
Fire Safety & Disaster Classes in Schools	WPFR
Flood Plan Manager	No
Government TV Access	No
Grant Writers	Yes
Home Safety Council	Yes
Information included in Utility Bills	Yes
Lahar Warning System	Yes
Planners	Yes
Planning Commission	Yes
Police Chief	Yes
Police Department	Yes
Public Utility	Yes
Public Works Department	Yes
Safe Streets Program	No
Safety Fairs	Yes
Stream Team	No
Surveyors	No

Table 3-3 Administrative Capability (Cont)

Administrative Tools (Agency, Departments or Programs)	Yes or No
<u>Regional Capabilities</u>	
Local Business Districts	Yes
Local Department of Emergency Management	Yes
Local Fire Agencies plus Mutual Aid with others	WPFR
Local Hospitals	Yes
Local Law Enforcement Agencies and Mutual Aid with others	Yes
Local Neighborhood Associations	Yes
Local Newspapers	Yes
Local Parks Commission/Board	Yes
Local Power Companies	Yes
Local Parent Teacher's Association	Yes
Neighboring Counties	Yes
Pierce County Department of Emergency Management	Yes
Pierce County Fire Chiefs Association	Yes
Pierce County Neighborhood Emergency Teams (PCNET)	Yes
Pierce County Police Chiefs Association	Yes
Pierce County Safe Kids Coalition	WPFR
Pierce County Sheriffs Department	Yes
Puget Sound Clean Air Agency	Yes
Puget Sound Energy	Yes
Puget Sound Regional Council	Yes
Puget Sound Water Quality Management Plan	Yes
Service Organizations	Yes
Tacoma/Pierce County Health Department	Yes
Tribes	Yes

Technical Capability

Table 3-4 Technical Tools

Technical Tools (Plans and Other)	Yes or No
Jurisdiction Capabilities	
After Action Reports of Any Incident	Yes
Capital Improvement Plan	Yes
Comprehensive Emergency Management Plan	Yes
Comprehensive Plan	Yes
Continuity of Governmental Services and Operations Plan (COOP and COG)	Yes
Critical Facilities Plan	No
Drainage Master Plan	No
Economic Development Plan	Yes
Emergency Evacuation Plan	No
Emergency Response Plan	Yes
Generator Placement Plan	In process
Habitat Plan	No
Hazardous Materials Response Plan	WPFR
Lahar Evacuation Plan	No
Pandemic Flu Plan	Yes
Post-Disaster Recovery Plan	Yes
Sewer/Wastewater Comprehensive Plan (Pierce Co. PW)	Yes
Storm Comprehensive Plan	Yes
Water Comprehensive Plan (Lakewood Water District)	Yes
Regional Capabilities	
Coordinated Water System Plan and Regional Supplement 2001	Yes
Local and Regional Emergency Exercises – All Types	Yes

Fiscal Capability

Table 3-5 Fiscal Capability

Fiscal Tools (Taxes, Bonds, Fees, and Funds)	Yes or No
<u>Jurisdiction Capabilities</u>	
TAXES:	
Authority to Levy Taxes	Yes
BONDS:	
Authority to Issue Bonds	Yes
FEES:	
Fees for Water, Sewer, Gas or Electric Service	No
Impact Fees for Homebuyers/Developers for New Developments/Homes	Yes
Local Improvement District (LID)	Yes
FUNDS:	
Capital Improvement Project Funds	Yes
Enterprise Funds	No
General Government Fund (Departments)	Yes
Internal Service Funds	Yes
Special Revenue Funds	Yes
Withhold Spending in Hazard-Prone Areas	N/A
<u>Regional Capabilities</u>	
Pierce County Land Conservancy	Yes
Cascade Land Conservancy	No

Specific Capabilities

Table 3-6 Specific Capabilities

Jurisdiction Specific Capabilities	
<u>Legal & Regulatory</u>	
<u>Administrative & Technical</u>	
<u>Fiscal</u>	

National Flood Insurance Program (NFIP) Capability

NFIP Status

The City entered the NFIP on October 25, 1977. The City is in good standing in the NFIP as certified by Washington State Department of Ecology. The last Community Assistance Visit (CAV) was completed in August of 2007.

Section 4

Risk Assessment Requirements

Identifying Hazards--- Requirement §201.6(c)(2)(i):

[The risk assessment **shall** include a] description of the type ... of all natural hazards that can affect the jurisdiction.

- Does the new or updated plan include a description of the types of all natural hazards that affect the jurisdiction?

Profiling Hazards---Requirement §201.6(c)(2)(i):

[The risk assessment **shall** include a] description of the ... location and extent of all natural hazards that can affect the jurisdiction. The plan **shall** include information on previous occurrences of hazard events and on the probability of future hazard events.

- Does the risk assessment identify (i.e., geographic area affected) of each hazard being addressed in the new or updated plan?
- Does the risk assessment identify the extent (i.e., magnitude or severity) of each hazard addressed in the new or updated plan?
- Does the plan provide information on previous occurrences of each hazard addressed in the new or updated plan?
- Does the plan include the probability of future events (i.e., chance of occurrence) for each hazard addressed in the new or updated plan?

Assessing Vulnerability: Overview---Requirement §201.6(c)(2) (ii):

[The risk assessment **shall** include a] description of the jurisdiction's vulnerability to the hazards described in paragraph (c)(2)(i) of this section. This description **shall** include an overall summary of each hazard and its impact on the community.

- Does the new or updated plan include an overall summary description of the jurisdiction's vulnerability to each hazard?
- Does the new or updated plan address the impacts of each hazard on the jurisdiction?

Assessing Vulnerability: Addressing Repetitive Loss Properties---Requirement §201.6(c)(2) (ii):

[The risk assessment] **must** also address the National Flood Insurance Program (NFIP) insured structures that have been repetitively damaged by floods.

- Does the new or updated plan describe vulnerability in terms of the types and numbers of repetitive loss properties located in the identified hazard areas?

Assessing Vulnerability: Identifying Structures---Requirement §201.6(c)(2) (ii)(A):

The plan **should** describe vulnerability in terms of the types and numbers of existing and future buildings, infrastructure, and critical facilities located in the identified hazard areas...

- Does the new or updated plan describe vulnerability in terms of the types and numbers of existing buildings, infrastructure, and critical facilities located in the identified hazard areas?
- Does the new or updated plan describe vulnerability in terms of the types and numbers of future buildings, infrastructure, and critical facilities located in the identified hazard areas?

Assessing Vulnerability: Estimating Potential Losses---Requirement §201.6(c)(2) (ii)(B):

[The plan **should** describe vulnerability in terms of an] estimate of the potential dollar losses to vulnerable structures identified in paragraph (c)(2)(ii)(A) of this section and a description of the methodology used to prepare the estimate...

- Does the new or updated plan estimate potential dollar losses for vulnerable structures?
- Does the new or updated plan describe the methodology used to prepare the estimate?

Assessing Vulnerability: Analyzing Development Trends---Requirement §201.6(c)(2) (ii)(c):

[The plan **should** describe vulnerability in terms of] providing a general description of land uses and development trends within the community so that mitigation options can be considered in future land use decisions.

- Does the new or updated plan describe land uses and development trends?

SECTION 4

REGION 5 ALL HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD RISK ASSESSMENT SECTION

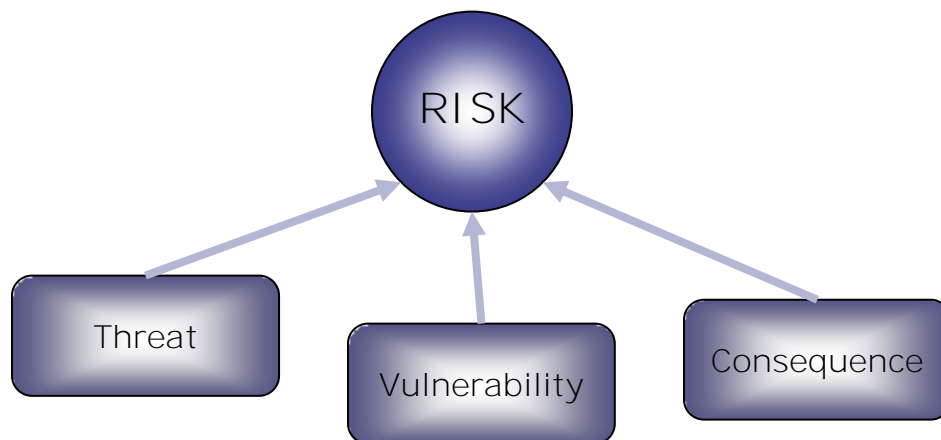
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Section Overview

The Risk Assessment portrays the threats of natural hazards, the vulnerabilities of a jurisdiction to the hazards, and the consequences of hazards impacting communities. Each hazard is addressed as a threat and is identified and profiled in the Hazard Identification. The vulnerabilities to and consequences of a given hazard are addressed in the Vulnerability Analysis. Vulnerability is analyzed in terms of exposure of both population and infrastructure to each hazard. Consequences are identified as anticipated, predicted, or documented impacts caused by a given hazard when considering the vulnerability analysis and the characteristics of the hazard as outlined in its identification.



The WA Region 5 **Hazard Identification** was used for this plan. Each jurisdiction's Vulnerability and Consequence Analysis are based on the Region 5 Hazard Identification. The Region 5 Hazard Identification can be found in the Base Plan. Each hazard is identified in subsections. The subsections are grouped by hazard-type (i.e., geological and meteorological hazards) and then alphabetically within each type. A summary table of the WA Region 5 Hazard Identification is included in this section as Table 4-1a and Table 4-1b.

The **Vulnerability Analysis** is displayed in six tables:

- Table 4-2 General Exposure
- Table 4-3 Population Exposure
- Table 4-4 General Infrastructure Exposure
- Table 4-5a Consequence Analysis Chart – Geological
- Table 4-5b Consequence Analysis Chart – Meteorological
- Table 4-5c Consequence Analysis Chart – Technological

Each jurisdiction has its own Vulnerability Analysis, and it is included in this section.

The **Consequence Identification** is organized by Threat. Each threat page summarizes the hazard, graphically illustrates exposures from the Vulnerability Analysis, and lists corresponding Consequences. Each jurisdiction has its own Consequence Identification and it is included in this section: avalanche, earthquake, landslide, tsunami, volcanic, drought, flood, severe weather, and wildland/urban interface fire.

Specific information and analysis of a jurisdiction's owned (public) infrastructure is addressed in the Infrastructure Section of its Plan.

Vulnerability and Hazard Impact Analysis

Identified Hazards Affecting the City of Lakewood

Through the Mitigation, Hazard Identification Risk Assessment (HIRA) and Comprehensive Emergency Management Plan (CEMP) planning processes, the City of Lakewood has identified seven major natural hazards and six technological hazards that may significantly affect the fire district. These hazards were chosen based on multiple criteria including high frequency and potential impact.

Geological:

- Earthquake
- Landslide
- Tsunami/Seiche
- Volcanic Eruption

Meteorological:

- Drought/WUI
- Flood
- Severe Weather

Technological / Human-caused

- Civil Disturbance
- Energy Emergency
- Epidemic/Pandemic
- Hazardous Materials
- Terrorism / Active Threat / Cyber Attack
- Transportation Incident

Introduction

- The National Threat and Hazard Identification and Risk Assessment (THIRA)¹ published by FEMA July 2019 provides scenarios and introduced an important concept: Plausible Concurrent Operations. Plausible Concurrent Operations represents ongoing response and recovery operations and are a way of recognizing that multiple events that have no connection to one another may overlap and impact an already vulnerable jurisdiction. An example of Plausible Concurrent Operations would be responding and recovering from a pandemic outbreak and then a month later there is an earthquake. Secondary hazards such as a tsunami, landslides, fires, liquefaction, hazardous material spills, energy emergency following an earthquake are incorporated with their primary hazard and are not listed as separate concurrent operations.

- May of 2020, we are several months into the COVID-19 pandemic outbreak as we are writing this analysis. The probability of a concurrent incident for all jurisdictions is extremely high during this COVID-19 outbreak.
- The concurrent scenarios/hazards of most concern include:
 - epidemic/pandemic
 - earthquake (includes tsunami, liquefaction, landslides, fire, hazardous material spills, energy emergency)
 - a lahar
 - severe weather
 - terrorism and active threat / attack tactics
- Although not listed above civil disturbance can have a high consequence when paired with epidemic/pandemic.

The vulnerability and impact analysis incorporated many findings from other assessments for a holistic approach. The assessments and key findings are briefly captured here but additional details can be found in the Base Plan Section 4: Pierce County Hazard Identification & Risk Assessment.

Geological

Earthquake

The Pierce County Region averages at least one earthquake every ten years. More recently they have become more frequent. There are three distinct earthquake threats in our region. Deep earthquakes, like the 2001 Nisqually Earthquake, which was a magnitude 6.8; earthquakes on the Seattle or Tacoma Faults that could have a magnitude up to 8.0; and the Cascadia Subduction Zone located off the Washington Coast that could have a magnitude 9.0 or higher earthquake. Any of these types of earthquakes could cause millions, if not billions of dollars in damage within the region.

Landslide

There are many areas throughout the City of Lakewood where slopes exceed 15% and the glacial till is overlain by well-drained soils. When these hillsides become wet, it is possible for the slope to fail. These areas are also at an increased risk of damage resulting from an earthquake or slope settlement. The largest steep slope hazard area lies along Chambers Creek, affecting both Lakewood and University Place.

Tsunami / Seiche

Pierce County has been impacted by three tsunamis generated in the Puget Sound in the past 120 years. The largest of these, the 1894 tsunami, originated in Commencement Bay and destroyed 300 feet of dock and sent a ten-foot wave into Old Town Tacoma. With eight miles of Puget Sound waterfront acting as the western border of the District, there is a risk of either a tsunami or seiche. A seiche is a standing wave in an enclosed or partially enclosed body of water. The phenomenon has been observed on lakes, reservoirs, swimming pools and bays.

Volcanic

While the City of Lakewood will not likely be directly impacted by a volcanic event, it will likely be an area of refuge from citizens throughout Pierce County whose homes would be affected if Mount Rainier erupted. Another risk is that of isolation. Many of the roads utilized for commerce and travel to work may be impacted in the event of a volcanic eruption.

Meteorological

Drought and WUI

The Puget Sound region is susceptible to a Wildland Urban Interface (WUI) fire event. While individual communities have different exposures, the development of extended dry conditions in the summer months leads to increased risk. The causes of WUI fires range from weather related events to human caused.

Flood

There are very few areas identified in the City of Lakewood as being at risk for floods. The areas at risk do not flood often, but during periods of heavy rain it is possible. The areas identified as being at risk of flood are the banks of Chambers Creek at the border of Lakewood and University Place, and Flett and Clover Creeks in Lakewood.

Severe Storm

The City of Lakewood enjoys a very mild climate. July is on average the warmest month, with the highest recorded temperature of over 100 degrees in 2009. The summer of 2015 was one of the hottest and driest on record with more than 10 days over 90 degrees. On average, December is the coolest month with the lowest recorded temperature being five degrees in 1985. The average yearly rainfall is 39 inches and the average snowfall is eight inches. The number of days with measurable precipitation on average each year is 146, with the remaining days varying from sunny, to partly cloudy, to overcast. Severe weather is a risk to the community. Windstorms, hail, snow, and ice storms have all impacted the City of Lakewood in the past. While tornadoes have not occurred in our jurisdiction, they have taken place recently in areas nearby.

Technological

Civil Disturbance

Civil disturbances are the result of groups or individuals feeling their needs or rights are being infringed upon, either by society at large, a segment thereof, or the current overriding political system. When this results in community disruption where intervention is required to maintain public safety it has become a civil disturbance. Civil disturbance spans a wide variety of actions and includes, but is not limited to; labor unrest, strikes, civil disobedience, demonstrations, riots or rebellion. Triggers could include; racial tension, immigration status, religious conflict, unemployment, a decrease in normally accepted goods or services such as water, food, or gas shortages, or unpopular political actions. There has never been an issue with civil disturbances

within the City of Lakewood's borders, but they have occurred in the County. Civil disturbances can affect the region's economic vitality should businesses be forced to close or highways and other infrastructure severely impacted.

Cyber Attack

- Cyber Critical Infrastructure CyberSecurity Consultants provides services to many in Pierce County including South Sound 9-1-1, Pierce County Radio Communications, Washington State Patrol, and many other local agencies. In 2019, the top threats were:
 1. Phishing/spearphishing.
 2. Ransomware.
 3. Distributed Denial-of-Service (DDoS) attack.
 4. Advance Persistent Threat.
 5. Fileless Malware.
- The top vectors or common access points were:
 1. Email attachments/links.
 2. Drive-by or download.
 3. Webserver/Web app.
 4. USB drives.
- During times of disaster there is a significant increase in scams, phishing attacks, and Advance Persistent Threat attacks to gain access to financial and cyber systems.
- In COVID-19 there was a significant increase in attacks against medical centers and collaboration platforms.

Energy Emergency

A utility emergency may involve one or more of the following; natural gas, heating oil, gasoline, coal, electricity or water. These types of emergencies can create a great risk to first responders. They must mitigate the hazards so neither they, nor the public are injured. No matter what type of utility is involved, when a lack of resources disrupts business and the day-to-day lives of citizens, it can become an emergency. This is especially true during periods of inclement weather or other types of disasters.

Epidemic/Pandemic

The Tacoma Pierce County Health Department informs local healthcare providers, including the EMS Division of the City of Lakewood, when there is some sort of communicable health risk in the region. The main health risk in recent years has been influenza. Many diseases that Americans have not worried about for years, if not decades have returned (such as measles). It is important for the City of Lakewood to heed these advisories to protect the health of first responders and the community alike. The impacts of epidemics/pandemics could include loss of life or short- or long-term debilitation of victims. It could create an economic impact due to missed work, which affects not only the employee, but the employer as well. A serious epidemic or pandemic would likely cause a strain on the current public health and medical resources, of which the City of Lakewood is a part. The City of Lakewood supports the local health

community in preventing an epidemic by offering necessary immunizations to employees and their families.

Hazardous Materials

There are several facilities in the fire district known to store and/or utilize hazardous materials. Assessing the known locations of hazardous materials and the routes those materials travel allows for preplanning and provides an overview of the level of risk from a hazardous materials event. There are approximately 30 facilities in the City of Lakewood required to complete Tier II reporting. Tier II reports are forms that organizations and businesses throughout the United States with hazardous chemicals above certain quantities, are required by the EPA to complete. Most of these facilities are in the commercial core of Lakewood. In 2018, a large hazardous materials facility opened its doors in Woodbrook. Another hazardous material that should be recognized is meth labs. Several have been found in wooded areas and are cleaned-up by Washington Department of Ecology.

Terrorism / Active Threat / Attack Tactics

Attacks can be perpetrated by many different actors with different motivations, such as terrorists, violent extremists, and targeted violent offenders. All use violent tactics to harm people and/or property. The consequences of the attack depend on the tactics employed by the threat actor, such as active shooter(s) events, bombings, arson, murder, vehicle ramming, kidnapping and hostage-taking, maritime attack, and hijacking or skyjacking. The threat of terrorism and violent extremism has grown with the interconnectedness afforded by the internet. Terrorist organizations can reach anyone around the world to support or participate in attacks. It remains nearly impossible to predict violent attacks, but security and intervention measures are continuing to evolve with the use of new tactics employed by threat actors. The city could be vulnerable to such attacks.

Transportation Incident

Interstate 5 runs through the City of Lakewood. The average daily vehicle count is over 120,000 vehicles per day with approximately 10 percent of these being large trucks. Of the large trucks on Interstate 5, approximately 3 percent transport a hazardous material and an additional 3 percent transport “super heavy” loads. West Pierce Fire & Rescue and the City of Lakewood responds on average to one collision per week involving a “heavy truck” on Interstate 5. As the population in our area continues to increase this will only worsen. Rail transit is also a risk in our community. There were two passenger train derailments in July and December of 2017 and there have been several freight train derailments in Pierce County over the past 10 years. There is also the potential of an aircraft incident as the runway for the airfield at Joint Base Lewis McChord borders the city. The Pierce County Ferry system is another transportation risk in the City of Lakewood.

Changes in Development

The City of Lakewood identified strategic business locations and residential neighborhoods that were ready for redevelopment. Targeted locations for revitalization in Lakewood included

Pacific Highway, the International District, and the neighborhoods of Woodbrook, Springbrook, Kendrick, Lakeview and Lake City. In addition, the Lakewood Downtown subarea plan, will replace the existing auto-oriented development and replace it with higher quality, denser urban redevelopment to make the city's commercial core accessible and inviting to pedestrians.

The City of Lakewood has moved toward higher density mixed use developments and will continue to do so for the foreseeable future. Due to the changes in development, the population of the city has increased over the last five years according to the US Census estimates. As development continues and population continues to grow in Lakewood, the risk of identified hazards impacting the population increases. Lakewood continues to monitor the infrastructure, incorporate elements of the mitigation plan into the existing plans, ordinances and programs to dictate land uses and educate the public of hazards through community outreach. Section 5, Mitigation Strategies identifies measures to build the resiliency within the City and have been reviewed and reprioritized to reflect the changes in development trends.

Table 4-1a WA Region 5 Hazard Identification Summary – Geological

HAZARD		DECLARATION # DATE/PLACE	PROBABILITY/ RECURRENCE	MAPS, FIGURES AND TABLES
<u>Geological</u>	<u>AVALANCHE</u>	Not Applicable	Yearly in the mountainous areas of the County including Mt. Rainier National Park and the Cascades.	Slab Avalanche Areas Vulnerable to Avalanche Pierce County Avalanches of Record
	<u>EARTHQUAKE</u>	N/A--7/22/2001 Nisqually Delta N/A--6/10/2001 Satsop DR-1361-WA--2/2001 Nisqually N/A--7/2/1999 Satsop DR-196-WA--4/29/1965 Maury Island, South Puget Sound N/A--4/13/1949 South Puget Sound N/A--2/14/1946 Maury Island	40 years or less occurrence Historical record—about every 23 years for intraplate earthquakes.	Types of Earthquakes Major Faults in the Puget Sound Basin Seattle and Tacoma Fault Segments Pierce County Seismic Hazard Major Pacific Northwest Earthquakes Notable Earthquakes Felt in Pierce County Salmon Beach, Tacoma Washington following Feb 2001 Earthquake Liquefaction Niigata Japan-1964 Lateral Spreading – March 2001
	<u>LANDSLIDE</u>	DR-1671-WA--2006 DR-1361-WA--2001 DR-1159-WA--12/96-2/1997 DR-852-WA--1/1990 DR-545-WA--12/1977 State proclamations: 20-02 – 01/20/2020 17-08 –05/18/2017 SR 410	Slides with minor impact (damage to five or less developed properties or \$1,000,000 or less damage) 10 years or less. Slides with significant impact (damage to six or more developed properties or \$1,000,000 or greater damage) 100 years or less.	Northeast Tacoma Landslide January 2007 Pierce County Landslide Deposits, Scarps and Flanks, and Susceptibility Landslide Facts for Pierce County – Shallow Landslide Susceptibility Pierce County Deep Landslide Hazard Area Pierce County Shallow Landslide Hazard Area Pierce County Slope Stability Areas Pierce County Comparison of Landslide Susceptible Areas Notable Landslides in Pierce County Ski Park Road – Landslide January 2003 SR-165 Bridge Along Carbon River – Landslide February 1996 Aldercrest Drive – Landslide
	<u>TSUNAMI</u>	N/A--A.D. 900 Seattle Fault EQ Sourced Tsunami N/A--1894 Puyallup River Delta N/A--1949 Tacoma Narrows	Due to the limited historic record, until further research can provide a better estimate a recurrence rate of plus or minus 100-200 years will be used.	Hawaii 1957 – Residents Explore Ocean Floor Before Tsunami Hawaii 1949 – Wave Overtakes a Seawall Tsunamis in Washington State Tsunami Inundation and Current Based on Earthquake Scenario Notable Tsunamis in Pierce County Salmon Beach, Pierce County 1949 – Tsunamigenic Subaerial Landslide Salmon Beach, Pierce County 1949 – Tsunamigenic Subaerial Landslide Damage in Tacoma from 1894 Tsunami
	<u>VOLCANIC</u>	DR-623-WA--5/1980	The recurrence rate for either a major lahar (Case I or Case II) or a major tephra eruption is 500 to 1000 years. The recurrence rate for either a major lahar (Case I or Case II) or a major tephra eruption is 500 to 1000 years.	Volcano Hazards Tephra Types and Sizes Lahars, Lava Flows and Pyroclastic Hazards of Mt. Rainier Estimated Lahar Travel Times for Lahars 10 ⁷ to 10 ⁸ Cubic Meters in Volume Pierce County Eruptive Events and Lahars

Table 4-1b WA Region 5 Hazard Identification Summary – Meteorological

HAZARD		DECLARATION # DATE/PLACE	PROBABILITY/ RECURRENCE	MAPS, FIGURES AND TABLES
<u>Meteorological</u>	<u>CLIMATE CHANGE</u>	Not Applicable	Not Applicable	IPCC Models on Global Temperature Change: 1900 to 2100 Recent and Projected Temperatures for the Pacific Northwest Puget Sound Projected Warming Puget Sound Projected Precipitation Change Projected Decline in Snowpack Projected Sea Level Risk – Tacoma Sea Level Rise Inundation Area in 2100 Tacoma Tideflats Climate Impacts and Natural Hazards Comparison of the South Cascade Glacier: 1928 to 2003 Lower Nisqually Glacier Retreat: 1912 to 2001
	<u>DROUGHT</u>	Many dry seasons but no declarations State proclamations: 18-05--7/31/2018	50 years or less occurrence	Sequence of Drought Impacts Palmer Drought Severity Index Pierce County Watersheds %Area of Basin in Drought Conditions Since 1895 %Time in Severe to Extreme Drought: 1895-2004 %Time in Severe to Extreme Drought: 1985-1995 Notable Droughts Affecting Pierce County Columbia River Basin USDA Climate Zones – Washington State
	<u>FLOOD</u>	DR-WA 1817--01/2009 DR-1734-WA--12/2007 DR-1671-WA--11/2006 DR-1499-WA--10/2003 DR-1159-WA--12/96-2/97 DR-1100-WA--1-2/1996 DR-1079-WA--11-12/1995 DR-896-WA--12/1990 DR-883-WA--11/1990 DR-852-WA--1/1990 DR-784-WA--11/1986 DR-545-WA--12/1977 DR-492-WA--12/1975 DR-328-WA--2/1972 DR-185-WA--12/1964	5 years or less occurrence Best available science--the frequency of the repetitive loss claims indicates there is approximately a 33 percent chance of flooding occurring each year.	Lower Puyallup River Historical Flooding in Lower Puyallup River Levees and Revetments in the Lower Puyallup River Summary of Damages to Lower Puyallup River Facilities Middle Puyallup River Historical Flooding in Middle Puyallup River Levees and Revetments in the Middle Puyallup River Summary of Damages to Lower Middle River Facilities Upper Puyallup River Historical Flooding in Upper Puyallup River Levees and Revetments in the Upper Puyallup River Summary of Damages to Upper Puyallup River Facilities Lower White River Historical Flooding in Lower White River Levees and Revetments in the Lower White River Summary of Damages to Lower White River Facilities Upper White River Historical Flooding in Upper White River Levees and Revetments in the Upper White River Summary of Damages to Upper White River Facilities Greenwater River Historical Flooding in Greenwater River Carbon River Historical Flooding in Carbon River South Prairie Creek Historical Flooding in South Prairie Creek Middle Nisqually River

<u>Meteorological</u>				Historical Flooding in Middle Nisqually River Upper Nisqually River Historical Flooding in Upper Nisqually River Levees and Revetments in the Upper Nisqually River Summary of Damages to Upper Nisqually River Facilities Mashel River Historical Flooding in Mashel River Nov 2006 Flooding River Park Estates – Along Puyallup River
	<u>SEVERE WEATHER</u>	DR-4056-WA – 01/2012 DR-1825- WA – 12/2008 – 01/2009 DR-1682-WA--12/2006 DR-1159-WA--12/96-2/1997 DR-1152-WA--11/19/1996 DR-981-WA--1/1993 Inauguration Day Storm DR-137-WA--10/1962 Columbus Day Storm State proclamations: 19-06--02/15/2019 (Dec. 2018 Winter Storm) 19-05--02/14/2019 Winter Storm Maya 17-08--5/18/2017 Severe rain 17-03--3/14/2017 17-02--1/19/2017 Winter Storm 15-18--12/24/2015 Windstorms and Flooding	The recurrence rate for all types of severe storms is 5 years or less.	Fujita Tornado Damage Scale Windstorm Tracks Pierce County Severe Weather Wind Hazard – South Wind Event Pierce County Severe Weather Wind Hazard – Enumclaw East Wind Event Notable Severe Weather in Pierce County Snowstorm January 2004 Downtown Tacoma Satellite Image – Hanukkah Eve Windstorm Before/After Tornado Damage Greensburg KS May 2007 County Road December 2006 Windstorm Tacoma Narrows Bridge – November 1940 Windstorm
	<u>WUI FIRE</u>	EM-3372-WA Aug-Sept. 2015 State proclamations: 17-12--9/2/2017 Norse Peak Fire 15-11--6/26/2015	Based on information from WA DNR the probability of recurrence for WUI fire hazard to Pierce County is 5 years or less.	Washington State Fire Hazard Map Pierce County Forest Canopy Industrial Fire Precaution Level Shutdown Zones Carbon Copy Fire August 2006 Washington State DNR Wildland Fire Statistics: 1973-2007 DNR Wildland Response South Puget Sound Region: 2002-2007 Pierce County DNR Fires

Table 4-2c WA Region 5 Hazard Identification Summary – Technological

HAZARD		DECLARATION # DATE/PLACE	PROBABILITY/ RECURRENCE	MAPS, FIGURES AND TABLES
Technological	<u>ABANDONED MINES</u>	Not Applicable	Based on information from WA DNR. The Pierce County Sheriff's Department reports that they have had very few incidents of citizens entering the abandoned mines in east Pierce Co. Isolated issues of minor subsidence have occurred, typically following flood events i.e. 2009/2010.	Pierce County – Mine Hazard Areas Map Based on WA DNR Information Schasse, Koler, Eberle, and Christie, <u>The Washington State Coal Mine Map Collection: A Catalog, Index, and User's Guide</u> , Open File Report 94-7, June 1984 Pierce County 2014 HIRA
	<u>CIVIL DISTURBANCE</u>	Not Applicable	In the past 150 + years there have been eleven major incidents giving a recurrence rate of every seven years.	Pierce County Civil Disturbance High Probability Locations Map Pierce County Civil Disturbance High Probability Locations Zoomed In Map
	<u>DAM FAILURE</u>	Not Applicable	No occurrences in Pierce County 50+ years recurrence for WA State	Reasons for Dam Failures Nationally PC Dams that Pose a High or Significant Risk to the Public Pierce County High and Significant Risk Dams Dam Failures in WA State Mud Mt. Dam Intake
	<u>ENERGY EMERGENCY</u>	Not Applicable	Power outages are the most frequent energy incident, via natural hazards (storms, ice) Recurrence rate – every five years (storms) Recurrence rate – 50+ years (major)	Tacoma Power Outage 1929, USS Lexington provides power
	<u>EPIDEMIC / PANDEMIC</u>	EM-3507-WA 03/12/2020	Epidemic: <ul style="list-style-type: none"> • 1976-2014 Ebola outbreaks • Flu occurs annually Pandemics: <ul style="list-style-type: none"> • 2009-2010 "Swine Flu" recurrence rate – 20 years 	Individuals hoping to avoid contacting disease
	<u>HAZARDOUS MATERIALS</u>	Not Applicable	<ul style="list-style-type: none"> • Dalco Passage oil spill of October 13, 2004 • Chlorine Spill Port of Tacoma February 12, 2007 Large incidents five year recurrence Small incidents one week recurrence	List of constituents or ingredients found in Bakken crude oil Environmental Protection Agency's Identified Top Five Facilities Exxon Valdez Oil Spill, 1989 Pierce County Spill data from May 2018 to May 2019 Dalco Passage oil spill (October 13, 2004)
	<u>PIPELINE FAILURE</u>	Not Applicable	<ul style="list-style-type: none"> • Northwest Pipeline Corporation natural gas incident May 1st 2003, in Sumner 10 years recurrence	Cities and Towns with interstate pipelines within, or within 1 mile of city limits Olympic Pipeline Rupture 06/10/99 Pierce County Pipelines Whatcom Falls Park, 2003
	<u>TERRORISM ACTIVE THREAT CYBER ATTACK</u>	Not Applicable	Minor incident – recurrence 1-year Major Incident – recurrence 10 years	250 Active Shooter Incidents in the U.S. from 2000-2017: Incidents per year 250 Active Shooter Incidents in the U.S. from 2000-2017: Casualty Breakdown per year 250 Active Shooter Incidents in the U.S. from 2000-2017: Location

				Categories Occurrences in the Puget Sound
	<u>TRANSPORTATION ACCIDENT</u>	Not Applicable State proclamations: 17-13--12/18/2017 Amtrak derailment 15-05--4/16/2015 SR 410 Bridge 15-04--3/11/15 Damage to I-5 Overpass	Minor incidents – recurrence daily Major incidents - recurrence 10 years	Airports in Pierce County Ferry Services in Pierce County Transportation Accidents/Catastrophic Failures in Pierce County

The City of Lakewood Hazard Maps and Overview of Data Source Descriptions

Regulated Floodplain²

Summary:

The flood hazard map delineates the flood hazard risk to the City of Lakewood. This map uses the new FEMA Flood Insurance Study (FIS) and Digital Flood Insurance Rate Map (DFIRM) for Pierce County, Washington and Incorporated Areas effective on March 7, 2017. These mapping products replace the FIS & FIRM issued to Unincorporated Pierce County in 1987 and the other incorporated communities between 1980 and 1985. The new DFIRM is a seamless countywide product adopted by every community participating in the National Flood Insurance Program (NFIP).

The intended purposes of this data are to support the National Flood Insurance Program so that flood insurance policies can be written for any qualifying structure in the community. In areas identified as Special Flood Hazard Areas (SFHA), a structure with a federally backed loan is required to purchase flood insurance. SFHA are designated as Zones beginning with the letter A or V (e.g. AE, AH and VE). Areas of moderate risk or areas suspected to be at risk of flooding but where no detailed flood study has been completed are shown as Zone X (Shaded). It is also to inform development in or near flood hazard areas so that new construction and redevelopment meets the purposes of the flood hazards areas described in Pierce County Code Title 18E.70.

Description:

The DFIRM is a composite of several flood studies, some dating back to the 1970s and as recent as 2016 that represent the best available date at the production deadline. The exception to this is the "secluded areas" that are near significant levees that effect the floodplain do not meet the federal standard (44 Code of Federal Regulations 65.10) to show an area protected by the levee. The secluded areas, in the lower Puyallup River and the Carbon River and Puyallup River near the City of Orting, continue to show the understanding of risk prior to the establishment of 44CFR65.10 as shown on the first FIRM. There are more recent hydraulic studies that show a better understanding of flood risk and Unincorporated Pierce County regulates to this better data which has been added to the Regulated Floodplain 2017 feature class. In areas where the regulated flood hazard varies from the DFIRM there are attributions indicating a different "insurance zone" or "insurance BFE".

The Digital Flood Insurance Rate Map (DFIRM) Database depicts flood risk information and supporting data used to develop the risk data. The primary risk classifications used are the 1-percent-annual-chance flood event, the 0.2-percent-annual-chance flood event, and areas of minimal flood risk. The DFIRM Database is derived from Flood Insurance Studies (FISs), previously published Flood Insurance Rate Maps (FIRMs), and flood hazard analyses performed in support of the FISs and FIRMs, and new mapping data, where available. The FISs and FIRMs are published by the Federal Emergency Management Agency (FEMA).

The FIRM is the basis for floodplain management, mitigation, and insurance activities for the National Flood Insurance Program (NFIP). Insurance applications include enforcement of the mandatory purchase requirement of the Flood Disaster Protection Act, which "... requires the

purchase of flood insurance by property owners who are being assisted by Federal programs or by Federally supervised, regulated or insured agencies or institutions in the acquisition or improvement of land facilities located or to be located in identified areas having special flood hazards, " Section 2 (b) (4) of the Flood Disaster Protection Act of 1973. In addition to the identification of Special Flood Hazard Areas (SFHAs), the risk zones shown on the FIRMs are the basis for the establishment of premium rates for flood coverage offered through the NFIP. The DFIRM Database presents the flood risk information depicted on the FIRM in a digital format suitable for use in electronic mapping applications. The DFIRM database is a subset of the Digital FIS database that serves to archive the information collected during the FIS.

Updates:

The October 2019 update to the Regulated Floodplain 2017 shows the changed flood hazard areas modified by FEMA in two Letter of Map Revisions (LOMR).

Some coastal areas of Puget Sound were modified by LOMR 19-10-0588P that became effective 4/22/2019.

A new flood study of Deer Creek within the City of Puyallup modified the flood hazard areas with LOMR 18-10-0841P that became effective 4/4/2019.

Landslide Susceptibility – Deep³

Summary:

These data sets were produced to provide attribute and spatial information on deep-seated landslide susceptibility in Pierce County, by the Washington State Department of Natural Resources, Washington Division of Geology and Earth Resources (DGER). The goal of this data is to estimate the extent of deep-seated landslide susceptible areas. This data is only an estimate of deep-seated landslide susceptible areas and can occur outside of the bounds of these polygons. This data is nonregulatory and is intended for informational purposes. It may not be suitable for legal, engineering, forestry, or surveying purposes; but it is intended to assist planners, homeowners, regulators, and others by identifying areas to seek further geologic investigation before developing, or areas to avoid. Users of this information should consider their intended application, and review or consult the accompanying documentation, to determine the usability of the data for themselves.

Description:

This is a polygon feature class intended to estimate areas susceptible to deep-seated landslides. To create this susceptibility dataset a landslide inventory was first created by using the methods described in the report accompanying these data. The constructed landslide inventory was then used, along with other necessary datasets, to create this deep-seated landslide susceptibility dataset by following protocol from Special Paper 48 (Burns and Mickelson, 2016). This feature class is part of a larger landslide susceptibility dataset for Pierce County, Washington.

Use Limitations:

The Washington Division of Geology and Earth Resources (DGER) shall not be held liable for improper or incorrect use of the data described and/or contained herein. This product is provided 'as is' without warranty of any kind, either expressed or implied, including, but not limited to, the

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Landslide Susceptibility – Shallow⁴

Summary:

These data sets were produced to provide attribute and spatial information on shallow landslide susceptibility in Pierce County, by the Washington State Department of Natural Resources, Washington Division of Geology and Earth Resources (DGER). The goal of this data is to estimate the extent of shallow landslide susceptible areas. This data is only an estimate of shallow landslide susceptible areas and can occur outside of the bounds of these polygons. This data is non-regulatory and is intended for informational purposes. It may not be suitable for legal, engineering, forestry, or surveying purposes; but it is intended to assist planners, homeowners, regulators, and others by identifying areas to seek further geologic investigation before developing, or areas to avoid. Users of this information should consider their intended application, and review or consult the accompanying documentation, to determine the usability of the data for themselves.

Description:

This is a polygon feature class intended to estimate areas susceptible to shallow landslides. To create this susceptibility dataset, the data listed in Special Paper 45 (Burns and others, 2012) as necessary data was obtained, and the Factor of Safety (FOS) portion of that protocol was followed. This feature class is part of a larger landslide susceptibility dataset for Pierce County, Washington.

Use Limitations:

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Liquefaction Potential⁵

Summary:

This is a subset of the original data clipped to Pierce County. The liquefaction susceptibility map details the risk potential throughout the City of Lakewood in a color gradient map.

The Washington State Department of Natural Resources, Division of Geology and Earth Resources received grant funding through the Hazard Mitigation Grant Program (HMGP) following the Nisqually earthquake of February 2001 (FEMA-1361-DRWA). This grant required the Division of Geology and Earth Resources to develop statewide liquefaction susceptibility and NEHRP (National Earthquake Hazards Reduction Program) site class maps.

Regional and local earthquake hazard maps such as these support hazard mitigation, emergency planning and response, planning of local zoning ordinances, and building code enforcement. The primary reason for producing this series of earthquake hazard maps is to support revisions to the State Hazard Mitigation Plan required in the implementation of final rules 44CFR201.4 and 44CFR201.6. These Federal code regulations require both state and local agencies to describe the location and extent of earthquake hazards that affect their jurisdictions. Additionally, these maps will serve a great variety of end-users that are crucial partners in earthquake hazard mitigation.

Description:

These data contain polygons that provide information regarding the relative liquefaction potential for Pierce County, Washington. This feature class is part of a geodatabase that contains statewide ground response data for Washington State. Liquefaction is a natural phenomenon in which saturated, sandy soils lose their strength and behave as liquid. Liquefaction is caused by severe ground shaking during earthquake events. Polygons are classified as having 'very low' to 'high' relative liquefaction susceptibility. Areas underlain by bedrock or peat are mapped separately as these earth materials are not liquefiable, although peat deposits may be subject to permanent ground deformation caused by earthquake shaking and require site-specific analysis under the International Building Code. Water and ice are also separately designated.

Use Limitations:

The Washington Division of Geology and Earth Resources (DGER) shall not be held liable for improper or incorrect use of the data described and/or contained herein. This product is provided 'as is' without warranty of any kind, either expressed or implied, including, but not limited to, the implied warranties of merchantability and fitness for a particular use. The Washington State Department of Natural Resources and the authors of this product will not be liable to the user of this product for any activity involving the product with respect to the following: (a) lost profits, lost savings, or any other consequential damages; (b) the fitness of the product for a particular purpose; or (c) use of the product or results obtained from use of the product. Although these data have been processed successfully on computers of DGER, no warranty, expressed or implied, is made by DGER regarding the use of these data on any other system, nor does the fact of distribution constitute or imply any such warranty. Appropriate use of these map data is the responsibility of each user. - Users must acknowledge the originators when using the data set as a source. - Data should not be used beyond the limits of the source scale. - The data set is not a survey document and should not be utilized as such. This map is meant only as a general guide to delineate areas prone to liquefaction. It is not a substitute for site-specific investigation to assess the potential for liquefaction for any development project. Because the data used in the

liquefaction susceptibility assessment have been subdivided based on regional geologic mapping, this map cannot be used to determine the presence or absence of liquefiable soils beneath any specific locality. This determination requires a site-specific geotechnical investigation performed by a qualified practitioner.

Hazardous Material

The Hazardous Material map has outlined the main arterial routes, railroad lines, airports, marine ferry routes and Tier II sites for which the GIS spatial analysis was taken where there is the potential risk for hazardous materials to be located at any given time. A 2,500-foot buffer was placed around these identified areas, in accordance with the Emergency Response Guidebook (ERG) for potential contaminated zones. This zone does not go into detail of identifying 3 control zones during a hazmat incident. It is intended for general planning purposes only. If an actual incident were to occur instructions would be given by the Incident Commander on site and buffer zones would be determined by the type of hazardous material released. To reduce clutter and overlapping of data the 2,500-buffer zone was not included on the map, but data was analyzed from within those perimeters.

2017 Tier II Sites

The Emergency Planning and Community Right-to-Know Act (EPCRA) of 1986 was created to help communities plan for chemical emergencies. It also requires industry to report on the storage, use and releases of hazardous substances to federal, state, and local governments. EPCRA requires state and local governments, and Indian tribes to use this information to prepare for and protect their communities from potential risks. In 2017 Pierce County Emergency Management secured a project to identify reported 2017 Tier II Sites within Pierce County. These sites were mapped based on their geographical location of identified hazardous substances reported. There are 12 reported Tier II sites identified within the City of Lakewood.

Pipelines

Summary:

These data sets are for the purpose of tracking all gas transmission pipelines, hazardous liquid transmission pipelines, and liquefied natural gas (LNG) plants in Pierce County. The data will be used to support the risk assessment associated with the county's liquid and gas pipeline infrastructure.

Description:

The National Pipeline Mapping System (NPMS) is a geographic information system (GIS) dataset containing the location and selected attributes of the major gas transmission and hazardous liquid transmission pipelines and liquefied natural gas (LNG) plants operating in Pierce County. Effective October 1, 2015 pipeline and LNG plant data is contributed annually by pipeline operators as required by the Pipeline Safety: Miscellaneous Changes to Pipeline Safety Regulations (<http://www.regulations.gov/#!documentDetail;D=PHMSA-2010-0026-0054>).

A 1,000-foot buffer was placed around these identified areas for potential hazard risks related to an accident/incident. It is intended for general planning purposes only. If an actual

accident/incident were to occur instructions would be given by the Incident Commander on site and traffic control zones, barriers or alternate routes would be determined by the type of accident/incident that occurred. To reduce clutter and overlapping of data the 1,000-buffer zone was not included on the map, but data was analyzed within those perimeters.

Use Limitations:

The NPMS pipeline data is available to pipeline operators and federal, state, and local government officials.

Use Constraints: Neither the United States Government nor any party involved in the creation and compilation of NPMS data and maps guarantees the accuracy or completeness of the products. NPMS data should be considered no more accurate than +/-500 feet and must never be used as a substitute for contacting the appropriate local one-call center prior to digging.

Last updated: 2018-07-30.

Transportation Accidents / Incidents

The Transportation Accident map, like the Hazardous Material Map has also outlined the main arterial routes, railroad lines, airports, marine ferry routes for which the GIS spatial analysis was taken where there is the potential risk for transportation accidents/incidents to occur at any given time. A 2,500-foot buffer was placed around these identified areas also for potential hazard risks related to the accident/incident. It is intended for general planning purposes only. If an actual accident/incident were to occur instructions would be given by the Incident Commander on site and traffic control zones, barriers or alternate routes would be determined by the type of accident/incident. To reduce clutter and overlapping of data the 2,500-buffer zone was not included on the map, but data was analyzed from within those perimeters.

Drought, Severe Weather, Civil Disturbance, Energy Emergency, Epidemic, and Terrorism

Due to the nature of these potential natural and human-caused hazards occurring anywhere within Pierce County or within a local jurisdiction, their total boundary figures are used when calculating the risk factors. These numbers will match their Base number's and will show the percent risk at 100% on the Vulnerability Analysis Tables for General Exposure, Population Exposure and General Infrastructure Exposure.

Hazard maps are not created for each of these hazards and for reference the Base map is in Profile Section 2 of this Mitigation Plan.

Vulnerability Analysis Data

A vulnerability analysis was conducted on each hazard map to determine the General Exposure, Population Exposure, and general Infrastructure Exposure risk.

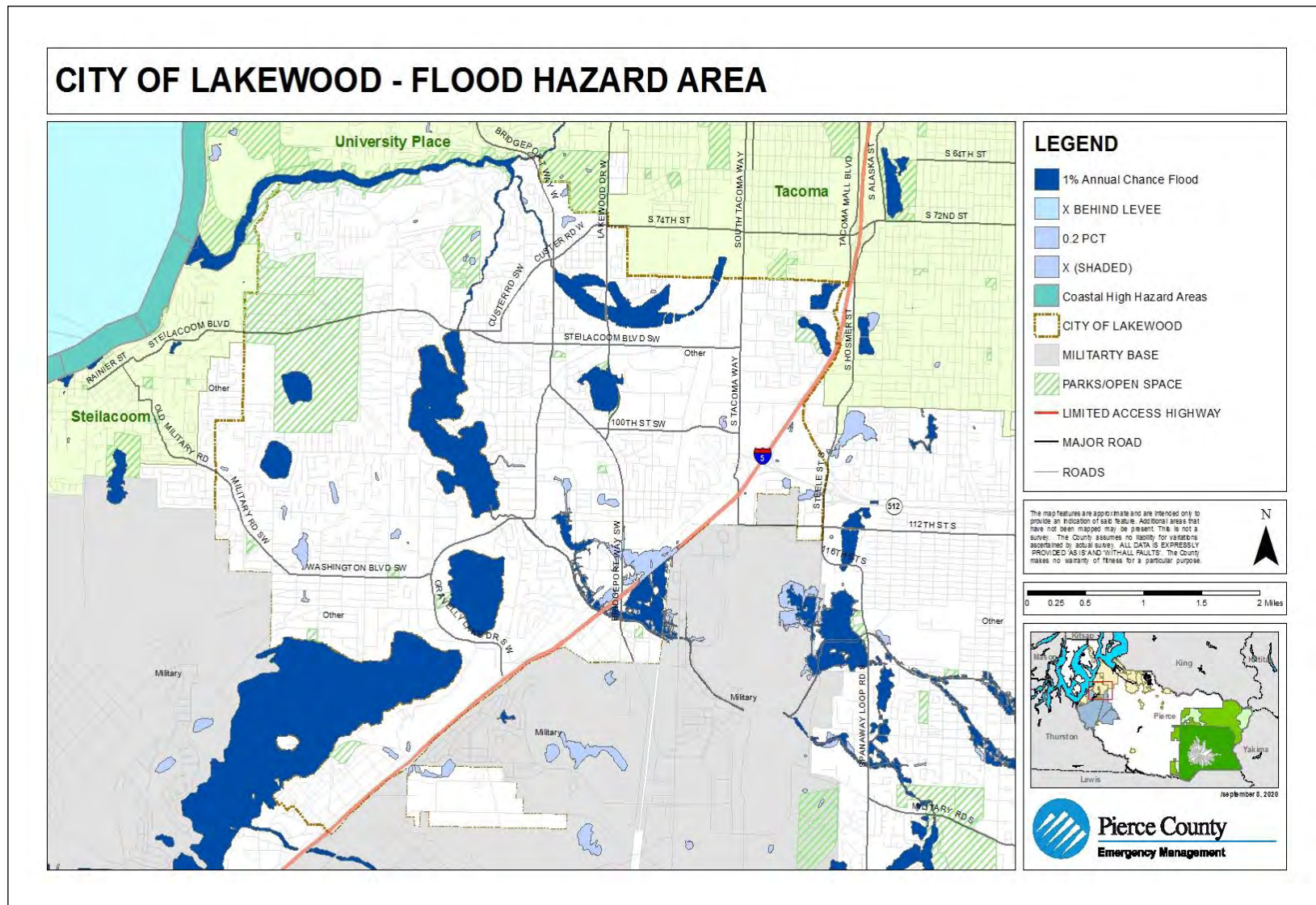
The Pierce County parcel geodatabase is derived from the Pierce County Assessor-Treasurer's Office and they edit and maintain their "parcel" geodatabases daily. The GIS polygon data includes condominium parcel information but does not include mobile home data. A "Total

Base” value is determined for each jurisdiction based on their boundaries and then an analysis is performed to determine the risk percent of each hazard within those boundaries. The tax parcel geodatabase provides information for the square miles, parcels, land value, improved value and total assessed values for the analysis and is identified in Tables 4-3 and 4-4.

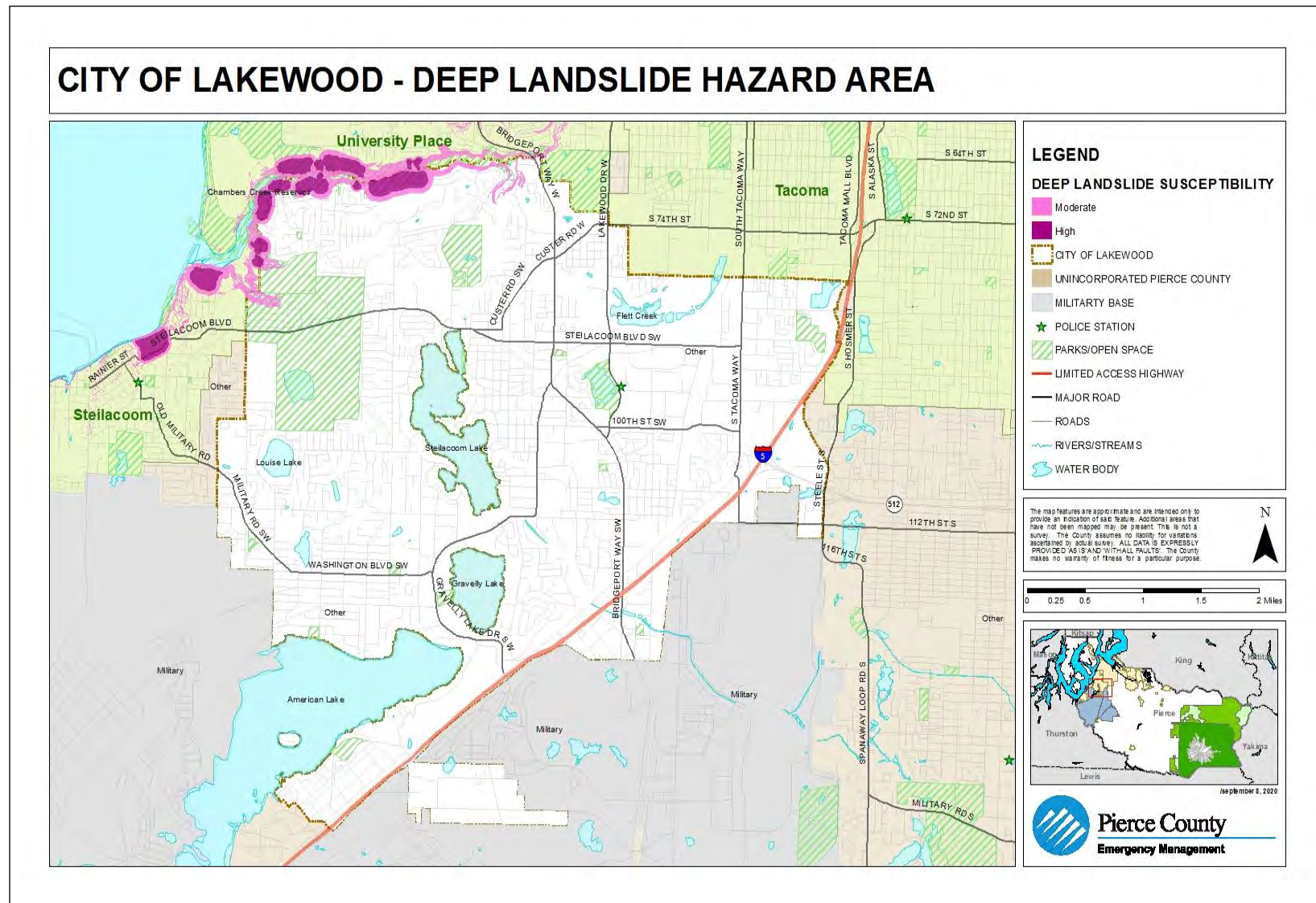
The original 2010 census data was downloaded by Pierce County GIS via the US Census Bureau server ftp and was available on October 14, 2011. All population base and hazard exposure data are derived from this dataset in determining the population exposure. At the current time with the mitigation plan updates this is the best available data that is county wide. As hazards do not have jurisdictional boundaries, a dataset is required that is county wide for analysis purposes. It is acknowledged that this population data is 10 years old and outdated and will be replaced within the plan once the 2020 census data becomes available sometime in 2021. Profile Section 2 provides a heading “Demographic Analysis” for jurisdictions to identify their current populations as best described by them.

The population density figures from Table 4-3 Vulnerability Analysis, Population Exposure calculate the total population density within each hazard area to identify the vulnerable population at risk. The population density is not calculated from the entire jurisdictional boundary.

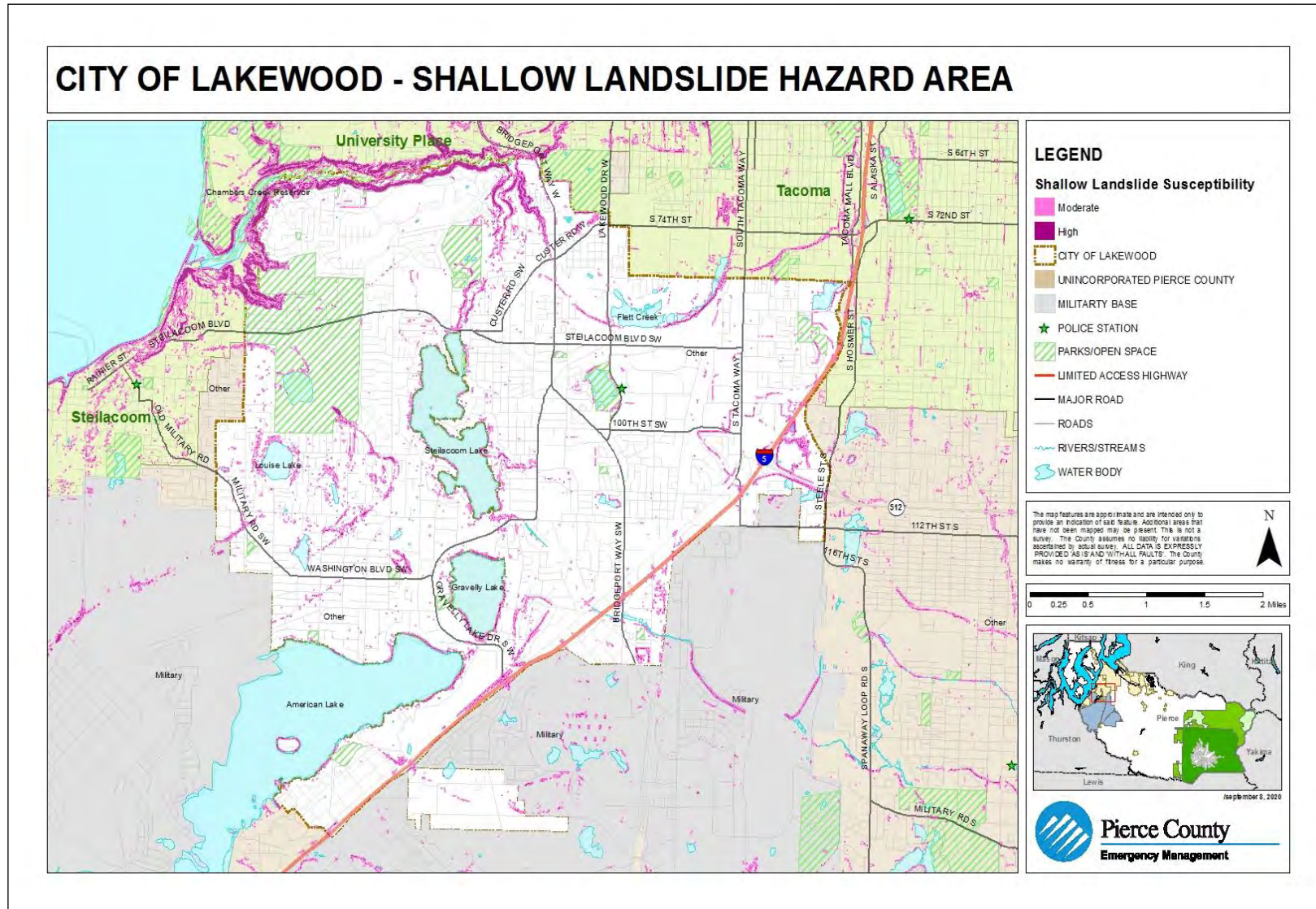
Map 4-1 City of Lakewood – Flood Hazard Map



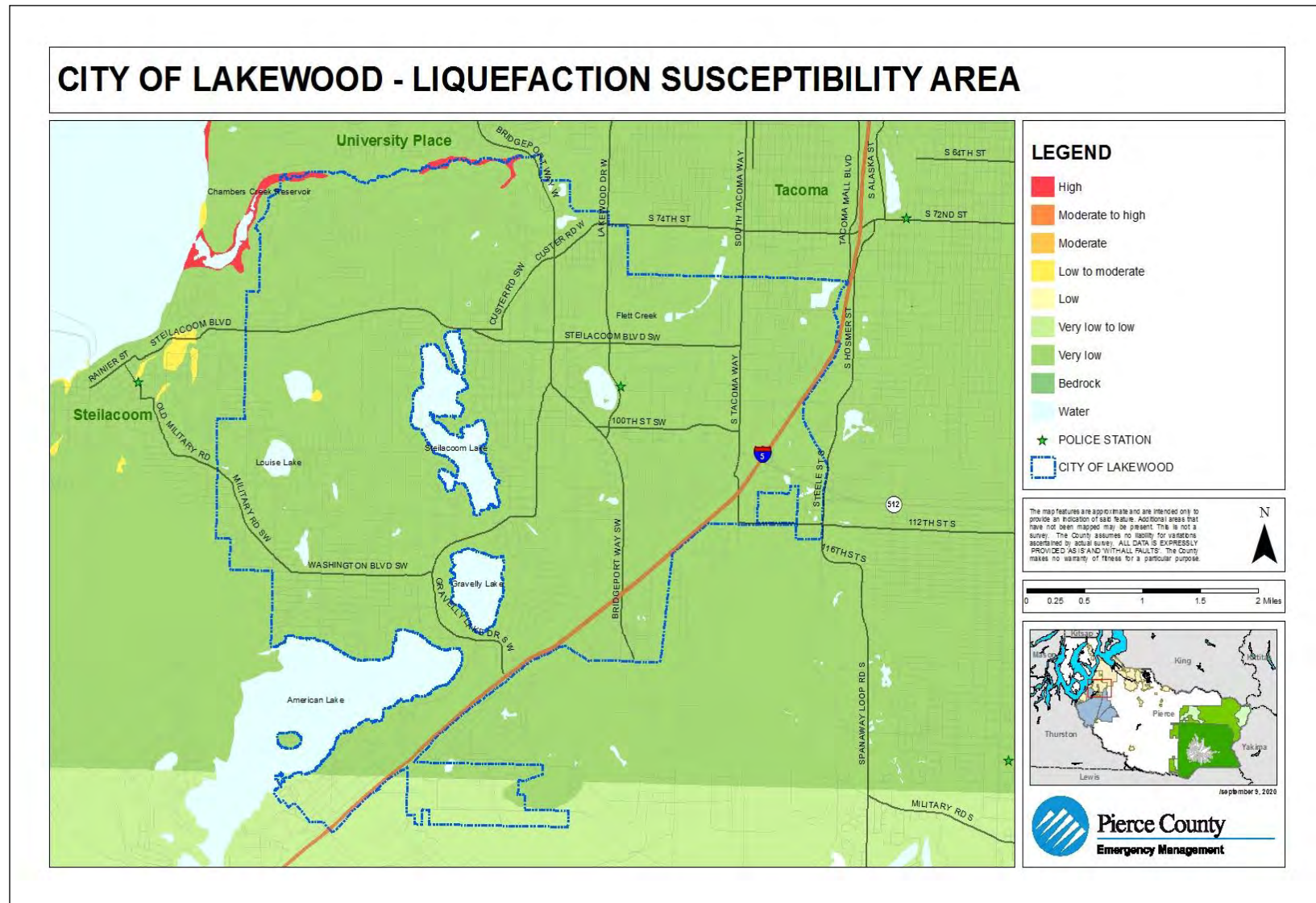
Map 4-2 City of Lakewood – Deep Landslide Hazard Map



Map 4-3 City of Lakewood – Shallow Landslide Hazard Map

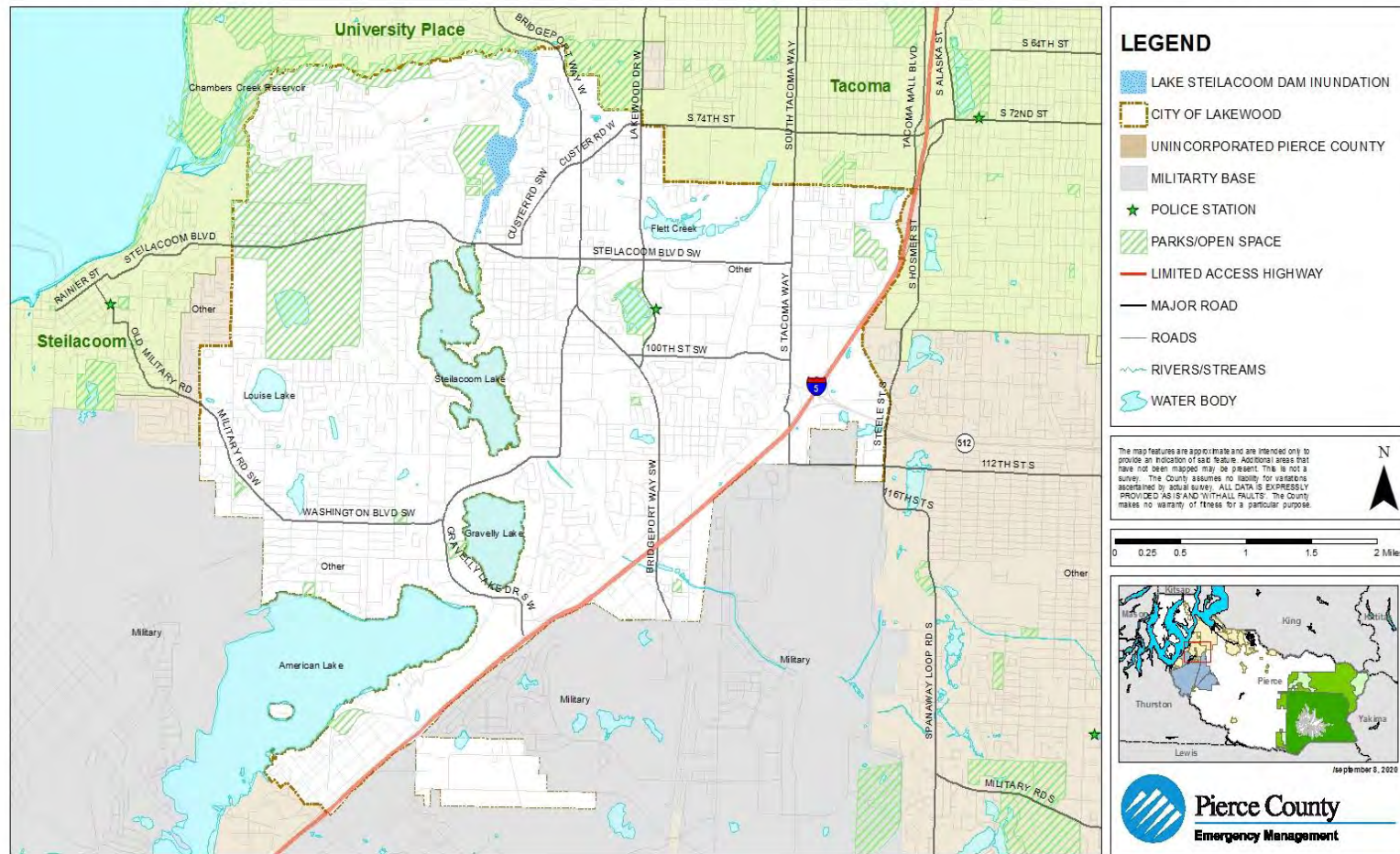


Map 4-4 City of Lakewood – Liquefaction Susceptibility Hazard Map

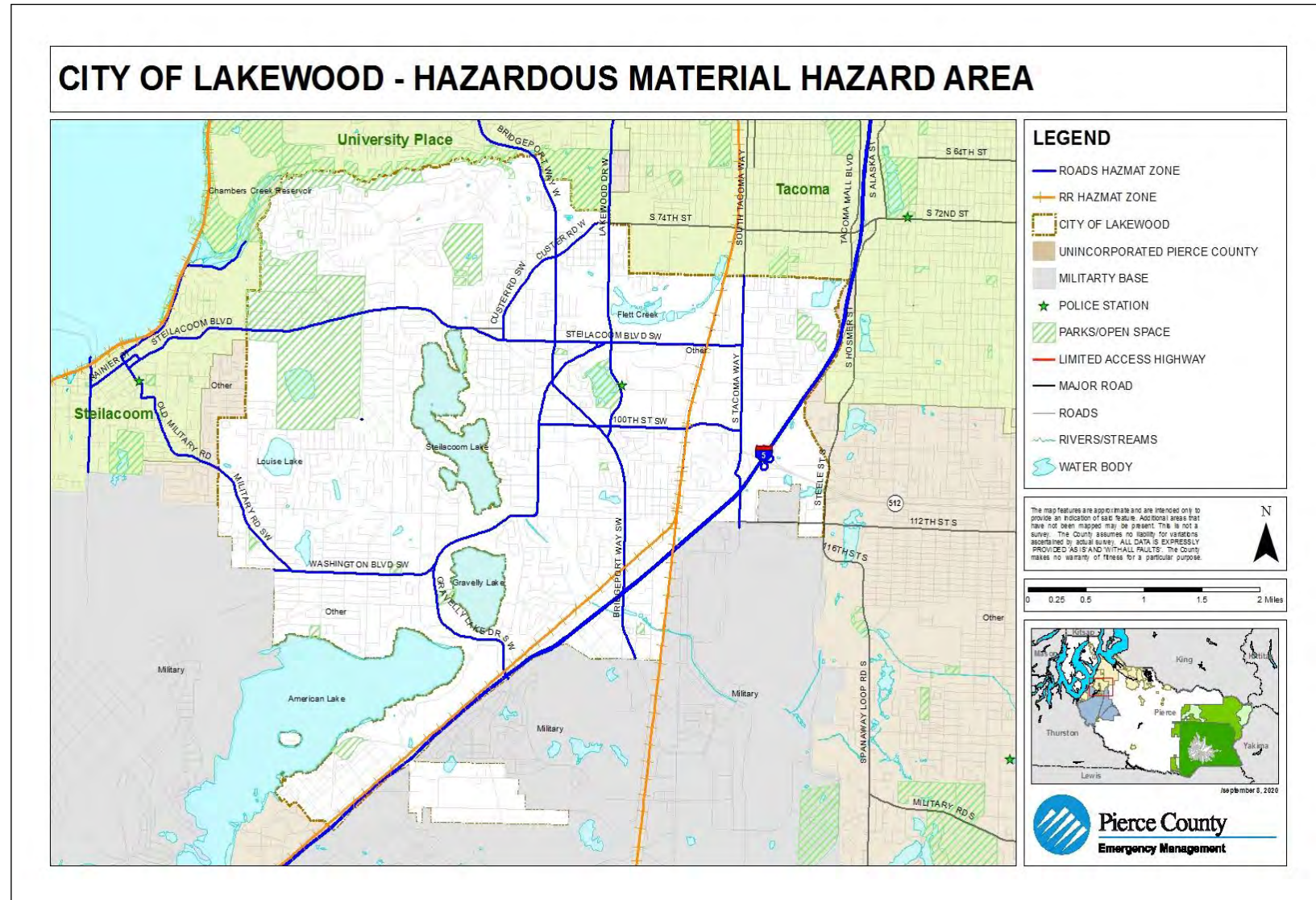


Map 4-5 City of Lakewood – Dam Failure Hazard Area Map

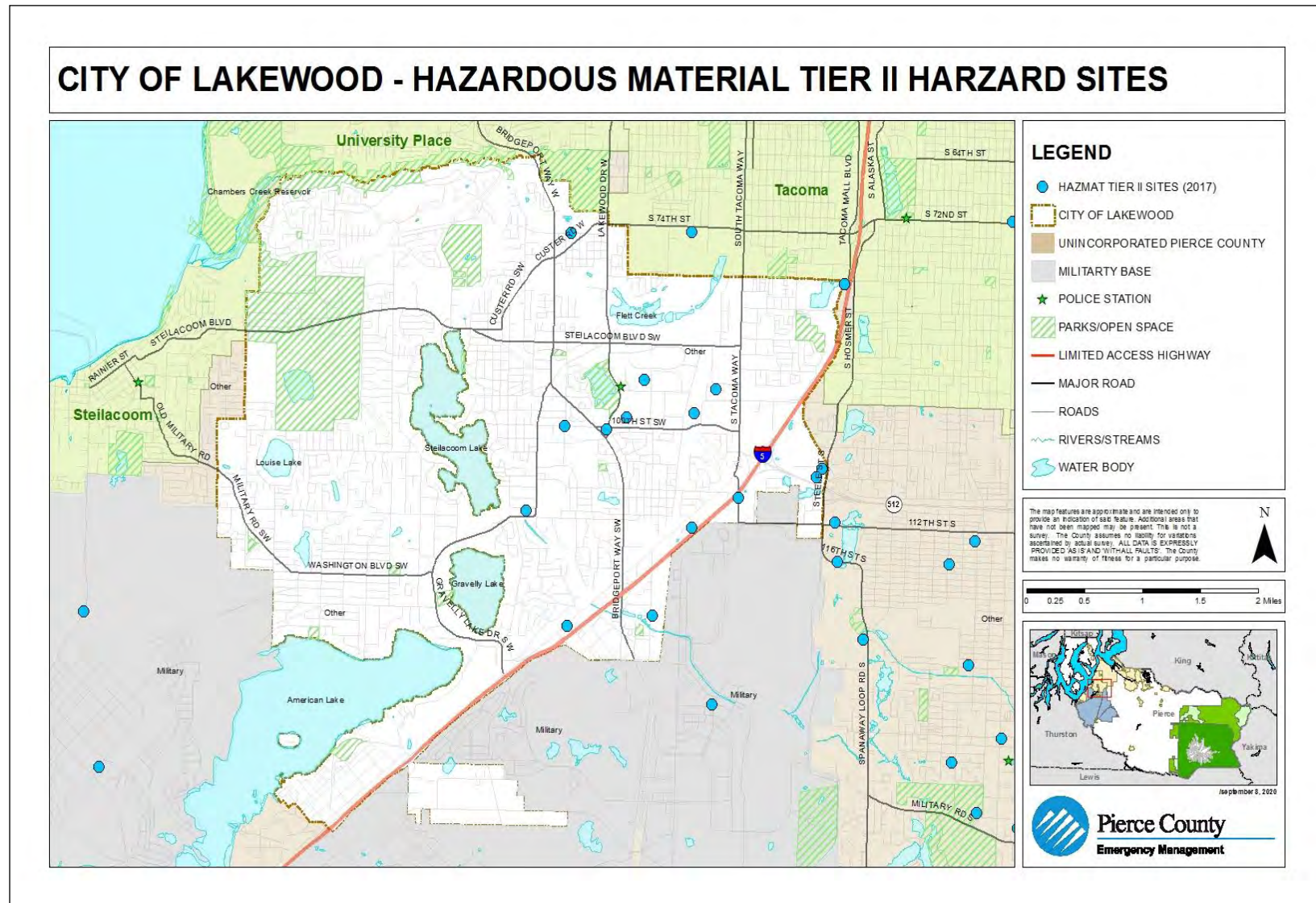
CITY OF LAKEWOOD - DAM FAILURE-LAKE STEILACOOM DAM BREACH



Map 4-6 City of Lakewood – Hazardous Material Hazard Area Map

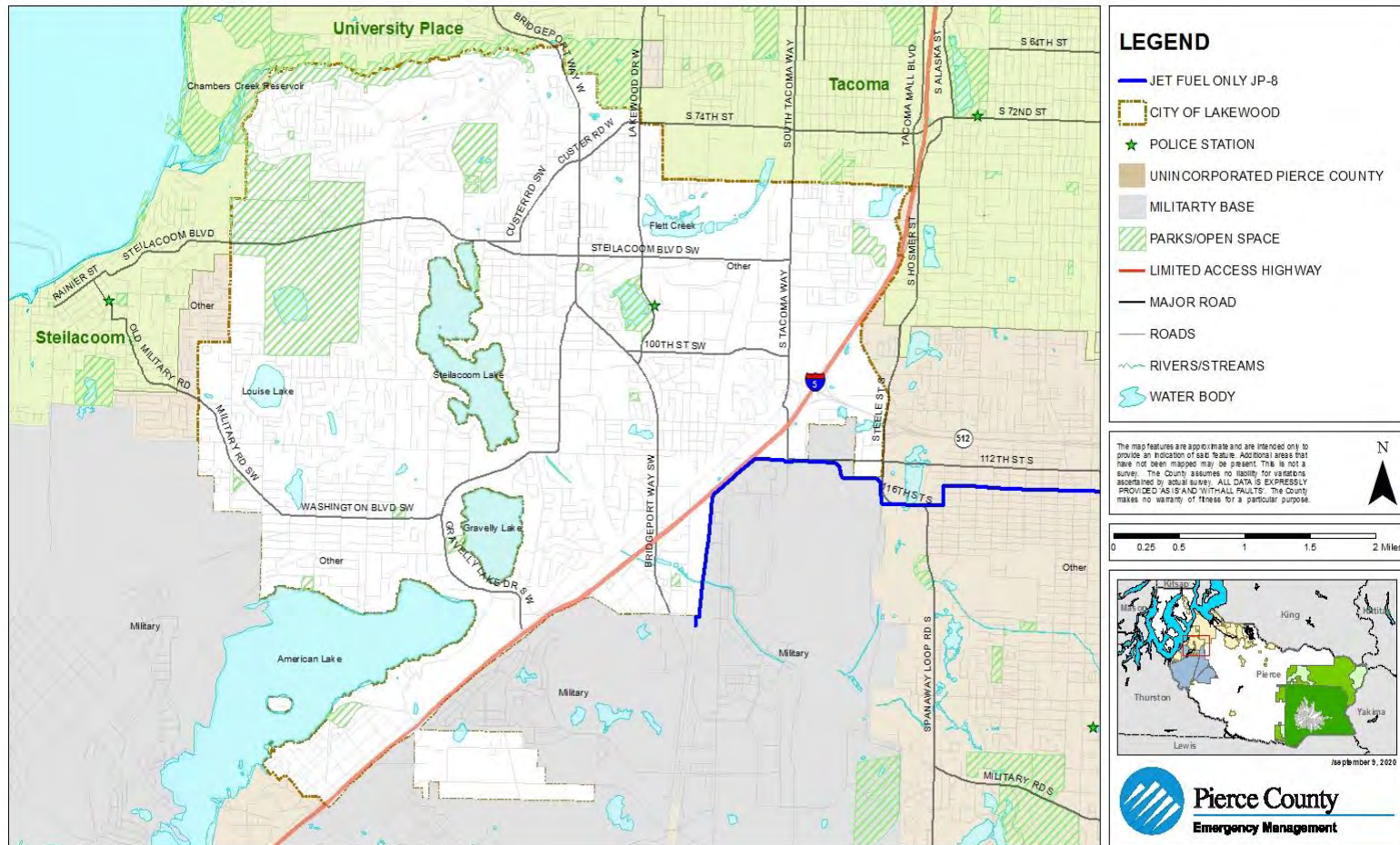


Map 4-7 City of Lakewood – Hazardous Material Tier II Sites 2017 Hazard Area Map

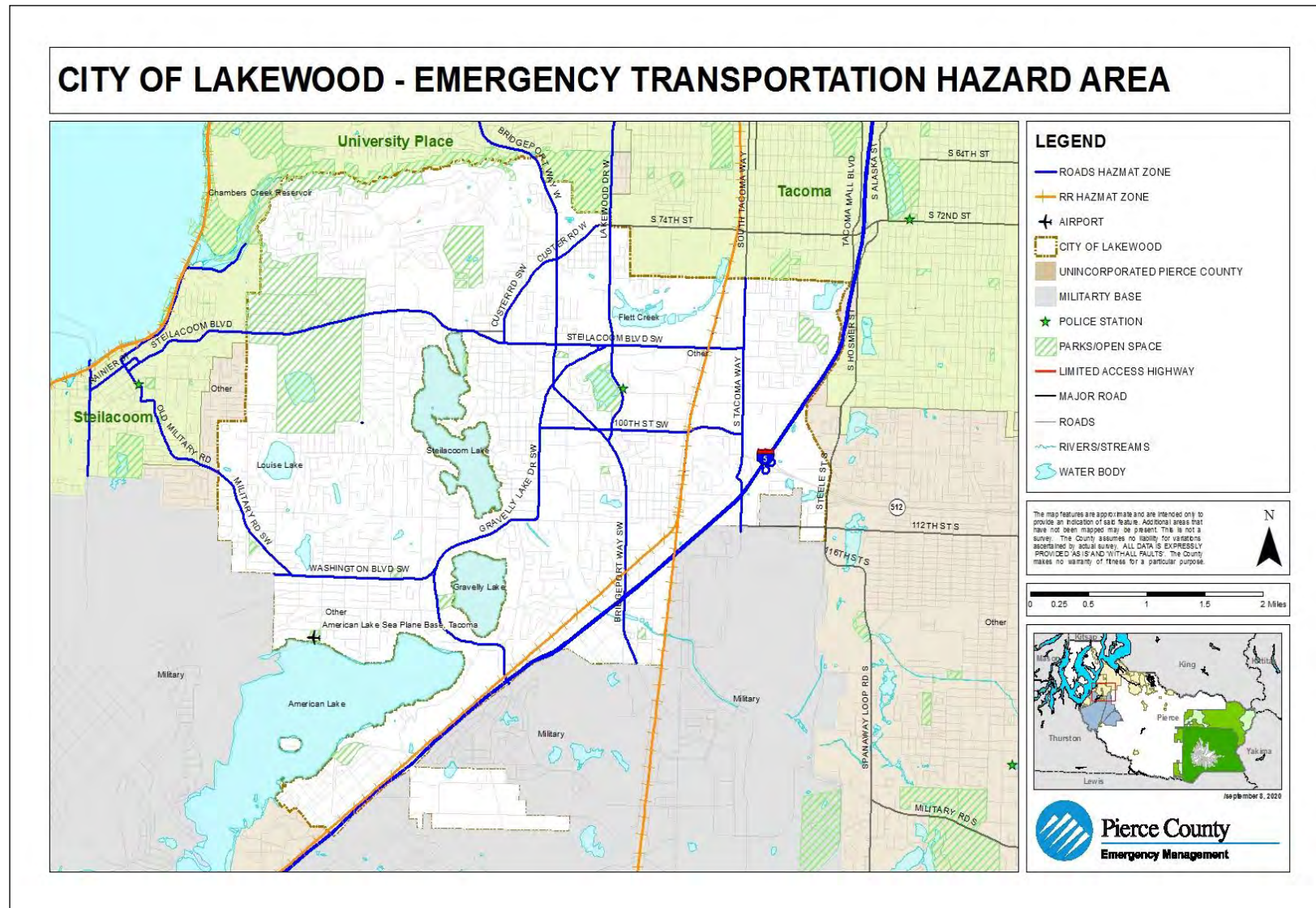


Map 4-8 City of Lakewood – Pipeline Hazard Area Map

CITY OF LAKEWOOD - PIPELINE HAZARD AREA



Map 4-9 City of Lakewood – Transportation Emergency Area Map



Vulnerability Analysis

Table 4-2 Vulnerability Analysis: General Exposure

THREAT		AREA (SQ MI)		PARCELS	
		Total	% Base	Total	% Base
BASE		14.32	100%	17,583	100%
Geological	Avalanche	NA	NA	NA	NA
	Liquefaction Susceptibility	.16	1.1%	27	3.2%
	Landslide-Deep	.97	6.7%	567	.7%
	Landslide-Shallow	6.83	47.7%	3,749	21.3%
	Tsunami	NA	NA	NA	NA
	Volcanic ⁶	14.32	100%	17,583	100%
Meteorological	Drought ⁷	14.32	100%	17,583	100%
	Flood	3.91	27.3%	1,457	8.3%
	Severe Weather	14.32	100%	17,583	100%
	WUI Fire ⁸	Insufficient GIS data to draw numbers from at this time or map susceptibility			
Technological	Abandoned Mines ⁹	NA	NA	NA	NA
	Civil Disturbance ¹⁰	14.32	100%	17,583	100%
	Dam Failure ¹¹	.27	1.9%	131	.75
	Energy Emergency ¹²	14.32	100%	17,583	100%
	Epidemic ¹³	14.32	100%	17,583	100%
	Hazardous Material ¹⁴	11.21	78.3%	13,192	75%
	Pipeline Hazard ¹⁵	.46	3.2%	142	.81%
	Terrorism ¹⁶ / Active Threat	14.32	100%	17,583	100%
	Transportation Accidents ¹⁷	11.21	78.3%	13,192	75%

Table 4-3 Vulnerability Analysis: Population Exposure¹⁸

THREAT ²		POPULATION			SPECIAL POPULATIONS (OF TOTAL EXPOSED POPULATION)			
		Total	% Base	Density (pop/sq mi)	65+ yrs		20- yrs	
					#	%	#	%
BASE		58,163	100%	4,061	7,735	10%	14,646	100%
Geological	Avalanche	NA	NA	NA	NA	NA	NA	NA
	Liquefaction Susceptibility	1,375	2%	8,480	345	4%	239	2%
	Landslide-Deep	5,369	9.2%	5,558	998	12.9%	1,072	7.3%
	Landslide-Shallow	36,396	62.6%	5,328	5,245	67.8%	8,874	60.6%
	Tsunami	NA	NA	NA	NA	NA	NA	NA
	Volcanic	58,163	100%	4,061	7,735	10%	14,646	100%
Meteorological	Drought	58,163	100%	4,061	7,735	10%	14,646	100%
	Flood	11,426	19.6%	2,925	1,517	20%	2,992	20%
	Severe Weather	58,163	100%	4,061	7,735	10%	14,646	100%
	WUI Fire	Insufficient GIS data to draw numbers from at this time or map susceptibility						
Technological	Abandoned Mines	NA	NA	NA	NA	NA	NA	NA
	Civil Disturbance	58,163	100%	4,061	7,735	10%	14,646	100%
	Dam Failure	740	1%	2,737	170	2%	132	1%
	Energy Emergency	58,163	100%	4,061	7,735	10%	14,646	100%
	Epidemic	58,163	100%	4,061	7,735	10%	14,646	100%
	Hazardous Material	49,660	85.4%	4,430	6,559	84.8%	12,623	86.2%
	Pipeline	2,726	5%	5,870	151	2%	853	6%
	Terrorism / Active Threat	58,163	100%	4,061	7,735	10%	14,646	100%
	Transportation Accidents	49,660	85.4%	4,430	6,559	84.8%	12,623	86.2%

Table 4-4 Vulnerability Analysis: General Infrastructure Exposure

THREAT ²		LAND VALUE			IMPROVED VALUE			TOTAL ASSESSED VALUE		
		Total (\$)	% Base	Avg. Value (\$)	Total (\$)	% Base	Avg. Value (\$)	Total (\$)	% Base	Avg. Value (\$)
BASE		\$3,841,746,300	100%	\$218,492	\$5,299,393,100	100%	\$301,424	\$9,141,685,400	100%	\$519,916
Geological	Avalanche	NA	NA	NA	NA	NA	NA	NA	NA	NA
	Liquefaction Susceptibility	\$3,869,600	.1%	\$143,319	\$3,937,400	.1%	\$145,830	\$7,807,000	.1%	\$289,148
	Landslide-Deep	\$85,692,000	2.2%	151,132	\$212,591,900	4%	\$374,942	\$298,283,900	3.3%	\$5,369
	Landslide-Shallow	\$1,358,502,00	35.4%	\$362,364	\$2,001,920,900	37.8%	\$533,988	\$3,360,422,900	36.8%	\$896,352
	Tsunami	NA	NA	NA	NA	NA	NA	NA	NA	NA
	Volcanic	\$3,841,746,300	100%	\$218,492	\$5,299,393,100	100%	\$301,424	\$9,141,685,400	100%	\$519,916
Meteorological	Drought	\$3,841,746,300	100%	\$218,492	\$5,299,393,100	100%	\$301,424	\$9,141,685,400	100%	\$519,916
	Flood	\$789,503,400	20.6%	\$541,869	\$831,874,700	15.7%	\$570,950	\$1,621,378,100	18%	\$1,112,820
	Severe Weather	\$3,841,746,300	100%	\$218,492	\$5,299,393,100	100%	\$301,424	\$9,141,685,400	100%	\$519,916
	WUI Fire	Insufficient GIS data to draw numbers from at this time or map susceptibility								
Technological	Abandoned Mines	NA	NA	NA	NA	NA	NA	NA	NA	NA
	Civil Disturbance	\$3,841,746,300	100%	\$218,492	\$5,299,393,100	100%	\$301,424	\$9,141,685,400	100%	\$519,916
	Dam Failure	\$29,033,900	.76%	\$221,633	\$26,556,100	.50%	\$202,718	\$55,590,00	.61%	\$424,351
	Energy Emergency	\$3,841,746,300	100%	\$218,492	\$5,299,393,100	100%	\$301,424	\$9,141,685,400	100%	\$519,916

	Epidemic	\$3,841,746,300	100%	\$218,492	\$5,299,393,100	100%	\$301,424	\$9,141,685,400	100%	\$519,916
	Hazardous Material	\$3,046,230,400	79.3%	\$230,915	\$4,228,726,000	79.8%	\$320,552	\$7,274,956,400	79.6%	\$551,467
	Pipeline Hazard	\$87,071,600	2.27%	613,180	\$96,598,400	1.82%	\$680,270	\$183,670,00	2.01%	\$1,293,451
	Terrorism / Active Threat	\$3,841,746,300	100%	\$218,492	\$5,299,393,100	100%	\$301,424	\$9,141,685,400	100%	\$519,916
	Transportation Accidents	\$3,046,230,400	79.3%	\$230,915	\$4,228,726,000	79.8%	\$320,552	\$7,274,956,400	79.6%	\$551,467

Table 4-5a Consequence Analysis Chart – Geological¹⁹²⁰

THREAT		CONSEQUENCE	YES OR NO
<i>Geological</i>	Avalanche	Impact to the Public	No
		Impact to the Responders	No
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No
	Earthquake	Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	Yes
		Impact to Property, Facilities and Infrastructure	Yes
		Impact to the Environment	Yes
		Impact to the Jurisdiction Economic Condition	Yes
		Impact to Reputation or Confidence in Jurisdiction	Yes
	Landslide	Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	Yes
		Impact to the Environment	Yes
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No
	Tsunami	Impact to the Public	No
		Impact to the Responders	No
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No
	Volcanic²¹	Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	Yes
		Impact to the Environment	Yes
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No

Table 4-5b Consequence Analysis Chart – Meteorological

THREAT		CONSEQUENCE	YES OR NO
<i>Meteorological</i>	Drought	Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	Yes
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No
	Flood	Impact to the Public	No
		Impact to the Responders	No
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No
	Severe Weather	Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	Yes
		Impact to the Environment	Yes
		Impact to the Jurisdiction Economic Condition	Yes
		Impact to Reputation or Confidence in Jurisdiction	Yes
	WUI Fire	Impact to the Public	No
		Impact to the Responders	No
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No

Table 4-5c Consequence Analysis Chart – Technological²²

THREAT		CONSEQUENCE	YES OR NO
<i>Technological</i>	Abandoned Mines	Impact to the Public	No
		Impact to the Responders	No
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No
	Civil Disturbance	Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	Yes
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	Yes
	Dam Failure	Impact to the Public	No
		Impact to the Responders	No
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	No

	Energy Emergency	Impact to Reputation or Confidence in Jurisdiction	No
		Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	Yes
		Impact to Property, Facilities and Infrastructure	Yes
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	Yes
		Impact to Reputation or Confidence in Jurisdiction	Yes
	Epidemic	Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	Yes
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	Yes
		Impact to Reputation or Confidence in Jurisdiction	Yes
	Hazardous Materials	Impact to the Public	No
		Impact to the Responders	No
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No
	Pipeline Hazards	Impact to the Public	No
		Impact to the Responders	No
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	No
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	No
	Terrorism	Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	Yes
		Impact to the Environment	No
		Impact to the Jurisdiction Economic Condition	Yes
		Impact to Reputation or Confidence in Jurisdiction	Yes
	Transportation Accident	Impact to the Public	Yes
		Impact to the Responders	Yes
		Impact to COG and/or COOP in the Jurisdiction	No
		Impact to Property, Facilities and Infrastructure	Yes
		Impact to the Environment	Yes
		Impact to the Jurisdiction Economic Condition	No
		Impact to Reputation or Confidence in Jurisdiction	Yes

Endnotes

¹ A THIRA expands on the existing hazard identification and risk assessment and provides a comprehensive approach for assessing risks and associated impacts using a scenario to assess a range of capabilities.

² Data source information for the Regulated Floodplain 2017 GIS Feature Class hazard layer is from the Metadata. For additional information contact Dennis Dixon with Pierce County, Planning and Public Works, Surface Water Management Division.

³ Data source information for the Deep Landslide Susceptibility GIS Feature Class hazard layer is from the Metadata. For additional information contact the Washington Geological Survey.

⁴ Data source information for the Shallow Landslide Susceptibility GIS Feature Class hazard layer is from the Metadata. For additional information contact the Washington Geological Survey.

⁵ Liquefaction susceptibility is assigned based on criteria described in: Palmer, Stephen P.; Magsino, Sammantha L.; Bilderback, Eric L.; Poelstra, James L.; Folger, Derek S.; Niggemann, Rebecca A., 2007, Liquefaction susceptibility and site class maps of Washington State, by county: Washington Division of Geology and Earth Resources Open File Report 2004-20, 78 plates, with 45 p. text.

[http://www.dnr.wa.gov/ResearchScience/Topics/GeologyPublicationsLibrary/Pages/pub_ofr04-20.aspx]

Data source information for the Liquefaction Susceptibility GIS Feature Class hazard layer is from the Metadata. For additional information contact the Washington Geological Survey, Washington Division of Geology and Earth Resources. Last updates to this data set on 2017-01-03.

⁶ The threat of volcanic ashfall affects the entire Region 5 however some jurisdictions are specifically threatened by lahar flows directly from Mt. Rainier; an active volcano.

⁷ The entire jurisdiction is vulnerable to Drought. There are three things that must be understood about the effect of Drought on the jurisdiction: 1) Drought is a region wide event. When it does affect Pierce County, it will affect every jurisdiction, 2) Drought will gradually develop over time. It is a gradually escalating emergency that may take from months to years to affect the jurisdiction. Initially lack of water may not even be noticed by the citizens. However, as the Drought continues, its effects will be noticed by a continually expanding portion of the community until it is felt by all, and 3) jurisdictions will be affected differently at different times as a drought develops. This will vary depending on the needs of each local jurisdiction. Some examples are: jurisdictions that have industry that requires a continuous supply of a large quantity of water; others have agriculture that requires water but may only require it at certain times of the year; and, some jurisdictions have a backup source of water while others do not.

⁸ According to the most recent information from the Department of Natural Resources, the City of Orting while undergoing development does not have large areas of forested land that could develop into a wildland/urban interface fire. Further study is needed to determine the extent of the area that could be affected.

⁹ The definition of Abandoned Mines comes from the 2020 Pierce County HIRA: Abandoned mines are any excavation under the surface of the earth, formerly used to extract metallic ores, coal, or other minerals, and that are no longer in production.

¹⁰ The definition of Civil Disturbance comes from the 2020 Pierce County HIRA: Civil Disturbance (unrest) is the result of groups or individuals within the population feeling, rightly or wrongly, that their needs or rights are not being met, either by the society at large, a segment thereof, or the current overriding political system. When this results in community disruption of a nature where intervention is required to maintain public safety it has become a civil disturbance. Additionally, the Region 5 Strategic Plan includes Operational Objectives 3 & 4: Intelligence Gathering, Indicators, Warnings, etc; and Intelligence and Information Sharing.

¹¹ The definition of Dam Failure comes from the 2020 Pierce County HIRA: A dam is any “barrier built across a watercourse for impounding water.”¹¹ Dam failures are catastrophic events “characterized by the sudden, rapid, and uncontrolled release of impounded water. The vulnerability analysis was based on the potential dam failure from Mud Mountain Dam and Lake Tapps using Pierce County’s GIS data which originated from each of the dam’s emergency plans inundation maps.

¹² The definition of an Energy Emergency comes from the 2020 Pierce County HIRA: Energy emergency refers to an out-of-the-ordinary disruption, or shortage, of an energy resource for a lengthy period of time. Additionally, the Region 5 Strategic Plan addresses Energy Emergencies in its Operational Objective 32, Restoration of Lifelines which addresses the restoration of critical services such as oil, gas, natural gas, electric, etc.

¹³ The definition of Epidemic comes from the Centers for Disease Control: A Pandemic is an epidemic occurring over a very wide area and usually affecting a large proportion of the population. Pandemics occur when a wholly new subtype of influenza A virus emerges. A “novel” virus can develop when a virulent flu strain that normally infects birds or animals infects a human who has influenza; the two viruses can exchange genetic material, creating a new, virulent flu virus that can be spread easily from person-to-person. Unlike the flu we see yearly, no one would be immune to this new flu virus, which would spread quickly, resulting in widespread epidemic disease – a pandemic.

¹⁴ The definition of Hazardous Materials comes from the 2020 Pierce County HIRA: Hazardous materials are materials, which because of their chemical, physical or biological properties, pose a potential risk to life, health, the environment, or property when not properly contained. A hazardous material release then is the release of the material from its container into the local environment. A general rule of thumb for safety from exposure to hazardous material releases is 1000ft; the Emergency Response Guidebook 2016, established by the US Dept of Transportation, contains advice per specific materials. The vulnerability analysis was broken into two sub sections for a better understanding of the hazard using Pierce County’s GIS data with a 500-foot buffer on either side of the railroads and major roadways.

¹⁵ The definition of Pipeline Emergency comes from the 2020 Pierce County HIRA: While there are many different substances transported through pipelines including sewage, water and even beer, pipelines, for the purpose of this chapter, are transportation arteries carrying liquid and gaseous fuels. They may be buried or above ground.

¹⁶ The definition of Terrorism comes from the 2020 Pierce County HIRA: Terrorism has been defined by the Federal Bureau of Investigation as, “the unlawful use of force or violence against persons or property to intimidate or coerce a Government, the civilian population or any segment thereof, in furtherance of political or social objectives.” These acts can vary considerably in their scope and not all are related to Terrorism which is why Active Threat / Attack Tactics has been added to the Pierce County HIRA as its own chapter.

¹⁷ The definition of Transportation Accident comes from the 2020 Pierce County HIRA: Transportation accidents as used in this assessment include accidents involving a method of transportation on the road, rail, air, and maritime systems within the confines of Pierce County. The vulnerability analysis was broken into three sub sections for a better understanding of the hazard using Pierce County’s GIS data; Commencement Bay to include inland rivers and streams, railroads, and roads. A 200-foot buffer was applied to all the shorelines and a 500-foot buffer on either side of the railroads and roadways.

¹⁸ All census data is based off the block level 2010 census from OFM. Once the 2020 census data becomes available these numbers will be updated.

¹⁹ In the Impact to Property, Facilities and Infrastructure, both Tables 4-5a and 4-5b, look at the impact to all property, facilities and infrastructure existing in the jurisdiction, not just to that owned by the jurisdiction.

²⁰ The consideration for each of these hazards, in both Tables 4-5a and 4-5b, as to whether an individual hazard’s consequences exist or not, is based on a possible worst-case scenario. It must also be understood that a “yes” means that there is a good possibility that the consequence it refers to could happen as a result of the hazard, not that it will. Conversely “No” means that it is highly unlikely that that consequence will have a major impact, not that there will be no impact at all.

²¹ While the major volcanic hazard from Mt. Rainier is from a lahar descending the main river valleys surrounding the mountain, it is not the only problem. Most jurisdictions could receive tephra in greater or lesser amounts, sometimes with damaging results. Consequence analyses in this section consider the possibility of tephra deposition in addition to a lahar.

²² The Technological Consequences are added herein to acknowledge the role of human-caused hazards in the health and safety of unincorporated Pierce County. The consequences noted are under the same criteria as natural hazards given their impacts to the departmental assets.

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Section 5

Mitigation Strategy Requirements

Mitigation Strategy---Requirement §201.6(c)(3):

The plan **shall** include a strategy that provides the jurisdiction's blueprint for reducing the potential losses identified in the risk assessment, based on existing authorities, policies, programs and resources, and its ability to expand on and improve these existing tools.

Local Hazard Mitigation Goals---Requirement §201.6(c)(3)(i):

[The hazard mitigation strategy **shall** include a] description of mitigation goals to reduce or avoid long-term vulnerabilities to the identified hazards.

- Does the new or updated plan include a description of mitigation goals to reduce or avoid long-term vulnerabilities to the identified hazards?

Identification and Analysis of Mitigation Actions---Requirement §201.6(c)(3) (ii):

[The mitigation strategy **shall** include a] section that identifies and analyzes a comprehensive range of specific mitigation actions and projects being considered to reduce the effects of each hazard, with particular emphasis on new and existing buildings and infrastructure.

Identification and Analysis of Mitigation Actions: National Flood Insurance Program (NFIP) Compliance--Requirement §201.6(c)(3)(ii):

[The mitigation strategy] must also address the jurisdiction's participation in the National Flood Insurance Program (NFIP), and continued compliance with NFIP requirements, as appropriate.

- Does the new or updated plan identify and analyze a comprehensive range of specific mitigation actions and projects for each hazard?
- Do the identified actions and projects address reducing the effects of hazards on new buildings and infrastructure?
- Do the identified actions and projects address reducing the effects of hazards on existing buildings and infrastructure?
- Does the new or updated plan describe the jurisdiction(s) participation in the NFIP?
- Does the mitigation strategy identify, analyze and prioritize actions related to continued compliance with the NFIP?

Implementation of Mitigation Actions---Requirement: §201.6(c)(3) (iii):

[The mitigation strategy section **shall** include] an action plan describing how the actions identified in section (c)(3)(ii) will be prioritized, implemented, and administered by the local jurisdiction. Prioritization **shall** include a special emphasis on the extent to which benefits are maximized according to a cost benefit review of the proposed projects and their associated costs.

- Does the new or updated mitigation strategy include how the actions are prioritized? (For example, is there a discussion of the process and criteria used?)
- Does the new or updated mitigation strategy address how the actions will be implemented and administered, including the responsible department, existing and potential resources and the timeframe to complete each action?
- Does the new or updated prioritization process include an emphasis on the use of cost-benefit review to maximize benefits?
- Does the updated plan identify the completed, deleted or deferred mitigation actions as a benchmark for progress, and if activities are unchanged (i.e., deferred), does the updated plan describe why no changes occurred?

SECTION 5

REGION 5 ALL HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD MITIGATION STRATEGY SECTION

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Mitigation Measure Overview

The measures having been identified, defined, and evaluated; the rest of the process involved prioritization. The process relied upon the identified risks and vulnerabilities, the planning team's local expertise, public participation, each organization's needs and capabilities, a cost/benefit review, and input from the chief elected officials. In order to promote implementation of the measures, they were grouped based on the level at which they would be implemented, as described in the Plan Maintenance Section. These levels were:

- **Startup Mitigation Measures:** Those mitigation measures already in existence within the organization and including the maintenance of the Mitigation Plan.
- **Hazard Mitigation Forum (HMF):** Multi-organizational implementation mechanism.
- **Organization-Wide Mitigation Measures:** Mechanism depends on organization.
- **Public Education Mitigation Measures:** Localized level based on targeted communities and their needs and vulnerabilities.

The measures are prioritized within each implementation category. In order to provide consistency, the evaluation process including the eight categories, was used as the basis for the prioritization of measures. This allows for emphasis on the extent to which each measure is cost-effective.

The planning team members from each organization prioritized their organization's potential mitigation measures based on goals addressed with special attention paid to the measure's benefit-cost review, its ability to be implemented, and the extent to which it would mitigate one or multiple relevant hazards.

Prioritization of Measures

The list was prioritized based on the calculated risk posed to the community and the relative need to apply the mitigation strategy. Specifically, we introduced a new strategy for Transportation Accidents as there have been several rail accidents in the vicinity and a new high-speed rail service is planned to re-start operations within 2020. Some measures reflect long term progress. For instance, our Road Maintenance and Operations Shop was able to be outfitted with a generator switch and electrical connections during the previous plan period. In this next plan period, we will work to purchase and install the actual generator. We based the city's mitigation measures on what seemed reasonable, possible, and plausible given the abilities and time of the city and its staff members.

Table 5-1 City of Lakewood Mitigation Strategy Matrix

Implementation Mechanism	Mitigation Measure (<i>Hazard(s)</i>) ¹	Lead Jurisdiction(s) / Department(s)	Timeline (years)	Plan Goals					
				Life and Property	Operations Continuity	Partnerships	Natural Resources	Preparedness	Sustainable Economy
Startup	1. Existing Mitigation Actions (<i>E,L,V,D,F,WUI,SW,MM</i>)	Lakewood - Administration	Ongoing	✓	✓	✓	✓	✓	✓
	2. Plan Maintenance (<i>E,L,V,D,F,WUI,SW,MM</i>)	Lakewood - Administration	Ongoing	✓	✓	✓	✓	✓	✓
HMF	1. Pierce County Hazard Mitigation Forum (<i>E,L,V,D,F,WUI,SW,MM</i>)	PC DEM; Lakewood - Administration	Ongoing	✓	✓	✓	✓	✓	✓
	1. Complete, Distribute, Train on a Multi-Agency response to Transportation Accident Plan (MM)	Lakewood	1 to 2	✓	✓	✓		✓	
	2. Purchase and Install generators at Road Operations & Parks Maintenance Shops (<i>E,SW,MM</i>)	Lakewood – Public Works Road Operations	5	✓	✓			✓	✗
	3. Identify Critical Routes and Determine Alternate Routes (<i>E,F,SW</i>)	Lakewood – Public Works Engineering	5	✓	✓	✓			
	4. Radio Communications Set-Up Between all City-Owned Vehicles, Buildings, and EOC (<i>E,F,SW</i>)	Lakewood	5	✓	✓				
	5. Evacuation Plan Template for Multiple Events (<i>E,V,F,SW,WUI,MM</i>)	Lakewood - Police with West Pierce Fire & Rescue	1 to 2	✓	✓	✓			—
	6. Seismic Review and Retrofit of all Bridges, Culverts, and Retaining Walls within and Through the City (<i>E,F,SW</i>)	Lakewood – Public Works Engineering	5	✓		✓			↘
	7. Battery Back-Up (Uninterrupted Power Supply (UPS)) for All Major Traffic Signals (<i>E,F,SW</i>)	Lakewood – Public Works Road Operations	5	✓	✓				
	8. Complete, Distribute, Train Staff on Continuity of Operations Plan (COOP) (<i>E,V,F,WUI,SW,MM</i>)	Lakewood	Ongoing	✓	✓	✓		✓	↘
	9. Essential Records Protection (<i>E,V,F,SW,WUI,MM</i>)	Lakewood - IT and City Clerk	Ongoing	✓	✓	□			↘
	10. Capability Identification and Evaluation (<i>E,L,V,D,F,WUI,SW,MM</i>)	Lakewood	Ongoing	✓	✓	✓	✓	✓	↘
	11. Illicit Discharge and Spill Response (<i>MM</i>)	Lakewood – Public Works Engineering	Ongoing	✓	□	✓	✓	✓	□
	12. Develop Mutual Aid Agreements with Other Public Agencies to Support in Roadway Clearing and Repair (<i>E,F,SW</i>)	Lakewood – Public Works Road Operations	Ongoing	✓	✓	✓			
	13. Significant Tree Pruning and Maintenance (<i>E,D,SW</i>)	Lakewood – Public Works Road Operations	Ongoing	✓	✓		✓	✓	
	14. Utility Coordination (<i>E,L,V,F,SW,WUI,MM</i>)	Lakewood	Ongoing	✓	✓	✓			

Implementation Mechanism	Mitigation Measure (<i>Hazard(s)</i>) ¹	Lead Jurisdiction(s) / Department(s)	Timeline (years)	Plan Goals					
				Life and Property	Operations Continuity	Partnerships	Natural Resources	Preparedness	Sustainable Economy
	15. Accurate Mapping of All Roadways- Electronic and Hard Copy (<i>E,L,F,SW,MM</i>)	Lakewood - GIS	Ongoing	✓	✓				
	16. Traffic Control Devices for Setting Blockades and Detour Routes (<i>E,L,F,SW,MM</i>)	Lakewood – Public Works Road Operations	Ongoing	✓	✓				
	17. Generator Retrofit of Park Operations Center (<i>E,SW,MM</i>)	Lakewood - Parks	Complete	✓	✓			✓	
	18. Structural Retrofit of Park Operations Center (<i>E,SW</i>)	Lakewood – Parks	Complete	✓	✓			✓	
	19. Create an Emergency Operations Center in Lakewood City Hall (<i>E,V,F,SW,WUI,MM</i>)	Lakewood - Emergency Management	Complete	✓	✓	✓		✓	✓
	20. National Flood Insurance Program (<i>F</i>)	Lakewood (Community Development); PC PWU	Complete	✓	✓	✓	✓	✓	□
<u>Public Education</u>	1. Flood Preparedness and Response (<i>F</i>)	Lakewood – Public Works	Ongoing	✓	✓	✓	✓		
	2. Disaster Preparedness Training (<i>E,V,F,SW,WUI,MM</i>)	Lakewood - HR and PC Health Department	Ongoing	✓	✓				

Startup Mitigation Measures

Existing Mitigation Actions

Hazards: E, L, V, D, F, WUI, SW¹, MM²

The City of Lakewood will integrate the hazard mitigation plan into existing plans, ordinances, and programs to dictate land uses within the jurisdiction. Further, Lakewood will continue to implement existing programs, policies, and regulations as identified in the Capability Identification Section of this Plan. This includes such actions as updating the Critical Area Regulations and any ensuing land use policies with best available science. It also includes continuing those programs that are identified as technical capabilities.

1. **Goal(s) Addressed** = Protect Life and Property; Promote A Sustainable Economy; Ensure Continuity of Operations; Increase Public Preparedness for Disasters; Preserve or Restore Natural Resources; Establish and Strengthen Partnerships for Implementation.
2. **Cost of Measure** = TBD
3. **FUNDING SOURCE AND SITUATION** = FUNDING COULD BE ACCOMPLISHED WITH LOCAL BUDGETS OR GRANTS.
4. **Lead Jurisdiction(s)** = City of Lakewood - Administration.
5. **Timeline** = Ongoing
6. **Benefit** = City-Wide
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
	X		
Comments			

Origin

Previous Plan	Current Plan
X	X

Plan Maintenance

Hazards: E, L, V, D, F, WUI, SW¹, MM²

Lakewood will adopt those processes outlined in the Plan Maintenance Section of this Plan.

1. **Goal(s) Addressed** = Protect Life and Property; Promote A Sustainable Economy; Ensure Continuity of Operations; Increase Public Preparedness for Disasters; Preserve or Restore Natural Resources; Establish and Strengthen Partnerships for Implementation.
2. **Cost of Measure** = TBD
3. **FUNDING SOURCE AND SITUATION** = FUNDING COULD BE OBTAINED THROUGH LOCAL BUDGET.
4. **Lead Jurisdiction(s)** = City of Lakewood - Administration
5. **Timeline** = Ongoing
6. **Benefit** = City-Wide
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
	X		
Comments			

Origin

Previous Plan	Current Plan
X	X

Hazard Mitigation Forum

Pierce County Hazard Mitigation Forum

Hazards: E, L, V, D, F, WUI, SW¹, MM²

Lakewood will work in conjunction with the County through the Pierce County Hazard Mitigation Forum (HMF). The Forum will continue as a means of coordinating mitigation planning efforts among all jurisdictions within the County that have completed a mitigation plan. This ensures efficient use of resources and a more cooperative approach to making a disaster resistant county. The HMF meets annually; every October. This is addressed in the Plan Maintenance Section of this Plan.

1. **Goal(s) Addressed** = Protect Life and Property; Promote A Sustainable Economy; Ensure Continuity of Operations; Increase Public Preparedness for Disasters; Preserve or Restore Natural Resources; Establish and Strengthen Partnerships for Implementation.
2. **Cost of Measure** = Minor
3. **FUNDING SOURCE AND SITUATION** = FUNDING COULD BE OBTAINED THROUGH LOCAL BUDGET.
4. **Lead Jurisdiction(s)** = PC DEM; City of Lakewood
5. **Timeline** = Ongoing
6. **Benefit** = Regional
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
	X		
Comments			

Origin

Previous Plan	Current Plan
X	X

City Government Mitigation Measures

Complete, Distribute, and Train on a Multi-Agency response to Transportation Accident Plan

Hazards: MM²

Lakewood will develop a Multi-Agency plan for responding to transportation accidents within the City. Sometime during 2020, high-speed passenger rail service is expected to resume along the existing tracks, which cut through the heart of the commercial and commuting corridor of the City. Interstate 5, with large volumes of commercial vehicle traffic, also travels through the City. Lakewood will work with our response partners to develop a plan for coordinating a response to large-scale transportation accidents using NIMS practices.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Establish and Strengthen Partnerships for Implementation, Increase Public Preparedness for Disasters, Promote a Sustainable economy.
2. **Cost of Measure** = TBD
3. **FUNDING SOURCE AND SITUATION** = FUNDING COULD BE OBTAINED THROUGH LOCAL BUDGET OR GRANTS.
4. **Lead Jurisdiction(s)** = City of Lakewood
5. **Timeline** = Short-term
6. **Benefit** = City-Wide
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
Comments			
This is a new mitigation strategy based on recent transportations accidents that have been experienced in the area.			

Origin

Previous Plan	Current Plan
	X

Purchase and Install Generators at Road Operations & Parks Maintenance Shops

Hazards: E, SW¹ MM²

Purchase and install generators at the Road Operations and Parks Maintenance shops so in the event power is lost, the operations center will have power to coordinate Road Operations activities. In addition the fuel for all Road Operations and Parks vehicles and equipment is located at the Parks Maintenance shop, which needs to power to operate. Both of these functions are critical to the City's ability to respond to natural and man-made disasters, particularly those affecting our critical infrastructure.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Increase Public Preparedness for Disasters, Promote a Sustainable economy.
2. **Cost of Measure** = \$20,000
3. **Funding Source and Situation** = There is no current funding source for tis strategy.
4. **Lead Jurisdiction(s)** = City of Lakewood
5. **Timeline** = Short-Term
6. **Benefit** = Road Operations and Parks Operations
7. **Life of Measure** = 30 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
Comments			
This is a new strategy. Under the previous plan, we were able to complete the electrical wiring necessary to attach generators to the facilities. In the next five years, we hope to install the actual generators.			

Origin

Previous Plan	Current Plan
	X

Identify Critical Routes and Determine Alternate Routes

Hazards: E, F, SW¹

The City is unique with its bodies of water and neighborhoods surrounded by other jurisdictional boundaries – effectively making them islands with one route in and out. Determine these isolated areas and determine alternate access routes (e.g. via through private property, via Fort/Base lands, etc.) and develop agreements as needed that can be implemented during an emergency.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Establish and Strengthen Partnerships for Implementation.
2. **Cost of Measure** = TBD
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Public Works
5. **Timeline** = Long-term
6. **Benefit** = Traveling public and/or emergency vehicles over specific route.
7. **Life of Measure** = 100 years

8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
			X
Comments			
This was in our last plan but was deferred. We want to include in the new plan.			

Origin

Previous Plan	Current Plan
X	X

Radio Communications Set-Up Between all City-Owned Vehicles, Buildings, and EOC

Hazards: E, F, SW¹

Provide radios for back-up radio communication (when all of the cell phones go down).
Determine if communication can also be set up with other public agencies providing mutual aid.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations.
2. **Cost of Measure** = TBD
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants and state or federal grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Public Works
5. **Timeline** = Short-term
6. **Benefit** = Traveling public and/or emergency vehicles over specific route.
7. **Life of Measure** = 100 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
			X
Comments			
This was in our last plan but was deferred. We want to include in the new plan.			

Origin

Previous Plan	Current Plan
X	X

Evacuation Plan Template for Multiple Events

Hazards: E, V, F, WUI, SW¹, MM²

The police department will develop an SOP that will outline recommended techniques and procedures for conducting an evacuation of an area or complex in the event of different types of emergencies such as a train derailment, active shooter, bomb threat, or other events.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Establish and Strengthen Partnerships for Implementation.
2. **Cost of Measure** = Staff Time
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood - Police Department and Lakewood Fire
5. **Timeline** = Ongoing
6. **Benefit** = Residents and businesses of Lakewood, visitors and regional partners
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
			X
Comments			
This was in our last plan but was deferred. We want to include in the new plan.			

Origin

Previous Plan	Current Plan
X	X

Seismic Review and Retrofit of all Bridges, Culverts, and Retaining Walls within and Through the City

Hazards: E, F, SW¹

All City bridges, culverts, and wall structures will be reviewed for seismic vulnerability and retrofitted as needed. These include WSDOT and RR bridges that are within City's jurisdictional boundaries.

1. **Goal(s) Addressed** = Protect Life and Property, Establish and Strengthen Partnerships for Implementation; Promote a Sustainable Economy.
2. **Cost of Measure** = TBD
3. **FUNDING SOURCE AND SITUATION** = FUNDING COULD BE OBTAINED THROUGH LOCAL BUDGETS GRANTS AND FEDERAL BRAC FUNDING.
4. **Lead Jurisdiction(s)** = City of Lakewood – Public Works
5. **Timeline** = Long-Term
6. **Benefit** = Traveling public and/or emergency vehicles over specific route.
7. **Life of Measure** = 100 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
		X	
Comments			
Load rating of all bridges complete. Now we need seismic review of each bridge.			

Origin

Previous Plan	Current Plan
X	X

Battery Back-Up (Uninterrupted Power Supply (UPS)) for All Major Traffic Signals

Hazards: E, F, SW¹

Provide UPS (battery back-up) for all major traffic signals in the City.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations.
2. **Cost of Measure** = TBD
3. **Funding Source and Situation** = Funding could be obtained through local budgets or grants and federal ITS grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Public Works
5. **Timeline** = Long-term
6. **Benefit** = Traveling public and/or emergency vehicles over specific routes.
7. **Life of Measure** = 100 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
		30% complete	
Comments			
Work in progress. Want to keep in new plan.			

Origin

Previous Plan	Current Plan
X	X

Complete, Distribute and Train Staff on Continuity of Operations Plan (COOP)

Hazards: E, V, F, WUI, SW¹, MM²

The City of Lakewood will develop a COOP that enables staff to prepare for an emergency or disaster situation.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Establish and Strengthen Partnerships for Implementation; Increase Public Preparedness for Disasters; Promote a Sustainable Economy.
2. **Cost of Measure** = Staff time and materials
3. **Funding Source and Situation** = Funding could be obtained through local budget.
4. **Lead Jurisdiction(s)** = City of Lakewood
5. **Timeline** = Ongoing
6. **Benefit** = Residents and businesses of Lakewood, visitors and regional partners
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
		X	
Comments			
We completed our COOP revision in June 2019. We will now move forward with training staff, exercising it, and then adjusting the plan.			

Origin

Previous Plan	Current Plan
X	X

Essential Records Protection

Hazards: E, V, F, WUI, SW¹, MM²

Protect and/or provide a safe backup of essential records. This will be accomplished by developing an essential records protection schedule and records prevention response and recovery procedures.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Promote a Sustainable Economy.
2. **Cost of Measure** = Staff time and possible storage fees
3. **Funding Source and Situation** = Funding could be obtained through local budget general fund or grants or Washington State Archives.
4. **Lead Jurisdiction(s)** = City of Lakewood - General Services and City Clerk
5. **Timeline** = Short-term
6. **Benefit** = City Hall employees, Community and citizens, regional partners
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
		X	
Comments			

Efforts to protect paper records have made progress. There has been substantial progress in upgrading and hardening the electronic storage facilities for essential records. We are close to having two separate electronic servers, in two different locations constantly backing each other up, providing full redundancy.

Origin

Previous Plan	Current Plan
X	X

Capability Identification and Evaluation

Hazards: E, L, V, D, F, WUI, SW¹, MM²

Lakewood will develop a consistent and replicable system for evaluating the City's capabilities. A comprehensive evaluation will lead to specific policy recommendations to more effectively achieve disaster resistant communities. Further, a capability evaluation involves measurable variables so that capabilities may eventually be tracked in conjunction with the implementation of all mitigation measures. This is a key component in evaluating the success of the City's overall mitigation strategy.

1. **Goal(s) Addressed** = N/A. Goals addressed are contingent upon the mitigation measures resulting from this priority.
2. **Cost of Measure** = TBD
3. **FUNDING SOURCE AND SITUATION** = FUNDING COULD BE OBTAINED THROUGH LOCAL BUDGET OR GRANTS.
4. **Lead Jurisdiction(s)** = City of Lakewood
5. **Timeline** = Short-term
6. **Benefit** = City-Wide
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
	X		
Comments			
Ongoing strategy with constant updates.			

Origin

Previous Plan	Current Plan
X	X

Illicit Discharge and Spill Response

Hazards: MM²

- Have trained personnel and materials on hand to respond to reports of illicit discharges and spills to surface and groundwater in the city.
 - Have contact information for spill response personnel from the state Department of Ecology and Pierce County.
 - Educate the public and target audiences (e.g., businesses with high potential for spills) regarding the impacts and consequences of illicit discharges.
1. **Goal(s) Addressed** = Protect Life and Property; Increase Public Preparedness for Disasters; Preserve or Restore Natural Resources; Establish and Strengthen Partnerships for Implementation.
 2. **Cost of Measure** = TBD
 3. **Funding Source and Situation** = Funding could be obtained through local budget or grants and state or federal grants.
 4. **Lead Jurisdiction(s)** = City of Lakewood - Public Works Department
 5. **Timeline** = Ongoing
 6. **Benefit** = Water bodies and groundwater in Lakewood/Citizens of Lakewood
 7. **Life of Measure** = Perpetual
 8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
	X		
Comments			
Continual strategy.			

Origin

Previous Plan	Current Plan
X	X

Develop Mutual Aid Agreements with Other Public Agencies to Support in Roadway Clearing and Repair

Hazards: E, F, SW¹

The City has no equipment of its own at this time. Therefore, the City would need outside assistance of equipment and operators to assist in clearing and repairing roadways. Other public agencies within Lakewood have readily available equipment and could be utilized for local support. For example: Lakewood Water District, Lakeview Light and Power, Pierce County Public Works, WSDOT, Camp Murray, Fort Lewis.

1. **Goal(s) Addressed** = Protect life and property; Ensure continuity of operations; Establish and strengthen partnerships for implementation.
2. **Cost of Measure** = TBD
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Public Works
5. **Timeline** = Short-term
6. **Benefit** = Traveling public and/or emergency vehicles over specific route.

7. **Life of Measure** = 100 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
	X		
Comments			
Ongoing strategy.			

Origin

Previous Plan	Current Plan
X	X

Significant Tree Pruning and Maintenance

Hazards: E, D, SW¹

Trees within the City of Lakewood Parks system will be pruned or removed according to a plan developed in 2005 to enhance the overall health and safety of the park trees.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Preserve or Restore Natural Resources; Increase Public Preparedness for Disasters.
2. **Cost of Measure** = \$15,000
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Parks Department
5. **Timeline** = Ongoing
6. **Benefit** = Park staff and City of Lakewood
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal would be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
	X		
Comments			
Continual strategy			

Origin

Previous Plan	Current Plan
X	X

Utility Coordination

Hazards: E, L, V, F, SW, WUI¹, MM²

Facilitate coordination of utility emergency response contacts.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Establish and Strengthen Partnerships for Implementation; Ensure Continuity of Operations.
2. **Cost of Measure** = Staff Time
3. **Funding Source and Situation** = Funding could be obtained through local budgets or grants and state or federal grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Public Works Department
5. **Timeline** = Ongoing
6. **Benefit** = City Departments, staff and citizens
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
	X		
Comments			
Continual strategy			

Origin

Previous Plan	Current Plan
X	X

Accurate Mapping of All Roadways- Electronic and Hard Copy

Hazards: E, L, F, SW¹, MM²

Mapping of all Cities public and private roadways will be mapped accurately in order to track and communicate blocked and alternate routes. This will be available electronically via network computer access; local computer access; and hard copy (for redundancy) and in case of power loss.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations.
2. **Cost of Measure** = TBD
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood - GIS
5. **Timeline** = Ongoing
6. **Benefit** = ALL traveling public, emergency and utility responders
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X	X		
Comments			
This has been completed but is a continual strategy to keep it up to date.			

Origin

Previous Plan	Current Plan
X	X

Traffic Control Devices for Setting Blockades and Detour Routes

Hazards: E, L, F, SW¹, MM²

Obtain trailer and traffic control devices (barricades, barrels, signs, etc.) for setting blockades and detour routes in case of road hazard.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations.
2. **Cost of Measure** = TBD
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants and state or federal grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Public Works
5. **Timeline** = Short-term
6. **Benefit** = ALL traveling public, emergency and utility responders
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X	X		
Comments			
This was essentially completed with the purchase of a trailer containing multiple types of barricades. However, we continue to add and replace additional barricades, so this could be continual.			

Origin

Previous Plan	Current Plan
X	X

Generator Retrofit of Park Operations Center

Hazards: E, SW¹ MM²

As part of the electrical upgrades to the shop, a generator switch will be installed so in the event power is lost, the operations center will have power to operate the fueling station, and most power operations in the shop.

9. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Increase Public Preparedness for Disasters.
10. **Cost of Measure** = Already implemented as electrical upgrades to operation center
11. **Funding Source and Situation** = Funding could be obtained through local budget and grants.
12. **Lead Jurisdiction(s)** = City of Lakewood - Parks Department

13. **Timeline** = Short-Term
 14. **Benefit** = Parks Operation Center, City of Lakewood
 15. **Life of Measure** = 30 years
 16. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X			
Comments			
This was completed.			

Origin

Previous Plan	Current Plan
X	X

Structural Retrofit of Park Operations Center

Hazards: E, SW¹

Collar ties were added to the roof to strengthen the integrity of the Park Operations Center. Posts and walls were added to strengthen the loft inside the shop, and a sheer wall was added inside to add strength against side to side movement.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Increase Public Preparedness for Disasters.
2. **Cost of Measure** = Already implemented as part of upgrades made to the operations center.
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Parks Department
5. **Timeline** = Short-term
6. **Benefit** = Park Operations, City of Lakewood
7. **Life of Measure** = 30 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X			
Comments			
This was completed.			

Origin

Previous Plan	Current Plan
X	X

Create an Emergency Operations Center in Lakewood City Hall

Hazards: E, V, F, WUI, SW¹, MM²

The City of Lakewood will create an emergency operations center that has the necessary equipment for city EOC staff to operate effectively.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Establish and Strengthen Partnerships for Implementation; Increase Public Preparedness for Disasters; Promote a Sustainable Economy.
2. **Cost of Measure** = Time and Materials and special equipment
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood - Emergency Management Director
5. **Timeline** = Short-term
6. **Benefit** = Residents and businesses of Lakewood, visitors, community and regional partners
7. **Life of Measure** = 5-10 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X			
Comments			
We created an EOC, but it was done in the Police Station and not City Hall. But it is complete.			

Origin

Previous Plan	Current Plan
X	X

National Flood Insurance Program

Hazards: F

Lakewood will ensure that the City is compliant with the National Flood Insurance Program by updating floodplain identification and mapping, enforcing the flood damage prevention ordinance, and providing public education on floodplain requirements and impacts. The City of Lakewood will be an active participant in the Pierce County Flood Control District.

1. **Goal(s) Addressed** = Protect life and property; Ensure Continuity of Operations; Increase Public Preparedness; Increase and Strengthen Partnerships; Protect the Environment; Increase Public Preparedness
2. **Cost of Measure** = Staff time, special materials required, permits
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants
4. **Lead Jurisdiction(s)** = Lakewood (Community Development); PC PWU
5. **Timeline** = On-going
6. **Benefit** = City-wide; Regional
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X			
Comments			
The City amended its floodplain regulations to be compliant back in 2017 or 2018. This was followed by adopting updated shoreline regulations. The City has since found an error in the federal floodplain map and has submitted a request to amend the map to fix this.			

Origin

Previous Plan	Current Plan
X	X

Public Education Mitigation Measures

Flood Preparedness and Response

Hazards: F¹

- Have sandbags and sand on hand and placed at strategic locations throughout the city.
 - Have a stockpile of signs and barricades for warning of water over roadways or road closures.
 - Have maps, rakes, boots, rain gear and other equipment on hand for City staff to assist in flood response.
 - Coordinate with the Pierce County Lakewood Road Shop and DEM for their assistance with flood response.
 - Coordinate personnel resources for sandbagging parties.
 - Have contracts on hand to hire private contractors to assist as needed.
1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Preserve or Restore Natural Resources; Establish and Strengthen Partnerships for Implementation.
 2. **Cost of Measure** = TBD
 3. **Funding Source and Situation** = Funding could be obtained through local budget or grants and state or federal grants.
 4. **Lead Jurisdiction(s)** = City of Lakewood - Public Works Department
 5. **Timeline** = Ongoing
 6. **Benefit** = Areas of city affected by flooding (neighborhoods, businesses, etc.)
 7. **Life of Measure** = Perpetual
 8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X	X		
Comments			
We have substantially completed this strategy, however, we continue to enhance with additional equipment and supplies.			

Origin

Previous Plan	Current Plan
X	X

Disaster Preparedness Training

Hazards: E, V, F, WUI, SW¹, MM²

Develop and implement training program for city employees on disaster preparedness.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations.
2. **Cost of Measure** = Staff time and materials
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants and state or federal grants.
4. **Lead Jurisdiction(s)** = City of Lakewood - Human Resources and PC Health Department
5. **Timeline** = Short-term
6. **Benefit** = City staff and citizens, visitors and regional partners
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X	X		
Comments			
We trained 100% of City Staff by 2018. However, we will continue this strategy as refresher training and for new employees.			

Origin

Previous Plan	Current Plan
X	X

Endnotes

¹ Hazard Codes:

WHERE NECESSARY, THE SPECIFIC HAZARDS ADDRESSED ARE NOTED AS FOLLOWS:

A:	AVALANCHE
E:	EARTHQUAKE
F:	FLOOD
D:	DROUGHT
T:	TSUNAMI
V (L OR T):	VOLCANIC (LAHAR OR TEPHRA-SPECIFIC)
SW:	SEVERE STORM (WIND-SPECIFIC)
L:	LANDSLIDE
WUI:	WILDLAND/URBAN INTERFACE FIRE
MM:	MANMADE TO INCLUDE TERRORISM
ALL:	ALL HAZARDS, INCLUDING SOME MAN MADE. WHERE ONLY NATURAL HAZARDS ARE ADDRESSED, IT IS NOTED.

² While this Plan is strictly a *Natural* hazard mitigation plan, where a measure stems from a facility recommendation (Infrastructure Section) that deals specifically with terrorism, the mitigation strategy will use that analysis. Other measures, such as those that deal with multi-hazard community preparedness or recovery planning, mitigate man-made hazards and are noted as such. It is not the intent of this notation to imply that all measures were analyzed with regards to man-made hazards or that measures were identified with that in mind. Rather, the notation merely illustrates the potential on this template for the inclusion of man-made hazard analysis.

Section 6

Infrastructure Requirements

Assessing Vulnerability: Identifying Structures---Requirement §201.6(c)(2) (ii)(A):

The plan **should** describe vulnerability in terms of the types and numbers of existing and future buildings, infrastructure, and critical facilities located in the identified hazard areas.

- Does the new or updated plan describe vulnerability in terms of the types and numbers of existing buildings, infrastructure, and critical facilities located in the identified hazard areas?
- Does the new or updated plan describe vulnerability in terms of the types and numbers of future buildings, infrastructure, and critical facilities located in the identified hazard areas?

Assessing Vulnerability: Estimating Potential Losses---Requirement §201.6(c)(2) (ii)(B):

The plan **should** describe vulnerability in terms of an] estimate of the potential dollar losses to vulnerable structures identified in paragraph (c)(2)(i)(A) of this section and a description of the methodology used to prepare the estimate.

- Does the new or updated plan estimate potential dollar losses to vulnerable structures?
- Does the new or updated plan describe the methodology used to prepare the estimate?

SECTION 6

REGION 5 ALL HAZARD MITIGATION PLAN 2015-2020 EDITION CITY OF LAKEWOOD INFRASTRUCTURE SECTION

The Infrastructure Section is exempt from public disclosure pursuant to RCW 42.56.420. Request for public disclosure of this document or parts thereof should be referred immediately to the City of Lakewood's City Manager.

Distribution or changes to this document without the express written consent of the City of Lakewood's City Manager is prohibited.

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Section 7

Plan Maintenance Procedures Requirements

Monitoring, Evaluating, and Updating the Plan---Requirement §201.6(c)(4)(i):

[The plan maintenance process **shall** include a] section describing the method and schedule of monitoring, evaluating, and updating the mitigation plan within a five-year cycle.

- Does the new or updated plan describe the method and schedule for monitoring the plan, including the responsible department?
- Does the new or updated plan describe the method and schedule for evaluating the plan, including how, when and by whom (i.e. the responsible department)?
- Does the new or updated plan describe the method and schedule for updating the plan within the five-year cycle?

Incorporation into Existing Planning Mechanisms---Requirement §201.6(c)(4) (ii):

[The plan **shall** include a] process by which local governments incorporate the requirements of the mitigation plan into other planning mechanisms such as comprehensive or capital improvement plans, when appropriate...

- Does the new or updated plan identify other local planning mechanisms available for incorporating the mitigation requirements of the mitigation plan?
- Does the new or updated plan include a process by which the local government will incorporate the mitigation strategy and other information contained in the plan (e.g., risk assessment) into other planning mechanisms, when appropriate?
- Does the updated plan explain how the local government incorporated the mitigation strategy and other information contained in the plan (e.g., risk assessment) into other planning mechanisms, when appropriate?

Continued Public Involvement---Requirement §201.6(c)(4) (iii):

[The plan maintenance process **shall** include a] discussion on how the community will continue public participation in the plan maintenance process.

- Does the new or updated plan explain how continued public participation will be obtained? (For example, will there be public notices, an on-going mitigation plan committee, or annual review meetings with stakeholders?)

SECTION 7

REGION 5 ALL HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD PLAN MAINTENANCE SECTION

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The updated planning process began in the spring of 2019 and is continuing to build on the foundation of breaking the disaster cycle by planning for a disaster resistant City of Lakewood and Pierce County Region 5. This Section details the formal process that will guarantee the City of Lakewood Hazard Mitigation Plan remains an active and relevant document. The Plan Maintenance Section includes a description of the documentation citing the Plan's formal adoption by the Lakewood City Council. The Section also describes: the method and schedule of monitoring, evaluating, and updating within a five-year cycle; the process for incorporating the mitigation strategy into existing mechanisms; and the process for integrating public participation throughout the plan maintenance. The section serves as a guide for implementation of the hazard mitigation strategy.

Plan Adoption

Upon completion of the City of Lakewood Plan, it will be submitted to Washington State Emergency Management Division (EMD) for a Pre-Adoption Review. The EMD has 30 days to then take action on the Plan and forward it to the Federal Emergency Management Agency (FEMA) Region X for review. This review, which is allowed 45 days by law, will address the federal criteria outlined in FEMA Interim Final Rule 44 CFR Part 201.6. In completing this review there may be revisions requested by the EMD and/or FEMA. Revisions could include changes to background information, editorial comments, and the alteration of technical content. Pierce County Department of Emergency Management (PC DEM) will call a Planning Team Meeting to address any revisions needed and resubmit the changes.

The Lakewood City Council is responsible for the adoption of the Plan after the Pre-Adoption Review by the EMD and the FEMA Region X. Once the City adopts the Plan, the Police Chief will be responsible for submitting it, with a copy of the resolution, to the State Hazard Mitigation Officer at the Washington State EMD. EMD will then take action on the Plan and forward it to the FEMA Region X for final approval. Upon approval by FEMA, the City will gain eligibility for both Hazard Mitigation Grant Program and Pre-Disaster Mitigation Grant Program funds.

Appendix A will list the dates and include a copy of the signed Resolution from the jurisdiction as well as a copy of the FEMA approval of the jurisdiction's Plan. In future updates of the Plan, Appendix C will be used to track changes and/or updates. This plan will have to be re-adopted and re-approved prior to the five year deadline of _____ 2025.

Maintenance Strategy

The City of Lakewood maintenance strategy for implementation, monitoring, and evaluation provides a structure that encourages collaboration, information transference, and innovation. Through a multi-tiered implementation method, the City of Lakewood will provide its citizens a highly localized approach to loss reduction while serving their needs through coordinated policies and programs. The method's emphasis, on all levels of participation, promotes public involvement and adaptability to changing risks and vulnerabilities. Finally, it will provide a tangible link between citizens and the various levels of government service, ranging from

community action to the Department of Homeland Security. Through this strategy, Lakewood will continue to break the disaster cycle and achieve a more disaster resistant community.

Implementation

In the past, a dedicated effort to promote the Mitigation Plan within the city’s “Family of Plans” was not as strong as it could have been. There has been some staff/middle-management turnover which may account for some of this as well as the lack of understanding of what mitigation is and how it can easily crosswalk into other plans.

In order to ensure efficient and effective implementation, the City of Lakewood will implement its mitigation strategy over the next five years primarily through its annual plan review, budget process and varying grant application processes. It is the intent of the City to better integrate and implement the Mitigation Plan with our external partners and plans managed by internal departments such as *Public Works, Engineering and Community Development in addition to Emergency Management*.

The City Administration will work in conjunction with those departments identified in both the Capability Identification Section and under each mitigation measure to initiate the mitigation strategy. For example, any infrastructure-related measures will be jointly implemented through the Capital Improvement Plan and the various departments involved through the normal budget schedule. Any regulatory and land use measures will continue to be implemented through collaboration with Community Development and its updates of the City’s Comprehensive Plan. Other measures will be implemented through collaboration with the identified jurisdictions/departments listed under each measure’s evaluation and through the mechanisms and funding sources identified in the Capability Identification Section.

These efforts fall under a broader implementation strategy that represents a county-wide effort. This strategy must be adaptable to change while being consistent in its delivery.

The mitigation implementation strategy is a three-tiered method that emphasizes localized needs and vulnerabilities while addressing Lakewood’s as well as multi-jurisdictional policies and programs. The first tier is implementation through individual citizen level—existing Public Education Programs in the City of Lakewood (for example, at the individual level through the Public Safety Fair and at the neighborhood level through PC NET). The second is the City-Wide mechanism for implementation, in this case the City Administration. The third tier is a more external and multi-jurisdictional mechanism, the Hazard Mitigation Forum (HMF).

This method ensures that implementation speaks to unique vulnerabilities at the most local level, allows for coordination among and between levels, and promotes collaboration and innovation. Further, it provides a structured system of monitoring implementation. Finally, it is a method that can adapt to the City’s changing vulnerabilities, the region, and the times. These three levels and their means of implementation and collaboration are described below.

Public Education Programs

At the individual citizen level, Lakewood's public education programs provide the City with a localized mechanism for implementation. This approach to mitigation can adapt to the varying vulnerabilities and needs within a growing City and region. Public education programs are also a means for involving the public in mitigation policy development. Departments conducting mitigation-related programs will provide the existing targeted neighborhoods and special-needs populations a catalogue of mitigation measures from which individuals can choose those that would be most effective in their neighborhood.

Jurisdiction-Wide: City Administration

The City Administration will be the body responsible for determining the direction of the Plan's implementation. The City Administration is responsible to the City Council for the day-to-day operations of the City and its departments, the annual budget, and personnel. The City Manager follows the general policy as set by the City Council. The City Manager's office is responsible for the City's selection, evaluation, and training of all city staff. It oversees, coordinates, and manages the activities of all city departments and offices in carrying out the requirements of ordinances, laws, rules and regulations.

Initially, the City Manager's office will be responsible for the overall review of the plan and will designate mitigation measures to those departments responsible for their implementation. This will be done with assistance from both the Police Chief and the Emergency Management Coordinator. The Emergency Management Coordinator will address the Plan on an annual basis during the month of October. The Police Chief and Emergency Management Coordinator will monitor the plan's implementation throughout the year and report to the City Manager at this annual meeting. Evaluation and updates will be completed at this meeting. Recommendations will be made to coincide with the normal budgeting processes and provide an ample time period for review and adoption of any necessary changes to the implementation schedule.

Eventually, the City Manager's office may choose to cede this responsibility to a special committee. This committee would be composed of representatives from the departments identified in the Capability Identification Section as having a role in hazard mitigation. The Committee would ultimately provide a mechanism for coordination among those departments engaged in mitigation to ensure that a comprehensive and efficient approach be undertaken in the City's efforts at all-hazards mitigation.

Hazard Mitigation Forum

The PC Hazard Mitigation Forum (HMF) represents a broader and multi-jurisdictional approach to mitigation implementation. The PC HMF will be comprised of all representatives from unincorporated Pierce County and all jurisdictions, partially or wholly, within its borders, that have undertaken mitigation planning efforts. The PC HMF will serve as coordinating body for projects of a multi-jurisdictional nature and will provide a mechanism to share successes and increase the cooperation necessary to break the disaster cycle and achieve a disaster resistant Pierce County. Members of the PC HMF will include the following jurisdictions who have completed, or who have begun the process of completing, DMA 2000 compliant plans:

- City of Bonney Lake
- City of Bonney Lake
- City of DuPont
- City of Fife
- City of Gig Harbor
- City of Milton
- City of Puyallup
- City of Sumner
- City of University Place
- Town of Eatonville
- Town of Steilacoom
- Unincorporated Pierce County
- East Pierce Fire and Rescue #22
- Graham Fire and Rescue #21
- Orting Valley Fire and Rescue #18
- Riverside Fire and Rescue #14
- Anderson Island Fire and Rescue #27
- West Pierce Fire and Rescue #3
- Clover Park School District
- Eatonville School District
- Franklin Pierce School District
- Pacific Lutheran University
- Puyallup School District
- Sumner School District
- University Place School District
- Crystal River Ranch HOA
- Pierce Transit
- Riviera Community Club
- Clear Lake Water District
- Fruitland Mutual Water Company
- Lakeview Light and Power
- City of Buckley
- City of Buckley
- City of Edgewood
- City of Fircrest
- City of Lakewood
- City of Orting
- City of Roy
- City of Tacoma
- Town of Carbonado
- Town of South Prairie
- Town of Wilkeson
- Central Pierce Fire and Rescue #6
- Gig Harbor Fire and Medic One #5
- Key Peninsula Fire Department #16
- Browns Point Fire Department #13
- Ashford Elbe Fire District #23
- South Pierce Fire and Rescue #17
- Carbonado School District
- Dieringer School District
- Fife School District
- Orting School District
- Peninsula School District
- Steilacoom School District
- Tacoma School District
- Crystal Village HOA
- Metropolitan Park District
- Port of Tacoma
- Taylor Bay Beach Club
- Firgrove Mutual Water Company
- Graham Hill Mutual Water Company
- Lakewood Water District

- Mt. View-Edgewood Water Company
- Parkland Light and Water Company
- Spanaway Water Company
- Valley Water District
- Community Health Care
- Kaiser Permanente
- Western State Hospital
- Tacoma Pierce County Health Dept.
- Ohop Mutual Light Company
- Peninsula Light Company
- Summit Water and Supply Company
- Cascade Regional Blood Services
- Franciscan Health System
- MultiCare Health System
- Puyallup Tribe of Indians
- Bethel School District

Coordinated by the PC DEM, the PC HMF will meet annually in November. The City of Lakewood will be an active participant in the PC HMF and will be represented by the designated Planning Partner or their representative. Only through this level of cooperation can these jurisdictions meet all their mitigation goals.

Regional Mitigation Planning

Pierce County, Region 5 was configured into 5 planning groups based on a commonality in geographical hazards for the 2020-2025 mitigation plan update to foster relationship building and resiliency planning amongst jurisdictions. Although much of the meeting and planning time focused on plan updates and fostered relationship building the resiliency planning component will continue within multi-jurisdictional groups working together to further reduce risk. This provides another opportunity for continued collaboration planning amongst jurisdictions working and partnering together. The meeting frequency will be driven by the mitigation implementation strategy and combines the three-tiered approach. The City of Lakewood will continue to engage within the “southwest group” geographical planning area and will provide the specific department representative to engage in and implement mitigation activities within this geographical group.

Plan Evaluation and Update

It should be noted this planning process began in early 2019 following the then current CFR 201.6 Hazard Mitigation Planning Requirements. Based on new requirements in the Stafford Act, the City of Lakewood will evaluate and update the plan to incorporate these new requirements as necessary. Furthermore, if there are additional Stafford Act changes affecting CFR 201.6 in the coming years, the planning process will incorporate those as well.

The Region 5 Plan will guide the City’s mitigation efforts for the foreseeable future. City of Lakewood representatives on the Planning Team have developed a method to ensure that regular review and update of the Plan occur within a five year cycle. The Emergency Management Coordinator will coordinate any reviews through October meeting noted above.

PC DEM will collaborate with the emergency management coordinator and the PC HMF to monitor and evaluate the mitigation strategy implementation. PC DEM will track this

implementation through Pierce County's GIS database. Findings will be presented and discussed at the annual meeting.

Police and Community Development Department will provide a report of the Plan's implementation to the City Manager at the annual meeting. This report will drive the meeting agendas and will include the following:

- Updates on implementation throughout the City;
- Updates on the PC HMF and mitigation activities undertaken by neighboring jurisdictions;
- Changes or anticipated changes in hazard risk and vulnerability at the City, county, regional, State, FEMA and Homeland Security levels;
- Problems encountered or success stories;
- Any technical or scientific advances that may alter, make easier, or create measures.

The City Manager and local experts will decide on updates to the strategy based on the above information and a discussion of:

- The various resources available through budgetary means as well as any relevant grants;
- The current and expected political environment and public opinion;
- Meeting the mitigation goals with regards to changing conditions.

PC DEM will work with the Emergency Management Coordinator to review the Risk Assessment Section to determine if the information should be updated or modified based on new information. This will be done during the regularly scheduled reviews of the Hazard Identification and Vulnerability Assessment and the Comprehensive Emergency Management Plan.

Additional reviews of this Plan will be required following disaster events and will not substitute for the annual meeting. Within ninety days following a significant disaster or an emergency event impacting the City, the emergency management coordinator will provide an assessment that captures any "success stories" and/or "lessons learned." The assessment will detail direct and indirect damages to the City and its infrastructure, response and recovery costs, as part of the standard recovery procedures that use EMD Forms 129, 130, and 140. This process will help determine any new mitigation initiatives that should be incorporated into the Plan to avoid similar losses due to future hazard events. In this manner, recovery efforts and data will be used to analyze mitigation activities and spawn the development of new measures that better address any changed vulnerabilities or capabilities. Any updates to the Plan will be addressed at the annual October meeting.

As per 44 CFR 201.6, the City of Lakewood must re-submit the Plan to the State and FEMA with any updates every five years. This process will be coordinated by PC DEM through the Pierce County Hazard Mitigation Forum. In 2025 and every five years following at the Hazard Mitigation Forum, the City of Lakewood City Manager and/or the Emergency

Management Coordinator will submit the updated plan to PC DEM. PC DEM's Mitigation and Recovery Program Manager will collect updates from the Region 5 Plan jurisdictions and submit them to the State EMD and FEMA.

Continued Public Involvement

The City of Lakewood is dedicated to continued public involvement and education in review and updates of the Plan. The Emergency Management Coordinator and those departments with hazard mitigation measures identified will retain copies of the Plan and will post it on the City of Lakewood's website.¹ Announcements regarding the Plan's adoption and the annual updates to the Plan will be advertised on the City of Lakewood website.

The three-tiered implementation method provides an opportunity for continuous public involvement. Public Education campaigns are a means of informing the public on updates and implementation activities. Further, prior to submitting the Plan to WA EMD and FEMA for the five year review, the City of Lakewood will hold a public information and comment meeting. These meetings will provide the public a forum for which it can express its concerns, opinions, or ideas about Lakewood. These meetings will be advertised in the City of Lakewood through a variety of media, including the city's "Connections" newsletter and local newspaper

The City of Lakewood will conduct a review on a yearly basis to ensure all elements of the mitigation plan are updated and accurate. Each of the 76 jurisdictions has been tasked with having to provide documentation on public involvement including a brief description for each public hearing held, a summary on attendance, any feedback received from the public and the an overall description of what was accomplished. Even further, the City of Lakewood will provide proof of their attempts for public involvement such as screenshots of websites including date ranges, flyers and other relevant material documenting the public involvement process. Lastly, the City of Lakewood will look for new innovative ways for public involvement.

Endnotes

¹<https://www.cityoflakewood.us/>

APPENDIX A

REGION 5 ALL HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD

Plan Adoption

The “Region 5 Hazard Mitigation Plan” was adopted by the City of Lakewood’s City Council on XXX by resolution number XXX. The following page shows a copy of that resolution.

(Placeholder for Resolution)

(Placeholder for Resolution)

The plan was reviewed and approved as follows:

AGENCY	REPRESENTATIVE	DATE
Washington State Military Dept., Emergency Management Division	Tim Cook Hazard Mitigation Programs Manager	Approved—
FEMA Region X	Tamra Biasco Chief, Risk Analysis Branch Mitigation Division	Approved—

FEMA letter of pre-approval and letter of approval follows below.

(Placeholder for FEMA-Approval Letter)

(Placeholder for FEMA-Approval Letter, page 2)

(Placeholder for FEMA-Approval Letter, page 3)

APPENDIX A

REGION 5 ALL HAZARD MITIGATION PLAN 2015-2020 EDITION CITY OF LAKEWOOD

Plan Adoption

The “Region 5 Hazard Mitigation Plan” was adopted by the City of Lakewood’s City Council on July 6, 2015 by resolution number 2015-21. The following page shows a copy of that resolution.

RESOLUTION NO. 2015-21

A RESOLUTION of the City Council of the City of Lakewood adopting the Region 5 All Hazard Mitigation Plan – 2015-2020 Edition and the City of Lakewood Addendum to the Region 5 Hazard Mitigation Plan; and Updating the 2004 Pierce County Natural Hazard Mitigation Plan.

WHEREAS, the Federal Disaster Mitigation Act of 2000 requires that for all disasters declared on or after November 1, 2004, applicants for sub-grants following any disaster must have an approved All Hazard Mitigation Plan in accordance with 44CFR 201.6 prior to receipt of Hazard Mitigation Grant Program project funding; and

WHEREAS, the Federal Disaster Mitigation Act of 2000 requires that for Pre-Disaster Mitigation grant program project funding on or after November 1, 2003, applicants must have an approved All Hazard Mitigation Plan in accordance with 44CFR 201.6 prior to receipt of project funding; and

WHEREAS, the All Hazard Mitigation Plan Update represents the commitment of the City of Lakewood along with other surrounding government entities to reduce the risks from natural, man-made and technological hazards, serving as a guide for decision makers as they commit resources to reducing the affects of hazards, and it is in the public interest to proceed with the planning process in a timely manner; and

WHEREAS, City of Lakewood has participated with the Pierce County Department of Emergency Management in the development of the City of Lakewood All Hazard Mitigation Plan Update, and recognizes the economic loss, personal injury, and damage that can arise from these hazards; and

WHEREAS, reduction of these impacts can be achieved through a comprehensive coordinated planning process which includes an updated risk assessment that provides the factual basis for activities proposed in the mitigation strategies to reduce losses and vulnerabilities, a five-year cycle for plan maintenance, and documentation of formal adoption by the City of Lakewood; and

WHEREAS, the 2015-2020 Region 5 All Hazard Mitigation Plan Edition has been completed and approved by the State and the Federal Emergency Management Agency; and

WHEREAS, the City of Lakewood could risk not receiving future disaster funding if the All Hazard Mitigation Plan Update is not adopted; and

WHEREAS, the City Council of the City of Lakewood reviewed the All Hazard Mitigation Plan Update.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON HEREBY RESOLVES, as follows:

Section 1. The Region 5 Hazard Mitigation Plan, 2015-2020 Edition, is hereby adopted as set forth in Exhibit A, which is attached.

Section 2. The City of Lakewood Addendum to the Region 5 Hazard Mitigation Plan, an update to the City of Lakewood All Hazard Mitigation Plan is hereby adopted and shall be in full force and effect upon passage and signatures hereon.

PASSED by the City Council this 6th day of July, 2015.


Don Anderson, Mayor

Attest:


Alice M. Bush, City Clerk

Approved as to form:


Heidi Waechter, City Attorney

The plan was reviewed and approved as follows:

AGENCY	REPRESENTATIVE	DATE
Washington State Military Dept., Emergency Management Division	Tim Cook Hazard Mitigation Programs Manager	Approved—
FEMA Region X	Tamra Biasco Chief, Risk Analysis Branch Mitigation Division	Approved— February 2, 2015

FEMA Pre-Adoption Letter and FEMA Letter of approval will follow below.



U.S. Department of Homeland Security
FEMA Region X
Federal Regional Center
130 228th Street, SW
Bothell, WA 98021-8627

FEMA

February 2, 2015

Mr. Tim Cook
Hazard Mitigation Programs Manager
Washington State Emergency Management Division
Building 20, MS TA-20
Camp Murray, Washington 98430-5122

Dear Mr. Cook:

As requested, the U.S. Department of Homeland Security's Federal Emergency Management Agency (FEMA) has completed a pre-adoption review of the **Region 5 Hazard Mitigation Plan**. The plan successfully contains the required criteria, excluding the adoption, for hazard mitigation plans, as outlined in 44 CFR Part 201. This letter serves as Region 10's commitment to approve the plan upon receiving documentation of its adoption by the participating jurisdictions.

The plan will not be formally approved by FEMA until it is adopted. Each jurisdiction is not eligible for mitigation project grants until the plan is formally approved by FEMA.

Please contact our Regional Mitigation Planning Manager, Kristen Meyers, at (425) 487-4543 with any questions.

Sincerely,

Tamra Biasco
Chief, Risk Analysis Branch
Mitigation Division

KM:bb

www.fema.gov



U.S. Department of Homeland Security
FEMA Region 10
130 228th Street, SW
Bothell, Washington 98021-8627

FEMA

October 9, 2018

The Honorable Douglas Richardson
Chair, Pierce County Council
930 Tacoma Avenue South
Tacoma, Washington 98402

Dear Chair Richardson:

On July 23, 2015, the U.S. Department of Homeland Security's Federal Emergency Management Agency (FEMA) Region 10, approved the *Region 5 (Pierce County) Hazard Mitigation Plan* as a multi-jurisdictional local plan as outlined in Code of Federal Regulations Title 44 Part 201. This approval provides the below jurisdictions eligibility to apply for the Robert T. Stafford Disaster Relief and Emergency Assistance Act's, Hazard Mitigation Assistance grants through July 22, 2020, through your state.

COUNTY / CITIES / TOWNS

City of Bonney Lake	City of Lakewood	Town of Eatonville
City of Buckley	City of Milton	Town of Carbonado
City of DuPont	City of Orting	Town of South Prairie
City of Edgewood	City of Roy	Town of Steilacoom
City of Fife	City of Sumner	Town of Wilkeson
City of Fircrest	City of Tacoma	Pierce County
City of Gig Harbor	City of Puyallup	

FIRE PROTECTION DISTRICTS

Anderson Island Fire & Rescue (PCFD #27)-	East Pierce Fire and Rescue	Orting Valley Fire & Rescue (PCFD #18)
Ashford Fire (PCFD #23)-	Gig Harbor Fire & Medic One (PCFD #5)	South Pierce Fire & Rescue (PCFD #17)
Browns Point – Dash Point Fire (PCFD #13)	Graham Fire & Rescue (PCFD #21)	Riverside Fire & Rescue (PCFD #14)
Central Pierce Fire & Rescue (PCFD #6)	Key Peninsula Fire (PCFD #16)	West Pierce Fire & Rescue (PCFD #3)

SCHOOL AND PARK DISTRICTS

Carbonado SD	Franklin Pierce SD	Steilacoom Historic SD No. 1
Clover Park SD	Metro Parks Tacoma	Sumner SD
Dieringer SD	Orting SD	Tacoma SD #10
Eatonville SD	Peninsula SD	University Place SD
Fife SD	Puyallup SD	White River SD

WATER DISTRICTS AND OTHERS

Clear Lake WD	Lakewood Water District	Pierce Transit
Port of Tacoma	Community Health Care	

www.fema.gov

Chair Richardson
October 9, 2018
Page 2

The updated list of approved jurisdictions includes the City of Puyallup and Community Health Care that recently adopted the City of Puyallup Addendum to the *Region 5 (Pierce County) Hazard Mitigation Plan*. To continue eligibility, jurisdictions must review, revise as appropriate, and resubmit the plan within five years of the original approval date.


Additionally, this letter acknowledges that the following organizations, while not local governments, participated in, and adopted the plan. These organizations may be eligible to apply for the Hazard Mitigation Grant Program as private non-profits.

OTHER PLAN PARTICIPANTS

Pacific Lutheran University	Firgrove Mutual Inc.	Fruitland Mutual Water Company
Graham Hill Mutual Water Co., Inc.	Mt. View-Edgewood Water Company	Ohop Mutual Light Company
Spanaway Water Company	Summit Water & Supply Company	Tanner Electric Company
Herron Island Homeowners Association	Crystal Village Homeowners Association	Taylor Bay Beach Club
Raft Island Improvement Association	Riviera Community Club	Crystal River Ranch Association
Cascade Regional Blood Services	Dynamic Partners	Group Health Cooperative
Western State Hospital	Lakeview Light & Power	Franciscan Health System

If you have questions regarding your plan's approval or FEMA's mitigation grant programs, please contact Derrick Hiebert, State Mitigation Strategist with Washington Emergency Management Division, at (253) 512-7142, who coordinates and administers these efforts for local entities.

Sincerely,



Mark Carey, Director
Mitigation Division

Enclosures

cc: Tim Cook, Washington Emergency Management Division

KS:rg

APPENDIX A

REGION 5 HAZARD MITIGATION PLAN 2008-2013 UPDATE CITY OF LAKEWOOD

Plan Adoption

The “Region 5 Hazard Mitigation Plan” was adopted by the City of Lakewood’s City Council on November 3, 2008 by resolution number 2008-33. The following page shows a copy of that resolution.

RESOLUTION NO. 2008-33

A RESOLUTION of the City Council of the City of Lakewood, Washington, adopting the City of Lakewood Hazard Mitigation Plan.

WHEREAS, the City of Lakewood has participated with the Pierce County Department of Emergency Management in the development of the Hazard Mitigation Plan ; and

WHEREAS, the Federal Emergency Management Agency has mandated that all local and state governmental entities develop and submit for approval a Hazard Mitigation Plan to address pre-disaster planning issues; and

WHEREAS, the Hazard Mitigation Plan is completed and ready for adoption by the City of Lakewood; and

WHEREAS the Hazard mitigation Plan has been submitted and approved by the State and the Federal Emergency Management Agency; and

WHEREAS, the Lakewood City Council reviewed the Hazard mitigation Plan preparation process in a Council Study Session on March 10, 2008; and

WHEREAS, the City of Lakewood could risk not receiving future disaster funding if the Hazard mitigation Plan is not adopted;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON HEREBY RESOLVES as follows:

Section 1. That the Hazard Mitigation Plan is hereby adopted.

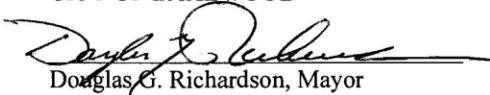
Section 2. This Resolution shall be in full force and effect upon passage and signatures hereon.

PASSED by the City Council this 3rd day of November, 2008.

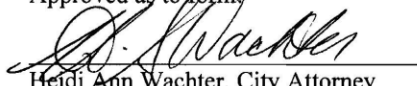
CITY OF LAKEWOOD

Attest:


Alice M. Bush, MMC, City Clerk


Douglas G. Richardson, Mayor

Approved as to form:


Heidi Ann Wachter, City Attorney

The plan was reviewed and approved as follows:

AGENCY	REPRESENTATIVE	DATE
FEMA Region X	Mark Carey Mitigation Division Director	Approved—

Letter of approval follows below.



FEMA

July 9, 2009

Mr. Steven C. Bailey, Director
Pierce County Department of Emergency Management
2501 South 35th Street
Tacoma, Washington 98409-7405

Dear Mr. Bailey:

On November 24, 2008, the U.S. Department of Homeland Security's Federal Emergency Management Agency (FEMA) approved the **Region 5 Hazard Mitigation Plan** as a multi-jurisdictional local plan as outlined in 44 CFR Part 201. With approval of this plan, the following entities are now eligible to apply for the Robert T. Stafford Disaster Relief and Emergency Assistance Act's hazard mitigation project grants through November 24, 2013:

Cities and Towns:	Fire Districts:	School Districts:	Utilities:
City of Buckley	Lakewood Fire Department (PCFD #2)	Carbonado SD	Clear Lake Water District
City of Dupont	Gig Harbor Fire & Medic One (PCFD #5)	Dieringer SD	Fruitland Mutual Water Company
City of Edgewood	Central Pierce Fire & Rescue (PCFD #6)	Eatonville SD	Graham Hill Mutual Water Company
City of Fife	PCFD #8	Fife SD	Lakeview Light and Power
City of Fircrest	PCFD #13	Franklin Pierce SD	Lakewood Water District
City of Gig Harbor	South Pierce Fire & Rescue (PCFD #15)	Orting SD	Mt. View-Edgewood Water Company
City of Lakewood	Key Peninsula Fire Department (PDFD #16)	Peninsula SD	Ohop Mutual Light Company
City of Milton	PCFD #18	Puyallup SD	Port of Tacoma
City of Orting	Graham Fire and Rescue (PCFD #21)	Steilacoom Historical SD	Summit Water and Supply Company
City of Tacoma	PCFD #23	Tacoma SD	Valley Water District
Town of Eatonville	PCFD #27	University Place SD	
Town of South Prairie		White River SD	
Town of Wilkeson		Pacific Lutheran University	

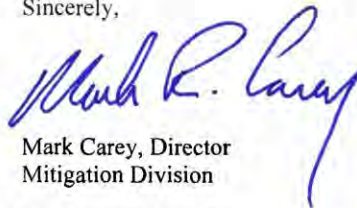
www.fema.gov

Mr. Bailey
July 9, 2009
Page 2

The list of approved jurisdictions has been updated to include the jurisdictions in italics above, which have recently adopted the Region 5 Hazard Mitigation Plan. To continue eligibility, the plan must be reviewed, revised as appropriate, and resubmitted within five years of the original approval date.

If you have questions regarding your plan's approval or FEMA's mitigation grant programs, please contact our State counterpart, Washington Emergency Management Division, which coordinates and administers these efforts for local entities.

Sincerely,



Mark Carey, Director
Mitigation Division

cc: Mark Stewart, Washington Emergency Management Division

KM:bb

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APPENDIX B

REGION 5 ALL HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD

Region 5 Hazard Mitigation Planning Team

City of Lakewood

NAME	TITLE	JURISDICTION-DEPARTMENT
Christine Badger	Emergency Management Coordinator	City of Lakewood
John Unfred	Emergency Deputy Director	City of Lakewood
Hallie McCurdy	Assistant Fire Chief	West Pierce Fire & Rescue

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APPENDIX C

REGION 5 ALL HAZARD MITIGATION PLAN
2020-2025 EDITION
CITY OF LAKEWOOD

Plan Revisions

RECORD OF CHANGES			
Change Number	Description of Change (with page numbers)	Date	Authorized by:

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APPENDIX D

REGION 5 ALL HAZARD MITIGATION PLAN

2020-2025 EDITION

CITY OF LAKEWOOD AND PIERCE COUNTY SCENARIO

OVERVIEW

This appendix contains the spatial results from the Hazus Earthquake Scenario results showing the Essential Facilities for 90% functionality for Day 1 and Day 7 following an earthquake event based on three earthquakes scenarios. Information was based on ShakeMaps developed by U.S. Geological Survey for a 7.1M earthquake occurring on the Tacoma Fault, 7.2M earthquake on the Nisqually Fault and a 7.2M earthquake on the SeaTac Fault. There was a total of four Essential Facilities that were modeled; fire stations, police stations, schools and hospitals. Additional information can be found in the Risk Assessment Section of the Pierce County All Hazard Mitigation Plan.

Inherent Errors

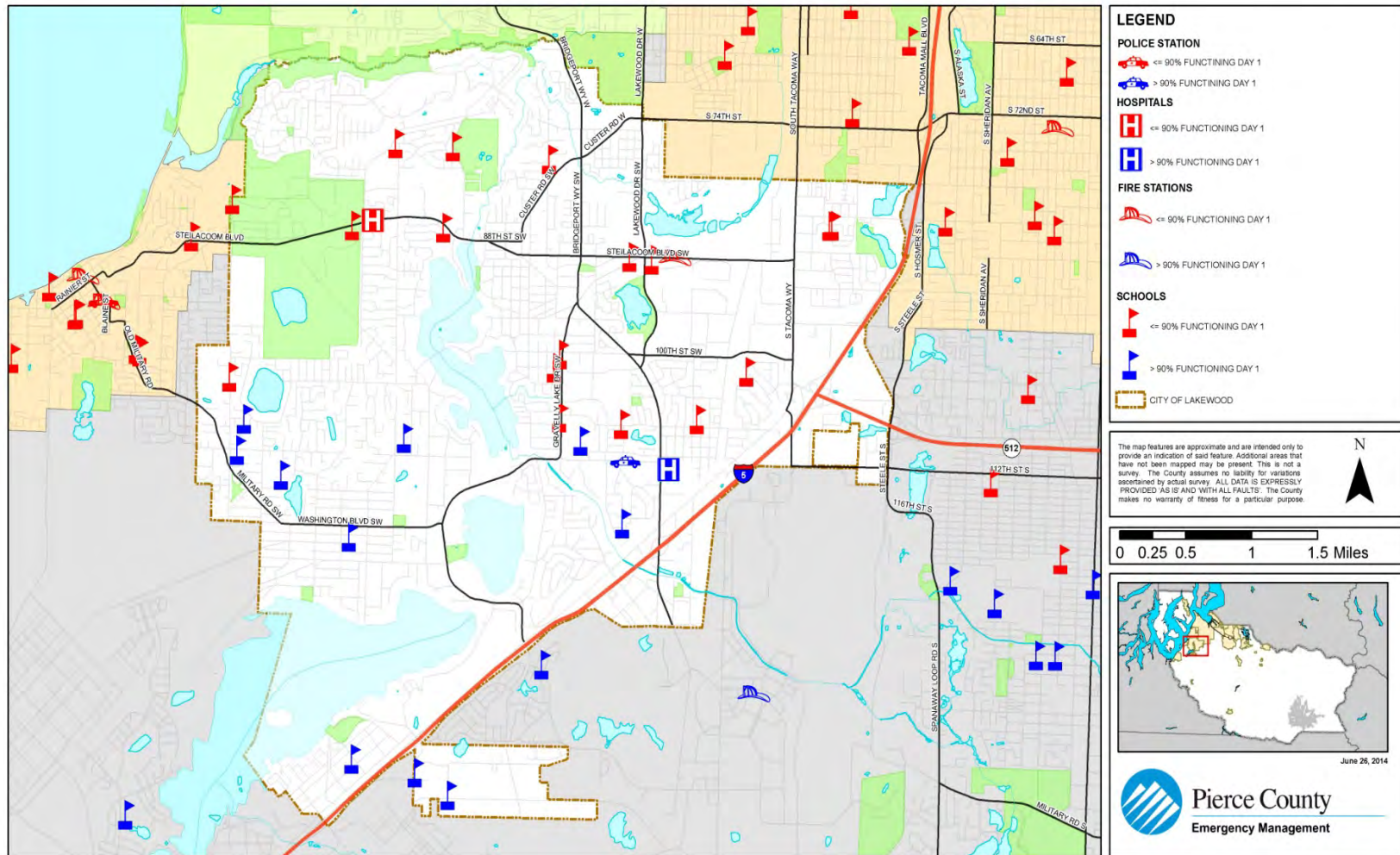
As a special note to the Gig Harbor and Key Peninsula areas St. Anthony's Hospital is not identified on Maps D-11, D-12, D-20, D-21, D-29 or D-30 due to the recent construction of St. Anthony's Hospital and lack of data. With future updates of the Region 5 All Hazard Mitigation Plan, St. Anthony's Hospital will be included in the scenario analysis. If this information becomes available prior to the five-year update in 2025, revised analysis will be done and the revised maps will be distributed to the City of Gig Harbor, Gig Harbor Fire & Medic One and the Key Peninsula Fire Department.

It has been identified that the police station located to the west side of Orting is not in the correct location as seen on Maps: D-9, D-10, D-18, D-19, D-27 and D-28. The police department shares a building with the Fire District #18 at 401 Washington Ave S, which is located in the middle of town. As Hazus-MH is updated the police station will show a co-location with the fire station at this same location. If this information becomes available prior to the five-year update in 2025, revised analysis will be done and the revised maps will be distributed to the City of Orting and to Pierce County Fire District #18.

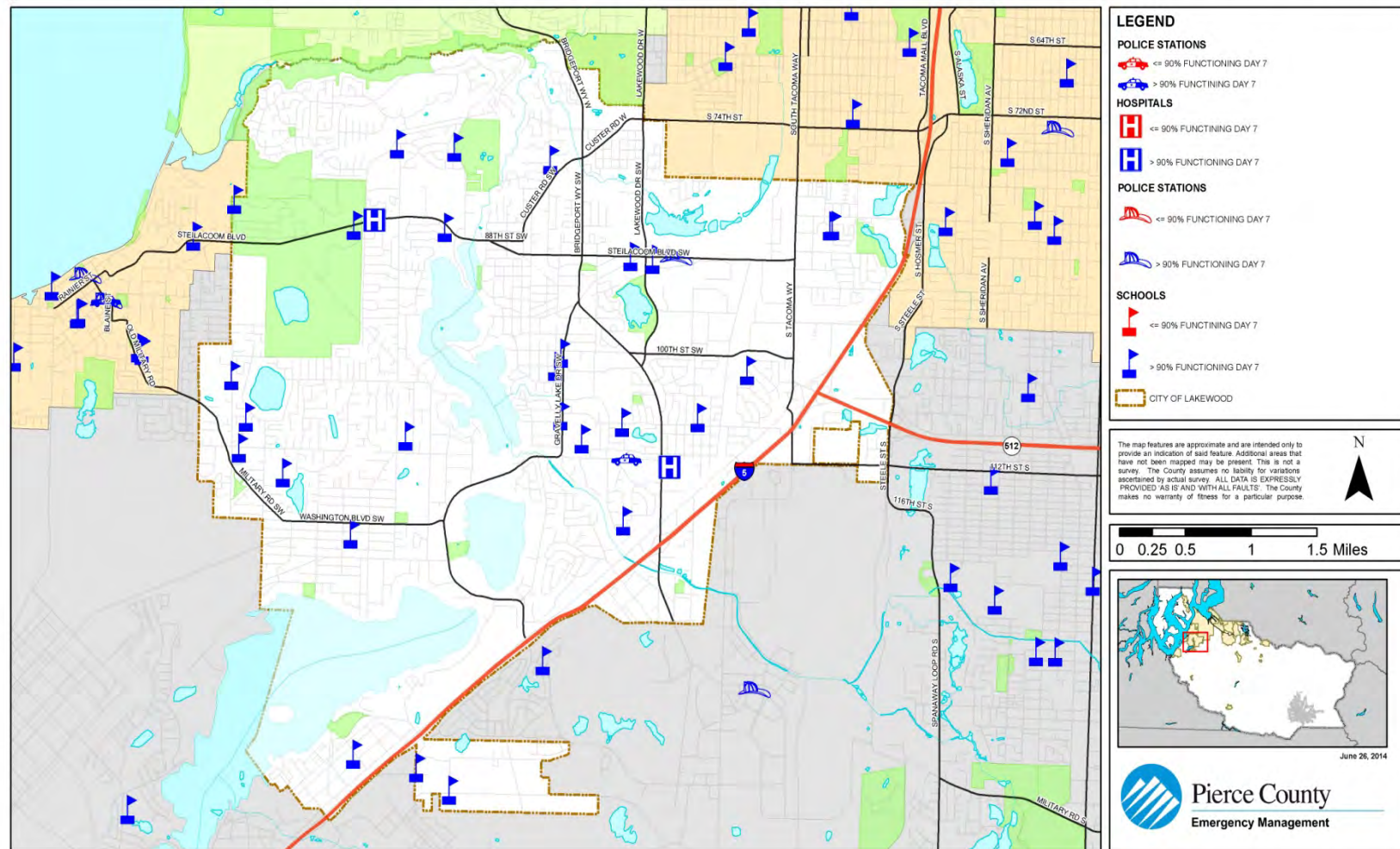
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Map D-1 City of Lakewood Tacoma Fault Scenario Essential Facilities Day 1

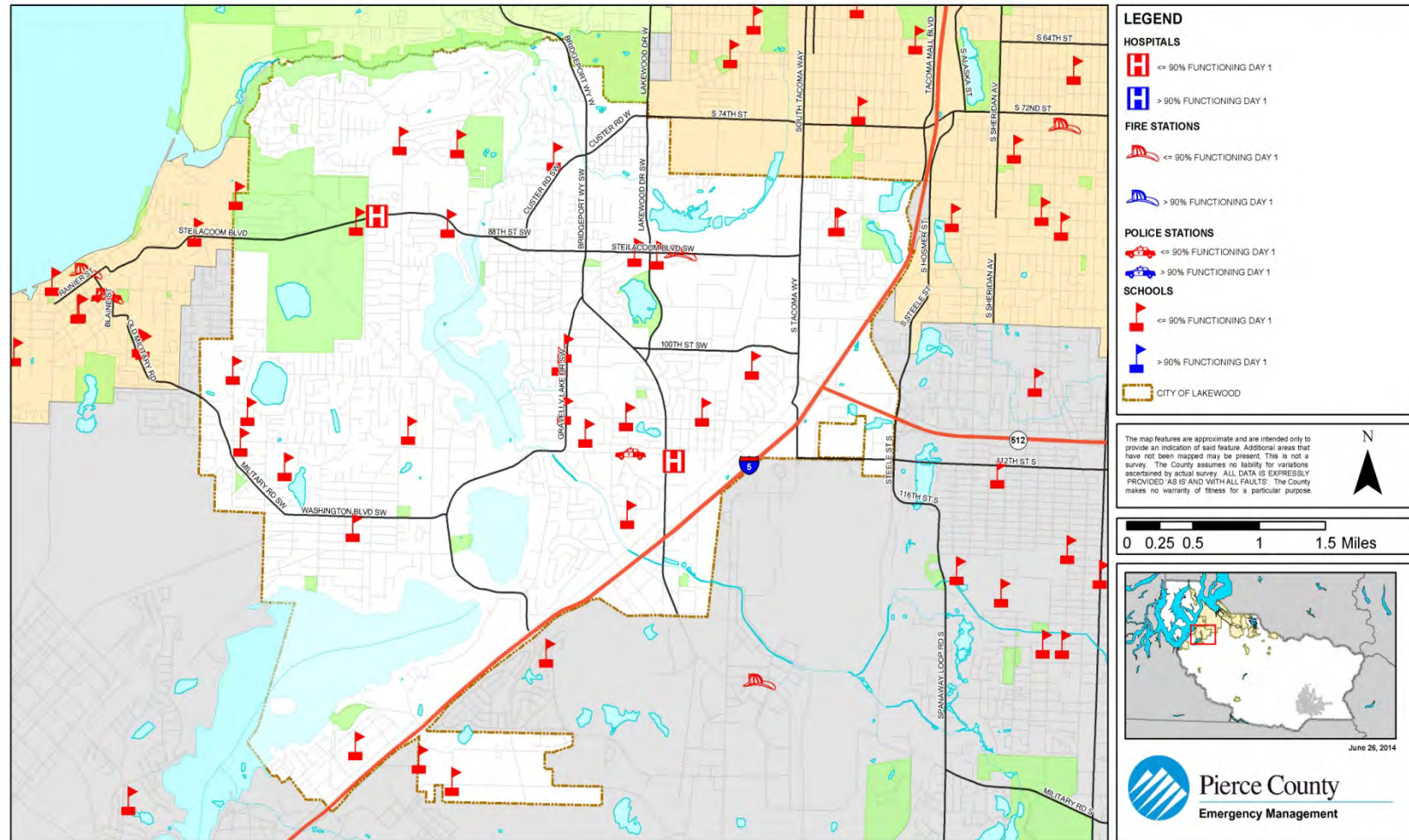
CITY OF LAKEWOOD - 7.1M TACOMA EARTHQUAKE SCENARIO - ESSENTIAL FACILITIES



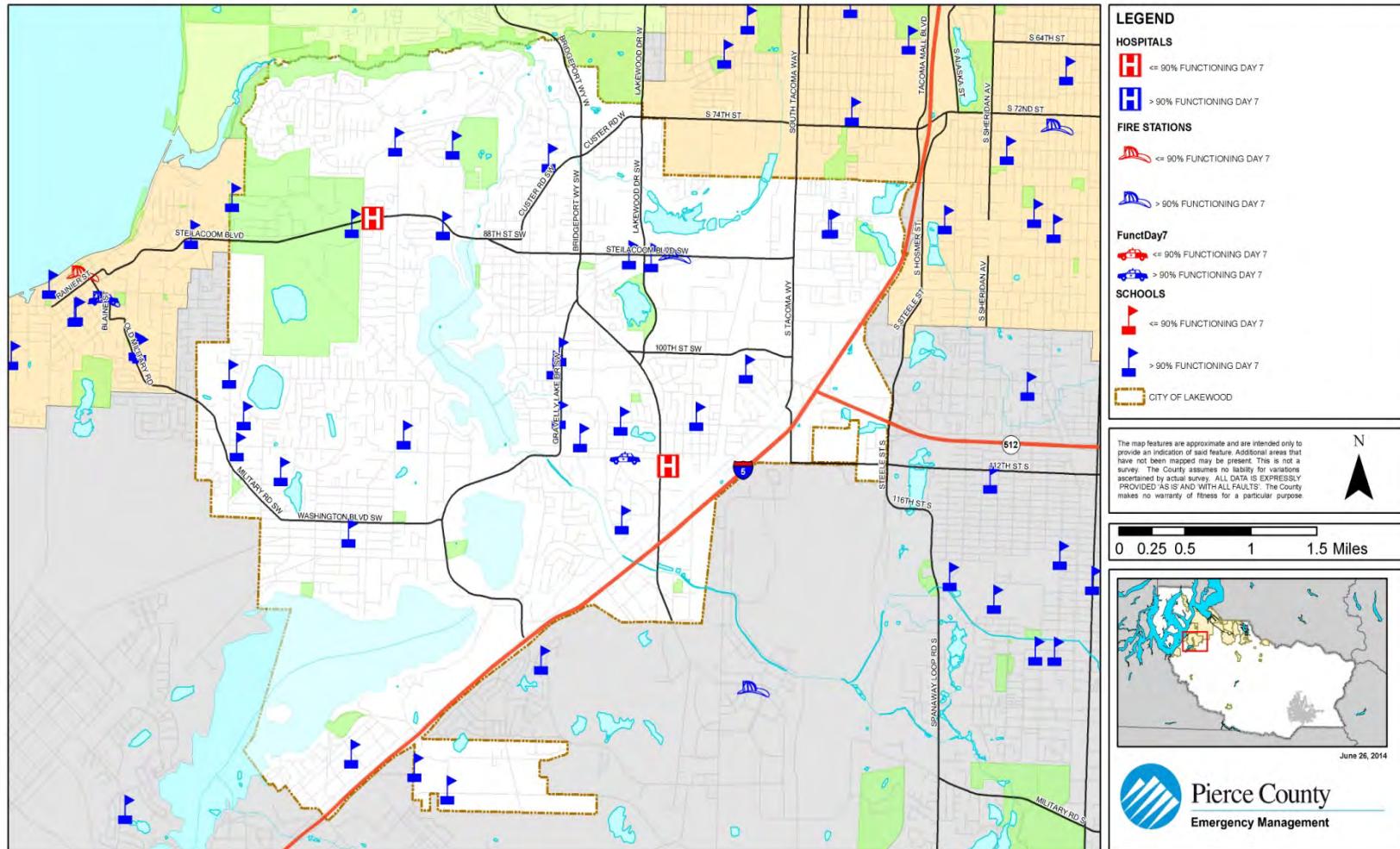
CITY OF LAKEWOOD - 7.1M TACOMA EARTHQUAKE SCENARIO - ESSENTIAL FACILITIES



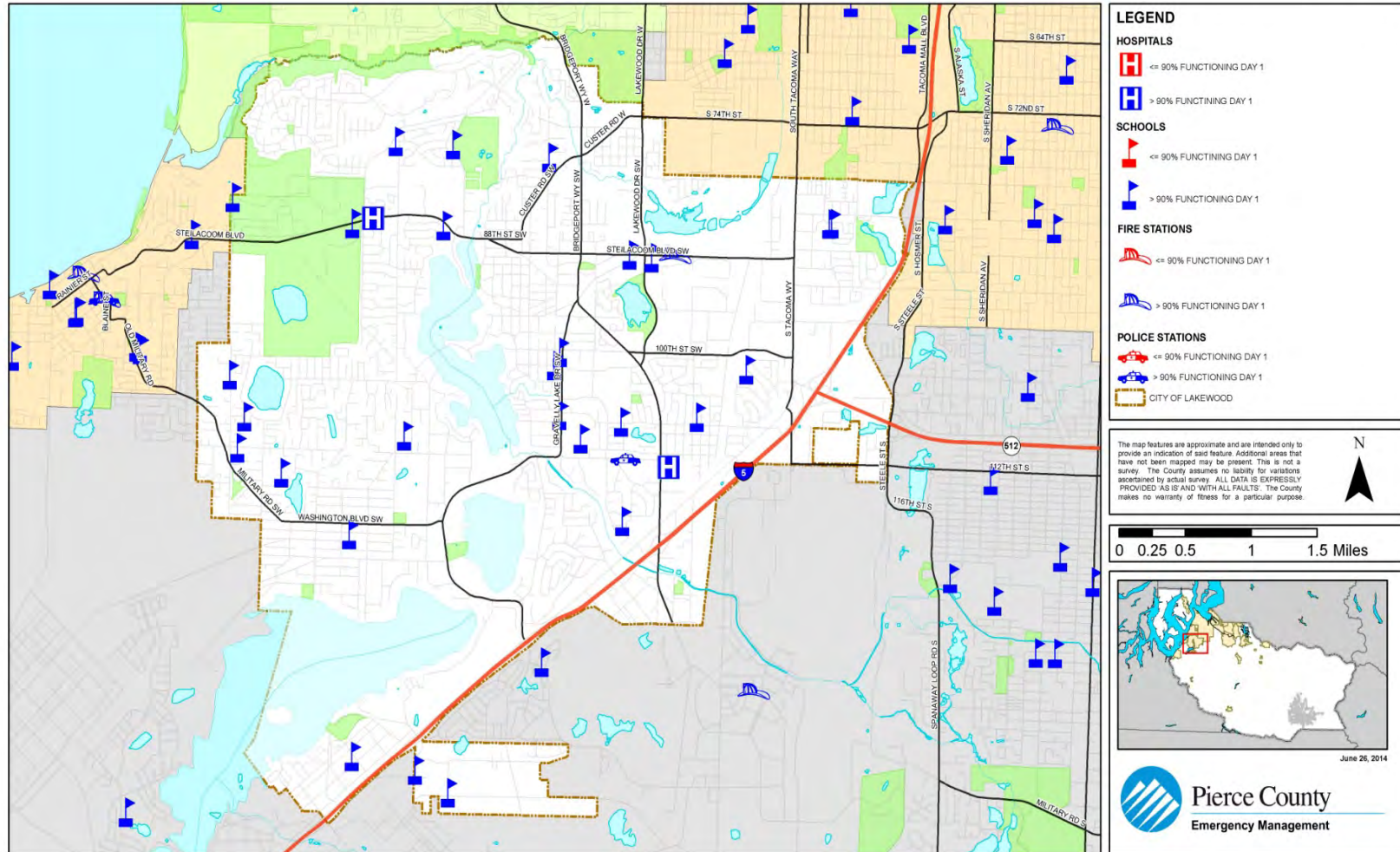
CITY OF LAKEWOOD - 7.2M NISQUALLY EARTHQUAKE SCENARIO - ESSENTIAL FACILITIES



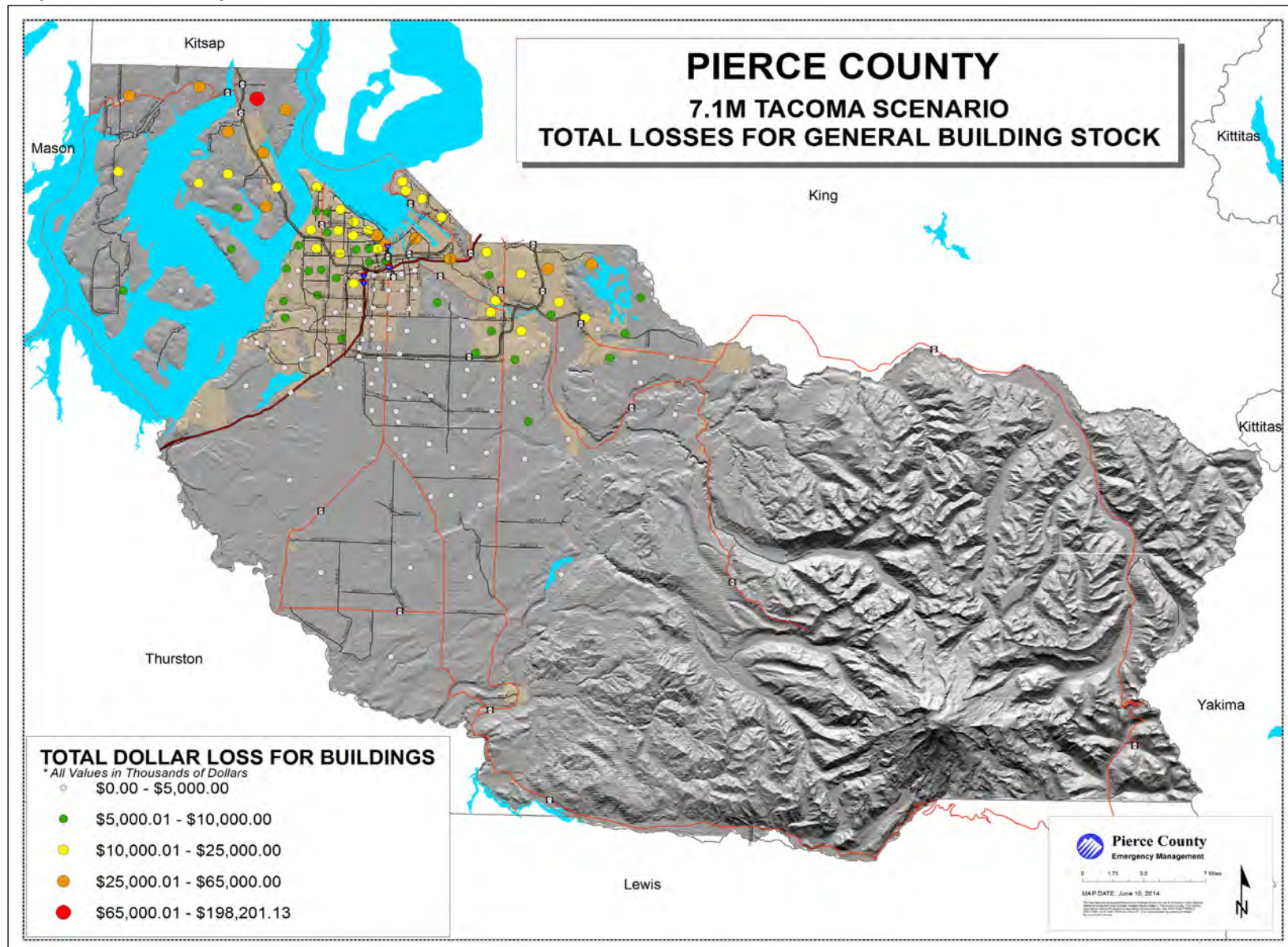
CITY OF LAKEWOOD - 7.2M NISQUALLY EARTHQUAKE SCENARIO - ESSENTIAL FACILITIES



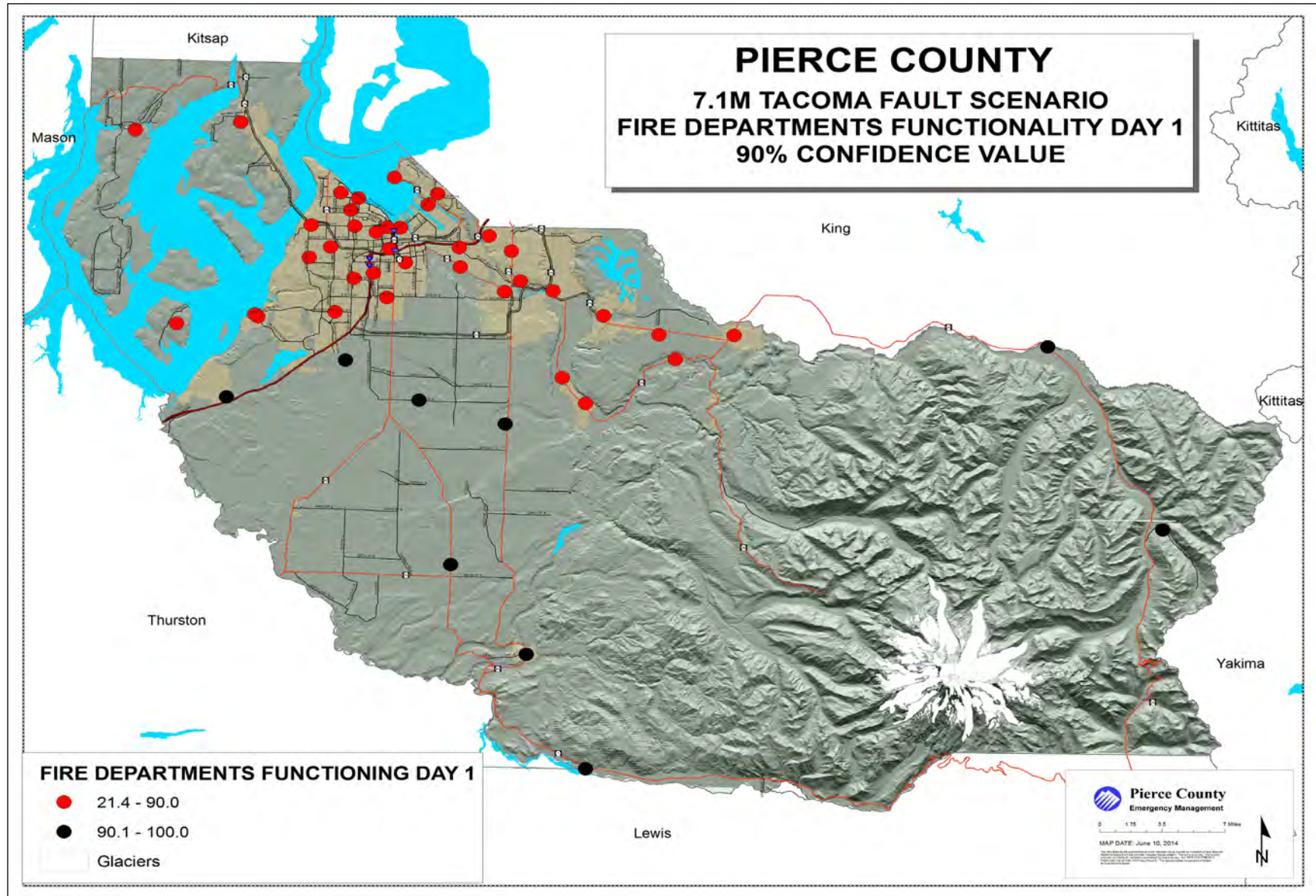
CITY OF LAKEWOOD - 7.2M SEATAC EARTHQUAKE SCENARIO - ESSENTIAL FACILITIES



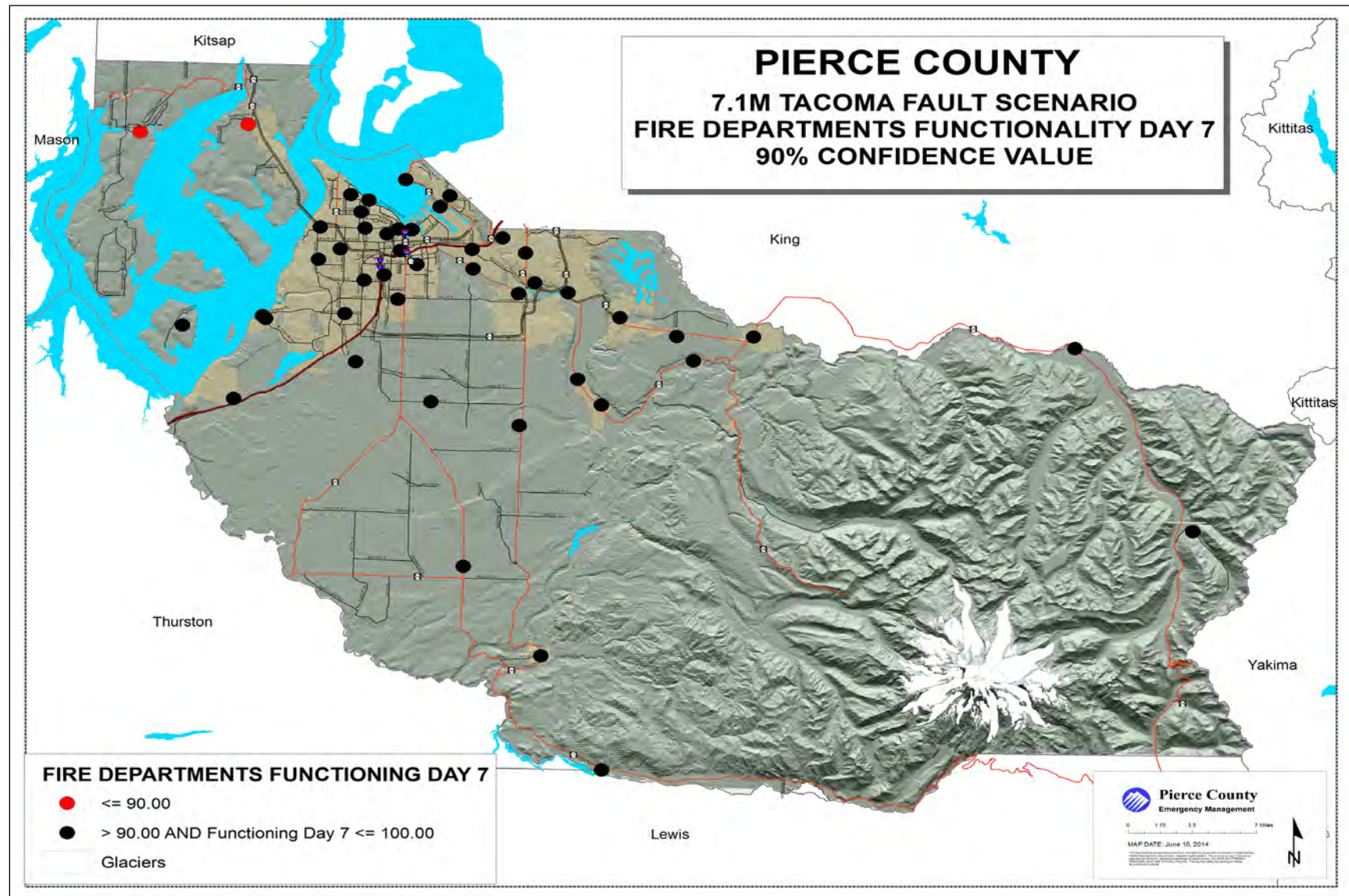
Map D-6 Pierce County Tacoma Fault Scenario Total Losses



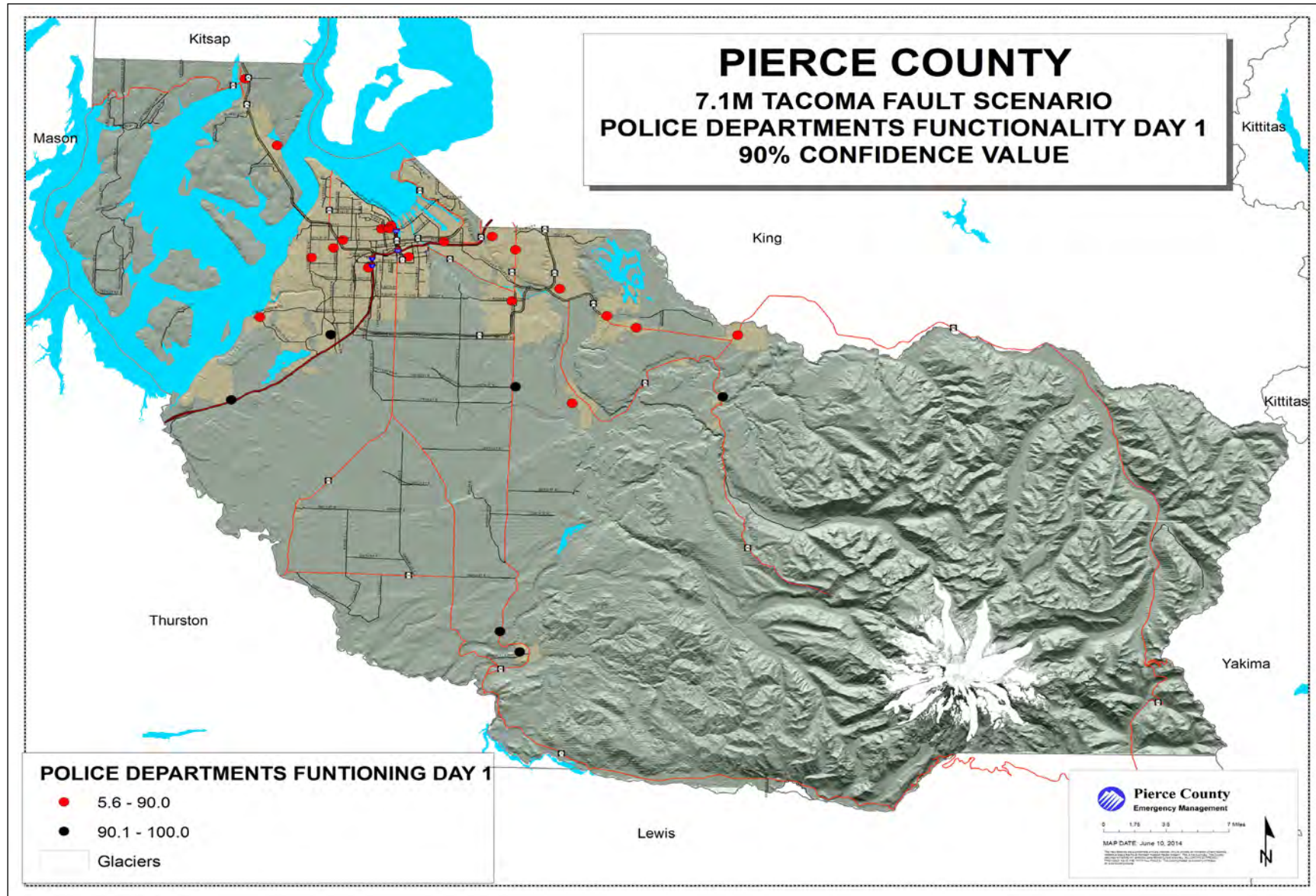
Map D-7 Pierce County Tacoma Fault Scenario Fire Department Functionality Day 1



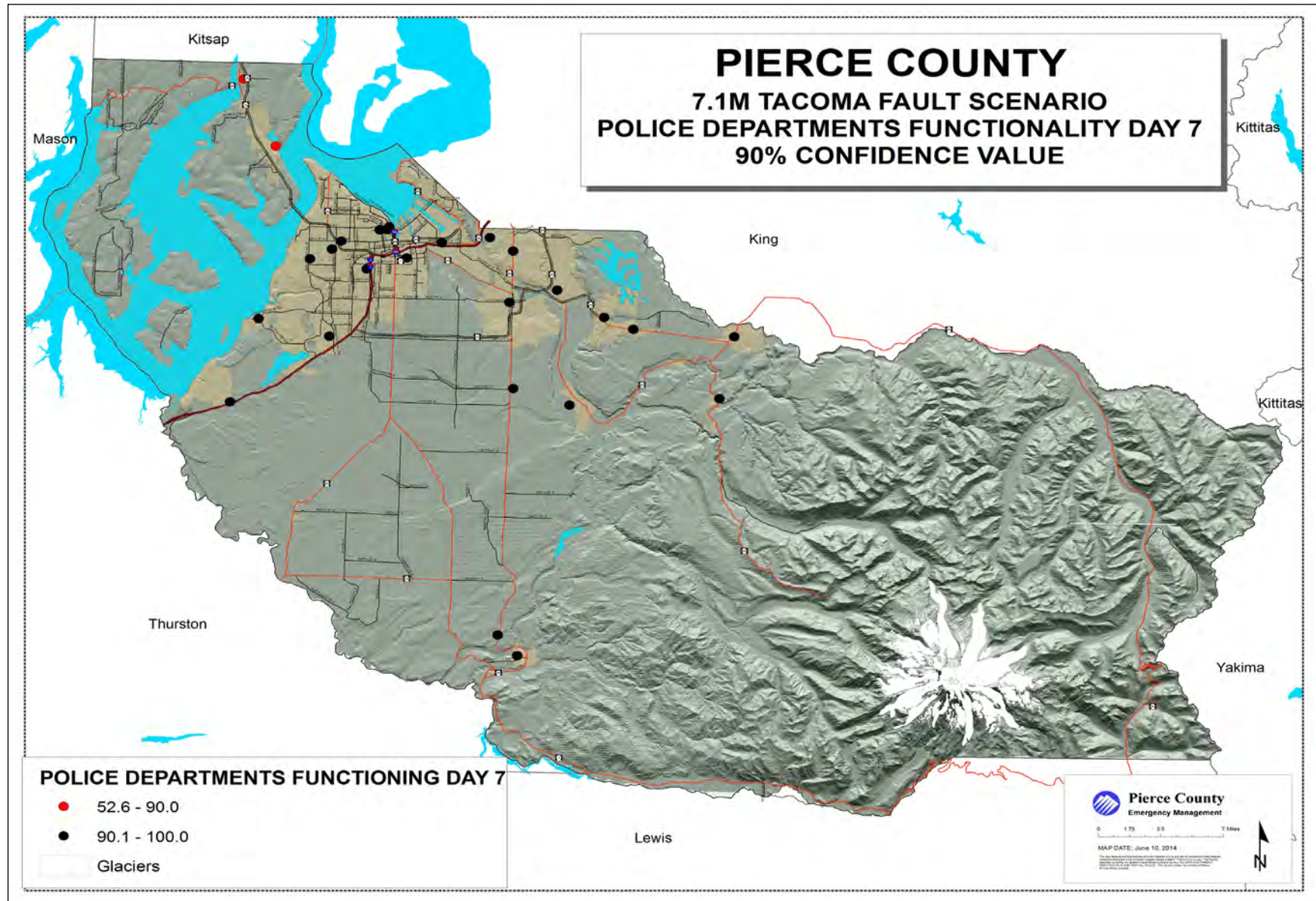
Map D-8 Pierce County Tacoma Fault Scenario Fire Department Functionality Day 7



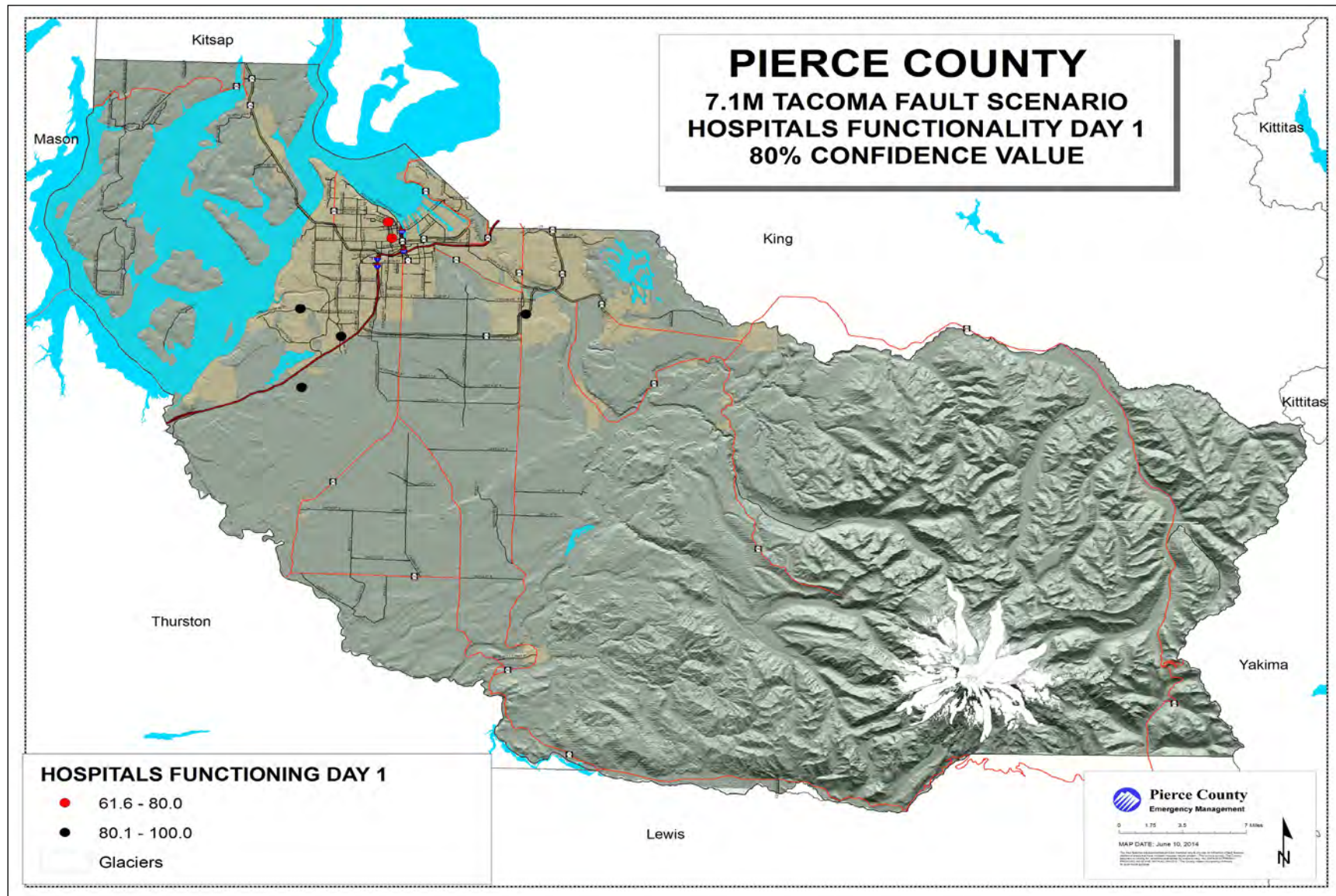
Map D-9 Pierce County Tacoma Fault Scenario Police Department Functionality Day 1¹



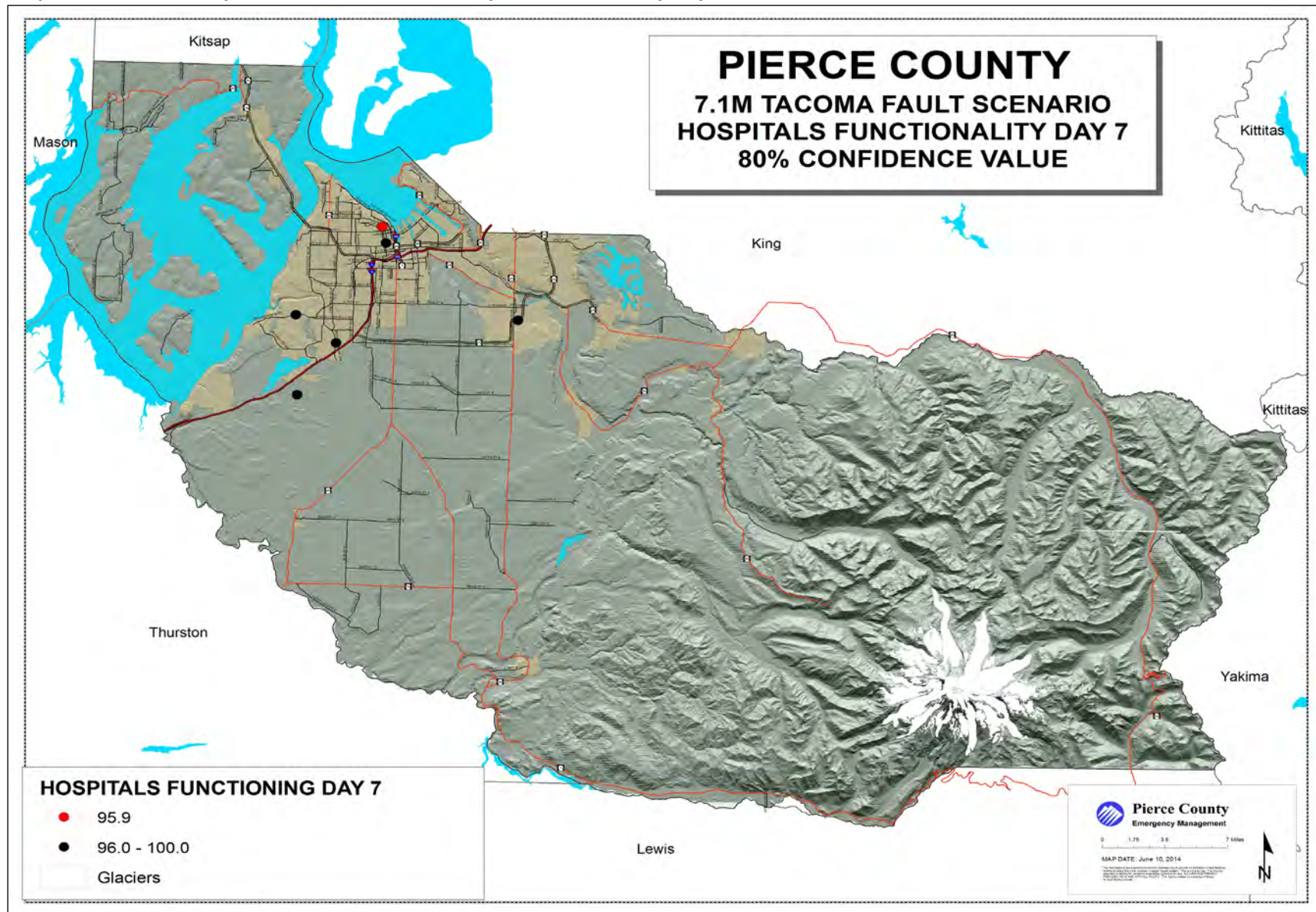
Map D-10 Pierce County Tacoma Fault Scenario Police Department Functionality Day 7²



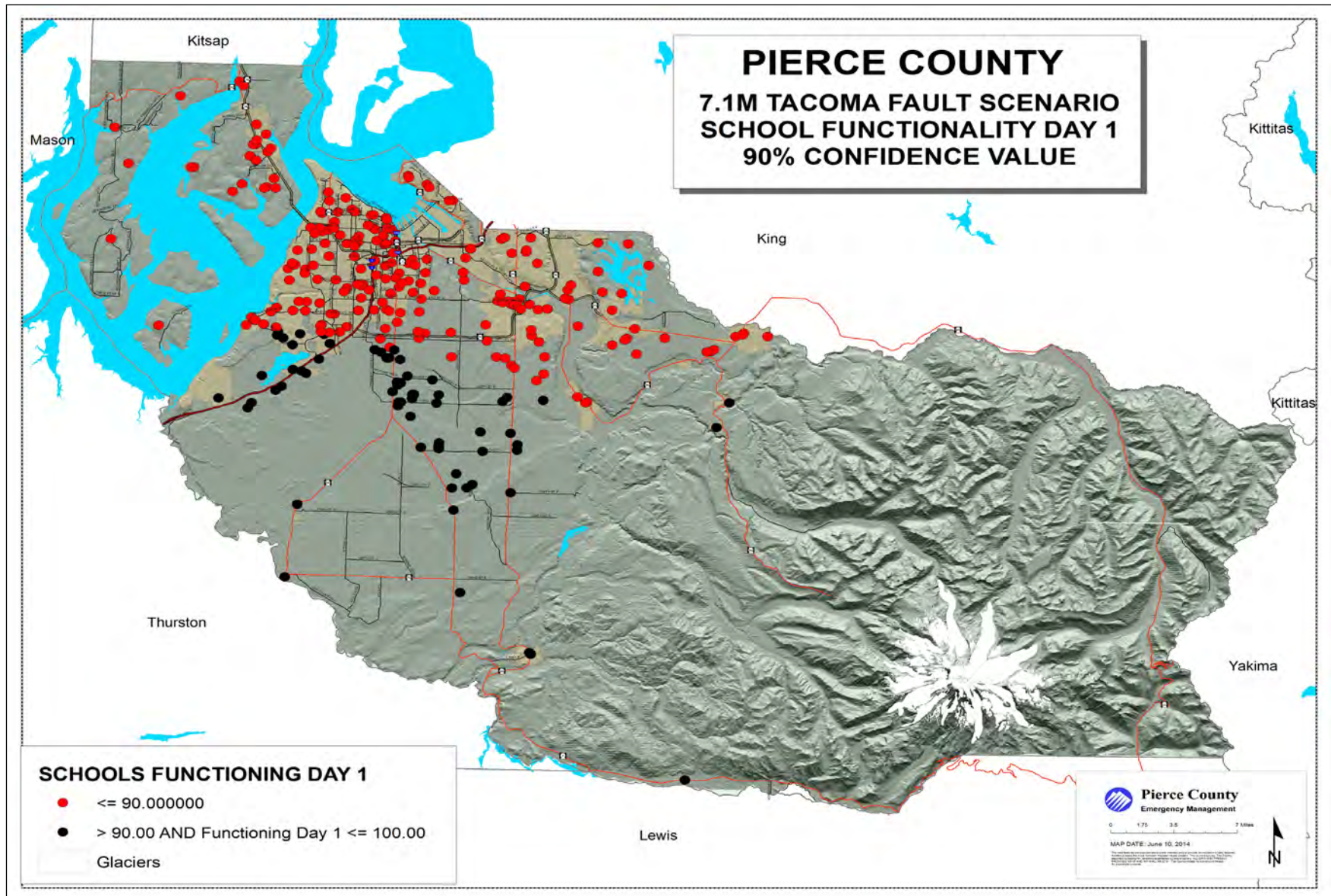
Map D-11 Pierce County Tacoma Fault Scenario Hospitals Functionality Day 1³



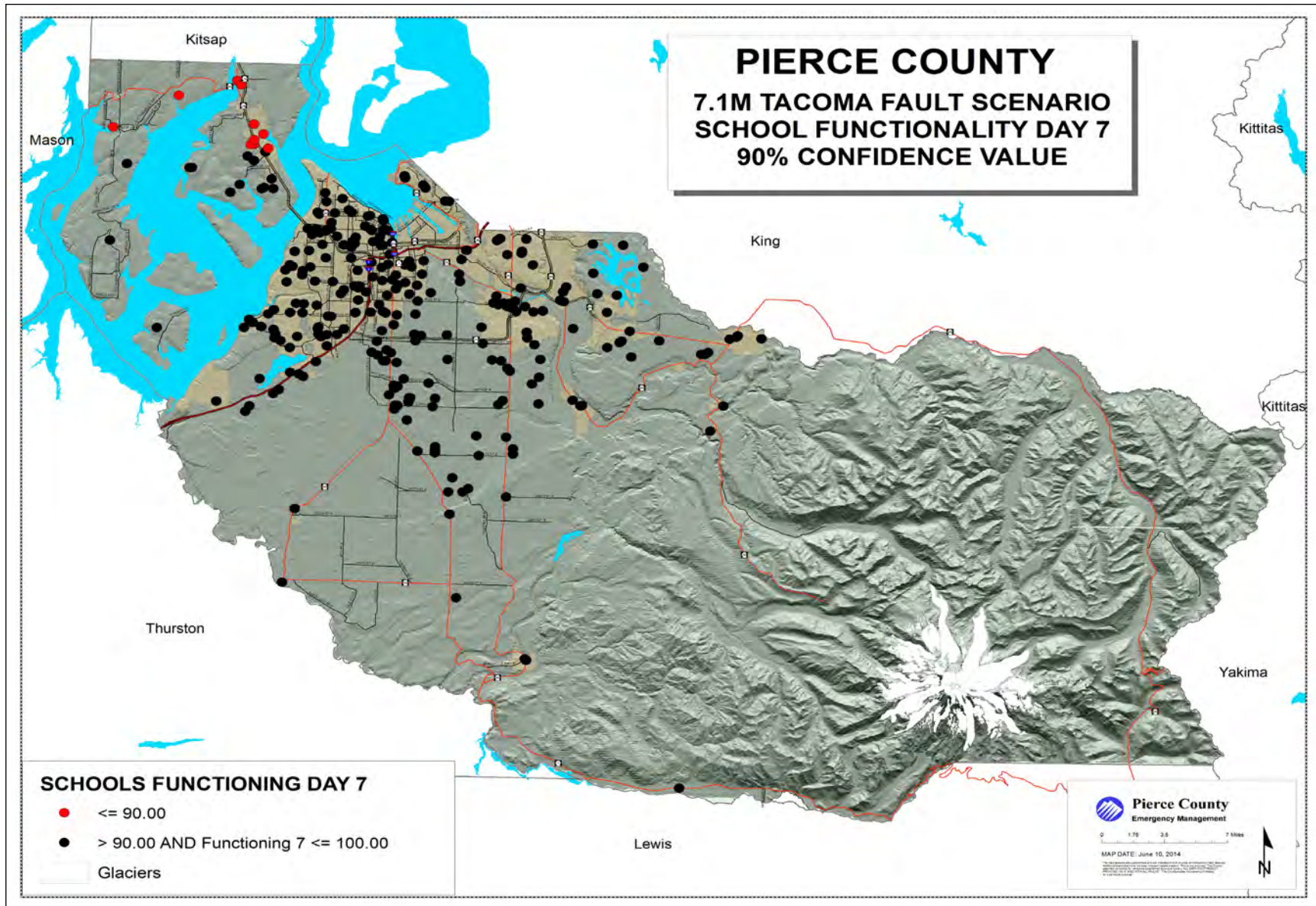
Map D-12 Pierce County Tacoma Fault Scenario Hospitals Functionality Day 7⁴



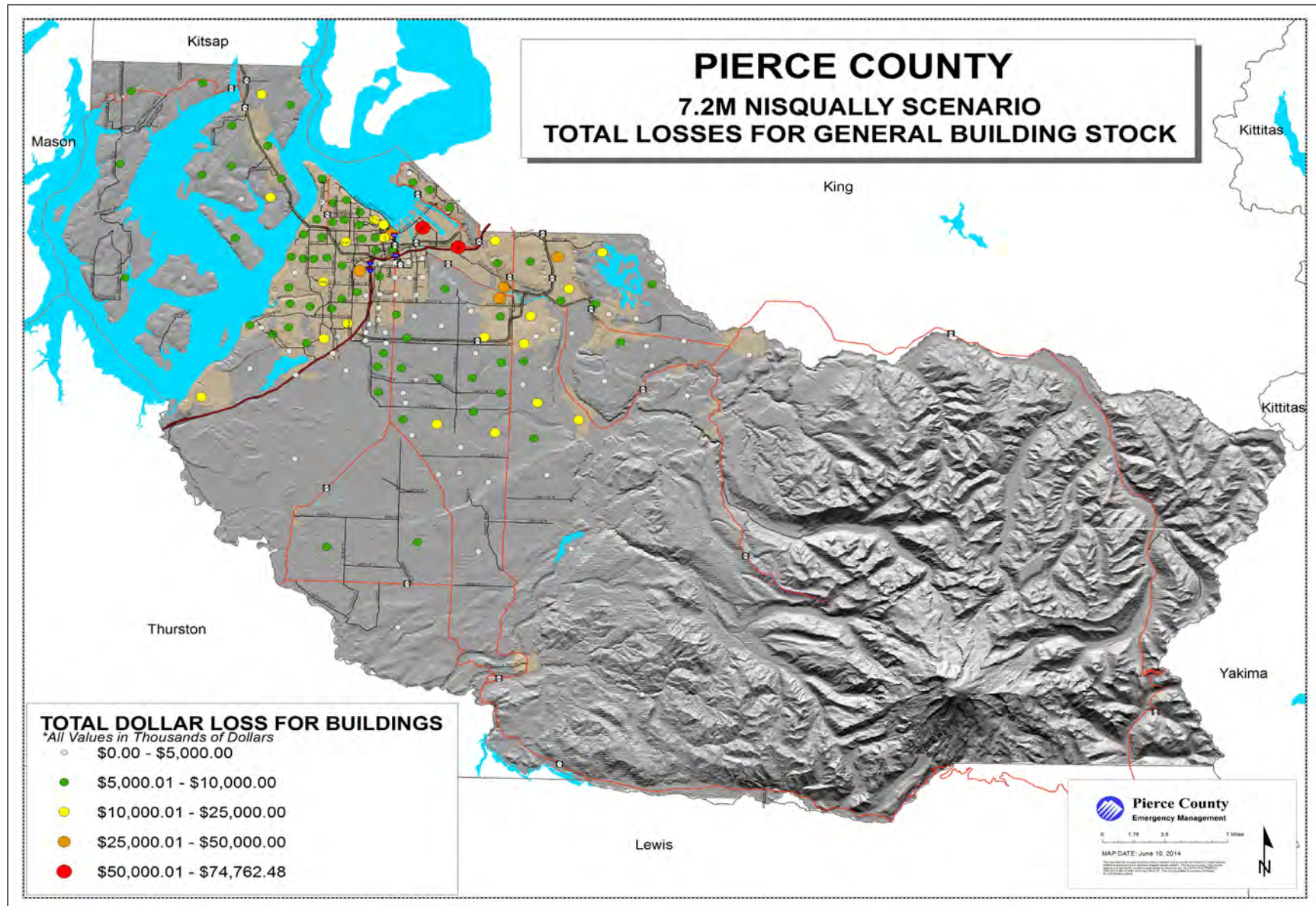
Map D-13 Pierce County Tacoma Fault Scenario School Functionality Day 1



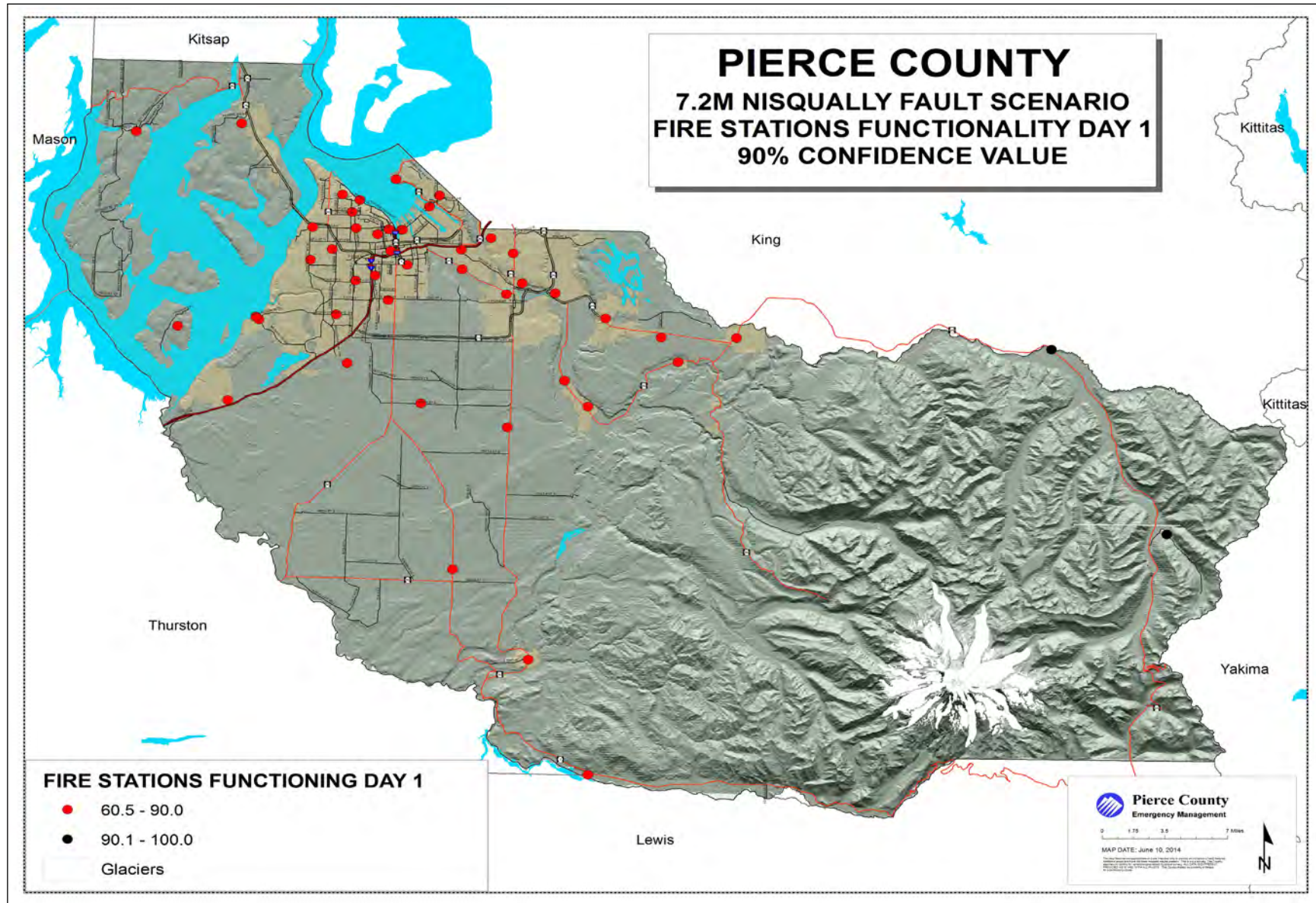
Map D-14 Pierce County Tacoma Fault Scenario School Functionality Day 7



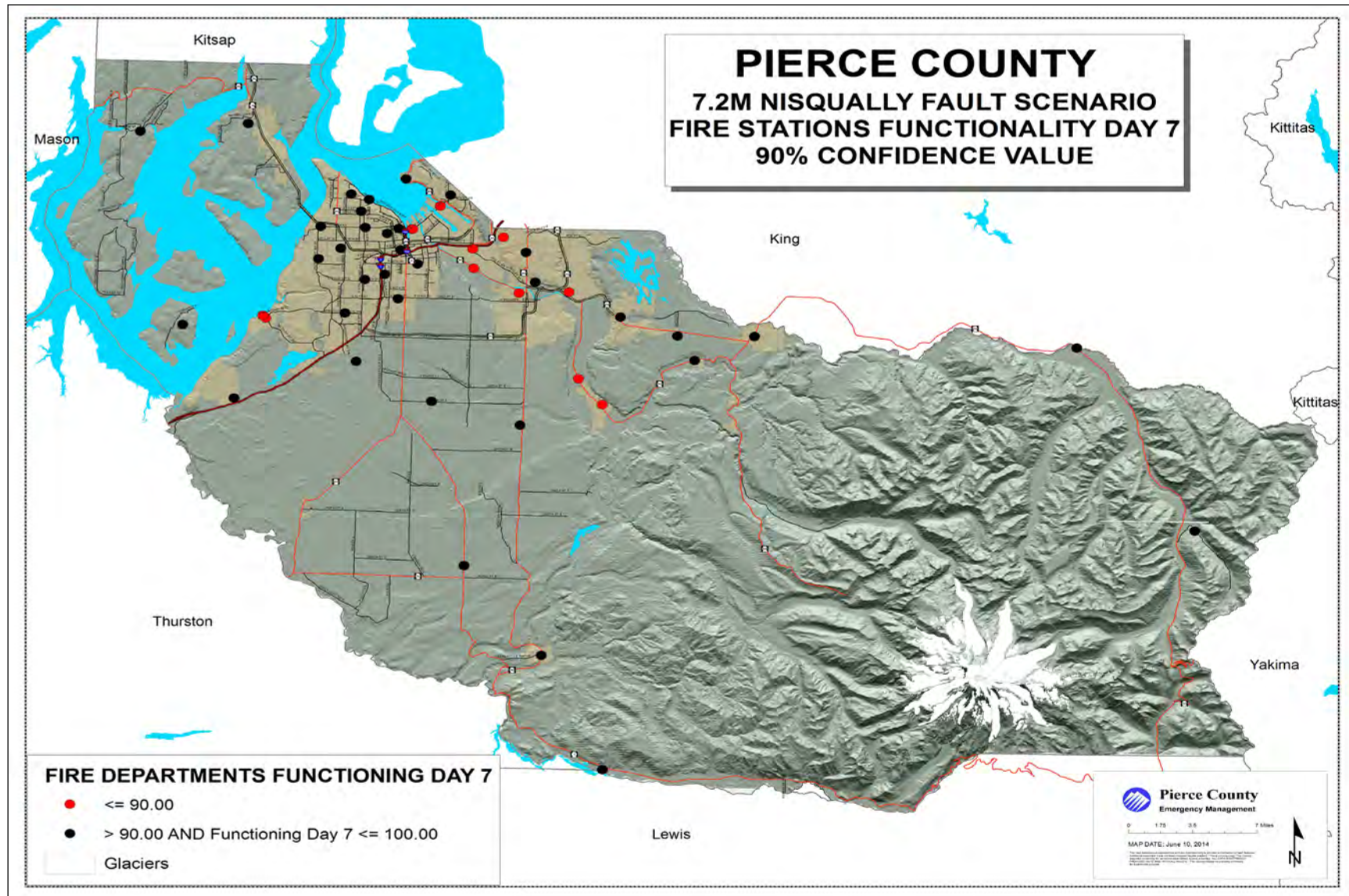
Map D-15 Pierce County Nisqually Fault Scenario Total Losses



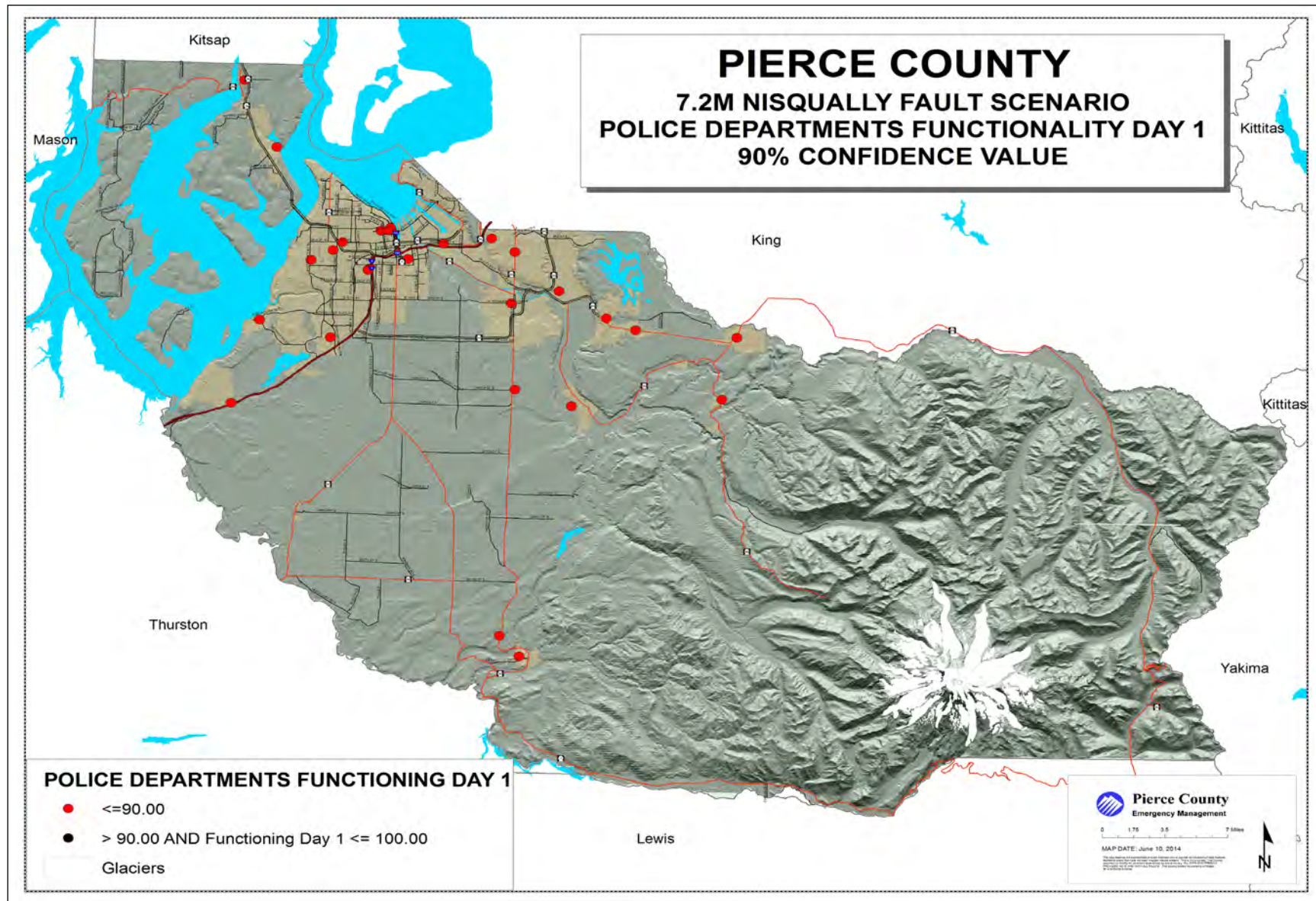
Map D-16 Pierce County Nisqually Fault Scenario Fire Stations Functionality Day 1



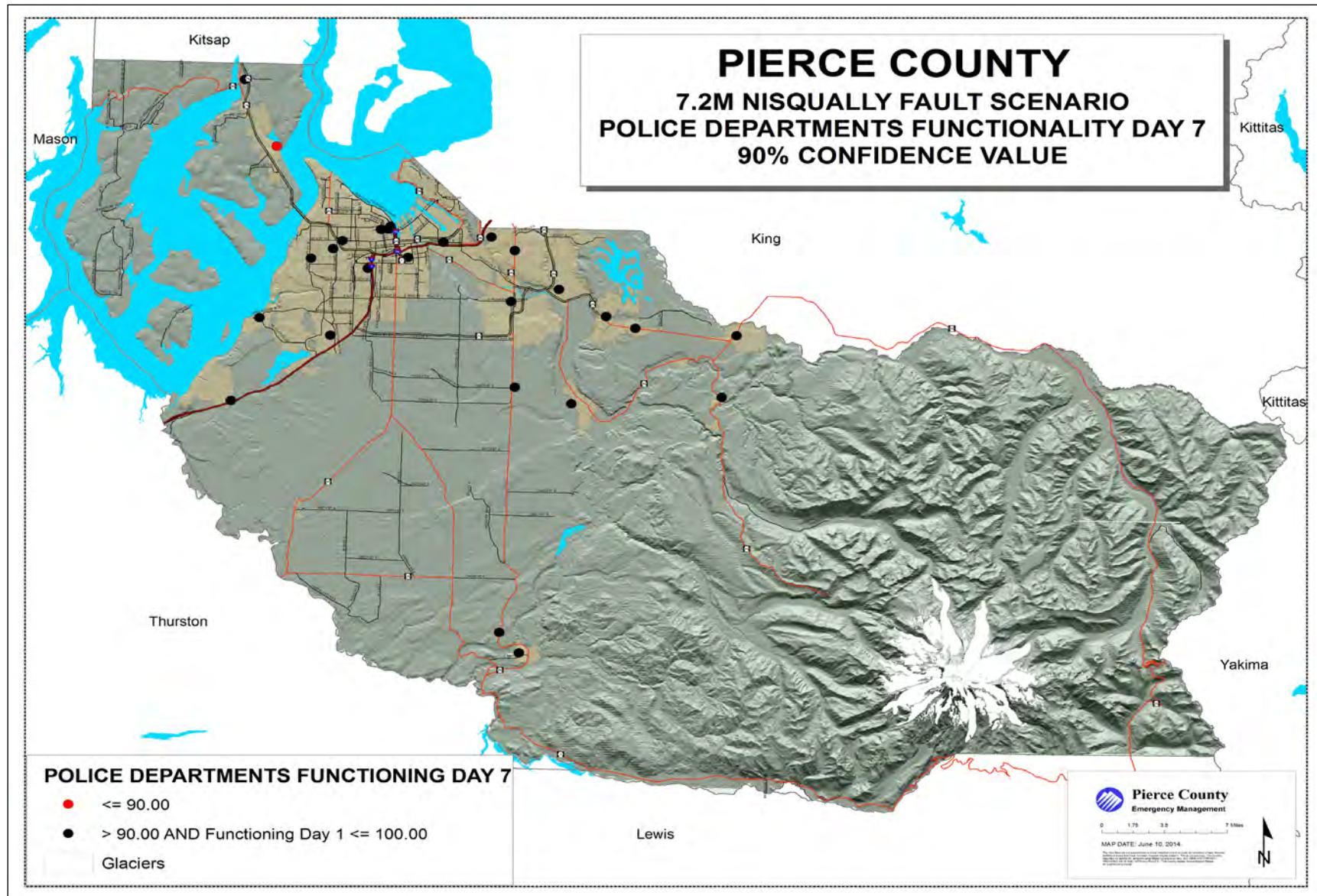
Map D-17 Pierce County Nisqually Fault Scenario Fire Stations Functionality Day 7



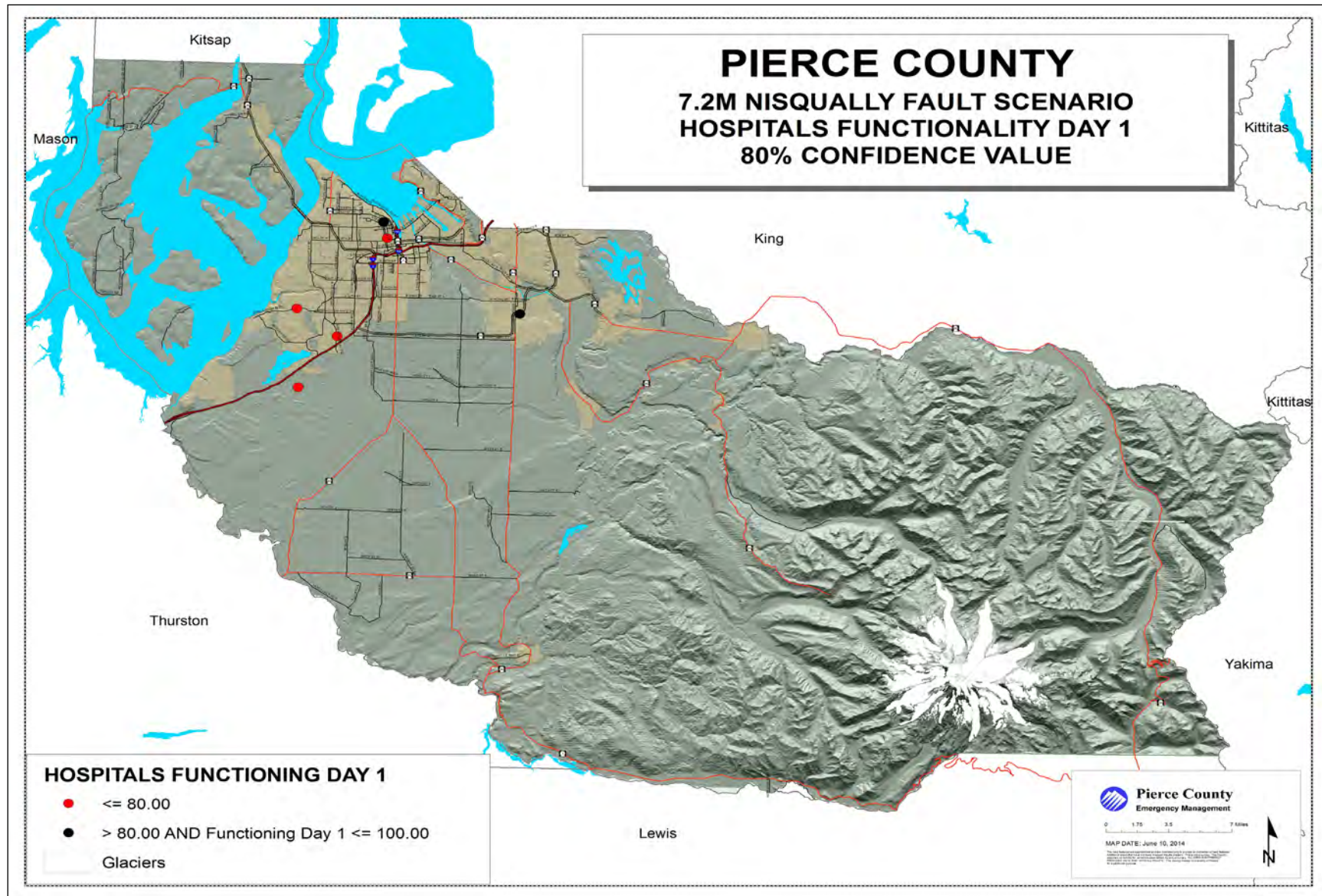
Map D-18 Pierce County Nisqually Fault Scenario Police Departments Functionality Day 1 ⁵



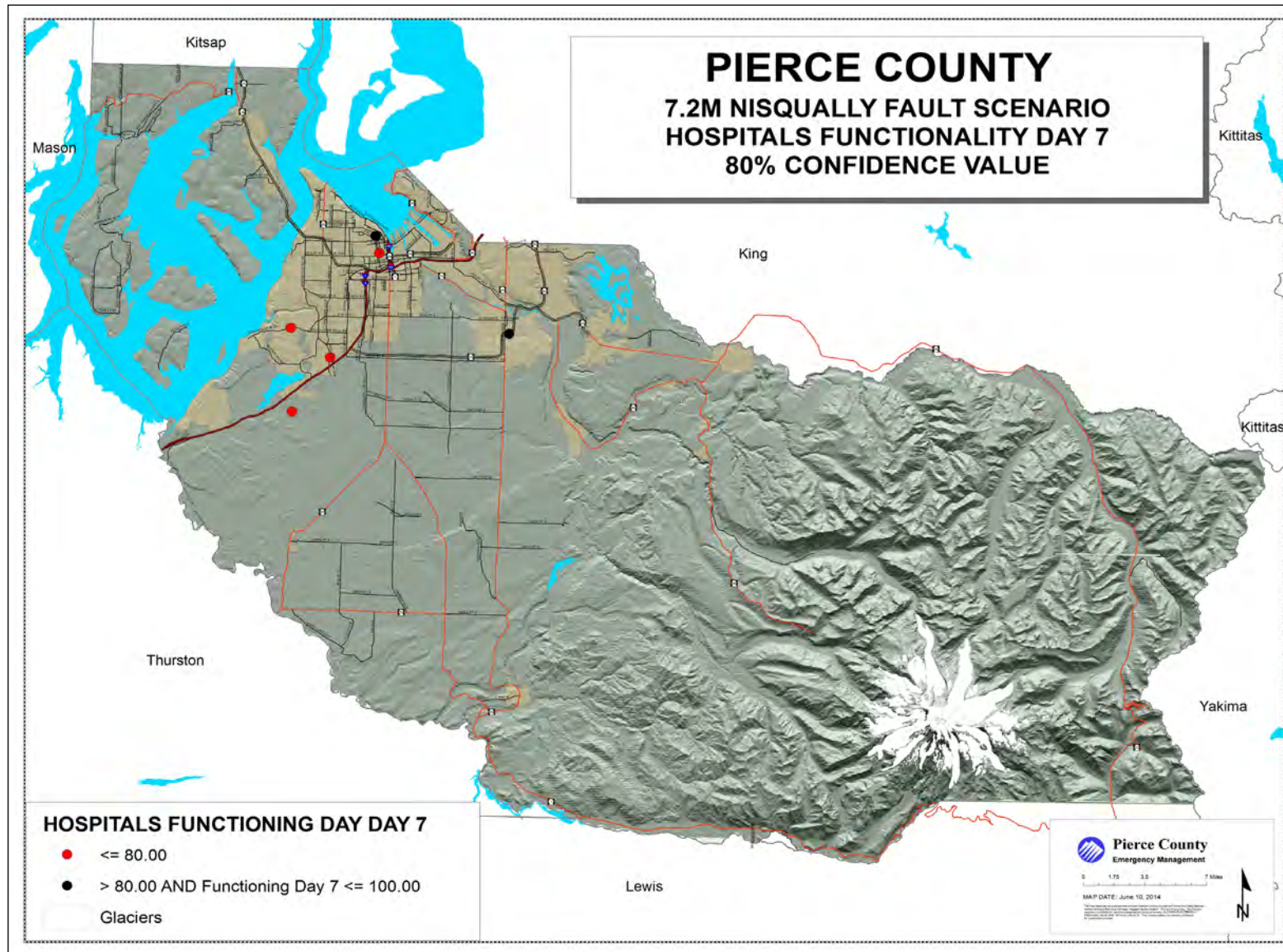
Map D-19 Pierce County Nisqually Fault Scenario Police Departments Functionality Day 7 ⁶



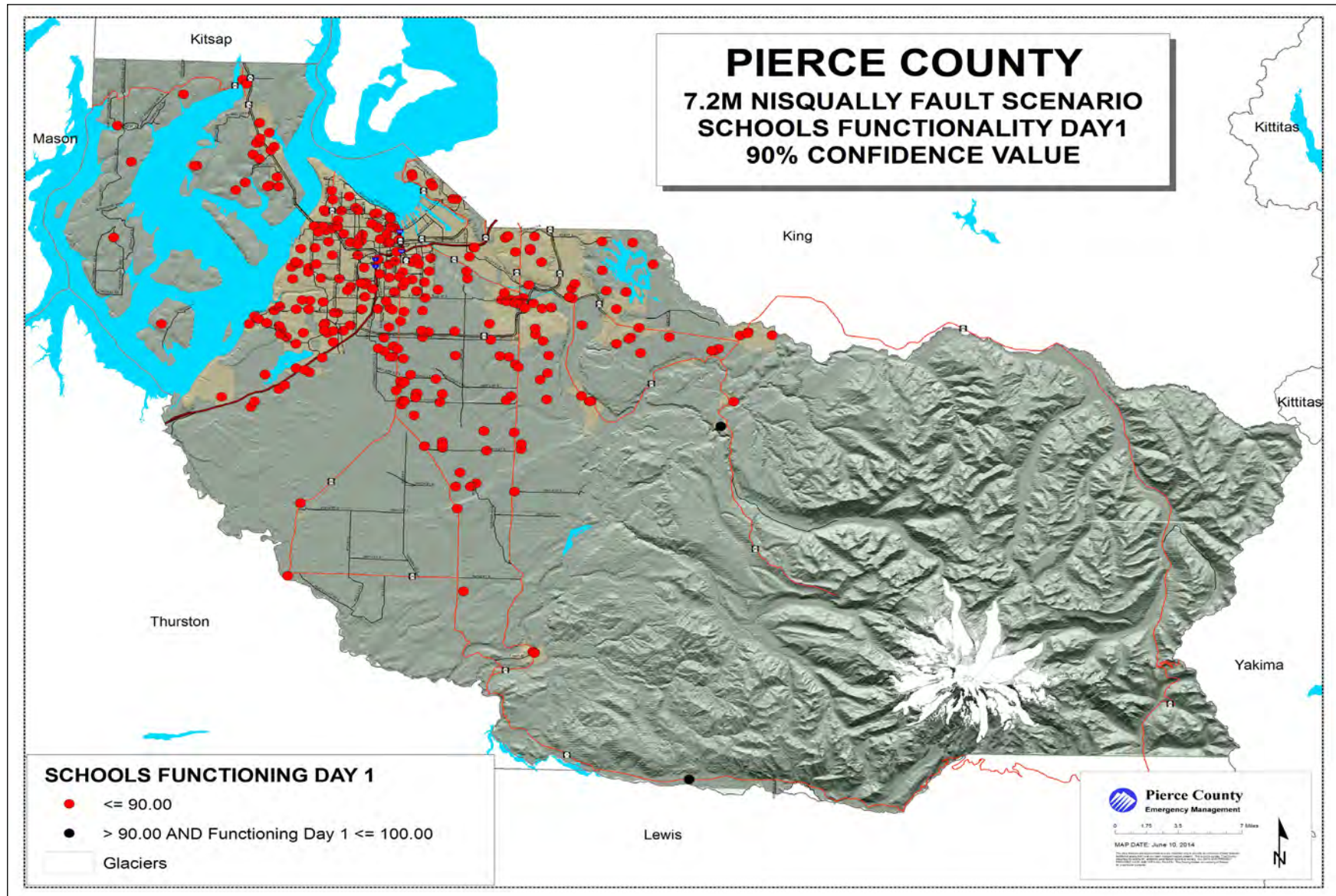
Map D-20 Pierce County Nisqually Fault Scenario Hospital Functionality Day 1 ⁷



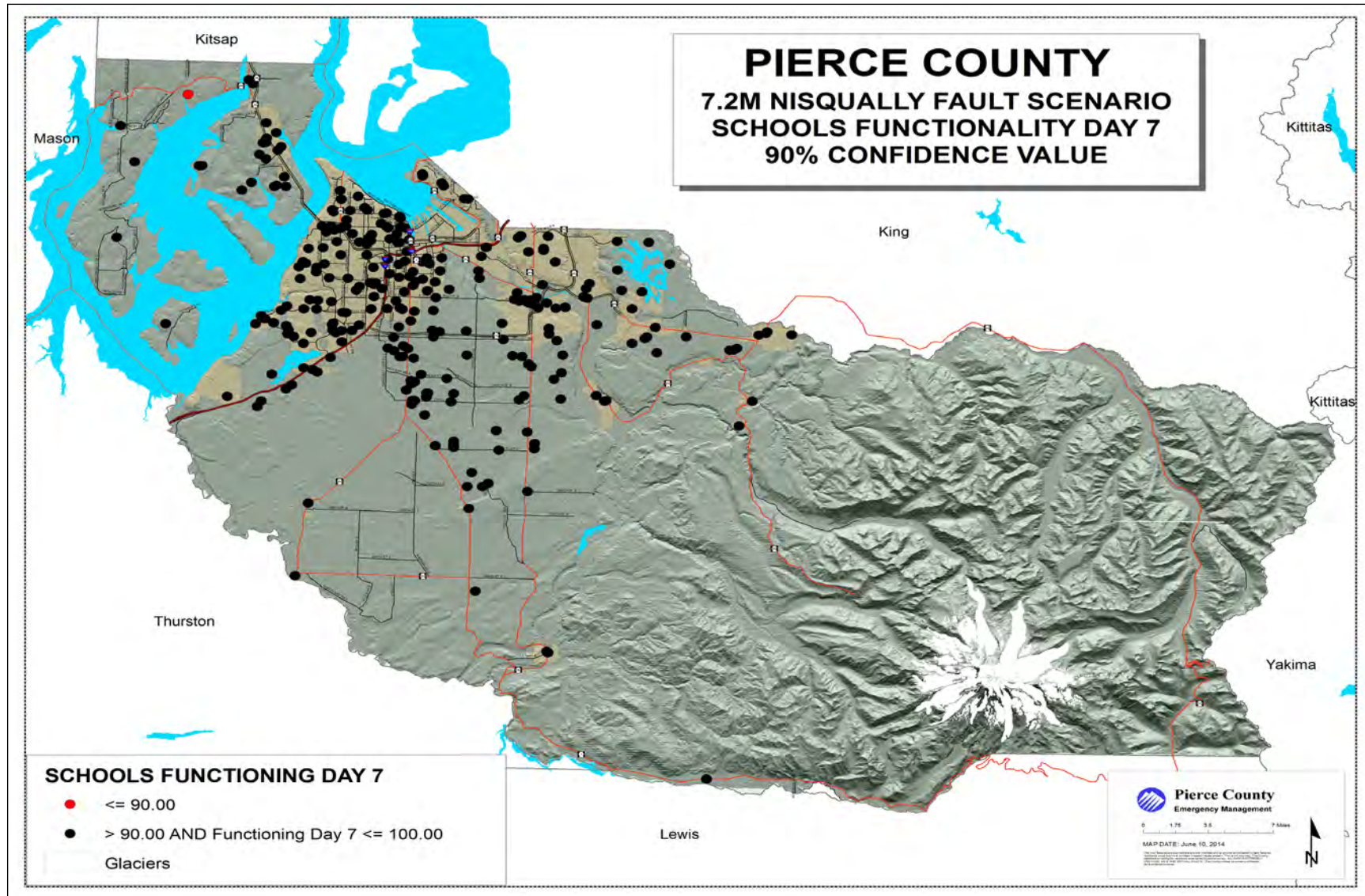
Map D-21 Pierce County Nisqually Fault Scenario Hospital Functionality Day 7⁸



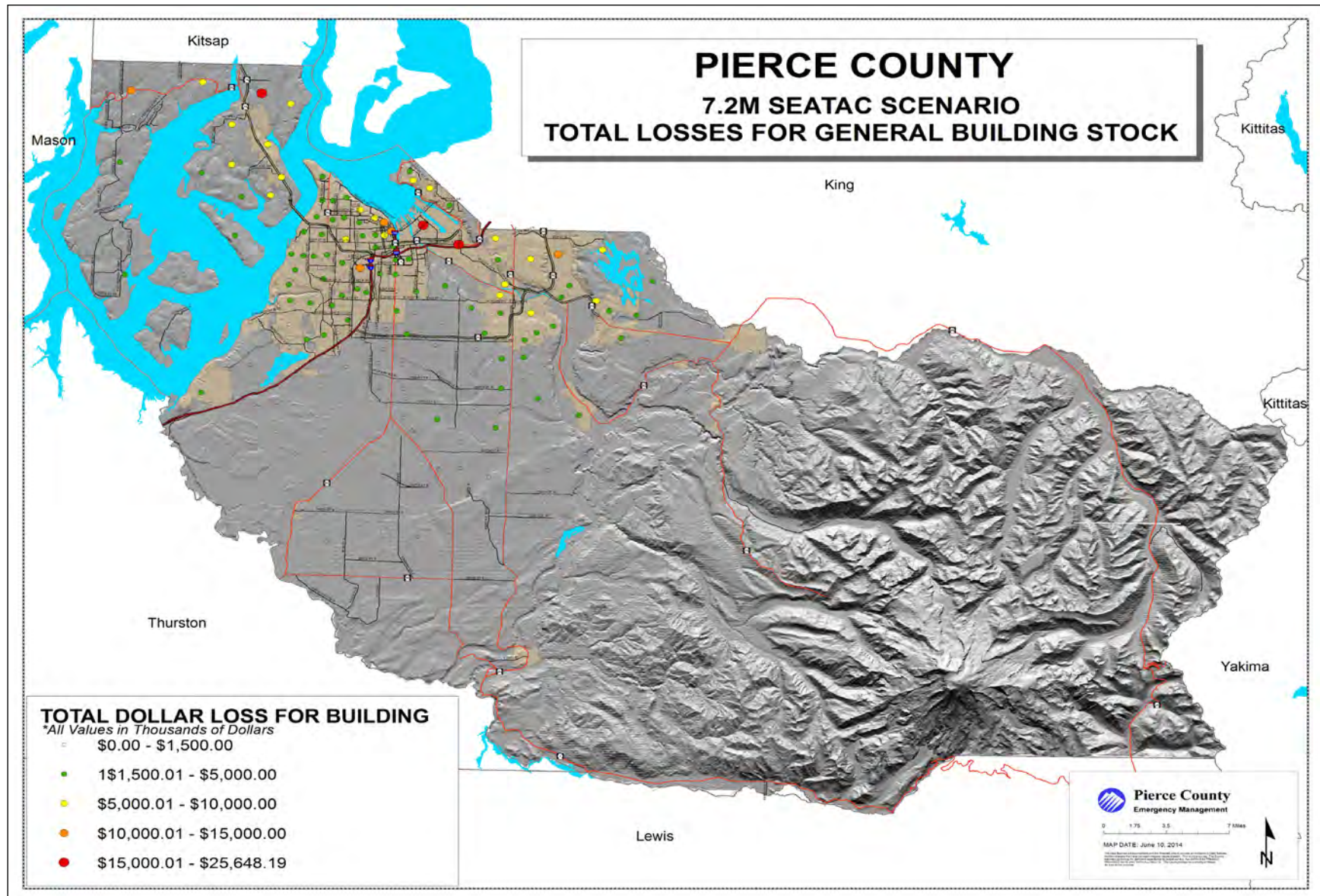
Map D-22 Pierce County Nisqually Fault Scenario Schools Functionality Day 1



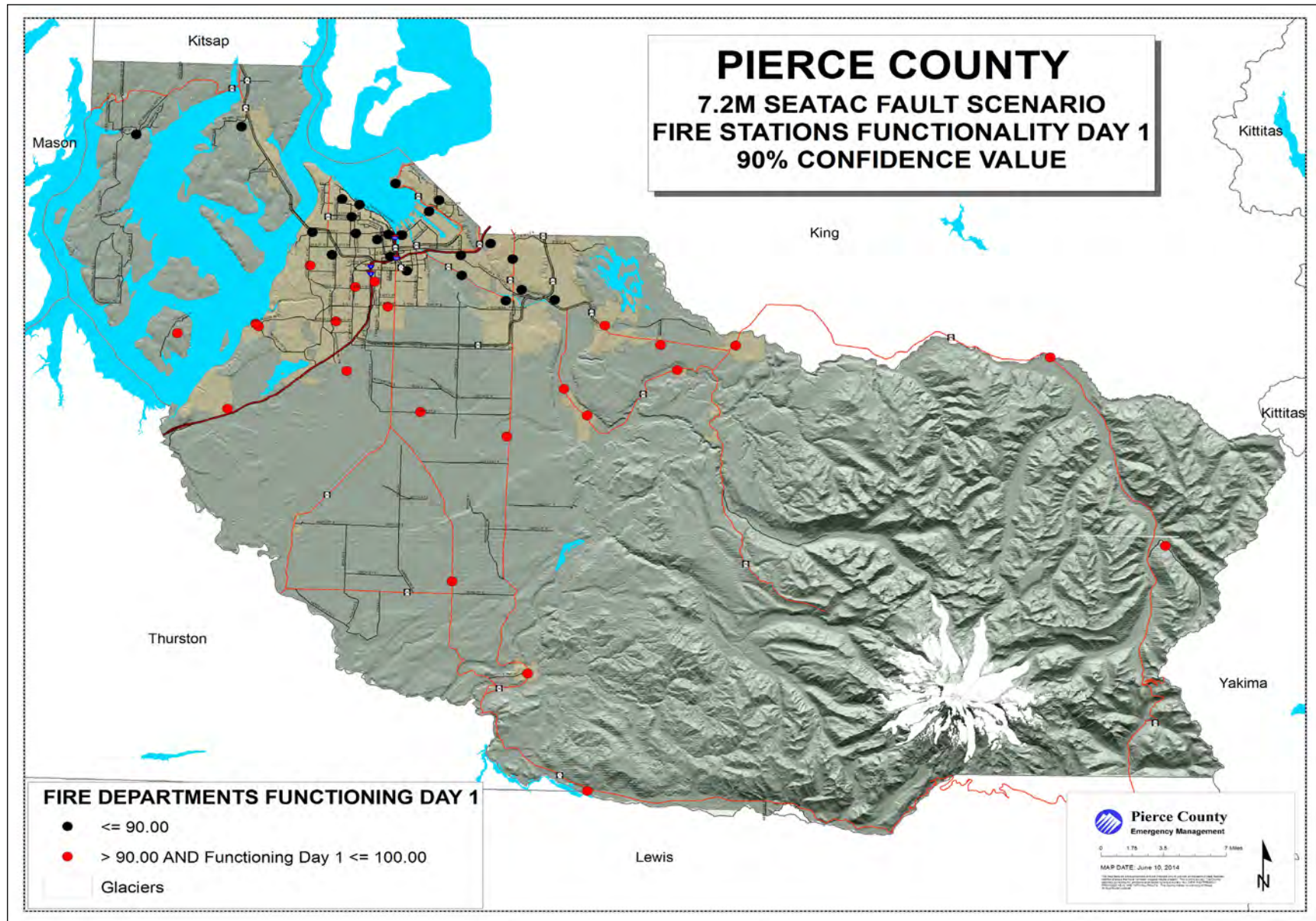
Map D-23 Pierce County Nisqually Fault Scenario Schools Functionality Day 7



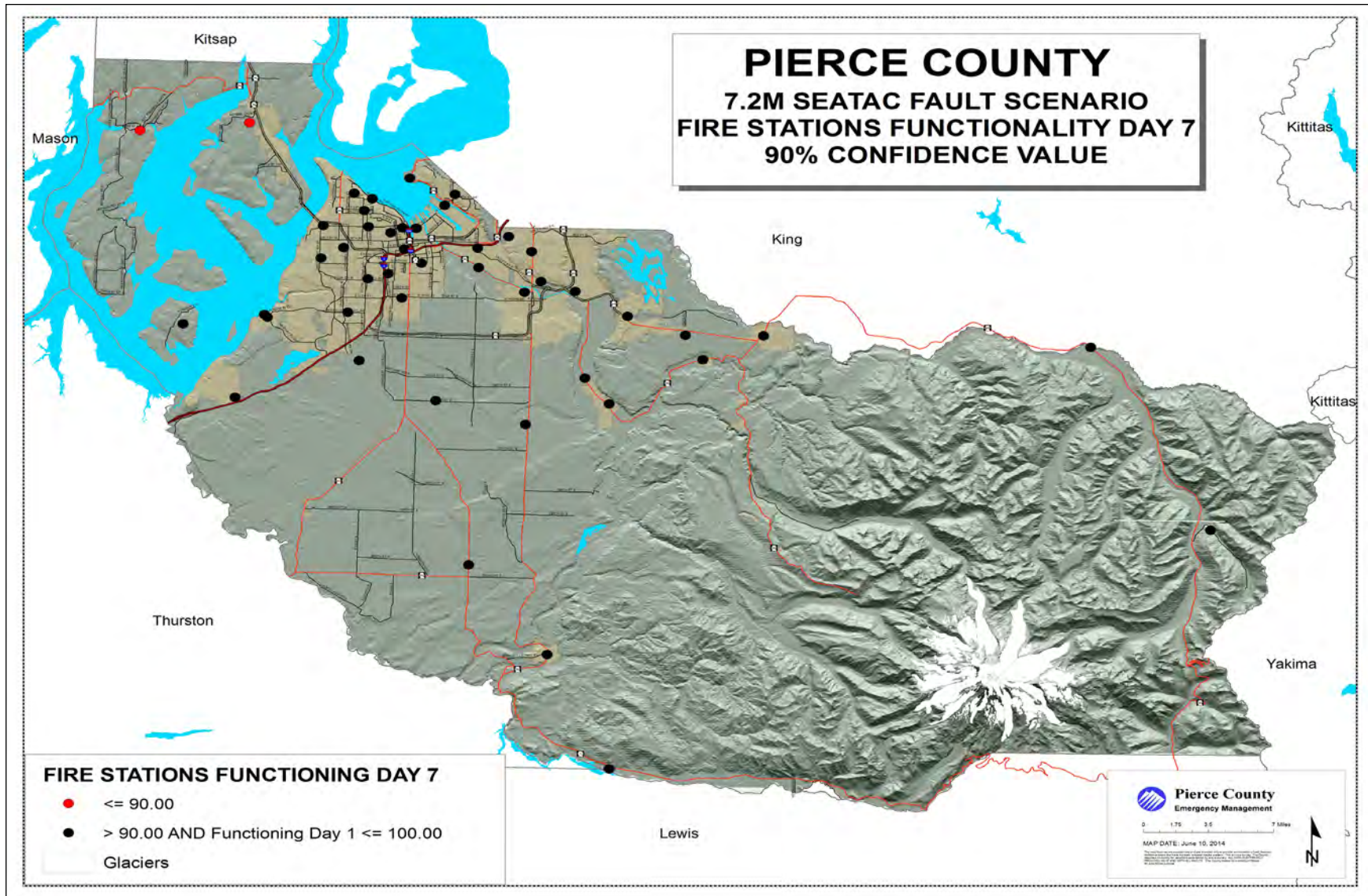
Map D-24 Pierce County SEATAC Fault Scenario Total Losses



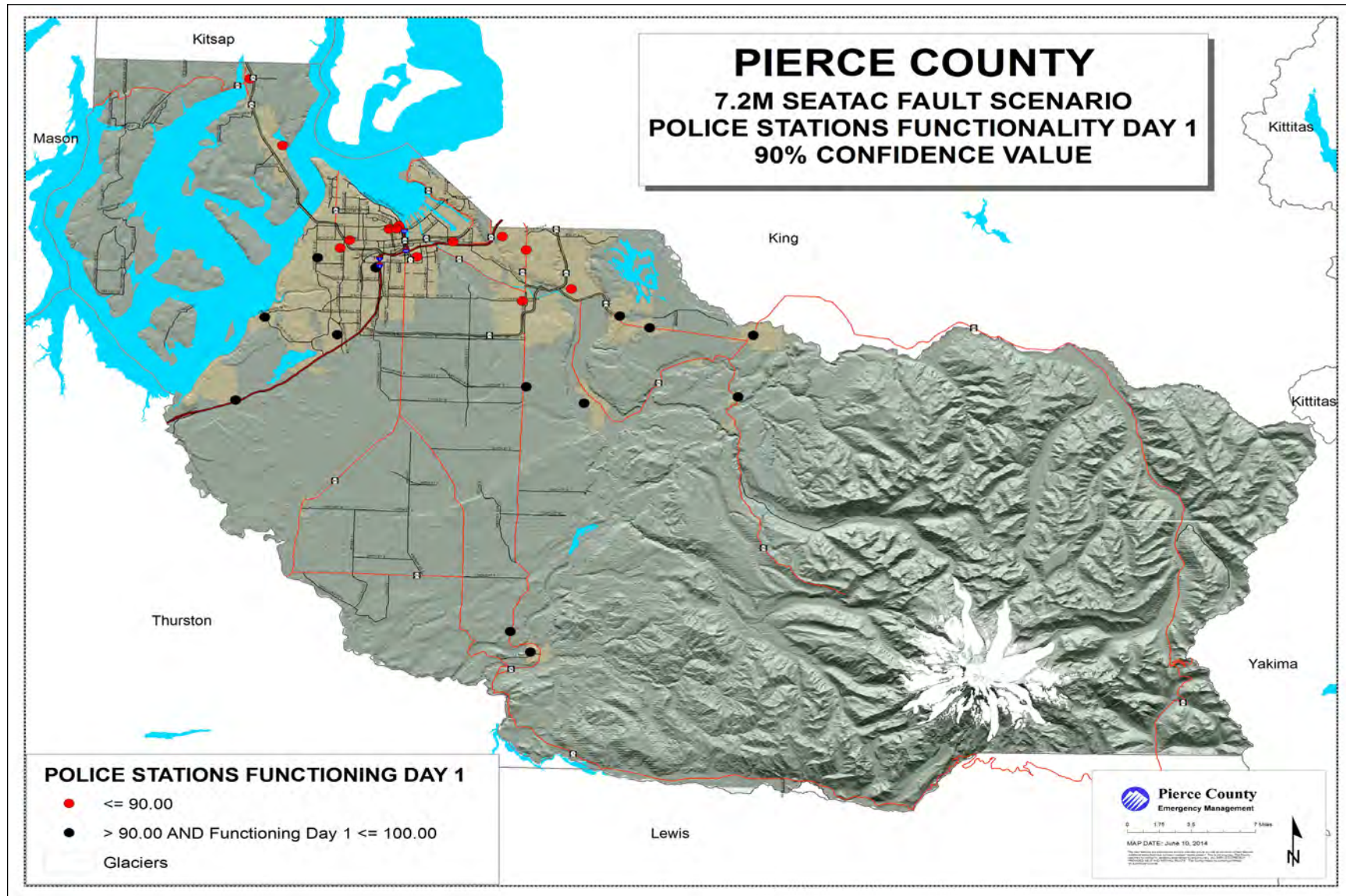
Map D-25 Pierce County SEATAC Fault Scenario Fire Stations Functionality Day 1



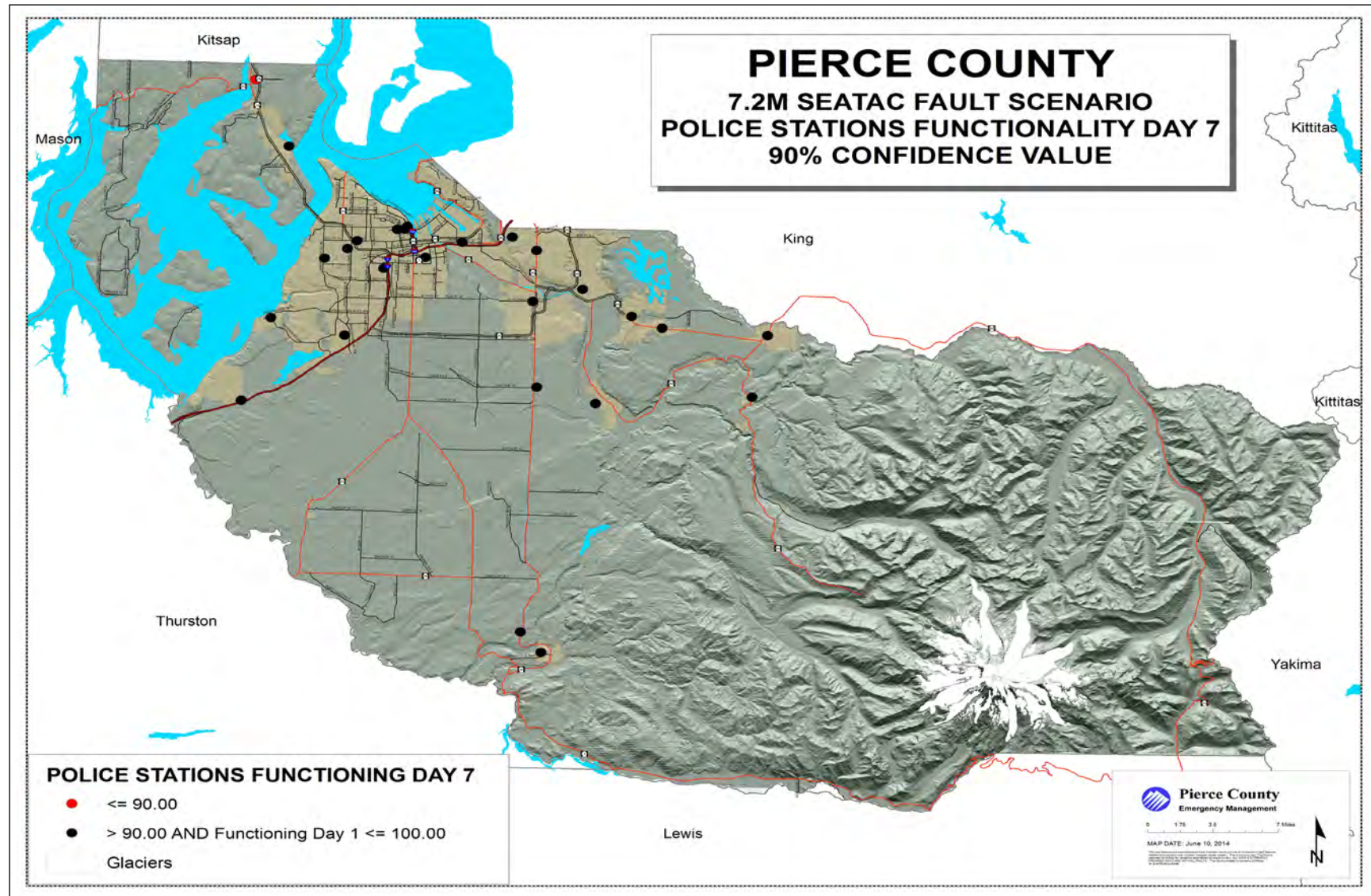
Map D-26 Pierce County SEATAC Fault Scenario Fire Stations Functionality Day 7



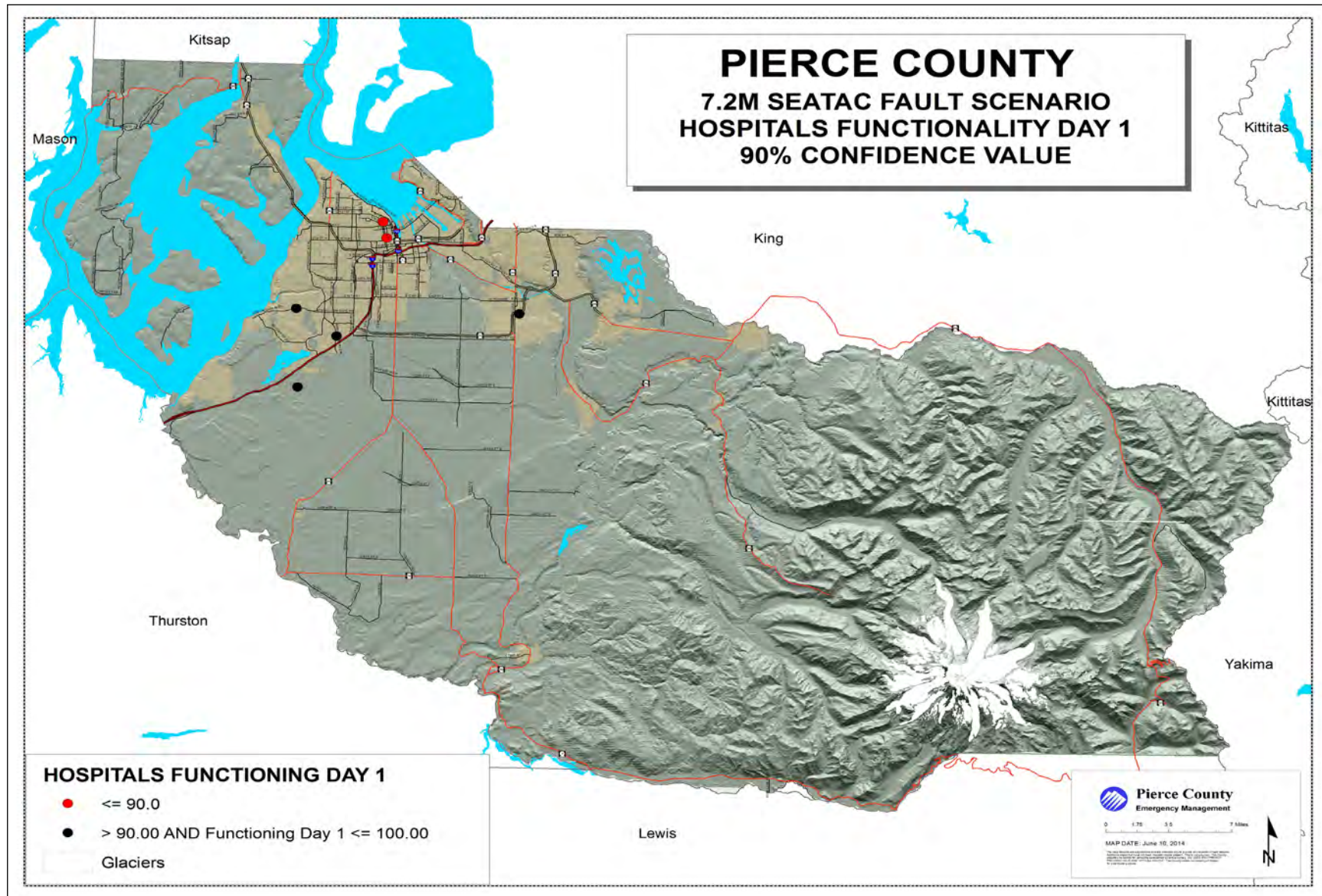
Map D-27 Pierce County SEATAC Fault Scenario Police Department Functionality Day 1 ⁹



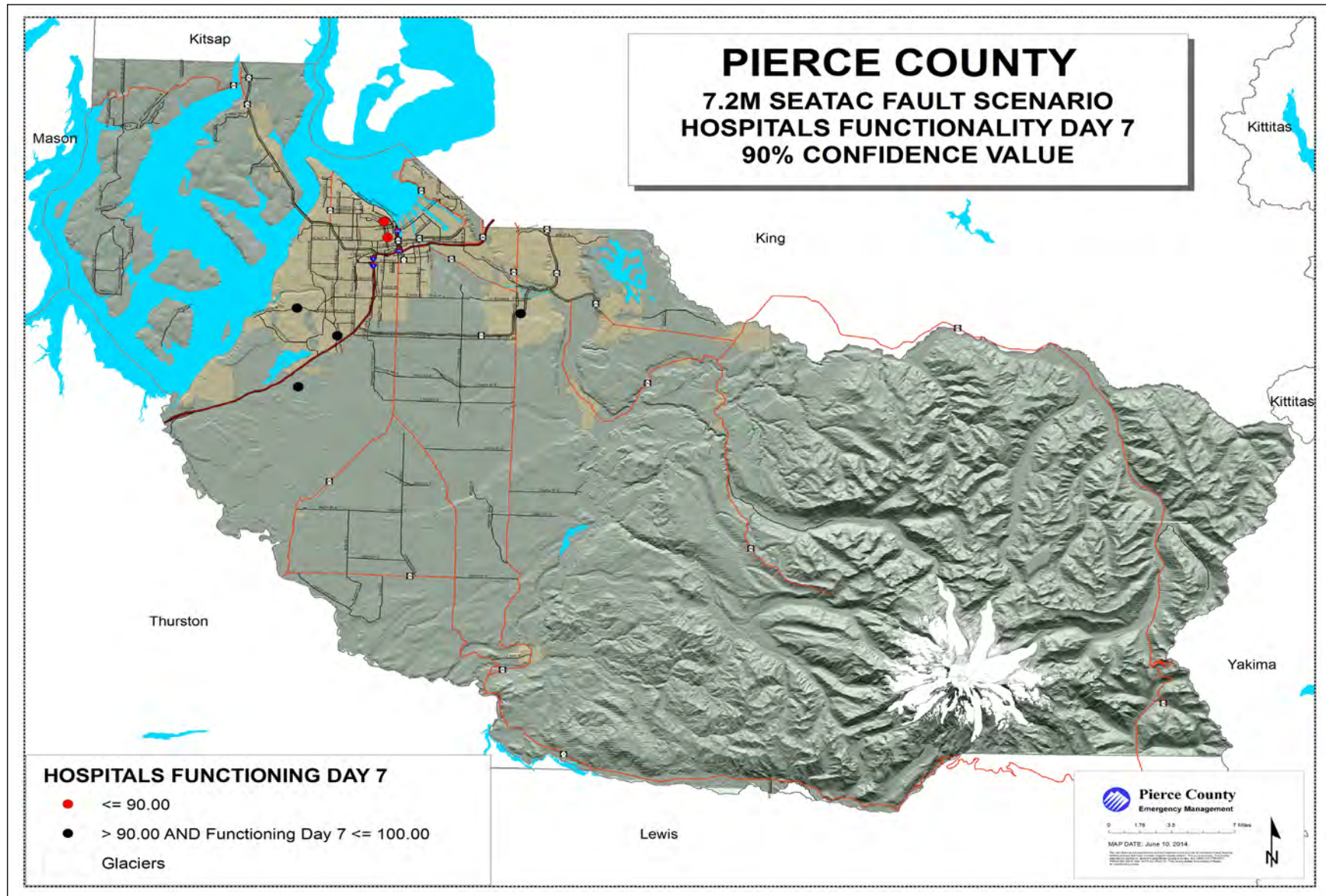
Map D-28 Pierce County SEATAC Fault Scenario Police Department Functionality Day 7¹⁰



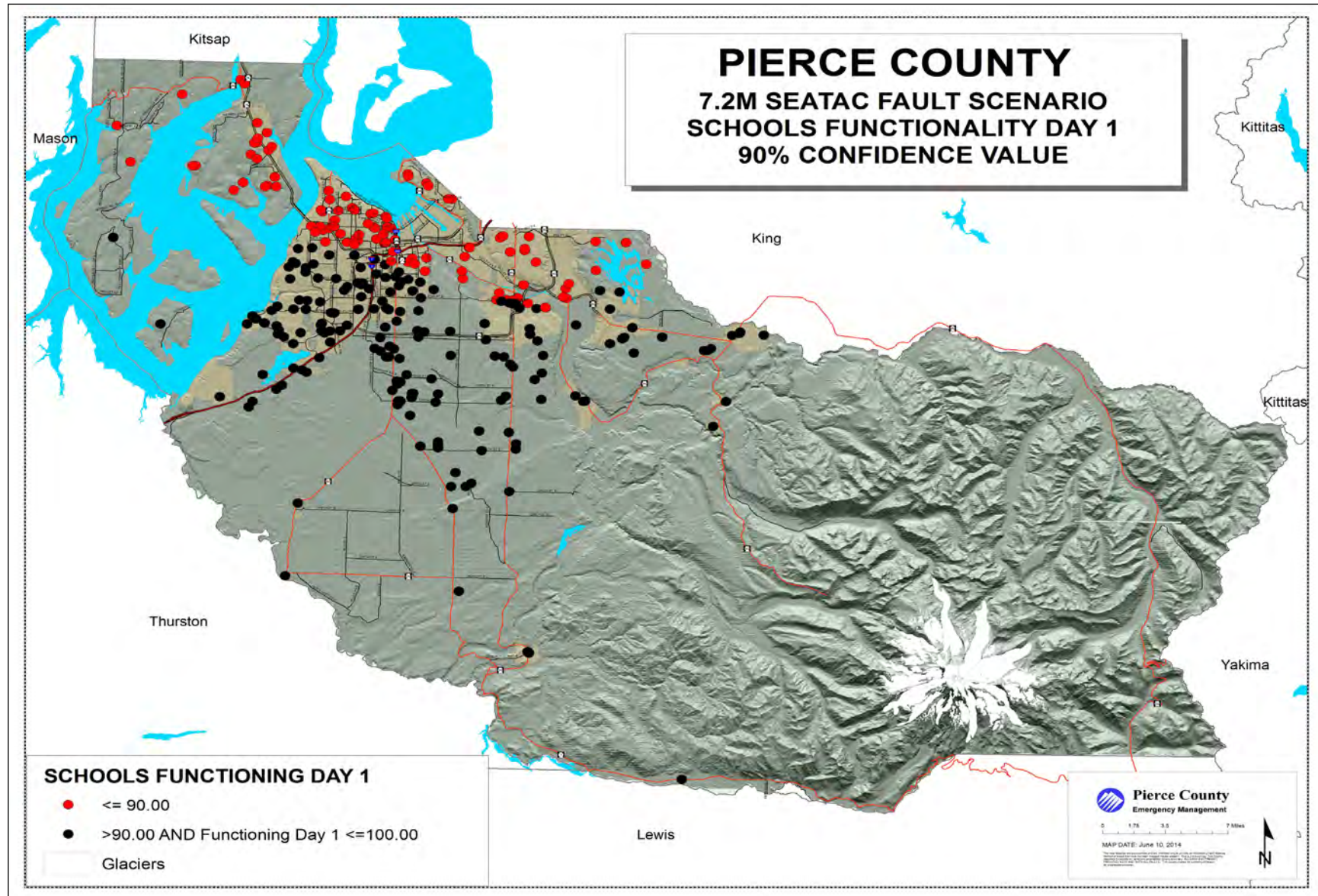
Map D-29 Pierce County SEATAC Fault Scenario Hospital Functionality Day 1 Map¹¹



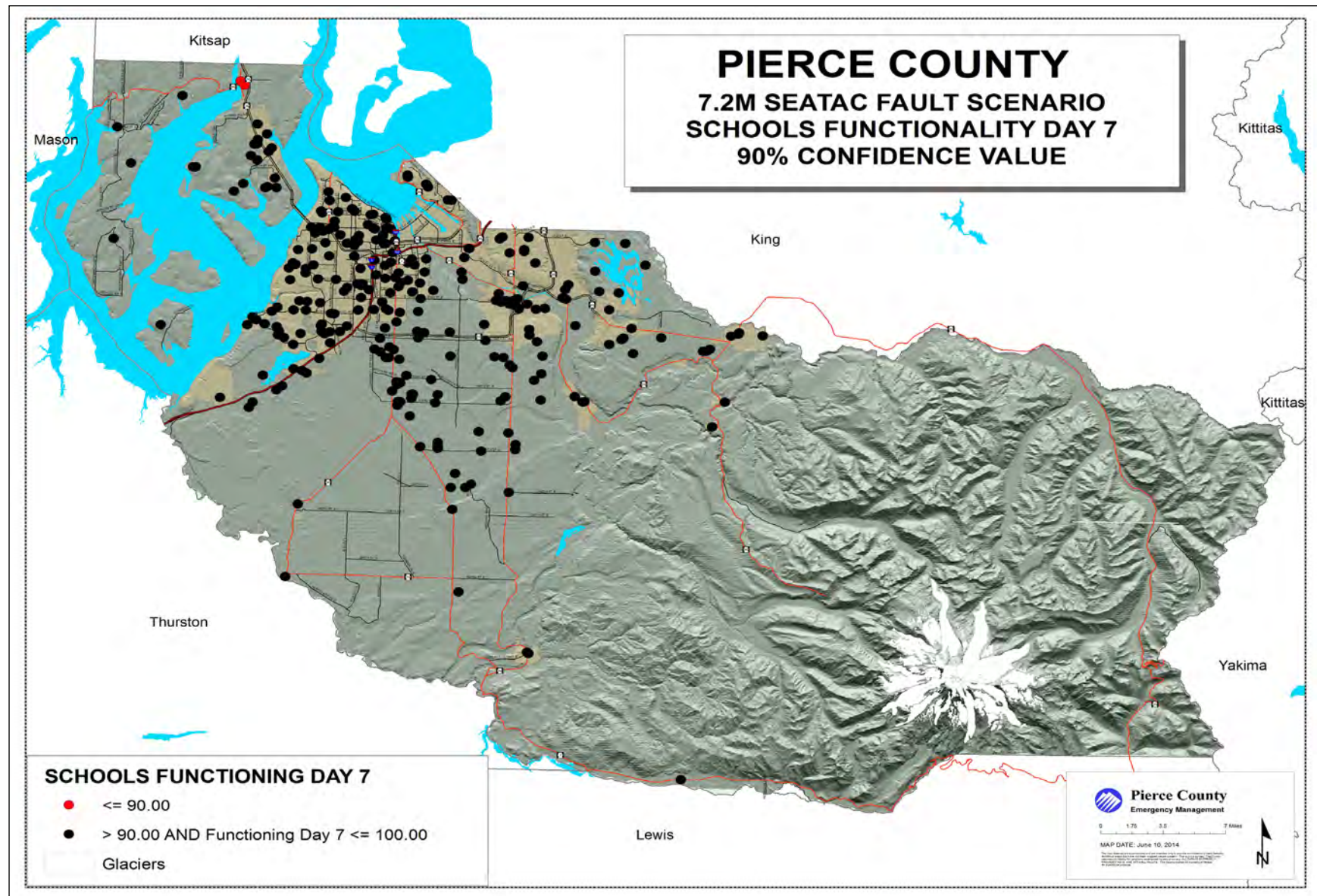
Map D-30 Pierce County SEATAC Fault Scenario Hospital Functionality Day 7¹²



Map D-31 Pierce County SEATAC Fault Scenario Schools Functionality Day 1



Map D-32 Pierce County SEATAC Fault Scenario Schools Functionality Day 7



Endnotes

¹ Hazus has placed the police station location incorrectly for the City of Orting. It should be located in the middle of the city with Fire District #18 as they share the same building.

² Hazus has placed the police station location incorrectly for the City of Orting. It should be located in the middle of the city with Fire District #18 as they share the same building.

³ St. Anthony's Hospital is not included on the map due to the recent construction of the hospital lack of data at the time the analysis was done.

⁴ St. Anthony's Hospital is not included on the map due to the recent construction of the hospital lack of data at the time the analysis was done.

⁵ Hazus has placed the police station location incorrectly for the City of Orting. It should be located in the middle of the city with Fire District #18 as they share the same building.

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¹⁰ Hazus has placed the police station location incorrectly for the City of Orting. It should be located in the middle of the city with Fire District #18 as they share the same building.

¹¹ St. Anthony's Hospital is not included on the map due to the recent construction of the hospital lack of data at the time the analysis was done.

¹² St. Anthony's Hospital is not included on the map due to the recent construction of the hospital lack of data at the time the analysis was done.

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APPENDIX E

REGION 5 ALL HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD DOCUMENTATION RECORDS

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SOCIAL MEDIA SCREEN SHOT – JANUARY 16, 2020 – FEBRUARY 15, 2020.....	11
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PUBLIC SAFETY ADVISORY COMMITTEE MINUTES – OCTOBER 2, 2019.....	13

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PUBLIC COMMENT DOCUMENTATION

Elected Officials Meeting – July 8, 2019



LAKEWOOD CITY COUNCIL STUDY SESSION AGENDA

Monday, July 8, 2019
7:00 P.M.

City of Lakewood
City Council Chambers
6000 Main Street SW
Lakewood, WA 98499

Page No.

CALL TO ORDER

ITEMS FOR DISCUSSION:

- (3) 1. Hazard Mitigation Plan Update. – *Ms. Debbie Bailey, Pierce County Emergency Management*
- (7) 2. Joint Lodging Tax Advisory Committee meeting. – (Workplan)
- (10) 3. Six Year (2019-2024) Information Technology Strategic Plan Update. – (Memorandum)

ITEMS TENTATIVELY SCHEDULED FOR THE JULY 15, 2019 REGULAR CITY COUNCIL MEETING:

- 1. Police Commissioning Ceremony.
- 2. Proclamation declaring July 27, 2019 as Pierce County Trails Day. – *Mr. Larry Leveen, Forever Green Trails*
- 3. Authorizing the execution of an interlocal agreement with the Washington State Department of Social and Health Services, in the amount of \$621,000, relative to the Western State Hospital community partnership program. – (Motion – Consent Agenda)
- 4. Authorizing the execution of an interlocal agreement with the Washington State Department of Social and Health Services, in the amount of \$90,000, relative to the Western State Hospital police protection program. – (Motion – Consent Agenda)
- 5. Appointing Carroll Ray Dodson to serve on the Public Safety Advisory Committee through August 6, 2022. – (Motion – Consent Agenda)
- 6. Approval of the special assessment roll for the American Lake – Lake Management District No. 1. – (Resolution – Consent Agenda)

The Council Chambers is accessible to persons with disabilities. Equipment is available for the hearing impaired. Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

<http://www.cityoflakewood.us>

7. Renewing interim controls initially adopted January 22, 2019 governing Wireless Service Facilities. – (Ordinance – Regular Agenda)
8. Adopting amendments to the Lakewood Comprehensive Plan, including the future land use and zoning map, and Lakewood Municipal Code Title 18A. – (Ordinance – Regular Agenda)

REPORTS BY THE CITY MANAGER**CITY COUNCIL COMMENTS****ADJOURNMENT**

The Council Chambers is accessible to persons with disabilities. Equipment is available for the hearing impaired. Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

<http://www.cityoflakewood.us>

- (3) A mitigation strategy that provides the jurisdiction's blueprint for reducing the potential losses identified in the risk assessment, based on existing authorities, policies, programs and resources, and its ability to expand on and improve these existing tools. This section **shall** include:
- (i) A description of mitigation goals to reduce or avoid long-term vulnerabilities to the identified hazards.
 - (ii) A section that identifies and analyzes a comprehensive range of specific mitigation actions and projects being considered to reduce the effects of each hazard, with particular emphasis on new and existing buildings and infrastructure.
 - (iii) An action plan describing how the actions identified in paragraph (c)(2)(ii) of this section will be prioritized, implemented, and administered by the local jurisdiction. Prioritization **shall** include a special emphasis on the extent to which benefits are maximized according to a cost benefit review of the proposed projects and their associated costs.
 - (iv) For multi-jurisdictional plans, there **must** be identifiable action items specific to the jurisdiction requesting FEMA approval or credit of the plan.
- (4) A plan maintenance process that includes:
- (i) A section describing the method and schedule of monitoring, evaluating, and updating the mitigation plan within a five-year cycle.
 - (ii) A process by which local governments incorporate the requirements of the mitigation plan into other planning mechanisms such as comprehensive or capital improvement plans, when appropriate.

- (iii) Discussion on how the community will continue public participation in the plan maintenance process.
- (5) Documentation that the plan has been formally adopted by the governing body of the jurisdiction requesting approval of the plan (e.g., City Council, County Commissioner, Tribal Council). For multi-jurisdictional plans, each jurisdiction requesting approval of the plan **must** document that it has been formally adopted.
- (d) **Plan review.**
- (1) Plans **must** be submitted to the State Hazard Mitigation Officer for initial review and coordination. The State will then send the plan to the appropriate IFMA Regional Office for formal review and approval.
 - (2) The Regional review will be completed within 45 days after receipt from the State, whenever possible.
 - (3) Plans **must** be reviewed, revised if appropriate, and resubmitted for approval within five years in order to continue to be eligible for HMGP project grant funding.

Successful HMGP applicants, as well as communities desiring to apply for mitigation grant funds in the future, will be required to develop an All Hazard Mitigation Plan utilizing these criteria.

For additional information please contact the HMGP Section at (253) 512-7073

*Jurisdictions with HMGP project or planning funds will have specific due dates for submittal of their plans. To receive Pre-Disaster Mitigation project grants, their mitigation plan must be submitted and approved prior to November 1, 2003.

Return to Agenda



State of Washington
**Emergency Management
Division**



Local Mitigation Planning Requirements Of the Disaster Mitigation Act of 2000

§201.6 of the Interim Final Regulations

November 2002

3

Local Mitigation Plan is the hazard mitigation plan required of a local or Indian tribal government acting as a **subgrantee as a condition of receiving a project subgrant under the HMGP** as outlined in 44 CFR 201.6.

Subgrantee means the government or other legal entity to which a subgrant is awarded and which is accountable to the grantee for the use of the funds provided. **Subgrantees can be a State agency, local government, private non-profit organizations, or Indian tribal government** as outlined in Sec. 206.433. Indian tribal governments acting as a subgrantee are accountable to the State grantee.

Plan requirement. (1) For all disasters declared on or after November 1, 2004*, local and tribal government applicants for subgrants, must have an approved local mitigation plan in accordance with 44 CFR 201.6 prior to receipt of HMGP subgrant funding. Until November 1, 2004*, local mitigation plans may be developed concurrent with the implementation of subgrants.

Sec. 201.6 Local Mitigation Plans

The local mitigation plan is the representation of the jurisdiction's commitment to reduce risks from natural hazards, serving as a guide for decision makers as they commit resources to reducing the effects of natural hazards. Local plans will also serve as the basis for the State to provide technical assistance and to prioritize project funding.

(a) Plan requirement.

- (1) For disasters declared after November 1, 2004, a local government **must** have a mitigation plan approved pursuant to this section in order to receive HMGP project grants. Until November 1, 2004, local mitigation plans may be developed concurrent with the implementation of the project grant.
- (2) Regional Directors may grant an exception to the plan requirement in extraordinary

circumstances, such as in a small and impoverished community, when justification is provided. In these cases, a plan will be completed within 12 months of the award of the project grant. If a plan is not provided within this timeframe, the project grant will be terminated, and any costs incurred after notice of grant's termination will not be reimbursed by FEMA.

- (3) Multi-jurisdictional plans (e.g. watershed plans) may be accepted, as appropriate, as long as each jurisdiction participates in the process, answers all the questions and has officially adopted the plan. State-wide plans will not be accepted as multi-jurisdictional plans.

(b) **Planning process.** An open public involvement process is essential to the development of an effective plan. In order to develop a more comprehensive approach to reducing the effects of natural disasters, the planning process **shall** include:

- (1) An opportunity for the public to comment on the plan during the drafting stage and prior to plan approval;
- (2) An opportunity for neighboring communities, local and regional agencies involved in hazard mitigation activities, and agencies that have the authority to regulate development, as well as businesses, academia and other private and non-profit interests to be involved in the planning process; and
- (3) Review and incorporation, if appropriate, of existing plans, studies, reports, and technical information.

(c) **Plan content.** The plan **shall** include the following:

- (1) Documentation of the planning process used to develop the plan, including how it was prepared, who was involved in the process, and how the public was involved.

- (2) A risk assessment that provides the factual basis for activities proposed in the strategy to reduce losses from identified hazards. Local risk assessments **must** provide sufficient information to enable the jurisdiction to identify and prioritize appropriate mitigation actions to reduce losses from identified hazards. The risk assessment **shall** include:

- (i) A description of the type, location, and extent of all natural hazards that can affect the jurisdiction. The plan **shall** include information on previous occurrences of hazard events and on the probability of future hazard events.
- (ii) A description of the jurisdiction's vulnerability to the hazards described in paragraph (c)(2)(i) of this section. This description **shall** include an overall summary of each hazard and its impact on the community. The plan **should** describe vulnerability in terms of:
 - (A) The types and numbers of existing and future buildings, infrastructure, and critical facilities located in the identified hazard areas;
 - (B) An estimate of the potential dollar losses to vulnerable structures identified in paragraph (c)(2)(i)(A) of this section and a description of the methodology used to prepare the estimate;
 - (C) Providing a general description of land uses and development trends within the community so that mitigation options can be considered in future land use decisions.
- (iii) For multi-jurisdictional plans, the risk assessment section **must** assess each jurisdiction's risks where they vary from the risks facing the entire planning area.

4

REGION 5 HAZARD MITIGATION PLANNING SUMMARY

Region 5 Hazard Mitigation Planning Effort Jurisdiction Participants (81)	
<u>21-Cities and Towns Group</u> <ol style="list-style-type: none"> 1. City of Bonney Lake 2. City of Buckley 3. City of Dupont 4. City of Edgewood 5. City of Fife 6. City of Fircrest 7. City of Gig Harbor 8. City of Lakewood 9. City of Milton 10. City of Orting 11. City of Puyallup 12. City of Roy 13. City of Sumner 14. City of Tacoma 15. City of University Place 16. Town of Carbonado 17. Town of Eatonville 18. Town of South Prairie 19. Town of Steilacoom 20. Town of Wilkeson 21. Pierce County Unincorporated 	<u>15-School District Group</u> <ol style="list-style-type: none"> 1. Carbonado School District 2. Clover Park School District 3. Dieringer School District 4. Eatonville School District 5. Fife School District 6. Franklin Pierce School District 7. Orting School District 8. Pacific Lutheran University 9. Peninsula School District 10. Puyallup School District 11. Steilacoom School District No. 1 12. Sumner/Bonney Lake School District 13. Tacoma School District 14. University Place School District 15. White River School District
<u>12-Fire District Group</u> <ol style="list-style-type: none"> 1. Fire District #3 - West Pierce 2. Fire District #5 - Gig Harbor) 3. Fire District #6 - Central Pierce 4. Fire District #13 - Browns Point 5. Fire District #14 - Riverside 6. Fire District #16 - Key Peninsula 7. Fire District #17 South Pierce 8. Fire District #18 - Orting 9. Fire District #21 - Graham 10. Fire District #22 – East Pierce 11. Fire District #23 – Ashford - Elbe 12. Fire District #27 – Anderson Island 	<u>14-Utility Group</u> <ol style="list-style-type: none"> 1. Clear Lake Water District 2. Firgrove Mutual District 3. Fruitland Mutual Water Company 4. Graham Hill Mutual Water Company 5. Lakeview Light and Power 6. Lakewood Water District 7. Mt. View-Edgewood Water Company 8. Ohop Mutual Light Company 9. Parkland Light and Water 10. Peninsula Light Company 11. Spanaway Water Company 12. Summit Water and Supply Company 13. Tanner Electric 14. Valley Water District
<u>10-Special Purpose Group</u> <ol style="list-style-type: none"> 1. American Red Cross 2. Crystal River Ranch 3. Crystal Village 4. Herron Island Improvement Club 5. Metro Parks 6. Pierce Transit 7. Port of Tacoma 8. Raft Island 9. Riviera Community Club 10. Taylor Bay 	<u>9-Health & Medical Group</u> <ol style="list-style-type: none"> 1. Cascade Regional Blood Services 2. Community Health Care 3. Dynamic Partners 4. Franciscan 5. Group Health 6. Madigan Hospital 7. MultiCare 8. Pierce County Health Department 9. Western State Hospital

PUBLIC COMMENT DOCUMENTATION

Farmers Market Outreach Booth – September 3, 2019

Hazard Mitigation 2020-2025



What is Hazard Mitigation?

Actions taken to reduce or eliminate the long-term risk to people, property, or the environment from hazards and their effects.

Examples:

- Hazardous Area Regulations
- Property Buy-Outs
- Elevating Homes
- Flood Control Dams
- Structural Seismic Retrofit
- Non-Structural Seismic Retrofit

What is Hazard Mitigation Planning? The process of determining the best means of how to reduce or eliminate the loss of life and property damage resulting from hazards.

Why do we need the plan?

The Disaster Mitigation Act of 2000 requires that local governments and public service agencies prepare natural hazard mitigation plans as a condition of future funding. Federal law requires an update, approval, and adoption of local mitigation plans every five years in order to remain eligible for



Hazard Mitigation 2020-2025

pre- and post-disaster mitigation funding. Since the first adoption of the plan, Pierce County has received hazard mitigation funds from five federally declared disasters totaling over \$5 million.

What are the goals of the Mitigation Plan?

- Protect life and property
- Ensure continuity of operations
- Establish and strengthen partnerships for implementation
- Preserve or restore natural resources
- Increase public preparedness for disasters
- Promote a sustainable economy

What is Pierce County's public participation process?

In order to ensure that the updated Region 5 All Hazard Mitigation Plan be comprehensive, the seven planning groups in conjunction with Pierce County Department of Emergency Management developed a public participation process of three components:

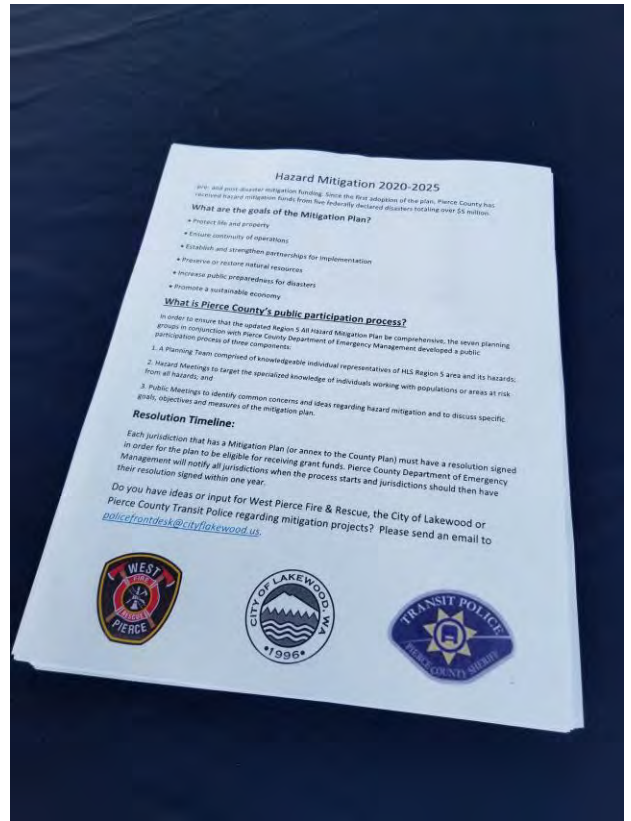
1. A Planning Team comprised of knowledgeable individual representatives of HLS Region 5 area and its hazards;
2. Hazard Meetings to target the specialized knowledge of individuals working with populations or areas at risk from all hazards; and
3. Public Meetings to identify common concerns and ideas regarding hazard mitigation and to discuss specific goals, objectives and measures of the mitigation plan.

Resolution Timeline:

Each jurisdiction that has a Mitigation Plan (or annex to the County Plan) must have a resolution signed in order for the plan to be eligible for receiving grant funds. Pierce County Department of Emergency Management will notify all jurisdictions when the process starts and jurisdictions should then have their resolution signed within one year.

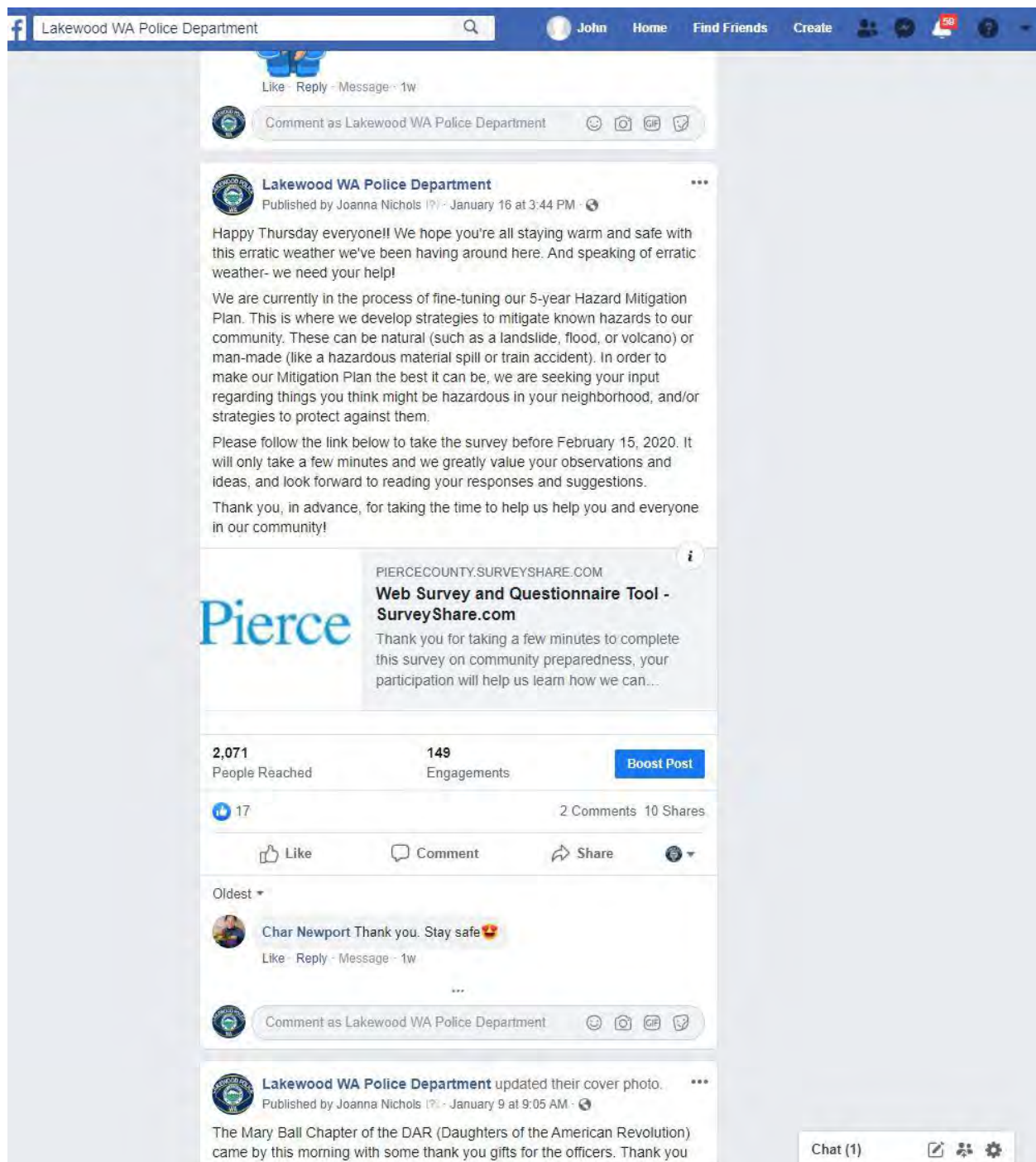
Do you have ideas or input for West Pierce Fire & Rescue, the City of Lakewood or Pierce County Transit Police regarding mitigation projects? Please send an email to policefrontdesk@cityflakewood.us.







Social Media Screen Shot – January 16, 2020 – February 15, 2020



Public Safety Advisory Committee Agenda – October 2, 2019



PUBLIC SAFETY ADVISORY COMMITTEE

Alan Hart • James Hairston • Ken Witkoe
Michael Arnett • Mark Peila • Ray Dotson • Mark Terry
Council Member Marie Barth, Committee Liaison
Police Chief Mike Zaro, Committee Staff Support

Regular Meeting, Wednesday, October 2, 2019 at 5:15 p.m

Lakewood Police Department
9401 Lakewood Drive SW
Lakewood, WA 98499

1. Call to Order
2. Roll Call
3. Approval of Minutes
4. Public Comments- (Members of the audience may comment on items that are not included on the agenda. Each person will be allowed 3 minutes to speak.)
5. Fire Department Comments
 - Presentation on Hazard Mitigation Plan
6. City Council Liaison Comments
7. Police Department Comments
8. Youth Council Comments
9. Unfinished Business
 - Fireworks Recommendation to Council
10. New Business
 - Preview of Next Meeting (December)
11. Reports from Board Members & Staff
 - Community Outreach Reports & Open Discussion on Public Safety
 - Pending Policy / Ordinance Changes
12. Adjournment

The Lakewood Police Station is accessible to persons with disabilities. Persons requesting special accommodations or language interpreters should contact Joanna Nichols, 830-5002, as soon as possible in advance of the meeting so that an attempt to provide the special accommodations can be made.

Public Safety Advisory Committee Minutes – October 2, 2019



PUBLIC SAFETY ADVISORY COMMITTEE

Regular Meeting Minutes
Wednesday, October 2, 2019
Lakewood Police Department
9401 Lakewood Drive SW
Lakewood, WA 98499

CALL TO ORDER

The meeting was called to order at 5:14 p.m.

ROLL CALL

Public Safety Advisory Committee Members Present: Alan Hart, James Hairston, Ken Witkoe, Michael Arnett, Ray Dotson, Mark Terry and Mark Peila

Public Safety Advisory Committee Members Excused: None

Public Safety Advisory Committee Members Absent: None

City Councilmember Present: Councilmember Marie Barth

Fire Department Staff Present: Assistant Fire Chief Hallie McCurdy

Lakewood Youth Council Present: Joo Gyeong Kim.

Staff Present: Chief Mike Zaro and Committee Staff Support; Joanna Nichols, Administrative Assistant

APPROVAL OF MINUTES

Mark Peila motioned to approve August's meeting minutes. All ayes; minutes were approved.

PUBLIC COMMENT

Kent Troy from the Oakbrook Neighborhood was in attendance and asked about a bicycle campaign, teaching proper rules for the road for both bicycles and cars sharing the road with the bicyclists, come spring.

FIRE CHIEF COMMENTS

Assistant Fire Chief Hallie McCurdy discussed the Mitigation Plan. Discussion ensued. Assistant Fire Chief McCurdy will email the plan to Joanna Nichols so she can send it out to PSAC members for perusal. Any comments or recommendations regarding the plan can be sent directly to Assistant Fire Chief McCurdy.

CITY COUNCIL LIAISON COMMENTS

Councilmember Marie Barth passed around the newest Community Economic Development brochure. Discussion ensued.

Michael Arnett asked about the development that was supposed to be going in on Military and 112th/Vernon. Councilmember Marie Barth stated she would try to find out and email it to Mr. Arnett.

POLICE CHIEF COMMENTS

Chief Michael Zaro went over the questions Chair Alan Hart had sent him prior to the meeting, which included shots fired reports on social media, car thefts/vehicle prowls, as well as discussing the Coffee with a Cop event which had one session that morning and another was happening now, during the meeting. Chief Zaro stated the Department intended to do another retail theft emphasis, also.

Chief Michael Zaro also stated he wanted to do another speed emphasis in the next month or two. Discussion ensued.

Alan Hart asked about the Fallen Officer Food Drive. Chief Michael Zaro stated it will be on November 26th this year, from 6 a.m. until 5:30 p.m. The Blood Drive portion will also be happening again, from 9 a.m. until 3 p.m.

Mark Peila asked about the Citizens Academy. Chief Michael Zaro gave a recap, stating there are only three classes left.

Ken Witkoe asked for a Reserve Program. Chief Michael Zaro stated he would have someone here at the next meeting.

YOUTH COUNCIL COMMENTS

Joo Gyeong Kim was present, but had to leave prior to her turn to share information.

UNFINISHED BUSINESS

The fireworks subcommittee/letter of recommendation was discussed. It was decided to vote on what recommendation to forward to the City Council. Vote was: 5 leave ordinance as is/status quo; 2 total ban. The subcommittee will write up a new recommendation letter, with help from Joanna Nichols, as needed; if anyone has anything else to add, they can send it to Ken or Joanna by October 31st. Ken will have the final letter to Joanna by November 15th.

NEW BUSINESS

Alan Hart stated that we need to start discussing SummerFest now. It will be on July 11th, 2020. A subcommittee was formed to work out the logistics for SummerFest, consisting of James Hairston, Mark Peila and Ray Dotson.

Alan Hart reminded everyone that each member had to take the OPMA (Open Public Meetings Act) training, and to speak with Joanna Nichols if you needed the links for the training to be resent.

Alan Hart stated that it was time to vote in the new Chair and Vice Chair.

REPORTS FROM BOARD MEMBERS & STAFF

Ray Dotson volunteered to attend the Tillicum Neighborhood Association Meetings. Other members gave their meeting reports, as applicable.

Michael Arnett asked some questions regarding protocols for Animal Control call outs. Discussion ensued.

ADJOURNMENT

Mark Peila motioned to adjourn the meeting; all ayes. Meeting adjourned at 6:40 p.m.

Public Safety Advisory Committee:

Attest:

Alan Hart, Vice Chair

Joanna Nichols, Secretary

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APPENDIX F

REGION 5 ALL HAZARD MITIGATION PLAN 2020-2025 EDITION CITY OF LAKEWOOD COMPLETED/DEFERRED MITIGATION STRATEGIES

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Deferred or Completed Mitigation Strategies

The following mitigation strategies were removed from the Section 5 Mitigation Plan and placed in Appendix F to retain them. They have either been deferred or completed. In the future as mitigation strategies are completed, they will be moved to this location to provide a history of accomplishments. Deferred mitigation strategies can be moved back into the “working” mitigation strategies of Section 5 at any time.

Table F-1 City of Lakewood Mitigation Strategy Matrix

Implementation Mechanism	Mitigation Measure (<i>Hazard(s)</i>) ¹	Lead Jurisdiction(s) / Department(s)	Timeline (years)	Plan Goals					
				Life and Property	Operations Continuity	Partnerships	Natural Resources	Preparedness	Sustainable Economy
<u>Deferred</u>	1. Identify Critical Routes and Determine Alternate Routes (<i>E,F,SW</i>)	Lakewood – Public Works Engineering	5	✓	✓	✓			
	2. Radio Communications Set-Up Between all City-Owned Vehicles, Buildings, and EOC (<i>E,F,SW</i>)	Lakewood	5	✓	✓				
	3. Evacuation Plan Template for Multiple Events (<i>E,V,F,SW,WUI,MM</i>)	Lakewood - Police with West Pierce Fire & Rescue	1 to 2	✓	✓	✓			
<u>Completed</u>	1. Generator Retrofit of Park Operations Center (<i>E,SW,MM</i>)	Lakewood - Parks	Complete	✓	✓			✓	
	2. Structural Retrofit of Park Operations Center (<i>E,SW</i>)	Lakewood – Parks	Complete	✓	✓			✓	
	3. Create an Emergency Operations Center in Lakewood City Hall (<i>E,V,F,SW,WUI,MM</i>)	Lakewood - Emergency Management	Complete	✓	✓	✓		✓	✓
	4. National Flood Insurance Program (<i>F</i>)	Lakewood (Community Development); PC PWU	Complete	✓	✓	✓	✓	✓	☐

Deferred Mitigation Measures

Identify Critical Routes and Determine Alternate Routes

Hazards: E, F, SW¹

The City is unique with its bodies of water and neighborhoods surrounded by other jurisdictional boundaries – effectively making them islands with one route in and out. Determine these isolated areas and determine alternate access routes (e.g. via through private property, via Fort/Base lands, etc.) and develop agreements as needed that can be implemented during an emergency.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Establish and Strengthen Partnerships for Implementation.
2. **Cost of Measure** = TBD
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Public Works
5. **Timeline** = Long-term
6. **Benefit** = Traveling public and/or emergency vehicles over specific route.
7. **Life of Measure** = 100 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
			X
Comments			
This was in our last plan but was deferred. We want to include in the new plan.			

Origin

Previous Plan	Current Plan
X	

Radio Communications Set-Up Between all City-Owned Vehicles, Buildings, and EOC

Hazards: E, F, SW¹

Provide radios for back-up radio communication (when all of the cell phones go down). Determine if communication can also be set up with other public agencies providing mutual aid.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations.
2. **Cost of Measure** = TBD
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants and state or federal grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Public Works

5. **Timeline** = Short-term
6. **Benefit** = Traveling public and/or emergency vehicles over specific route.
7. **Life of Measure** = 100 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
			X
Comments			
This was in our last plan but was deferred. We want to include in the new plan.			

Origin

Previous Plan	Current Plan
X	

Evacuation Plan Template for Multiple Events

Hazards: E, V, F, WUI, MM, SW¹

The police department will develop an SOP that will outline recommended techniques and procedures for conducting an evacuation of an area or complex in the event of different types of emergencies such as a train derailment, active shooter, bomb threat, or other events.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Establish and Strengthen Partnerships for Implementation.
2. **Cost of Measure** = Staff Time
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood - Police Department and Lakewood Fire
5. **Timeline** = Ongoing
6. **Benefit** = Residents and businesses of Lakewood, visitors and regional partners
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
			X
Comments			
This was in our last plan but was deferred. We want to include in the new plan.			

Origin

Previous Plan	Current Plan
X	

Completed Mitigation Measures

Generator Retrofit of Park Operations Center

Hazards: E, MM, SW¹

As part of the electrical upgrades to the shop, a generator switch will be installed so in the event power is lost, the operations center will have power to operate the fueling station, and most power operations in the shop.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Increase Public Preparedness for Disasters.
2. **Cost of Measure** = Already implemented as electrical upgrades to operation center
3. **Funding Source and Situation** = Funding could be obtained through local budget and grants.
4. **Lead Jurisdiction(s)** = City of Lakewood - Parks Department
5. **Timeline** = Short-Term
6. **Benefit** = Parks Operation Center, City of Lakewood
7. **Life of Measure** = 30 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X			
Comments			
This was completed.			

Origin

Previous Plan	Current Plan
X	

Structural Retrofit of Park Operations Center

Hazards: E, SW¹

Collar ties were added to the roof to strengthen the integrity of the Park Operations Center. Posts and walls were added to strengthen the loft inside the shop, and a sheer wall was added inside to add strength against side to side movement.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Increase Public Preparedness for Disasters.
2. **Cost of Measure** = Already implemented as part of upgrades made to the operations center.
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood – Parks Department
5. **Timeline** = Short-term
6. **Benefit** = Park Operations, City of Lakewood

7. **Life of Measure** = 30 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X			
Comments			
This was completed.			

Origin

Previous Plan	Current Plan
X	

Create an Emergency Operations Center in Lakewood City Hall

Hazards: E, V, F, WUI, MM, SW¹

The City of Lakewood will create an emergency operations center that has the necessary equipment for city EOC staff to operate effectively.

1. **Goal(s) Addressed** = Protect Life and Property; Ensure Continuity of Operations; Establish and Strengthen Partnerships for Implementation; Increase Public Preparedness for Disasters; Promote a Sustainable Economy.
2. **Cost of Measure** = Time and Materials and special equipment
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants.
4. **Lead Jurisdiction(s)** = City of Lakewood - Emergency Management Director
5. **Timeline** = Short-term
6. **Benefit** = Residents and businesses of Lakewood, visitors, community and regional partners
7. **Life of Measure** = 5-10 years
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X			
Comments			
We created an EOC, but it was done in the Police Station and not City Hall. But, it is complete.			

Origin

Previous Plan	Current Plan
X	

National Flood Insurance Program

Hazards: F

Lakewood will ensure that the City is compliant with the National Flood Insurance Program by updating floodplain identification and mapping, enforcing the flood damage prevention ordinance, and providing public education on floodplain requirements and impacts. The City of Lakewood will be an active participant in the Pierce County Flood Control District.

1. **Goal(s) Addressed** = Protect life and property; Ensure Continuity of Operations; Increase Public Preparedness; Increase and Strengthen Partnerships; Protect the Environment; Increase Public Preparedness
2. **Cost of Measure** = Staff time, special materials required, permits
3. **Funding Source and Situation** = Funding could be obtained through local budget or grants
4. **Lead Jurisdiction(s)** = Lakewood (Community Development); PC PWU
5. **Timeline** = On-going
6. **Benefit** = City-wide; Regional
7. **Life of Measure** = Perpetual
8. **Community Reaction** = the proposal is likely to be endorsed by the entire community.

Status Update: 2020 – 2025 Edition

Complete	Ongoing	Partially Complete	Deferred
X			
Comments			
The City amended its floodplain regulations to be compliant back in 2017 or 2018. This was followed by adopting updated shoreline regulations. The City has since found an error in the federal floodplain map and has submitted a request to amend the map to fix this.			

Origin

Previous Plan	Current Plan
X	

Endnotes

¹ Hazard Codes:

WHERE NECESSARY, THE SPECIFIC HAZARDS ADDRESSED ARE NOTED AS FOLLOWS:

A:	AVALANCHE
E:	EARTHQUAKE
F:	FLOOD
D:	DROUGHT
T:	TSUNAMI
V (L OR T):	VOLCANIC (LAHAR OR TEPHRA-SPECIFIC)
SW:	SEVERE STORM (WIND-SPECIFIC)
L:	LANDSLIDE
WUI:	WILDLAND/URBAN INTERFACE FIRE
MM:	MANMADE TO INCLUDE TERRORISM
ALL:	ALL HAZARDS, INCLUDING SOME MAN MADE. WHERE ONLY NATURAL HAZARDS ARE ADDRESSED, IT IS NOTED.

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TO: Mayor and City Councilmembers

FROM: David Bugher, Assistant City Manager for Development Services, and
Jeff Gumm, Housing Program Manager

THROUGH: John J. Caulfield, City Manager *John J. Caulfield*

DATE: April 19, 2021, City Council Meeting Date

SUBJECT: Lakewood CDBG CARES Act CDBG-CV 2

Background: The purpose of this memorandum is threefold:

- 1) To provide Council with an introduction to the Coronavirus Aid, Relief, and Economic Security Act (CARES Act) CDBG-CV2 program including eligible funding options;
- 2) Provide recommendations on CDBG-CV2 funding; and
- 3) Lays out a timeline for compliance and project completion.

CDBG-CV2 Funding Information: On April 1, 2021 the City received notification from the Washington State Department of Commerce the City was eligible for \$136,706 in CDBG-CV2 funding through its status as a CDBG entitlement community. Funding for the CDBG-CV2 program would be provided through \$15 million in funds set aside by Commerce as part of the Coronavirus Aid, Relief, and Economic Security Act (CARES Act), Public Law 116-136. Allocations are based upon a population/LMI based formula and can be used for locally-determined priority activities. Program requirements are similar to CDBG-CV1 & 3, which the City received directly from the federal government.

What Can The Funds Be Used For? As provided under the CARES Act, CDBG-CV funding must to be used to prevent, prepare for, and respond to the social and economic impacts of the coronavirus outbreak. Eligible activities include: 1) Public service activities; 2) Housing-related activities; 3) Public improvements and facilities; 4) Real property acquisition; 5) Economic development activities; and 6) General administrative and planning activities. All CDBG funded activities must benefit low-and moderate-income persons or meet the CDBG urgent need national objective criteria.

Common uses of funds include:

- Microenterprise Assistance – assistance to businesses with 5 or fewer employees;
- Service-type Assistance – services including legal services, homeless services, health-related services, subsistence payments (rental assistance payments which are one-time or short-term not to exceed 6 months), food banks, housing counseling, etc.;
- Public Facilities – assistance to homeless facilities and neighborhood facilities, including acquisition, construction, and rehabilitation; and
- Planning and Administration – general planning and administration associated with federal regulation compliance.

* For a detailed description of eligible activities, see attached CARES Act CDBG-CV2 Grants – Eligible Activities Guide.

Funding Recommendation: The following table lists funding recommendations to assist low- and moderate-income Lakewood residents adversely affected by the coronavirus pandemic. Recommendations include an alternate funding option should funds not be fully expended for primary activities:

FUNDING RECOMMENDATIONS	CDBG-CV2	CDBG Eligible Activity
Living Access Support Alliance (LASA) Client Service Center Rehab	\$121,706	Public Facilities (03E) - Neighborhood Facilities. Rehab of existing facility to include access to washing machines, bathroom facilities, and general hygiene to homeless persons.
Administration	\$15,000	General Administration (21A) – General administration associated with construction and monitoring.
Total Funding	\$136,706	
<i>Alternate: Living Access Support Alliance (LASA) Client Service Center Homeless Programs</i>	<i>Any unexpended funds remaining</i>	<i>Public Services (03T) – Homeless Programs. Costs associated with the operation of homeless programs.</i>

Staff is making the above funding recommendations in part due to the short timelines involved and the previous CDVG-CV1 and 3 program's focus on business assistance, and rental & mortgage assistance. Staff recommends funding Living Access Support Alliance (LASA) Client Services Center Rehab in the amount of \$121,706. Funds would be used to remodel and reorient laundry facilities and bathroom access along the W elevation, away from the facility's main entrance to provide access to homeless individuals looking to wash clothing and access to general hygiene. LASA recently went through general design and project estimation to develop this project. Early estimate for this project was approximately \$85,000; however, additional bidding and construction requirements subject to CDBG regulations have City estimates at approximately \$100,000 - \$115,000. Additional funding

for administration is recommended at \$15,000 to cover staff costs for Davis-Bacon construction compliance and overall CDBG program requirements. Alternative funding for LASA homeless programs are recommended should any funds remain unexpended after construction is completed.

Program Timeline: Table 2 outlines CDBG-CV2 milestones, including upcoming public participation requirements, submission deadlines, and program closeout. These timelines are very short and will require staff to closely monitor progress.

TABLE 2 CDBG-CV2 TIMELINE & MAJOR MILESTONES	
April 1, 2021	Commerce notified Lakewood of its eligibility to receive \$136,706 in CDBG-CV2 funding.
April 16, 2021	Notice required in TNT of Public Hearing on April 21, 2021.
<i>April 19, 2021</i>	<i>Council review of proposed use of CDBG-CV2 funds.</i>
April 16 – April 22, 2021	Formal public comment period on use of CDBG-CV2 funds.
April 21, 2021	Public Hearing before CSAB to receive public input on use of CDBG-CV2 funds.
April 30, 2021	Deadline to submit CDBG-CV2 funding application to Commerce.
May 2021	Contract execution with Commerce.
May-June 2021	Execute contract(s) with program subrecipient(s)/ Begin program rollout.
June 1 – June 15, 2023	Conduct a public hearing to review and receive public comment on program performance prior to program closeout.
June 30, 2023	CDBG-CV2 program end date with Commerce.

CARES Act CDBG-CV2 Grants

Eligible Activities Guide

ELIGIBLE ACTIVITIES

CDBG eligible activities are listed in the Housing and Community Development Act of 1974, Section 105 (the Act) [here](#). The primary CDBG-CV2 eligible activities are defined in the following pages. Activities and local assistance programs determined by a CDBG entitlement jurisdiction as CDBG-CV eligible will be accepted in most cases by the state CDBG program.

CDBG-CV2 funds can extend or expand CV1 (and CV3) funded activities for expedient use of funds and to continue successful programs. If new local priorities have emerged, CDBG-CV2 can fund a new CDBG eligible activity that prevents, prepares for, or responds to coronavirus. Funding new activities may require an amendment.

Following are definitions, HUD activity codes and further guidance on CDBG-CV2 eligible activities.

MICROENTERPRISE ASSISTANCE

HUD CODE
18C

HUD defines a microenterprise as a commercial enterprise having five or less employees, including the owner(s). To be eligible for CDBG-CV funding, the business must have a physical location and be able to document loss of revenue due to COVID-19 impacts. Or the funding can expand a microenterprise that provides COVID-19 response services such as medical, food delivery, cleaning and other services to support home health and quarantine.

CDBG funds can support a local microenterprises assistance program by funding:

1. Provisions of credit (including grants) for the establishment, stabilization, and expansion of microenterprises. Initial amounts generally not to exceed \$25,000. Grant money may be used only for the operating expenses of the awarded business. The operating expenses are defined as the day-to-day trading operations of the business such as covering payroll and rent.
2. General and technical assistance, advice and business support services to owners of microenterprises and persons developing microenterprises.
3. Operation of the fund including staffing for pre- and post-loan technical assistance, loan servicing activities and loan collection activities.

The CDBG-CV grantee either directly administers a local microenterprise assistance program or, in most cases, passes the funds to a local or regional economic/community development organization. This organization uses the funds to provide technical assistance, package loans/grants, and manage loan portfolio payments for qualifying microenterprises.

The CDBG-funded technical and financial assistance must be structured to benefit LMI households. The CDBG COVID-19 Microenterprise Assistance Guide with template forms for developing and implementing a CDBG-funded microenterprise assistance program is available on Commerce's CDBG-CV website and [here](#).

PUBLIC SERVICES DEFINITIONS AND CODES

CDBG can fund a wide range of public services that meet a CDBG national objective. Below is an excerpt from HUD's database manual providing definitions and codes for CDBG eligible public services. Services that most likely respond to COVID-19 impacts are highlighted.

Subsistence Payments are short-term (no more than six month) emergency payments for utilities, rent or mortgage. The CDBG COVID-19 Subsistence Payment Guide with template forms for developing and implementing a CDBG-funded utility, rent and/or mortgage emergency assistance program is available on Commerce's CDBG-CV website and [here](#).

CDBG funded public services may cover labor, supplies, materials, and other costs directly tied to service delivery. Planning and community organization activities, not directly linked to the delivery of an eligible public service to a client, are not eligible. CDBG cannot fund general outreach activities or partnership building activities, unless you can count and report on the clients receiving a direct service from those activities.

HUD CODE	Type of Services and Definition
05A	Senior Services Services for the elderly. 05A may be used for an activity that serves both the elderly and persons with disabilities provided it is intended primarily to serve elderly. <i>If the activity is intended primarily to serve persons with disabilities, use 05B instead.</i>
05B	Services for Persons with Disabilities Services for the persons with disabilities, regardless of age. <i>If the activity is intended primarily for elderly persons, use 05A instead.</i>
05C	Legal Services Services providing legal aid to low- and moderate-income (LMI) persons. <i>If the legal service is only for the settlement of tenant/landlord disputes, use 05K.</i>
05D	Youth Services Services for young people age 13 to 19 that include, for example, recreational services limited to teenagers and teen counseling programs. Also use 05D for counseling programs that target teens but include counseling for the family as well. <i>For services for children age 12 and under, use 05L; for services for abused and neglected children, use 05N.</i>
05E	Transportation Services General transportation services. <i>Transportation services for a specific client group should use the matrix code for that client group. For example, use 05A for transportation services for the elderly.</i>
05F	Substance Abuse Services Substance abuse recovery programs and substance abuse prevention/education activities.
05G	Services for victims of domestic violence, dating violence, sexual assault or stalking Services for victims of domestic violence, dating violence, sexual assault or stalking. <i>For services limited to abused and neglected children, use 05N.</i>
05H	Employment Training

HUD CODE	Type of Services and Definition
	Assistance to increase self-sufficiency, including literacy, independent living skills, resume writing, job coaching, “how to get and keep a job” training, or training students in a particular field on skill when there is no tie to a specific position or business.
05I	Crime Awareness/Prevention Promotion of crime awareness and prevention, including crime prevention education programs and paying for security guards.
05J	Fair Housing Activities (subject to Public Services subrecipient) Fair housing services (3.g., counseling on housing discrimination) that meet a national objective.
05K	Tenant/Landlord Counseling Counseling to help prevent or settle disputes between tenants and landlords.
05L	Child Care Services Services that will benefit children (generally < age 13), including parenting skills classes.
05M	Health Services Services addressing the physical health needs of residents of the community. <i>For mental health services, use 05O.</i>
05N	Services for Abused and Neglected Children Daycare and other services exclusively for abused and neglected children.
05O	Mental Health Services Services addressing the mental health needs of residents of the community.
05P	Screening for Lead Poisoning Activities undertaken primarily to provide screening for lead poisoning.
05Q	Subsistence Payments One-time or short-term (no more than three month) emergency payments on behalf of individuals or families, generally for the purpose of preventing homelessness. Examples include utility payments to prevent cutoff of service and rent/mortgage payments to prevent eviction. <i>Note: Direct payments to individuals for their food, clothing, utilities or other income payments are not eligible. Income payments or emergency vouchers paid to a third party for no more than six consecutive months may be eligible.</i>
05R	Homebuyer Downpayment Assistance - Excluding Housing Counseling If needed, see the full definition in HUD's list of CDBG Eligible Activity Codes .
05S	Rental Housing Subsidies Tenant subsidies exclusively for rental payments for more than six months. Activities providing this form of assistance must be carried out by CBDOs or 105(a)(15) entities.
05T	Security Deposits Tenant subsidies exclusively for payment of security deposits.
05U	Housing Counseling only Housing counseling for renters, homeowners, and/or potential new homebuyers that is provided as an independent public service.

HUD CODE	Type of Services and Definition
05Ua	Energy Conservation Counseling Energy conversation counseling and testing for renters, homeowners, and/or potential new homebuyers that is provided as an independent public.
05V	Neighborhood Cleanups One-time or short-term efforts to remove trash and debris from neighborhoods. Examples include neighborhood cleanup campaigns and graffiti removal.
05W	Food Banks Costs associated with the operation of food banks, community kitchens, and food pantries, such as staff costs, supplies, utilities, maintenance, and insurance.
05X	Housing Information and Referral Services An activity that provides housing information, education, and referral services, or general budget/financial counseling that does not meet the 24 CFR 5.100 Housing Counseling.
05Y	Housing Counseling - Supporting Homebuyer Downpayment Assistance (05R) Housing Counseling that is provided to in conjunction with homebuyer downpayment assistance (05R) as a public service.
03T	Homeless/AIDS Patients Programs Costs associated with the operation of programs for the homeless or for AIDS patients, such as staff costs, utilities, maintenance, and insurance. <i>Note: Contact the state CDBG program for a list of noncongregate shelters receiving FEMA operating funds and at risk when the FEMA funds end.</i>
05Z	Other Public Services Examples are client referrals to social services, neighborhood cleanup, graffiti removal, and food distribution (community kitchen, food bank, and food pantry services).

Here is guidance on specific types of services that are eligible under only limited conditions:

- **Resource Referral** - If CDBG is to fund a staff person (such as a receptionist) who refers clients to both COVID-19 and non COVID-19 response services, then CDBG-CV can cover the referral service costs only in the proportion of total persons receiving COVID-19 response services, as well as meet the LMI National Objective. Contact CDBG if seeking to fund resource referral services.
- **Outreach costs** - Outreach is not an eligible CDBG public service in of itself. Outreach to inform clients of a specific program can be an allowable expense, but only as part of that program's service delivery costs. Specific partnership building costs necessary to provide a direct client service could be an allowable expense as part of that program's service delivery costs, with that Program Name and service description then to be listed in the ZoomGrants Project Description table.
- **Housing Related Services** – Many housing related activities are not eligible as a CDBG public service because they are eligible instead for a housing rehabilitation, facility or homeownership assistance type of CDBG grant. Make sure your service description of any housing related services clearly shows how the service meets the definition of an eligible CDBG public service.

Please note: While the CARES Act [Coronavirus Relief Fund \(CRF\)](#) can fund similar local government activities that may be considered a public related service, the CRF differs from the CARES Act CDBG-CV funding requirements. A comparison of eligible activities is available on Commerce's [CDBG-CV website](#). Also, activities eligible for America Rescue Plan Act local government funds are not necessarily eligible for CDBG-CV funding.

PUBLIC (OR NON-PROFIT) FACILITIES DEFINITIONS AND CODES

CDBG can fund a wide range of public facilities. CDBG-CV2 funds are available for public health, emergency response, temporary housing or other facilities that address COVID-19 impacts. The primary HUD codes for facilities most likely to be targeted in response to COVID-19 impacts are listed below. For the full list of 03 public facility codes, here is a link to [HUD's list of CDBG Eligible Activity Codes](#).

HUD CODE	Type of Facilities and Definitions
03C	<p>Homeless Facilities (not operating costs)</p> <p>Acquisition, construction, conversion of buildings, or rehabilitation of temporary shelters and transitional housing for the homeless, including victims of domestic violence, dating violence, sexual assault or stalking, disaster victims, runaway children, drug offenders, and parolees.</p> <p><i>Note: For the construction of permanent housing for the homeless, use code 12; for the rehabilitation of such housing, use the appropriate 14* code; for facilities for abused and neglected children, use 03Q.</i></p>
03E	<p>Neighborhood Facilities</p> <p>Acquisition, construction, or rehabilitation of facilities that are principally designed to serve a neighborhood and that will be used for social services or for multiple purposes (including recreation). Such facilities may include libraries and community centers.</p>
03P	<p>Health Facilities</p> <p>Acquisition, construction, or rehabilitation of physical or mental health facilities. Examples of such facilities include neighborhood clinics, hospitals, nursing homes, and convalescent homes.</p> <p><i>Health facilities for a specific client group should use the matrix code for that client group. For example, use 03Q for the construction or rehabilitation of health facilities for abused and neglected children.</i></p>
03Z	<p>Other Public Improvements Not Listed in 03A-03S</p> <p>This matrix code replaces matrix code 03. Only use this code when an activity does not fall under a more specific 03A – 03S matrix code. Check the following before using this matrix code:</p> <ul style="list-style-type: none"> • Grantees may only have one public facility in an activity. Grantees must set up a separate activity for each public facility. When two or more related facilities are funded by CDBG, the grantee needs to set up a separate activity for each facility or improvement. • 03Z can be used for seawalls, bus shelters, retaining walls, and wind turbines. • 03Z can be used for activities that assist persons with disabilities by removing architectural barriers from or providing ADA improvements to government buildings (activities that otherwise would not be eligible for CDBG funding).

PLANNING AND GENERAL ADMINISTRATION DEFINITIONS AND CODES

CDBG can fund some planning and administration costs.

CDBG-CV funded plans must prevent, prepare for, or respond to coronavirus and principally benefit low- and moderate-income persons. Planning may be done to address inequities amongst socioeconomic groups that were revealed and exacerbated by the coronavirus – such as access to health care or affordable housing. By addressing these inequities, local government can reduce further harm during the COVID-19 pandemic, and in advance of future public health concerns. Planning-only activities can be conducted by the local government grantee and/or by a grantee procured planning consultant.

General administration includes those local government grantee activities necessary to administer the CDBG-CV2 contract with Commerce, and do not include direct project administration.

HUD CODE	Type of Services and Definition
20A	Planning-Only Activities 24 CFR 570.483(b)(5) and (c)(3) Program planning activities in which planning is the only activity, or in which planning activities are unrelated to any other activity funded as part of the grant.
21A	General Program Administration 24 CFR 570.206 or 24 CFR 570.489(a)(3) Overall CDBG program administration, including (but not limited to) salaries, wages, and related costs of grantee staff or others engaged in program management, monitoring, and evaluation. Costs associated with direct service delivery or other direct project costs are generally eligible under that project code and are not considered general administration. <i>Note: CDBG-CV funds for the grantee's general administration costs are limited, cannot exceed 5 percent, and must be justified in the ZoomGrants application budget.</i>

CARES Act CDBG-CV2 Grants

Citizen Participation Guide

Before applying for state CDBG funds, a local government must show it has provided its residents the opportunity to comment on proposed use of funds and meet other federal citizen participation requirements (24 CFR 570.486). Commerce is seeking to minimize the CDBG-CV2 citizen participation steps while meeting these CDBG citizen participation requirements.

Both CDBG entitlement and CDBG nonentitlement CDBG-CV1 grantees can apply for a state CDBG-CV2 grant. While the citizen participation requirements are similar, they differ between these two types of applicants.

Public Hearing

CDBG citizen participation regulations require a local government conduct a CDBG public hearing prior to submitting a CDBG application to the state, including the CDBG-CV2 ZoomGrants application. Both CDBG entitlement and CDBG nonentitlement grantees completed a public hearing(s) for their current CDBG-CV funds. This public hearing can meet the CDBG-CV2 public hearing requirement if that public hearing:

- ☐ Was for CDBG-CV funds, providing an opportunity to receive public input on community development and housing needs to prevent, prepare for and respond to coronavirus,
- ☐ Occurred within the last 18 months,
- ☐ Met local public hearing requirements, and
- ☐ Records are maintained and available for review.

In addition, CDBG entitlement grantees must attest in their CDBG-CV2 ZoomGrants application that:

- ☐ Their CDBG Citizen Participation Plan does not require they conduct another public hearing for their proposed CDBG-CV2 activities.

✓ **Helpful Hint:** Determine if another public hearing is required before submitting the CDBG-CV application. If required, schedule the public hearing as soon as possible.

If a CDBG nonentitlement grantee intends to apply for more than one type of state CDBG grant during the same year, contact state CDBG-CV staff to discuss how to coordinate these citizen participation and public hearing requirements.

Citizen Participation Documentation

The CDBG-CV2 ZoomGrants application includes the following documentation actions:

- Date the applicable CDBG-CV public hearing
- Upload a copy of the applicable CDBG-CV public hearing's published notice
- Attest that the public hearing was "conducted in a manner to meet the needs of non-English speaking residents where a significant number of non-English speaking residents can reasonably be expected to participate"

Before a CDBG-CV2 contract is closed, a grantee must conduct a public hearing to review and received comments on program performance of the CDBG-CV2 funded activity(s). This public hearing can occur in conjunction with annual performance evaluation reports or future CDBG applications.

Citizen Participation Resources

CDBG entitlement grantees must follow their CDBG-CV citizen participation plan and local public hearing requirements.

CDBG nonentitlement grantees may use the [CDBG-CV1 Citizen Participation Guide](#) for more detailed guidance on conducting a CDBG-CV public hearing and documenting compliance.

Additional information is available in the CDBG-CV regulations found [here](#).



TO: Mayor and City Council

FROM: Bill Adamson, Program Director SSMCP

THROUGH: John Caulfield, City Manager

DATE: 7 APRIL 2021

SUBJECT: SSMCP 2021-22 WORK PLAN

ATTACHMENTS: (1) POWERPOINT SLIDE

Summary:

The attached slide outlines the SSMCP Work Plan for calendar years 2021-22 for the April 19 study session as part of the City Manager's report. This Plan was accepted by the SSMCP Steering Committee in September 2020 and further reviewed and approved by voice vote of the SSMCP Elected Officials Council on 12 November 2020. It has been in effect since then.

This presentation will focus on the way ahead and any outstanding challenges SSMCP anticipate and expects to resolve over the two year period of the Work Plan. Of note, the update to the 2010 Growth Coordination Plan which is on-going will provide needed guidance and direction concerning the future priorities of SSMCP.

2020 KEY ACCOMPLISHMENTS

1.) Completed 3 OEA grant studies - Office of Local Defense Community Cooperation (OLDCC)

Task 1: Analyze Local Transportation Impacts in the Vicinity of Joint Base Lewis-McChord (JBLM)

Task 2: Establish a coordination process for off installation housing of active duty service members in the vicinity of Joint Base Lewis-McChord (JBLM) neighboring communities

Task 3: Expand Conservation Banking and Conservation Partnering Opportunities In Thurston And Pierce Counties.

2.) Completed a 2020 update to JBLM Regional Economic Impact Analysis


3.) Developed 2020 & 2021 Legislative Agendas:

- Advocated for and received Defense Community Compatibility Account (DCCA) program funding
- Obtained additional Clear Zone funding
- Advocated for I-5 highway improvements

2021-22 WORK PLAN PRIORITIES

- Recast Social Services Work Group - prioritize work on child care initiatives
- Formalize a new *Work Group* focused on attainable housing for lower enlisted
- **Assess the continued relevance of the 2010 Growth Coordination Plan - conduct GCP Impact survey, publish new GCP in 2022**
- Secure federal and state funding, modify / manage Cooperative Agreement to resolve McChord Airfield Clear Zone Encroachment while assisting City of Lakewood in regulating and enforcing NCZ activities
- Implement strategies for traffic congestion management to improve JBLM access
- Enhance & Expand Regional SSMCP Coordination & Participation – serve as a *convener of resources promoting collaboration* between community and military leaders
- Inform & Educate Stakeholders on Military Impacts across the South Puget Sound – ***use Regional Economic Impact Analysis as a marketing tool***
- Inform and educate state legislators about SSMCP priorities and participate & advocate in State Level Activities



To: Mayor and City Councilmembers
From: Tho Kraus, Assistant City Manager/Chief Financial Officer
Through: John J. Caulfield, City Manager 
Date: April 19, 2021
Subject: City of Lakewood Transportation Benefit District 2020 Annual Report

Report Purpose

This report provides information on the status of the City of Lakewood Transportation Benefit District ("District") and fulfills the requirements of the State of Washington and the District for an annual report. Below are excerpts from the relevant state law and local documents.

RCW 36.73.160(2): A district shall issue an annual report, indicating the status of transportation improvement costs, transportation improvement expenditures, revenues, and construction schedules, to the public and to newspapers of record in the district.

Action taken by the City Council

- Adopted the TBD Annual Report for FY 2019 on May 4, 2020 (Reviewed at April 27, 2020 Study Session)

Next Steps

- Approve the TBD Annual Report for FY 2020 on May 3, 2021 Consent Agenda

Transportation Benefit District Financial Summary

The following table provides a summary of the Transportation Benefit District Fund for fiscal year 2020.

Fund 103 Transportation Benefit District	2020 Budget	2020 Actual
Revenues:		
\$20 Vehicle License Fee	\$ 835,000	\$ 881,849
Interest Earnings	-	904
Total Revenues	\$ 835,000	\$ 882,753
Expenditures (Transfers to Transportation CIP Fund):		
302.0005 Chip Seal Program - Local Access Roads	52,456	52,456
302.0135 JBLM North Access Improvements	195,000	195,000
Total Expenditures	\$ 247,456	\$ 247,456
Beginning Fund Balance, January 1	\$ 52,456	\$ 52,457
Ending Fund Balance, December 31	\$ 640,000	\$ 687,754

Note – The Washington State Department of Licensing (DOL) collects the fees and distributes the net proceeds to the City on a monthly basis. DOL automatically deducts one percent (1%) of the \$20 fee at the time of collection for administration and collections expenses incurred. The administration fee is the maximum amount permitted by RCW 82.80.140.

TBD Funded Projects – Revenues, Expenditures & Construction Schedules

The following is an update of TBD funded projects in 2020.

302.0005 Chip Seal Program – Local Access Roads

302.0005 Chip Seal Program - Local Access Roads	2019 Actual	2020 Actual
Revenues:		
Real Estate Excise Tax	\$ 114,000	\$ 307,543
TBD \$20 Vehicle License Fee	246,000	52,457
Other Project Savings	-	1,439
Total Revenues	\$ 360,000	\$ 361,439
Expenditure:		
Capital	342,363	446,020
302.0135 JBLM North Access Improvements	\$ 342,363	\$ 446,020
Beginning Fund Balance, January 1	\$ 66,944	\$ 84,581
Ending Fund Balance, December 31	\$ 84,581	\$ -

Note: 2019 balance from TBD funds totaling \$84,581 rolled over to 2020 Chip Seal Program.

The focus of the 2020 chip seal program was on the area east of Clover Park High School, north of 112th Street, west of Pack Highway and primarily south of 108th. Some streets were chip sealed north of 108th in the vicinity of Douglas Drive, Filbert Lane, and just west of Bridgeport Way.

The focus of the 2021 chip seal program will be the area bounded by Veteran's Drive to the north and Gravelly Lake Drive to the east and American Lake to the south.

109th Street Before:



109th Street After:



Montgrove Drive Before:



Montgrove Drive After:



302.0135 JBLM North Access Improvements

302.0135 JBLM North Access Improvements	2020 Actual
Revenues:	
TBD \$20 Vehicle License Fee	\$ 195,000
General Obligation Bonds	659,570
Real Estate Excise Tax	548,722
SWM Funds	218,900
Interest Earnings	251
Total Revenues	\$ 1,622,443
302.0135 JBLM North Access Improvements	
Expenditure:	
Capital	1,579,661
Total Expenditures	\$ 1,579,661
Beginning Fund Balance, January 1	\$ 167,768
Ending Fund Balance, December 31	\$ 210,550

In 2020, TBD funds were used to assist in the design of the JBLM North Access Improvements Project. Phase I of the construction will be constructed starting in May of 2021. The first phase of construction is expected to take 18 months to complete.