



LAKEWOOD CITY COUNCIL AGENDA

Monday, May 3, 2021

7:00 P.M.

City of Lakewood

The State Legislature passed a Concurrent Resolution modifying the Open Public Meetings Act (OPMA) during the COVID-19 pandemic until the end of the emergency. As a result, City Hall Council Chambers will **NOT** be open for City Council meetings.

Residents can virtually attend City Council meetings by watching them live on the city's YouTube channel: <https://www.youtube.com/user/cityoflakewoodwa>

Those who do not have access to YouTube can participate via Zoom by either visiting <https://us02web.zoom.us/j/86872632373> or calling by telephone: Dial +1(253) 215- 8782 and enter participant ID: 868 7263 2373.

To participate in Public Comment and/or Public Testimony:

Public Comments and Public Testimony on Public Hearings will be accepted by mail, email or by live virtual comment. Send comments by mail or email to Briana Schumacher, City Clerk at 6000 Main Street SW Lakewood, WA 98499 or bschumacher@cityoflakewood.us. Comments received up to one hour before the meeting will be provided to the City Council electronically.

Virtual Comments: If you would like to provide live Public Comments or Public Testimony during the meeting, you will need to join the Zoom meeting as an attendee by calling by telephone Dial +1(253) 215- 8782 and enter participant ID: 868 7263 2373 or visiting <https://us02web.zoom.us/j/86872632373>.

By Phone: For those participating by calling in by telephone (+1(253) 215- 8782 and enter participant ID: 868 7263 2373), the Mayor will call on you during the Public Comment and/or Public Hearings portions of the agenda. When you are unmuted please provide your name and city of residence. Each speaker will be allowed (3) three minutes to speak during Public Comment and at each Public Hearing.

By ZOOM: For those using the ZOOM link (<https://us02web.zoom.us/j/86872632373>), upon entering the meeting, please enter your name or other chosen identifier. Use the "Raise Hand" feature to be called upon by the Mayor during the Public Comments and/or Public Hearings portions of the agenda. When you are unmuted please provide your name and city of residence. Each speaker will be allowed (3) three minutes to speak during Public Comment and at each Public Hearing.

Outside of Public Comments and Public Hearings, all attendees on ZOOM will continue to have the ability to virtually raise your hand for the duration of the meeting. You will not be acknowledged and your microphone will remain muted except for when you are called upon.

Page No.

CALL TO ORDER

ROLL CALL

PLEDGE OF ALLEGIANCE

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

<http://www.cityoflakewood.us>

PROCLAMATIONS AND PRESENTATIONS

- (5) 1. Proclamation declaring May 9, 2021 through May 15, 2021 as National Police Week. – *Police Chief Mike Zaro*
2. Police Commissioning Ceremony.
3. Youth Council Report.
4. Clover Park School District Report.

PUBLIC COMMENTS**C O N S E N T A G E N D A**

- (6) A. Approval of the minutes of the City Council meeting of April 5, 2021.
- (12) B. Approval of the minutes of the City Council study session of April 12, 2021.
- (16) C. Approval of claims vouchers, in the amount of \$2,806,465.01, for the period of March 16, 2021 through April 15, 2021.
- (66) D. Approval of payroll checks, in the amount of \$2,467,857.81, for the period of March 16, 2021 through April 15, 2021.
- (68) E. Motion No. 2021-25
- Authorizing the execution of an agreement with Landscape Structures, Inc. for the Harry Todd Park Playground Replacement Project.
- (72) F. Motion No. 2021-26
- Approving the 2020 Transportation Benefit District (TDB) Annual Report.
- (78) G. Motion No. 2021-27
- Authorizing the execution of an agreement with Greater Lakes Mental Health for healthcare professionals.

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

- (85) H. Items filed in the Office of the City Clerk:
1. Community Services Advisory Board meeting minutes of March 17, 2021.
 2. Planning Commission meeting minutes of April 7, 2021.

R E G U L A R A G E N D A

PUBLIC HEARINGS AND APPEALS

- (89) This is the date set for a public hearing on the 2021 Carry Forward Budget Adjustment.

ORDINANCE

- (145) Ordinance No. 751

Adopting the Lakewood Station District Subarea Plan as an element of the Comprehensive Plan, amending the Lakewood Municipal Code by establishing a new Title 18C Lakewood Station District Development Code and amending Lakewood Municipal Code Chapters 18A.10 Basic Provisions and 18A.20 Administration. – *continued from April 19, 2021*

- (273) Ordinance No. 752

Adopting a SEPA Planned Action related to the Lakewood Station District Subarea. – *continued from April 19, 2021*

- (367) Ordinance No. 753

Amending Title 8 Health and Safety, Chapter 8.16 Public Nuisances and adding a new Chapter 8.18, Activities that are not deemed public nuisances and amending Chapter 8.26 Public Nuisance Declared.

RESOLUTION

- (375) Resolution No. 2021-07

Adopting the Fiscal Year 2021 (July 1, 2021 – June 30, 2022) Joint Tacoma and Lakewood Consolidated Annual Action Plan, including Amendment to Fiscal Year 2017, 2019 and 2020 Consolidated Annual Action Plans.

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

UNFINISHED BUSINESS

NEW BUSINESS

REPORTS BY THE CITY MANAGER

(425) Chambers Creek Canyon Trail Update.

(430) Park Sign Update.

CITY COUNCIL COMMENTS

ADJOURNMENT

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

CITY OF LAKEWOOD



PROCLAMATION

WHEREAS, each May during the week surrounding May 15th, our nation salutes the American men and women who put their lives on the line every day to maintain public safety and hold accountable those who break the law; and

WHEREAS, over the past year, the COVID-19 pandemic placed first responders at additional risk, dramatically impacting how they interact with the public and presenting new challenges never faced before; and

WHEREAS, there are more than 900,000 law enforcement officers serving in communities across the United States, including the 99 dedicated members of the Lakewood Police Department; and

WHEREAS, nearly 60,000 assaults against law enforcement officers are reported each year, resulting in approximately 17,000 injuries; and

WHEREAS, since the first recorded death in 1786, more than 21,000 law enforcement officers in the United States have been killed in the line of duty, including four members of the Lakewood Police Department; and

WHEREAS, it is with heavy hearts, we mourn the heroes taken from us only because they chose to serve and we rededicate ourselves to carrying forward their noble legacy; and

WHEREAS, the names of these dedicated public servants are engraved on the walls of the National Law Enforcement Officers Memorial in Washington, D.C.; and

WHEREAS, on May 13, 2021 in honor of the heroes taken from us, a Blood Drive will be held at the Lakewood Police Station from 9:00 a.m. to 3:00 p.m.

NOW THEREFORE, BE IT RESOLVED that the Lakewood City Council officially recognizes May 9, 2021 through May 15, 2021 as

NATIONAL POLICE WEEK

in the City of Lakewood, and publicly salutes the service of law enforcement officers in our community and in communities across the nation.

PROCLAIMED this 3rd day of May, 2021.

Don Anderson, Mayor



LAKEWOOD CITY COUNCIL MINUTES

Monday, April 5, 2021

City of Lakewood

<https://www.youtube.com/user/cityoflakewoodwa>

Telephone via Zoom: +1(253) 215-8782

Participant ID: 86872632373

CALL TO ORDER

Mayor Anderson called the meeting to order at 7:00 p.m.

ROLL CALL

Councilmembers Present: 7 – Mayor Anderson, Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, Patti Belle, Linda Farmer and Paul Bocchi.

PROCLAMATIONS AND PRESENTATIONS

Proclamation honoring and expressing deepest appreciation for Ron Lucas.

MAYOR ANDERSON PRESENTED A PROCLAMATION TO MS. JAN LUCAS WHO WAS JOINED BY STEPHANIE LUCAS AND TOWN OF STEILACOOM ADMINISTRATOR PAUL LOVELESS, HONORING AND EXPRESSING DEEPEST APPRECIATION FOR MR. RON LUCAS.

Presentation of the City of Lakewood's 25th Anniversary Art Contest.

Parks and Recreation Program Coordinator Martinez provided an overview of the City's 25th Anniversary Art Contest that highlighted the themes of LOVE Lakewood and UNITY. Ms. Connie Coleman-Lacadie, President, Lakewood Sister Cities Association, recognized Friendship City Gimhae, South Korea key leaders and participants of the contest.

Mayor Anderson then acknowledged and thanked Twin Star Credit Union for being the sponsor of the monetary prizes, the Clover Park School District for advertising the contest to students and families, the Lakewood Chamber of Commerce and The Pierce County Library System for providing raffle prizes, the Lakewood Sisters Cities Association for partnering and facilitating the participation of students from Gimhae and the judges who scored the entries.

Program Coordinator Martinez then shared a flipbook highlighting the winning artwork.

Clover Park School District Report.

Clover Park School District (CPSD) Boardmember Alyssa Anderson Pearson shared that all grade levels are back in the building for some form of in person learning. She provided a list of colleges that CPSD students have been accepted too and shared that all teachers and staff are now eligible for vaccinations. She also shared that CPSD is drafting an equity policy and they have over 80 stakeholders who are providing input.

She then spoke about academic recovery, social and emotional learning, emergency waivers for graduation credits and support for students as they return to the classroom.

PUBLIC COMMENTS

Speaking before Council were:

Dennis Haugen, Sioux Falls South Dakota, spoke about open borders, illegal immigration, Presidential competency gaps based on policies, illegal drugs and human trafficking.

Seth Borman, Temecula California, spoke on behalf of Steven Borman, who owns 8.8 acres of land within the Lakewood Station District Subarea. He spoke about density, parking and open space requirements proposed in Lakewood Station District Subarea Plan.

C O N S E N T A G E N D A

- A. Approval of the minutes of the City Council meeting of March 1, 2021.
- B. Approval of the minutes of the City Council study session of March 8, 2021.
- C. Approval of the minutes of the City Council meeting of March 15, 2021.
- D. Approval of claims vouchers, in the amount of \$1,583,133.94, for the period of February 17, 2021 through March 15, 2021.
- E. Approval of payroll checks, in the amount of \$2,300,388.89, for the period of February 16, 2021 through March 15, 2021.
- F. Motion No. 2021-20

Authorizing the execution of an agreement with Hemisphere Design, in the amount of \$141,500, for the Build Your Better Here imaging campaign.
- G. Motion No. 2021-21

Authorizing the execution of an extra duty police services contract with the Clover Park School District for School Resources Officers.

H. Motion No. 2021-22

Appointing Linn Larsen to serve on the Planning Commission through December 15, 2021.

I. Items filed in the Office of the City Clerk:

1. Lakewood Arts Commission meeting minutes of February 1, 2021.
2. Planning Commission meeting minutes of February 17, 2021.
3. Parks and Recreation Advisory Board meeting minutes of February 23, 2021.

COUNCILMEMBER MOSS MOVED TO ADOPT THE CONSENT AGENDA. SECONDED BY COUNCILMEMBER BOCCHI. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

PUBLIC HEARINGS AND APPEALS

This is the date set for a public hearing on the Lakewood Station District Subarea Plan (LSDS), Development Code and SEPA Planned Action.

COUNCILMEMBER MOSS MOVED TO APPEND TO THE PUBLIC HEARING RECORD THE PUBLIC COMMENT FROM SETH BORMAN. SECONDED BY COUNCILMEMBER BOCCHI. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

Speaking before Council were:

Seth Borman, Temecula California, spoke on behalf of Steven Borman, who owns 8.8 acres of land within the Lakewood Station District Subarea. He spoke about density, parking and open space requirements proposed in Lakewood Station District Subarea Plan.

James Guerro, Lakewood resident, shared a presentation related current parking requirements, associated costs and the future of transportation.

Dennis Haugen, Sioux Falls South Dakota, spoke about creating incentives for economic development and job creation.

There being no further testimony, the public hearing was declared closed at 8:11 p.m.

ORDINANCE

Ordinance No. 750 Amending Chapter 9.06 of the Lakewood Municipal Code related to Controlled Substances.

DEPUTY MAYOR WHALEN MOVED TO ADOPT ORDINANCE NO. 750 AND STRIKE THE WORD IMMEDIATELY FROM THE THIRD WHEREAS. SECONDED BY COUNCILMEMBER BOCCHI. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

UNFINISHED BUSINESS

None.

NEW BUSINESS

None.

REPORTS BY THE CITY MANAGER**Review Cooperative Use Agreement with the Clover Park School District.**

Parks, Recreation and Community Services Director Dodsworth shared that the City and Clover Park School District (CPSD) have worked together to draft an agreement to address cooperative use of facilities. She shared that the agreement addresses scheduling, priorities, responsibility and communications. Discussion ensued and it was noted that the agreement will come forward for City Council consideration on April 19, 2021.

South Sound Housing Affordability Partners (SSHA³P) Update.

City Manager Caulfield shared that as a follow up to prior conversations related to creating access to affordable and attainable housing SSHA³P has provided a draft intergovernmental agreement for City Council review and consideration. He shared that if Lakewood participates it is estimated that financial contributions will total \$7,000 in 2021, \$18,000 in 2022 and \$18,630 in 2023. Discussion ensued.

City Manager Caulfield provided an update on state and federal legislative initiatives.

He shared that Pierce County is still planning to use the Lakewood Senior Activity Center for jury trials therefore the City will still not have access to the facility for activities.

He reported that the City provides four extra duty police officers to Pierce Transit for police services and Pierce Transit inquired as to whether Lakewood would be interested in providing six dedicated Pierce Transit Police Officers, more information will be provided as discussions continue.

He then commented on the 2020 Financial Report which will come forward for City Council review on April 26th.

He congratulated the Human Resources Department and Wellness Committee for receiving the Well City Award again this year noting that the City has been a Well City since 2004 with the exception of 2010 and those who earn the recognition receive a 2% discount on medical premiums.

He shared that the City has submitted this years MLK Ceremony video to the Association of Washington Cities (AWC) Municipal Excellence Award Program, the new restrooms at Fort Steilacoom park are now open and the Lakewood Community Clean-Up event will be held on Saturday, April 17th and Sunday, April 18th, to volunteer please contact City Clerk Briana Schumacher.

He then shared that on April 1st, Sound Transit kicked off the public input online open house for the Lakewood Station Access Improvement Project and will be accepting feedback through April 20th.

He reported that the 99th Annual Lakeview Light and Power Annual meeting will be held virtually on April 22nd and after discussion the City's voting member will be Councilmember Belle.

CITY COUNCIL COMMENTS

Councilmember Farmer commented on the discussion at the City Council retreat related to diversity, equity and inclusion and the joint Resolution with the Clover Park School District. Discussion ensued and it was requested that the City Council discuss an equity statement during the April 12th study session.

Councilmember Bocchi commented on the 25th Anniversary Art Contest and the new restrooms at Fort Steilacoom.

Councilmember Belle commented on the creativity of the artwork submitted for the 25th Anniversary Art Contest.

Councilmember Moss shared that she attended the AWC Lobby Day and she conducted an interview with the Tacoma News Tribune related to diversity in jury pools.

Councilmember Brandstetter also commented on the artwork and descriptions submitted for the 25th Anniversary Art Contest. He spoke about Mr. Guerrero's presentation related to parking regulations.

Mayor Anderson commented on the public testimony related to the LSDS Plan. He shared that County Councilmember Hitchen has scheduled a meeting for April 19th related to American Rescue Plan Act (ARPA) funding ideas.

Deputy Mayor Whalen commented on the artwork and descriptions submitted for the 25th Anniversary Art Contest. He shared that he attended the Arts Commission Public Art Subcommittee meeting where they narrowed down submissions to six candidates.

Mayor Anderson announced that the City Council will recess into Execution Session for approximately 15 minutes pursuant to RCW 42.30.110(1)(b) to consider the selection of a site or the acquisition of real estate by lease or purchase when public knowledge regarding such consideration would cause a likelihood of increased price. The City Council is not expected to take action following the Executive Session.

The City Council recessed into Executive Session at 9:48 p.m. and reconvened at 10:03 p.m.

ADJOURNMENT

There being no further business, the meeting adjourned at 10:03 p.m.

DON ANDERSON, MAYOR

ATTEST:

BRIANA SCHUMACHER
CITY CLERK



LAKEWOOD CITY COUNCIL STUDY SESSION MINUTES

Monday, April 12, 2021

City of Lakewood

<https://www.youtube.com/user/cityoflakewoodwa>

Telephone via Zoom: +1(253) 215- 8782

Participant ID: 868 7263 2373

CALL TO ORDER

Mayor Anderson called the meeting to order at 7:00 p.m.

ROLL CALL

Councilmembers Present: 7 – Mayor Don Anderson, Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, Patti Belle, Paul Bocchi and Linda Farmer.

ITEMS FOR DISCUSSION:

Review of Year-End (2020) and 1st Quarter (2021) Police Report.

Chief Zaro reported reviewed the 1st Quarter 2021 Police Report. He highlighted crimes against population, persons crimes at Western State Hospital which totaled 2, property crimes which totaled 1029 and society crimes by quarter which decreased to 84. He shared that overall total crime has decreased and is the lowest for 1st Quarter since 2016.

He shared that accidents in 1st quarter totaled 261 and vehicle thefts are on the rise with a total of 128. He highlighted notable incidents including a homicide, a child abuse case, shooting on Interstate 5, as well as street racing and intersection takeovers which took place. Discussion ensued.

Chief Zaro then reviewed the 2020 Lakewood Police Department (LPD) Annual Report. He highlighted the department's response to the Sumner Grade fires and continued efforts related to the Coronavirus pandemic. He then reviewed annual stats and police by numbers. He provided an overview of the Pierce County Force Investigation Team and shared that in 2020 LPD held their second Citizens Academy. He reviewed stats related to the use of drones, property room inventory and K-9 captures. He shared that the Behavioral Health Contact Team responded to 429 episodes of care, there were 114 incidents involving use of force and 24 officer-involved collisions. He then reviewed the personnel related events from the year including retirements and new hires. Discussion ensued.

Review of Region 5 All Hazard Mitigation Plan.

Assistant Chief Unfred reported that the Hazard Mitigation Plan, a plan associated with a series of Emergency Management Plans, is required by Federal Emergency Management Agency (FEMA) and must be reviewed and updated every five years. He shared that if the City does not have a local mitigation plan in place, it would not be eligible for federal mitigation or post disaster grants. He shared that the plan represents the city's commitment to reducing risks from natural hazards and he highlighted each of the sections of the plan and the mitigation planning process. Discussion ensued.

Review statement expressing a commitment to valuing diversity, equity and inclusion.

Mayor Anderson shared that a statement on equity has been provided to City Councilmembers for review and feedback. The City Council reviewed and edited the draft statement and after further discussion it was recommended that the revised statement come forward for City Council approval in the form of a Resolution at the April 19th regular meeting.

ITEMS TENTATIVELY SCHEDULED FOR THE APRIL 19, 2021 REGULAR CITY COUNCIL MEETING:

1. Recognition of Azalia Riviere, Boys and Girls Clubs of South Puget Sound Youth of the Year.
2. Proclamation declaring April 24, 2021 as Parks Appreciation Day.
3. Proclamation recognizing JP Bega, Cody Orcutt, Mokia Tennant and Alex Thomason for attaining the rank of Eagle Scout.
4. Youth Council Report.
5. Authorizing the execution of a cooperative use agreement with the Clover Park School District. – (Motion – Consent Agenda)
6. Authorizing the acquisition of a parcel in support of future parking for American Lake Park. – (Motion – Consent Agenda)
7. This is the date set for a Public Hearing on the FY2021 Community Development Block Grant (CDBG) Annual Action Plan. – (Public Hearings and Appeals – Regular Agenda)
8. Adopting the Lakewood Station District Subarea Plan (LSDS) and SEPA Planned Action. – (Ordinance – Regular Agenda)
9. Expressing a commitment to valuing diversity, equity and inclusion. – (Resolution – Regular Agenda)

10. Adopting the Region 5 All Hazard Mitigation Plan (2020-2025) Edition and the 2021 Addendum to the Region 5 Hazard Mitigation Plan. – (Resolution – Regular Agenda)
11. Review of 2021 SSMCP Work Plan. – (Reports by the City Manager)
12. Review of Transportation Benefit District (TBD) Annual Report. – (Reports by the City Manager)

REPORTS BY THE CITY MANAGER

Acting City Manager Kraus shared that the Spring Community Clean-Up event will be held on Saturday, April 17th and Sunday, April 18th and the Washington State Transportation Commission will host a virtual meeting in Lakewood on April 20th and April 21st.

CITY COUNCIL COMMENTS

Councilmember Moss shared that this evening she provided Council Remarks at the Clover Park School District Board meeting.

Councilmember Brandstetter commented on questions he had related to the the parking requirements presentation from James Guerrero during the Lakewood Station District Subarea Plan public hearing.

Councilmember Farmer shared that she will need coverage for the Pierce County Regional Council meeting and plans to volunteer this weekend at the Spring Community Cleanup event.

Councilmember Belle shared that she will be volunteering at the Spring Community Cleanup event and last week she attended the Lake City Neighborhood Association meeting and the 25th Anniversary Planning meeting.

Councilmember Bocchi shared that last week he attended the Planning Commission meeting where there was a presentation from the Clover Creek Council related to restoration. He also commented on Pierce County reverting to Phase 2 and shared that he was vaccinated at the Lakewood Seamar Clinic.

Deputy Mayor Whalen shared that this week he will attend Pierce County Regional Council meeting and a walk through with artists at the Colonial Plaza and today he attended the Pierce Transit Board of Commissioners meeting.

Mayor Anderson shared that this afternoon he filmed congratulatory remarks for the City of Gimhae, South Korea's 40th Anniversary and tomorrow is virtual Coffee with the Mayor.

He then commented on Pierce County moving back to Phase 2 and the lack of transparency and information on the death rates in Pierce County and demanded that Pierce County receive every vaccination eligible for on a pro-rata basis.

ADJOURNMENT


There being no further business, the meeting adjourned at 9:48 p.m.

DON ANDERSON, MAYOR

ATTEST:

BRIANA SCHUMACHER
CITY CLERK



To: Mayor and City Councilmembers
From: Tho Kraus, Assistant City Manager/Chief Financial Officer
Through: John J. Caulfield, City Manager 
Date: May 3, 2021
Subject: Claims Voucher Approval

Check Run Period: March 16, 2021- April 15, 2021

Total Amount: \$ 2,806,465.01

Checks Issued:

03/31/21	Checks 94391-94447	\$ 223,746.51
04/15/21	Checks 94448-94532	\$ 230,006.31

EFT Checks Issued:

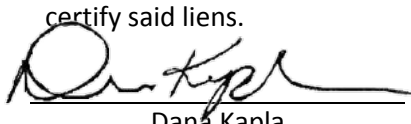
03/31/21	Checks 17139-17207	\$ 918,136.74
04/15/21	Checks 17208-17283	\$ 1,434,965.28

Voided Checks:

03/26/21	Check 94328	\$ 144.74
04/05/21	Check 93893	\$ 245.09

Grand Total \$ 2,806,465.01

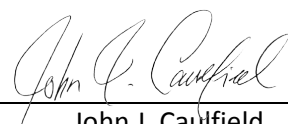
I, the undersigned, do hereby certify under penalty of perjury that the materials have been furnished, the services rendered, or the labor performed as described herein and that the claims are just and due obligations against the City of Lakewood, Washington, and that I am authorized to authenticate and certify said liens.



Dana Kapla
Finance Supervisor



Tho Kraus
Assistant City Manager/
Chief Financial Officer



John J. Caulfield
City Manager

City of Lakewood - Accounts Payable Voucher Report

Heritage Bank							Page 1 of 49
Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
17139	3/31/2021	011591		911 SUPPLY INC,			\$245.06
001.0000.15.521.22.31.008			3/8/2021	INV-2-9122	PD Pants: Bentz	245.06	
17140	3/31/2021	011713		ALLSTREAM,			\$1,098.41
503.0000.04.518.80.42.001			3/8/2021	17394499	IT 03/08-04/07 Phone	1,098.41	
17141	3/31/2021	001685		AMAYA ELECTRIC CORP,			\$1,794.30
101.0000.11.542.64.48.001			3/26/2021	9312-16	PKST Custer Elementary School	280.47	
504.0000.09.518.39.48.001			3/30/2021	9312-7	RM CL# 2020-0078	1,513.83	
17142	3/31/2021	001693		AMERICAN REPORTING COMPANY,			\$40.00
190.4999.52.559.32.41.001			3/24/2021	2734685	CDBG MHRS-12 Rodrigues	20.00	
190.4999.52.559.32.41.001			3/18/2021	2731677	CDBG MHR-181 Lucas	20.00	
17143	3/31/2021	003085		ANDERSON, JAY			\$500.00
301.0035.11.594.76.41.001			3/23/2021	Anderson Reimb	PK Reimb: J. Anderson Pd For S	500.00	
17144	3/31/2021	007445		ASSOCIATED PETROLEUM PRODUCTS,			\$22,897.77
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	116.45	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	23.63	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	148.65	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	64.73	
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501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	112.00	
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501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	221.94	
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501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	212.35	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	33.57	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	130.49	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	184.27	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	68.50	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	184.95	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	116.11	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	121.59	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	55.49	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	25.00	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	136.32	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	275.37	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	222.97	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	230.85	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	202.76	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	167.14	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	153.78	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	208.58	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	83.57	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	84.60	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	75.01	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	116.45	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	162.69	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	21.58	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	84.60	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	78.78	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	97.61	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	213.38	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	188.72	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	191.80	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	183.24	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	176.39	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	15.07	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	190.09	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	41.79	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	32.88	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	84.94	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	165.77	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	192.83	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	42.47	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	210.64	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	281.19	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	63.36	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	81.52	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	41.44	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	18.84	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	215.43	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	59.25	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	248.31	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	26.72	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	170.91	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	45.21	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	38.02	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	33.22	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	30.14	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	53.09	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	44.87	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	45.90	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	271.60	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	129.81	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	224.34	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	142.48	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	58.23	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	95.56	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	116.79	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	388.74	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	93.85	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	14.73	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	97.27	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	103.78	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	41.79	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	139.06	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	69.19	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	116.45	
180.0000.15.521.21.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	50.35	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	65.42	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	10.96	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	39.39	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	208.24	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	41.44	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	48.29	
180.0000.15.521.21.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	63.36	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	36.99	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
180.0000.15.521.21.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	29.80	
180.0000.15.521.21.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	92.82	
501.0000.51.521.10.32.001			3/9/2021	0307796-IN	PDFL 2/25-3/9	43.73	
501.0000.51.548.79.32.001			3/2/2021	0305781-IN	PKFL 2/17-3/2	16.19	
501.0000.51.548.79.32.001			3/2/2021	0305781-IN	PKFL 2/17-3/2	10.93	
501.0000.51.548.79.32.002			3/2/2021	0305781-IN	PKFL 2/17-3/2	39.25	
501.0000.51.548.79.32.002			3/2/2021	0305781-IN	PKFL 2/17-3/2	10.12	
501.0000.51.548.79.32.002			3/2/2021	0305781-IN	PKFL 2/17-3/2	64.75	
501.0000.51.548.79.32.002			3/2/2021	0305781-IN	PKFL 2/17-3/2	97.52	
501.0000.51.548.79.32.001			3/2/2021	0305781-IN	PKFL 2/17-3/2	60.70	
501.0000.51.548.79.32.001			3/2/2021	0305781-IN	PKFL 2/17-3/2	168.34	
501.0000.51.548.79.32.001			3/2/2021	0305781-IN	PKFL 2/17-3/2	178.86	
501.0000.51.548.79.32.001			3/2/2021	0305781-IN	PKFL 2/17-3/2	66.77	
501.0000.51.548.79.32.002			3/2/2021	0305781-IN	PKFL 2/17-3/2	89.84	
501.0000.51.548.79.32.002			3/2/2021	0305781-IN	PKFL 2/17-3/2	86.19	
501.0000.51.548.79.32.002			3/2/2021	0305781-IN	PKFL 2/17-3/2	91.44	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	217.80	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	91.63	
180.0000.15.521.21.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	60.16	
180.0000.15.521.21.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	18.66	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	33.42	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	62.39	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	50.41	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	179.92	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	108.62	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	46.23	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	204.43	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	137.87	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	195.80	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	49.30	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	192.45	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	73.25	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	25.62	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	150.68	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	65.17	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	145.39	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	172.12	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	141.21	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	168.50	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	237.02	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	29.80	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	83.00	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	196.08	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	265.43	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	178.53	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	154.58	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	184.10	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	147.61	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	155.69	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	83.28	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	148.45	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	37.88	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	65.45	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	161.82	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	33.98	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	47.35	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	18.66	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	30.36	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	209.17	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	28.41	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	191.34	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	199.14	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	149.01	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	170.17	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	34.26	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	36.76	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	56.26	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	73.53	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	59.60	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	80.21	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	119.76	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	255.40	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	6.68	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	218.08	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	100.27	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	28.41	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	9.19	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	66.84	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	110.29	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	141.76	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	220.58	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	11.70	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	143.44	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	69.63	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	64.06	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	87.45	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	32.31	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	77.98	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	153.46	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	47.07	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	195.80	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	77.98	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	64.89	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	238.97	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	23.67	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	52.08	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	99.15	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	116.14	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	42.89	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	311.10	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	61.00	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	133.13	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	111.69	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	109.74	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	35.65	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	34.81	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	25.90	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	62.67	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	154.58	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	143.44	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	95.53	
180.0000.15.521.21.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	85.23	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	110.01	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	35.65	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	37.32	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	41.50	
180.0000.15.521.21.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	42.89	
501.0000.51.521.10.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	39.27	
180.0000.15.521.21.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	45.68	
180.0000.15.521.21.32.001			3/23/2021	0313244-IN	PDFL 3/10-3/23	66.24	
17145	3/31/2021	012259		BEYLER CONSULTING LLC,			\$350.00
302.0001.21.595.12.41.001			2/22/2021	8901	PWCP 01/24-02/13 Edgewater Par	350.00	
17146	3/31/2021	008226		BIO CLEAN INC,			\$846.24
501.0000.51.521.10.48.005			3/9/2021	12034	PDFL Detail	423.12	
501.0000.51.521.10.48.005			3/15/2021	12052	PDFL Detail	423.12	
17147	3/31/2021	011701		BUENAVISTA SERVICES INC,			\$18,973.62
502.0000.17.518.30.41.001			3/20/2021	8539	PKFC 03/21 Custodial Svcs	4,524.21	
502.0000.17.521.50.48.001			3/20/2021	8539	PKFC 03/21 Custodial Svcs	2,309.72	
502.0000.17.542.65.48.001			3/20/2021	8539	PKFC 03/21 Custodial Svcs	1,098.96	
001.0000.11.576.81.41.001			3/20/2021	8539	PKFC 03/21 Custodial Svcs	449.07	
502.0000.17.518.30.41.001			12/31/2020	8332	PKFC 2020 Annual Carpet Cleani	5,800.00	
502.0000.17.521.50.41.001			12/31/2020	8332	PKFC 2020 Annual Carpet Cleani	2,685.00	
502.0000.17.542.65.48.001			12/31/2020	8332	PKFC 2020 Annual Carpet Cleani	580.00	
001.0000.11.576.81.41.001			12/31/2020	8332	PKFC 2020 Annual Carpet Cleani	400.00	
502.0000.17.521.50.48.001			1/1/2021	8312	PKFC 12/20 Day Porter Svcs	1,126.66	
17148	3/31/2021	010262		CENTURYLINK,			\$1,113.97
503.0000.04.518.80.42.001			3/19/2021	253-588-4697 855B	IT 03/19-04/19 Phone	50.35	
503.0000.04.518.80.42.001			3/14/2021	253-589-8734 340B	IT 03/14-04/14 Phone	170.95	
503.0000.04.518.80.42.001			3/16/2021	253-582-0174 486B	IT 03/16-04/16 Phone	266.63	
503.0000.04.518.80.42.001			3/16/2021	253-582-0669 467B	IT 03/16-04/16 Phone	240.14	
503.0000.04.518.80.42.001			3/16/2021	253-582-1023 738B	IT 03/16-04/16 Phone	65.00	
503.0000.04.518.80.42.001			3/16/2021	253-582-7426 582B	IT 03/16-04/16 Phone	124.23	
503.0000.04.518.80.42.001			3/2/2021	253-581-8220 448B	IT 03/02-04/02 Phone	58.45	
503.0000.04.518.80.42.001			3/1/2021	253-584-5364 399B	IT 03/01-04/01 Phone	58.45	
503.0000.04.518.80.42.001			3/1/2021	253-584-5364 399B	IT 03/01-04/01 Phone	79.77	
17149	3/31/2021	000536		CITY TREASURER CITY OF TACOMA,			\$2,227.23
101.0000.11.542.63.47.006			3/16/2021	100349419 03/16/21	PKST 01/14-03/15 7502 Lkwd Dr	25.65	
101.0000.11.542.64.47.005			3/16/2021	100350986 03/16/21	PKST 01/14-03/15 8800 Custer R	131.09	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
101.0000.11.542.63.47.006			3/16/2021	100440754 03/16/21	PKST 02/12-03/15 7211 BPW W St	20.99	
101.0000.11.542.64.47.005			3/16/2021	100463727 03/16/21	PKST 01/14-03/15 7919 Custer R	4.01	
101.0000.11.542.64.47.005			3/16/2021	100520997 03/16/21	PKST 01/14-03/15 7609 Custer R	64.38	
101.0000.11.542.64.47.005			3/16/2021	100892477 03/16/21	PKST 01/14-03/15 8108 John Dow	116.44	
101.0000.11.542.63.47.006			3/16/2021	100898201 03/16/21	PKST 02/12-03/15 7729 BPW W	162.72	
101.0000.11.542.64.47.005			3/12/2021	100463729 03/12/21	PKST 01/12-03/11 8203 Custer R	4.01	
101.0000.11.542.64.47.005			3/12/2021	100575626 03/12/21	PKST 01/12-03/11 8901 BPW SW	83.38	
101.0000.11.542.64.47.005			3/12/2021	100681481 03/12/21	PKST 01/12-03/11 8601 BPW SW S	115.82	
001.0000.11.576.81.47.005			3/22/2021	100384880 03/22/21	PKFC 01/21-03/19 8700 Steil Bl	36.04	
001.0000.11.576.81.47.005			3/18/2021	100384879 03/18/21	PKFC 01/16-03/17 8750 Steil Bl	72.08	
101.0000.11.542.63.47.006			3/18/2021	100415564 03/18/21	PKST 02/17-03/17 9450 Steil Bl	74.70	
101.0000.11.542.63.47.006			3/18/2021	100415566 03/18/21	PKST 02/17-03/17 9000 Steil Bl	73.84	
101.0000.11.542.63.47.006			3/18/2021	100415597 03/18/21	PKST 02/17-03/17 10000 Steil B	80.19	
101.0000.11.542.63.47.006			3/18/2021	100471519 03/18/21	PKST 02/17-03/17 8312 87th St	45.01	
101.0000.11.542.64.47.005			3/18/2021	100658937 03/18/21	PKST 01/16-03/17 10300 Steil B	76.55	
101.0000.11.542.64.47.005			3/18/2021	100687561 03/18/21	PKST 01/16-03/17 8623 87th Ave	60.00	
001.0000.11.576.81.47.005			3/18/2021	101076847 03/18/21	PKFC 01/16-03/17 8750 Steil Bl	214.39	
101.0000.11.542.64.47.005			3/18/2021	101086773 03/18/21	PKST 02/17-03/17 9550 Steil Bl	26.91	
101.0000.11.542.64.47.005			3/24/2021	100228932 03/24/21	PKST 01/23-03/23 8300 Steil Bl	283.86	
101.0000.11.542.64.47.005			3/24/2021	100228949 03/24/21	PKST 01/23-03/23 8200 Steil Bl	133.16	
101.0000.11.542.64.47.005			3/23/2021	100228710 03/23/21	PKST 01/22-03/22 8915 Meadow R	56.19	
101.0000.11.542.64.47.005			3/23/2021	100228892 03/23/21	PKST 01/22-03/22 9299 Whitman	51.89	
101.0000.11.542.64.47.005			3/23/2021	100433653 03/23/21	PKST 01/22-03/22 5460 Steil Bl	4.01	
101.0000.11.542.64.47.005			3/25/2021	100228868 03/25/21	PKST 01/26-03/24 10099 GLD SW	47.36	
101.0000.11.542.64.47.005			3/25/2021	100665891 03/25/21	PKST 02/24-03/24 7309 Onyx Dr	19.18	
101.0000.11.542.64.47.005			3/26/2021	100228748 03/26/21	PKST 01/27-03/25 11170 GLD SW	88.15	
101.0000.11.542.64.47.005			3/26/2021	100254732 03/26/21	PKST 02/25-03/25 11023 GLD SW	19.74	
101.0000.11.542.64.47.005			3/26/2021	100707975 03/26/21	PKST 02/25-03/25 7403 Lkwd Dr	35.49	
17150	3/31/2021	005786		CLASSY CHASSIS,			\$95.29
501.0000.51.521.10.48.005			3/12/2021	5129	PDFL Oil Change	95.29	
17151	3/31/2021	000099		CLOVER PARK SCHOOL DISTRICT,			\$1,020.52
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	75.58	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	39.04	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	27.68	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	51.48	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	139.85	

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501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	76.47	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	48.02	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	33.71	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	105.26	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	164.62	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	87.50	
501.0000.51.548.79.32.001			3/1/2021	20278	PKFL 02/21 Fuel	171.31	
17152	3/31/2021	008201		CONSTRUCTION TESTING,			\$670.00
302.0134.21.595.30.41.001			3/22/2021	141332	PWCP 03/02 Veterans Dr Improve	670.00	
17153	3/31/2021	010648		DIAMOND MARKETING SOLUTIONS,			\$2,571.63
001.0000.99.518.40.42.002			3/31/2021	03/21 Postage	ND 03/21 Replenish Postage	2,483.44	
311.0000.01.535.30.42.002			3/31/2021	03/21 Postage	PWSC 03/21 Postage Est.	88.19	
17154	3/31/2021	012931		DIMENSION TOWNHOUSES LLC,			\$5,500.40
190.2003.53.559.32.41.001			3/25/2021	AG 2021-081	CDBG AG 2021-081 Rent Assist:	2,139.60	
190.2003.53.559.32.41.001			3/25/2021	AG 2021-082	CDBG AG 2021-082 Rent Assist:	3,360.80	
17155	3/31/2021	003828		EFFICIENCY INC,			\$328.60
503.0000.04.518.80.48.003			3/12/2021	2724	IT Annotation Suite FTR 6.4	328.60	
17156	3/31/2021	003435		EMERALD HILLS COFFEES,			\$219.36
001.0000.99.518.40.45.004			3/25/2021	8525244	ND Water Cooler Rental	219.36	
17157	3/31/2021	005190		FASTENAL,			\$15.39
501.0000.51.548.79.31.006			3/16/2021	WALA246304	PKFL Square Single Wire Snappe	15.39	
17158	3/31/2021	009689		FLO HAWKS,			\$573.02
401.0000.11.531.10.48.001			3/8/2021	66132649	PKSW 03/08 Maint. Drain Cleani	286.51	
401.0000.11.531.10.48.001			12/11/2020	66123251	PKSW 12/11/20 Inspection 11325	286.51	
17159	3/31/2021	012975		FOSTER GARVEY PC,			\$7,350.00
192.0007.07.594.58.61.007			3/23/2021	2782264	SSMP Thru 02/28 Tactical Tailo	7,050.00	
001.0000.06.515.30.41.001			3/23/2021	2782265	LG Thru 02/28/21Swan Propertie	300.00	
17160	3/31/2021	012971		GREENFIELD APARTMENTS LLC,			\$4,000.00
190.2003.53.559.32.41.001			3/25/2021	AG 2021-076	CDBG AG 2021-076 Rent Assist:	4,000.00	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
17161	3/31/2021	005137		HAMILTON, RYAN			\$186.00
180.0000.15.521.21.43.004			3/30/2021	04/10-04/15 Per Diem	PD WSNIA Conf: R. Hamilton	186.00	
17162	3/31/2021	012411		HERRERA-VELASQUEZ, MURIEL			\$2,300.00
001.9999.11.565.10.41.020			3/24/2021	42	PKHS AG 2019-168 03/16-03/30 L	2,300.00	
17163	3/31/2021	012308		HONEY BUCKET,			\$586.48
001.0000.11.576.81.41.001				0552013242	PKFC 03/18-04/14 8714 87th Ave	-101.00	
001.0000.11.576.81.41.001				0552016023	PKFC Customer Credit 8714 87th	-1,200.00	
001.0000.11.576.81.41.001			1/21/2021	0551920914	PKFC 01/15-02/17 8714 87th Ave	283.13	
302.0134.21.595.30.63.001			1/28/2021	0551931017	PWCP 01/28-02/24 9222 Veterans	384.85	
001.0000.11.576.81.41.001			2/18/2021	0551962211	PKFC 02/18-03/17 8714 87th Ave	101.00	
302.0134.21.595.30.63.001			2/25/2021	0551972586	PWCP 02/25-03/24 9222 Veterans	384.85	
001.0000.11.576.81.41.001			3/18/2021	0552005134	PKFC 03/18-04/14 8714 87th Ave	101.00	
001.0000.11.576.81.41.001			3/25/2021	0552017932	PKFC 01/01-03/29 8714 87th Ave	632.65	
17164	3/31/2021	004036		HORIZON AUTOMATIC RAIN CO,			\$297.32
101.0000.11.542.70.31.030			3/26/2021	3N124148	PKST Herbicide	269.76	
502.0000.17.518.30.31.001			3/18/2021	3N123709	PKFC PVC Wire Saw	27.56	
17165	3/31/2021	008466		KBH CONSTRUCTION CO,			\$70,338.00
301.0006.11.594.76.63.001			3/23/2021	1	PK AG 2020-296 2020 Gateway &	62,658.00	
301.0036.11.594.76.63.001			3/23/2021	1	PK AG 2020-296 2020 Gateway &	11,382.00	
301.0000.00.223.40.00.000			3/23/2021	1	PK AG 2020-296 Retainage	-3,702.00	
17166	3/31/2021	010946		KRAUS, THO			\$70.00
001.0000.04.514.20.49.003			3/22/2021	To The Rescue 03/21	FN "To The Rescue" Webinar: Kr	70.00	
17167	3/31/2021	000299		LAKEVIEW LIGHT & POWER CO.,			\$3,084.85
101.0000.11.542.64.47.005			3/14/2021	67044-004 03./14/21	PKST 02/10-03/10 108th St SW &	72.79	
101.0000.11.542.64.47.005			3/14/2021	67044-010 3/14/21	PKST 02/10-03/10 108th St SW &	67.78	
101.0000.11.542.64.47.005			3/14/2021	67044-017 03/14/21	PKST 02/10-03/10 112th St SW &	67.34	
101.0000.11.542.64.47.005			3/14/2021	67044-030 03/14/21	PKST 02/10-03/10 112th ST SW &	73.50	
101.0000.11.542.63.47.006			3/14/2021	67044-072 03/14/21	PKST 02/10-03/10 11302 Kendric	121.53	
502.0000.17.542.65.47.005			3/14/2021	67044-073 03/14/21	PKFC 02/10-03/10 11420 Kendric	394.36	
101.0000.11.542.64.47.005			3/7/2021	67044-002 03/07/21	PKST 02/03-03/03 Pac Hwy & STW	76.63	
101.0000.11.542.64.47.005			3/7/2021	67044-012 03/07/21	PKST 02/03-03/03 Hwy 512 & STW	104.93	
101.0000.11.542.63.47.006			3/7/2021	67044-014 03/07/21	PKST 02/03-03/03 Hwy 512 & STW	91.54	
101.0000.11.542.64.47.005			3/7/2021	67044-016 03/07/21	PKST 02/03-03/03 40th Ave SW	62.43	

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101.0000.11.542.64.47.005			3/7/2021	67044-031 03/07/21	PKST 02/03-03/03 84th St S & S	67.70	
101.0000.11.542.64.47.005			3/7/2021	67044-032 03/07/21	PKST 02/03-03/03 100th ST SW &	86.36	
001.0000.11.576.80.47.005			3/7/2021	67044-048 03/07/21	PKFC 02/03-03/03 2716 84th St	80.78	
101.0000.11.542.64.47.005			3/7/2021	67044-050 03/07/21	PKST 02/03-03/03 LKWD Dr SW/St	78.05	
101.0000.11.542.64.47.005			3/7/2021	67044-053 03/07/21	PKST 02/03-03/03 4648 Steil Bl	58.96	
101.0000.11.543.50.47.005			3/7/2021	67044-074 03/07/21	PKST 02/03-03/03 9424 Front St	561.83	
001.0000.11.576.80.47.005			3/7/2021	67044-075 03/07/21	PKFC 02/03-03/03 8807 25th Ave	183.02	
101.0000.11.542.64.47.005			3/7/2021	67044-078 03/07/21	PKST 02/03-03/03 100th St SW &	107.51	
101.0000.11.542.64.47.005			3/7/2021	67044-079 03/07/21	PKST 02/03-03/03 96th St S & S	219.02	
101.0000.11.542.64.47.005			3/7/2021	67044-080 03/07/21	PKST 02/03-03/03 8802 STW	78.50	
101.0000.11.542.64.47.005			3/7/2021	67044-081 03/07/21	PKST 02/03-03/03 3601 Steil Bl	74.22	
101.0000.11.542.63.47.006			3/7/2021	67044-083 03/07/21	PKST 02/03-03/03 40th & 100th	101.53	
101.0000.11.542.64.47.005			3/7/2021	67044-084 03/07/21	PKST 02/03-03/03 Steil & Lkvw	78.23	
101.0000.11.542.63.47.006			3/7/2021	67044-085 03/07/21	PKST 02/03-03/03 26th & 88th S	46.36	
101.0000.11.542.63.47.006			3/7/2021	67044-087 03/07/21	PKST 02/03-03/03 123rd & BPW S	79.13	
101.0000.11.542.63.47.006			3/7/2021	67044-089 03/07/21	PKST 02/03-03/03 9520 Front ST	50.82	
17168	3/31/2021	000280		LAKESIDE CHAMBER OF COMMERCE,			\$12,706.65
104.0005.01.557.30.41.001			3/21/2021	01/21	HM AG 2021-092 01/21 Tourism L	5,886.46	
104.0005.01.557.30.41.001			3/21/2021	02/21	HM AG 2021-092 02/21 Tourism L	6,137.19	
001.0000.13.558.70.49.001			5/1/2021	SC9676	ED Lkwd Chamber Of Commerce Me	683.00	
17169	3/31/2021	000288		LAKESIDE HARDWARE & PAINT INC,			\$299.64
001.0000.11.576.80.31.001			3/24/2021	643846	PKFC Keys	13.13	
001.0000.11.576.80.31.001			3/18/2021	643297	PKFC Nozzle	35.57	
101.0000.11.542.30.31.001			3/17/2021	643206	PKST Brooms	29.64	
001.0000.11.576.81.31.001			3/11/2021	642736	PKFC Basket	24.71	
001.0000.11.576.80.31.001			3/16/2021	643107	PKFC Bolt, Washer	1.95	
001.0000.11.576.80.31.001			3/16/2021	643112	PKFC Roller Covers, Oil	194.64	
17170	3/31/2021	002021		LAKESIDE HISTORICAL SOCIETY,			\$5,165.59
104.0008.01.557.30.41.001			3/18/2021	03/18/21	HM AG 2021-091 Lodging Tax Gra	5,165.59	
17171	3/31/2021	005490		LAKESIDE PLAYHOUSE,			\$449.51
104.0013.01.557.30.41.001			3/10/2021	03/10/21	HM AG 2021-090 Lodging Tax Gra	449.51	
17172	3/31/2021	000298		LAKESIDE TOWING,			\$208.43
001.0000.15.521.10.41.070			3/11/2021	226212	PD 03/09	126.00	
001.0000.15.521.10.41.070			3/18/2021	226272	PD 03/10	82.43	

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17173	3/31/2021	010674		MACKAY COMMUNICATIONS INC,			\$49.98
503.0000.04.518.80.42.001			3/27/2021	SB078377	IT PD 02/21 Air-Time AQ01968	49.98	
17174	3/31/2021	011494		MARTIN, BRIAN			\$325.00
503.0000.04.518.80.41.001			3/18/2021	2106	IT 02/21 Training In Adobe Afte	130.00	
503.0005.04.518.80.41.001			3/18/2021	2107	IT 03/21 Research & Troublesho	195.00	
17175	3/31/2021	009724		MILES RESOURCES LLC,			\$514,072.88
101.0000.11.542.90.49.018			3/15/2021	318927	PKST Recycle Waste Products	6.30	
101.0000.11.542.63.31.030			3/15/2021	319043	PKST Hot Mix Asphalt	529.66	
101.0000.11.542.90.49.018			3/15/2021	319043	PKST Recycle Waste Products	162.10	
101.0000.11.542.63.31.030			3/15/2021	319070	PKST Hot Mix Asphalt	1,631.96	
101.0000.11.542.30.31.030			2/28/2021	318426	PKST Cold Mix	102.43	
302.0134.21.595.30.63.001			3/25/2021	2020-083 PP # 10	PWCP AG 2020-083 02/01-02/28 V	511,226.39	
101.0000.11.542.90.49.018			3/22/2021	319137	PKST Recycle Waste Products	36.70	
101.0000.11.542.30.31.030			3/22/2021	319138	PKST Hot Mix Asphalt	304.84	
101.0000.11.542.90.49.018			3/22/2021	319215	PKST Recycle Waste Products	72.50	
17176	3/31/2021	012969		NRB PROPERTY MANAGEMENT,			\$3,060.00
190.2003.53.559.32.41.001			3/25/2021	AG 2021-078	CDBG AG 2021-078 Rent Assist:	3,060.00	
17177	3/31/2021	009317		OPTIC FUSION INC,			\$1,524.28
503.0000.04.518.80.42.001			3/1/2021	95-19018	IT 03/21 Internet Connectivity	1,524.28	
17178	3/31/2021	012500		O'REILLY AUTO PARTS,			\$173.51
501.0000.51.548.79.31.006			3/24/2021	2863-223697	PKFL Wiper Blades	19.67	
501.0000.51.548.79.31.006			3/24/2021	2863-223697	PKFL Wiper Blades	19.67	
501.0000.51.548.79.31.006			3/24/2021	2863-223697	PKFL Wiper Blades	19.67	
501.0000.51.548.79.31.006			3/24/2021	2863-223697	PKFL Wiper Blades	19.67	
501.0000.51.548.79.31.006			3/24/2021	2863-223697	PKFL Wiper Blades	19.68	
501.0000.51.548.79.31.006			3/23/2021	3626-322150	PKFL Wiper Blades	75.15	
17179	3/31/2021	010255		PAPE' MACHINERY EXCHANGE,			\$945.31
001.0000.11.576.80.35.001			2/18/2021	250841	PKFC Power Trim Edger	945.31	
17180	3/31/2021	007033		PARAMETRIX,			\$29,283.56
302.0135.21.595.12.41.001			3/17/2021	25406	PWCP AG 2020-019 Thru 02/27 JB	29,283.56	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
17181	3/31/2021	000407		PIERCE COUNTY,			\$1,071.39
001.0000.02.237.11.00.002			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	558.88	
001.0000.15.521.10.41.125			3/22/2021	CI-299932	PD 02/21 Jail Scvs	512.51	
17182	3/31/2021	012974		PIERCE COUNTY ALLIANCE,			\$9,437.50
195.0021.02.512.53.41.001			3/15/2021	2019-2021	MC 2019-2021 UAs	9,437.50	
17183	3/31/2021	000428		PIERCE COUNTY SEWER,			\$161.48
001.0000.11.576.81.47.004			3/21/2021	02029430 03/21/21	PKFC 01/01-02/28 9101 Angle Ln	121.65	
001.0000.11.576.81.47.004			3/14/2021	02020548 03/14/21	PKFC 02/21 9115 Angle LN SW Sh	39.83	
17184	3/31/2021	010630		PRINT NW,			\$40.67
001.0000.01.511.60.49.005			3/10/2021	30980501	CC Business Cards: Patti Belle	40.67	
17185	3/31/2021	009928		PROFAST SUPPLY LLC,			\$718.95
001.0000.11.576.80.31.001			3/25/2021	31057	PKFC Supplies	718.95	
17186	3/31/2021	012650		QUIGG BROS INC,			\$74,111.85
301.0003.11.594.76.63.001			3/10/2021	6	PK AG 2020-103 02/21 Harry Tod	77,644.35	
301.0000.00.223.40.00.000			3/10/2021	6	PK AG 2020-103 02/21 Retainage	-3,532.50	
17187	3/31/2021	010522		RICOH USA INC,			\$42.14
503.0000.04.518.80.45.002			3/21/2021	5061660586	IT 02/21-03/20 Add't Images	20.91	
503.0000.04.518.80.45.002			12/18/2020	29184	IT 02/18-03/17 Add'l Images	21.23	
17188	3/31/2021	005984		SCHALER, GERALD			\$60.00
001.0000.11.576.81.31.008			3/20/2021	7874	PKFC Schaler Work Boot Allowan	60.00	
17189	3/31/2021	011411		SMARSH INC,			\$1,781.10
503.0000.04.518.80.42.001			1/31/2021	INV00642643	IT 01/01-12/31 Archiving Platf	989.10	
503.0000.04.518.80.42.001			1/31/2021	INV00642643	IT 01/01-12/31 Verizon - MG Cl	792.00	
17190	3/31/2021	012410		SOLON, LISA			\$1,928.00
001.9999.11.565.10.41.020			3/24/2021	43	PKHS AG 2019-169 03/16-03/30 L	1,928.00	
17191	3/31/2021	003267		SOUTH TACOMA GLASS SPECIALISTS,			\$582.48
501.0000.51.521.10.48.005			3/9/2021	45967	PDFL Auto Glass	582.48	
17192	3/31/2021	002881		SPRAGUE PEST SOLUTIONS CO,			\$216.40

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
502.0000.17.521.50.48.001			3/24/2021	4461665	PKFC 03/24 Pest Control PD	126.39	
001.0000.11.576.81.41.001			3/22/2021	4454083	PKFC 03/22 Gen Pest Svcs	90.01	
17193	3/31/2021	000516		SPRINT,			\$120.47
503.0000.04.518.80.42.001			3/18/2021	482477812-160	IT 02/15-03/14 Phone	120.47	
17194	3/31/2021	009493		STAPLES ADVANTAGE,			\$846.68
105.0002.07.559.20.31.001			3/11/2021	3471853735	AB Cardstock, Folders	46.17	
001.0000.07.558.60.31.001			3/12/2021	3471920502	CD/PKST Office Supplies	17.57	
101.0000.21.544.20.31.001			3/12/2021	3471920502	CD/PKST Office Supplies	27.37	
001.0000.09.518.10.31.001			3/4/2021	3471381140	HR Cert Holdr, Frames	70.98	
001.0000.15.521.10.31.001			3/5/2021	3471451418	PD Office Supplies	65.39	
001.0000.15.521.10.31.001			3/5/2021	3471451419	PD Office Supplies	9.19	
190.6002.52.559.31.31.001			3/5/2021	3471451420	CDBG/PWSW Office Supplies	257.16	
190.0005.52.559.31.31.001			3/5/2021	3471451420	CDBG/PWSW Office Supplies	123.84	
401.0000.41.531.10.31.001			3/5/2021	3471451420	CDBG/PWSW Office Supplies	15.41	
190.6002.52.559.31.31.001			3/5/2021	3471451421	CDBG Folders	43.16	
001.0000.04.514.20.31.001			3/5/2021	3471451422	FN Office Supplies	107.78	
001.0000.04.514.20.31.001			3/5/2021	3471451423	FN Tape	7.08	
001.0000.02.512.50.31.001				3471451417	MC Supplies/Headsets Cr For I	-513.52	
001.0000.15.521.10.31.001			2/27/2021	3470745624	PD Office Supplies	357.08	
001.0000.15.521.10.31.001			2/27/2021	3470745634	PD Office Supplies	141.17	
001.0000.15.521.10.31.001			2/27/2021	3470745639	PD Office Supplies	70.85	
17195	3/31/2021	002458		SUMMIT LAW GROUP,			\$938.00
001.0000.06.515.30.41.001			3/19/2021	122840	LG 02/21 General Labor	938.00	
17196	3/31/2021	006497		SYSTEMS FOR PUBLIC SAFETY,			\$33,137.75
501.0000.51.521.10.48.005			3/17/2021	39360	PDFL Electrical	1,052.78	
501.0000.51.521.10.48.005			3/17/2021	39470	PDFL Oil Change	86.52	
501.0000.51.521.10.48.005			3/17/2021	39470	PDFL Safety Inspection	22.38	
501.0000.51.521.10.48.005			3/17/2021	39501	PDFL Oil Change	86.52	
501.0000.51.521.10.48.005			3/17/2021	39501	PDFL Ult Car Washes 03/11/17-0	22.38	
501.0000.51.521.10.48.005			3/10/2021	39095	PDFL Oil Change	93.09	
501.0000.51.521.10.48.005			3/10/2021	39095	PDFL Safety Inspection	27.84	
501.0000.51.521.10.48.005			3/10/2021	39095	PDFL Other	1,612.16	
501.0000.51.521.10.48.005			3/10/2021	39291	PDFL Other	945.15	
501.0000.51.521.10.48.005			3/10/2021	39391	PDFL Oil Change	78.92	
501.0000.51.521.10.48.005			3/10/2021	39391	PDFL Safety Inspection	465.70	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.521.10.48.005			3/10/2021	39391	PDFL Wipers	32.66	
501.0000.51.521.10.48.005			3/10/2021	39391	PDFL Radiator	516.17	
501.0000.51.521.10.48.005			3/10/2021	39391	PDFL Other	133.12	
501.0000.51.521.10.48.005			3/10/2021	39412	PDFL Oil Change	89.43	
501.0000.51.521.10.48.005			3/10/2021	39412	PDFL Safety Inspection	70.89	
501.0000.51.521.10.48.005			3/10/2021	39412	PDFL Brakes	1,008.74	
501.0000.51.521.10.48.005			3/10/2021	39412	PDFL Electrical	75.56	
501.0000.51.521.10.48.005			3/10/2021	39412	PDFL Tires	231.83	
501.0000.51.521.10.48.005			3/10/2021	39438	PDFL Oil Change	90.80	
501.0000.51.521.10.48.005			3/10/2021	39438	PDFL Safety Inspection	25.55	
501.0000.51.521.10.48.005			3/10/2021	39438	PDFL Other	429.45	
501.0000.51.521.10.48.005			3/10/2021	39438	PDFL Tire	233.21	
501.0000.51.521.10.48.005			3/10/2021	39445	PDFL Other	49.05	
501.9999.51.594.21.64.005			3/15/2021	38667	PDFL Build	21,607.98	
501.0000.51.521.10.48.005			3/15/2021	39432	PDFL Oil Change	88.51	
501.0000.51.521.10.48.005			3/15/2021	39432	PDFL Safety Inspection	23.26	
501.0000.51.521.10.48.005			3/15/2021	39432	PDFL Electrical	227.68	
501.0000.51.521.10.48.005			3/15/2021	39432	PDFL Brakes	331.55	
501.0000.51.521.10.48.005			3/15/2021	39432	PDFL Wipers	179.89	
501.0000.51.521.10.48.005			3/15/2021	39432	PDFL Tire Repair	74.64	
501.0000.51.521.10.48.005			3/15/2021	39433	PDFL Other	175.49	
501.0000.51.521.10.48.005			3/15/2021	39464	PDFL Tire	49.05	
501.0000.51.521.10.48.005			3/15/2021	39465	PDFL Electrical	68.54	
501.0000.51.521.10.48.005			3/15/2021	39466	PDFL Tire	968.59	
501.0000.51.521.10.48.005			3/15/2021	39469	PDFL Tire	29.43	
501.0000.51.521.10.48.005			3/15/2021	39482	PDFL Tire	228.28	
501.0000.51.521.10.48.005			3/15/2021	39482	PDFL Wipers	38.64	
501.0000.51.521.10.48.005			3/15/2021	39482	PDFL Electrical	32.70	
501.0000.51.521.10.48.005			3/25/2021	39479	PDFL A/C	772.96	
501.0000.51.521.10.48.005			3/25/2021	39479	PDFL Tire Repair	149.28	
501.0000.51.521.10.48.005			3/25/2021	39479	PDFL Other	13.56	
501.0000.51.521.10.48.005			3/25/2021	39490	PDFL Diagnostics	49.05	
501.0000.51.521.10.48.005			3/25/2021	39496	PDFL Other	49.05	
181.0000.51.521.21.48.005			3/25/2021	39508	PDFL Oil Change	80.33	
181.0000.51.521.21.48.005			3/25/2021	39508	PDFL Safety Inspection	27.71	
181.0000.51.521.21.48.005			3/25/2021	39508	PDFL Battery	391.68	
17197	3/31/2021	008186		TRCVB,			\$15,004.57

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
104.0016.01.557.30.41.001			2/28/2021	LW-2021-02	HM AG 2021-087 02/21 Lodging T	10,489.01	
104.0016.01.557.30.41.001			1/31/2021	LW-2021-01	HM AG 2021-087 01/21 Lodging T	4,515.56	
17198	3/31/2021	007355		TRIPPER'S TRAILER SERVICE &			\$384.65
501.0000.51.548.79.48.005			3/8/2021	5475	PKFL Trailer Drum Bearing Svc	384.65	
17199	3/31/2021	000153		TYLER TECHNOLOGIES INC,			\$109.40
503.0000.04.518.80.49.004			3/22/2021	020-128533	IT 04/15-05/14 Tyler Supervisi	109.40	
17200	3/31/2021	012365		US CAD HOLDINGS LLC,			\$1,232.25
001.0000.07.558.60.41.090			3/15/2021	INV46450	IT Revu Std, Bluebeam Revu Std	1,232.25	
17201	3/31/2021	009372		VENTEK INTERNATIONAL,			\$90.00
503.0000.04.518.80.42.001			3/1/2021	125760	IT 03/21 CCU Server Hosting, D	90.00	
17202	3/31/2021	000593		WASHINGTON STATE TREASURER,			\$41,934.39
001.0000.02.237.10.00.002			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	16,677.03	
001.0000.02.237.10.00.001			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	9,681.40	
001.0000.02.237.30.00.000			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	58.00	
001.0000.02.386.89.15.001			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	32.10	
001.0000.02.237.10.00.007			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	1,579.23	
001.0000.02.386.89.16.001			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	176.78	
001.0000.02.386.89.14.001			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	118.13	
001.0000.02.237.10.00.008			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	3,159.56	
001.0000.02.237.10.00.009			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	1,195.12	
001.0000.02.386.89.12.001			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	99.99	
001.0000.02.386.89.13.001			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	99.99	
001.0000.02.237.10.00.003			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	8,016.55	
001.0000.02.237.10.00.006			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	503.01	
001.0000.02.237.10.00.004			3/23/2021	02/21 Bldg. Code	MC 02/21 State Bldg. Code	537.50	
17203	3/31/2021	010239		WEST PIERCE FIRE & RESCUE,			\$220.00
001.0000.11.576.81.41.001			3/23/2021	341C-002-000-001 '21	PKFC Fire Code Permit 9115 Ang	110.00	
502.0000.17.518.35.41.001			3/9/2021	384D-001-003-000 '21	PKFC 384D-001-003-000 CH Fire	110.00	
17204	3/31/2021	012671		WILLIAMS KASTNER & GIBBS PLLC,			\$459.96
105.0001.07.559.20.41.001			3/23/2021	618064	AB Thru 02/28 Terry Emmert	459.96	
17205	3/31/2021	011031		XIOLOGIX LLC,			\$3,011.85

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
503.0000.04.518.80.48.002			3/3/2021	8352	IT - Fortinet Renewal, FortiNa	2,180.56	
503.0000.04.518.80.48.002			3/3/2021	8352	IT - Fortinet Renewal	559.98	
503.0000.04.518.80.48.002			3/3/2021	8352	Sales Tax	271.31	
17206	3/31/2021	008553		ZONES INC,			\$9,920.24
503.0000.04.518.80.31.001			3/17/2021	K16978810101	IT Labels	236.29	
503.0000.04.518.80.48.003			2/27/2021	K16863780101	IT 1 Yr. Traps Advanced Endpoi	8,901.90	
503.0000.04.518.80.35.030			3/24/2021	B47798540101	IT Startech USB-C Multiport Ad	264.42	
503.0000.04.518.80.35.030			3/24/2021	K17041790101	IT Labelwriter Print Server	161.55	
503.0000.04.518.80.35.030			3/25/2021	B47798540102	IT Startech USB-C Multiport Ad	356.08	
17207	3/31/2021	001272		ZUMAR INDUSTRIES INC,			\$3,016.77
101.0000.11.542.64.31.001			3/5/2021	35547	PKST Signs	3,016.77	
17208	4/15/2021	011591		911 SUPPLY INC,			\$1,436.03
001.0000.15.521.22.31.008			3/25/2021	INV-2-9613	PD Belts	969.01	
001.0000.15.521.22.31.008			4/9/2021	INV-2-9946	PD Shirts, Jackets, Pants: L C	467.02	
17209	4/15/2021	002831		ADVANCED TRAFFIC PRODUCTS INC,			\$290.31
101.0000.11.542.64.35.014			4/5/2021	0000029292	PKST Polara 6" Push Button Ext	290.31	
17210	4/15/2021	001685		AMAYA ELECTRIC CORP,			\$26,921.14
504.0000.09.518.39.48.001			3/31/2021	9312-1	RM CLAIM 2020-0079 South Tacom	20,314.02	
504.0000.00.223.40.00.000			3/31/2021	9312-1	RM CLAIM 2020-0079 Retainage	-1,015.70	
302.0004.21.595.30.63.001			3/31/2021	9312-11	PWCP Steilacoom Blvd & Custer	4,104.16	
302.0000.00.223.40.00.000			3/31/2021	9312-11	PWCP Steil. Blvd. & Custer Rd	-205.21	
101.0000.11.542.64.48.001			3/31/2021	9312-12	PWST Steil. @ CPTC St. Light R	546.60	
101.0000.00.223.40.00.000			3/31/2021	9312-12	PWST Steil. @ CPTC St Light Re	-27.33	
302.0080.21.595.30.63.001			3/31/2021	9312-13	PWCP 108th St SW & Pacific Hwy	3,373.26	
302.0000.00.223.40.00.000			3/31/2021	9312-13	PWCP 108th/Pac Hwy Retainage	-168.66	
17211	4/15/2021	009961		AMB TOOLS & EQUIPMENT,			\$486.42
501.0000.51.548.79.31.006			4/12/2021	T325845	PKFL Hose, Gun, Wand, Coupler,	486.42	
17212	4/15/2021	007445		ASSOCIATED PETROLEUM PRODUCTS,			\$980.31
501.0000.51.548.79.32.001			3/30/2021	0316293-IN	PKFL 3/17-3/30	36.25	
501.0000.51.548.79.32.002			3/30/2021	0316293-IN	PKFL 3/17-3/30	63.23	
501.0000.51.548.79.32.002			3/30/2021	0316293-IN	PKFL 3/17-3/30	100.93	
501.0000.51.548.79.32.002			3/30/2021	0316293-IN	PKFL 3/17-3/30	35.38	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.548.79.32.001			3/30/2021	0316293-IN	PKFL 3/17-3/30	9.28	
501.0000.51.548.79.32.001			3/30/2021	0316293-IN	PKFL 3/17-3/30	49.31	
501.0000.51.548.79.32.002			3/30/2021	0316293-IN	PKFL 3/17-3/30	77.15	
501.0000.51.548.79.32.001			3/30/2021	0316293-IN	PKFL 3/17-3/30	144.44	
501.0000.51.548.79.32.001			3/30/2021	0316293-IN	PKFL 3/17-3/30	183.30	
501.0000.51.548.79.32.002			3/30/2021	0316293-IN	PKFL 3/17-3/30	43.51	
501.0000.51.548.79.32.001			3/30/2021	0316293-IN	PKFL 3/17-3/30	76.28	
501.0000.51.548.79.32.001			3/30/2021	0316293-IN	PKFL 3/17-3/30	104.41	
501.0000.51.548.79.32.001			3/30/2021	0316293-IN	PKFL 3/17-3/30	56.84	
17213	4/15/2021	011039		BERK CONSULTING INC,			\$2,972.50
001.9999.07.558.65.41.001			3/30/2021	10472-02-21	CD AG 2020-043 02/21 Lkwd Stat	900.00	
001.9999.07.558.65.41.001			4/13/2021	10472-03-21	CD AG 2020-043 03/21 Lkwd Stat	2,072.50	
17214	4/15/2021	012280		BLUE PEAK LOGIC INC,			\$934.15
503.0000.04.518.80.48.003			6/30/2021	1691	IT 07/01/21-06/30/22 Skills Ma	934.15	
17215	4/15/2021	009742		CED,			\$1,982.28
502.0016.17.594.18.63.001			2/17/2021	8541-1001432	PKFC Lights	142.16	
502.0016.17.594.18.63.001			2/18/2021	8541-1001592	PKFC Lights	585.49	
502.0016.17.594.18.63.001			2/18/2021	8541-1001595	PKFC Lights	1,254.63	
17216	4/15/2021	010262		CENTURYLINK,			\$280.42
503.0000.04.518.80.42.001			3/23/2021	206-T31-6789 758B	IT 03/23-04/23 Phone	83.14	
503.0000.04.518.80.42.001			4/1/2021	253-584-2263 463B	IT 04/01-05/01 Phone	79.98	
503.0000.04.518.80.42.001			4/1/2021	253-584-5364 399B	IT 04/01-05/01 Phone	58.65	
503.0000.04.518.80.42.001			4/2/2021	253-581-8220 448B	IT 04/02-05/02 Phone	58.65	
17217	4/15/2021	000536		CITY TREASURER CITY OF TACOMA,			\$47,267.16
101.0000.11.542.63.47.006			4/7/2021	100223530 04/07/21	PKST 03/04-04/01 9315 GLD SW	2,258.79	
101.0000.11.542.64.47.005			4/7/2021	100436443 04/07/21	PKST 02/06-04/06 8103 83rd Ave	40.84	
101.0000.11.542.64.47.005			4/7/2021	101129625 04/07/21	PKST 02/06-04/06 7804 83rd Ave	46.90	
001.0000.15.521.10.49.020			4/1/2021	91023098	PD Q2/21 Radio User Fees	36,620.25	
502.0000.17.518.35.47.005			3/31/2021	100113209 03/31/21	PKFC 03/02-03/30 6000 Main St	8,074.48	
101.0000.11.542.63.47.006			3/31/2021	100218262 03/31/21	PKST 03/02-03/30 10601 Main St	61.24	
101.0000.11.542.63.47.006			3/31/2021	100218270 03/31/21	PKST 03/02-03/30 10602 Main St	10.16	
101.0000.11.542.64.47.005			3/31/2021	100218275 03/31/21	PKST 03/02-03/30 10511 GLD SW	68.26	
101.0000.11.542.63.47.006			3/31/2021	100262588 3/31/21	PKST 01/30-03/30 6100 Lkwd Tow	86.24	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
17218	4/15/2021	005786		CLASSY CHASSIS,			\$940.39
501.0000.51.521.10.48.005			3/26/2021	5130	PDFL Oil Change	100.49	
501.0000.51.521.10.48.005			3/26/2021	5130	PDFL Carwash	38.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	11.20	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	9.72	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	12.96	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	18.37	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	36.89	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	27.54	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	21.87	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	21.87	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	27.54	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	12.96	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	29.16	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	21.87	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	21.87	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	21.87	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	36.45	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	12.20	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	14.58	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	6.40	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.29	
501.0000.51.521.10.48.005			3/31/2021	W-979	PDFL Carwash	7.80	
501.0000.51.521.10.48.005			2/1/2021	34980	PDFL Carwash	12.96	
17219	4/15/2021	000099		CLOVER PARK SCHOOL DISTRICT,			\$1,302.01
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	75.93	
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	142.97	
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	29.10	
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	124.10	
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	224.94	
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	62.76	
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	71.32	
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	71.38	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	199.94	
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	39.58	
501.0000.51.548.79.32.001			4/5/2021	20279	PKFL 03/21 Fuel	259.99	
17220	4/15/2021	010648		DIAMOND MARKETING SOLUTIONS,			\$100.00
001.0000.99.518.40.42.002			4/6/2021	346170	ND 03/21 Daily Mail	100.00	
17221	4/15/2021	011679		DOOLITTLE CONSTRUCTION LLC,			\$15,278.22
302.0000.00.223.40.00.000			4/14/2021	AG 2020-104 Ret Rel	PWCP AG 2020-104 Retainage Rel	15,278.22	
17222	4/15/2021	011920		EILEEN OBRIEN CONSULTING,			\$1,125.00
195.0021.02.512.53.41.001			3/30/2021	02/21 & 03/21	MC 02/21 & 03/21 BJA Drug Cour	1,125.00	
17223	4/15/2021	005190		FASTENAL,			\$131.79
502.0016.17.594.18.63.001			3/30/2021	WALA246503	PKFC Machine Screws	3.85	
502.0000.17.521.50.31.001			3/18/2021	WALA246327	PKFC Maint. Supplies	104.26	
502.0000.17.521.50.31.001			3/16/2021	WALA246303	PKFC Maint. Supplies	23.68	
17224	4/15/2021	012935		FOREST VILLIAGE,			\$3,410.00
190.2003.53.559.32.41.001			4/13/2021	AG 2021-112	CDBG AG 2021-112 Rent Assist:	3,410.00	
17225	4/15/2021	011891		GLS US,			\$222.61
001.0000.99.518.40.42.002			3/31/2021	4422814	ND 03/21 Mail Pick Up	222.61	
17226	4/15/2021	007965		GORDON THOMAS HONEYWELL,			\$7,779.17
001.0000.03.513.10.41.001			3/31/2021	March 2021 1014	CM AG 2020-275 03/21 Gov'tl Af	4,862.50	
192.0000.00.558.60.41.001			3/31/2021	March 2021 1185	SSMCP AG 2020-110 03/21 Gov'tl	2,916.67	
17227	4/15/2021	012971		GREENFIELD APARTMENTS LLC,			\$4,000.00
190.2003.53.559.32.41.001			4/13/2021	AG 2021-113	CDBG AG 2021-113 Rent Assist:	4,000.00	
17228	4/15/2021	012958		GRM INFORMATION MANAGEMENT,			\$185,950.80
503.0018.04.594.14.64.003			3/31/2021	2300058	IT - Doc. Mgmt. 2 Yr VisualVau	169,200.00	
503.0018.04.594.14.64.003			3/31/2021	2300058	Sales Tax	16,750.80	
17229	4/15/2021	011900		HEMISPHERE DESIGN INC,			\$15,000.00
001.9999.13.558.70.41.001			4/1/2021	COL210406	ED AG 2021-110 04/21 Build You	15,000.00	
17230	4/15/2021	012806		HOLOSUN TECHNOLOGIES INC,			\$2,127.90

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
001.0000.15.521.10.35.010			3/25/2021	48292	PD HS515GM- 20mm micro optica	2,127.90	
17231	4/15/2021	012308		HONEY BUCKET,			\$250.72
302.0134.21.595.30.63.001			3/25/2021	0552016193	PWCP 03/25-03/29 9222 Veterans	68.72	
001.0000.02.523.30.47.004			3/8/2021	0551987860	MC 03/08-04/04 8714 87th Ave S	91.00	
001.0000.02.523.30.47.004			4/5/2021	0552033099	MC 04/05-05/02 8714 87th Ave S	91.00	
17232	4/15/2021	004036		HORIZON AUTOMATIC RAIN CO,			\$118.32
001.0000.11.542.70.31.001			4/14/2021	3N125174	PKST Sprayer	118.32	
17233	4/15/2021	011300		HORWATH LAW PLLC,			\$44,242.51
001.0000.02.512.51.41.004			4/8/2021	04/08/21	MC AG 2020-203 03/21 2021 Publ	38,750.00	
001.9999.02.512.51.41.001			4/8/2021	04/08/21	MC 03/21 Social Worker Service	2,007.51	
001.9999.02.512.51.41.001			4/8/2021	04/08/21	MC 03/21 Investigator Services	3,485.00	
17234	4/15/2021	000234		HUMANE SOCIETY FOR TACOMA & PC,			\$12,544.67
001.0000.15.554.30.41.008			4/1/2021	IVC0002321	PD AG 2020-261 04/21 Animal Sh	12,544.67	
17235	4/15/2021	003820		KNIGHT FIRE PROTECTION INC,			\$1,747.41
502.0000.17.518.35.41.001			4/6/2021	71288	PKFC 02/01 CH Annual Inspectio	692.37	
502.0000.17.521.50.41.001			4/6/2021	71289	PKFC 02/01 PD Annual Inspectio	538.51	
502.0000.17.542.65.48.001			4/6/2021	71290	PKFC 02/01 Transit Stn Annual	516.53	
17236	4/15/2021	011410		L.N. CURTIS AND SONS,			\$1,682.57
001.0000.15.521.26.31.020			3/23/2021	INV474347	PD Sponge Exact Impact 40MM Ro	1,377.90	
001.0000.15.521.26.31.020			3/23/2021	INV474347	Sales Tax	136.41	
001.0000.15.521.26.31.020			3/31/2021	INV476746	PD Sponge Exact Impact 40MM Ro	153.10	
001.0000.15.521.26.31.020			3/31/2021	INV476746	Sales Tax	15.16	
17237	4/15/2021	000299		LAKEVIEW LIGHT & POWER CO.,			\$25,982.29
101.0000.11.542.63.47.006			3/23/2021	232	PKST Q1/21 LED Street Lighting	12,599.38	
502.0000.17.521.50.47.005			3/21/2021	117448-001 03/21/21	PKFC 02/17-03/17 LKWD Police S	9,022.75	
101.0000.11.542.64.47.005			3/21/2021	67044-001 03/21/21	PKST 02/17-03/17 100th St SW &	67.34	
101.0000.11.542.64.47.005			3/21/2021	67044-003 03/21/21	PKST 02/17-03/17 Motor Ave & W	83.68	
101.0000.11.542.64.47.005			3/21/2021	67044-005 03/21/21	PKST 02/17-03/17 BP Wy SW & Lk	75.64	
101.0000.11.542.64.47.005			3/21/2021	67044-006 03/21/21	PKST 02/17-03/17 108th St SW &	70.02	
101.0000.11.542.64.47.005			3/21/2021	67044-019 03/21/21	PKST 02/17-03/17 BPW SW & 100t	72.16	
101.0000.11.542.64.47.005			3/21/2021	67044-020 03/21/21	PKST 02/17-03/17 59th AVE SW &	88.86	
101.0000.11.542.64.47.005			3/21/2021	67044-022 03/21/21	PKST 02/17-03/17 GLD SW & BPW	93.23	

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101.0000.11.542.64.47.005			3/21/2021	67044-024 03/21/21	PKST 02/17-03/17 GLD SW & Stei	66.55	
001.0000.11.576.80.47.005			3/21/2021	67044-034 03/21/21	PKFC 02/17-03/17 10506 Russell	41.36	
101.0000.11.542.63.47.006			3/21/2021	67044-039 03/21/21	PKST 02/17-03/17 5700 100th St	51.81	
101.0000.11.542.64.47.005			3/21/2021	67044-044 03/21/21	PKST 02/17-03/17 100th SW & LK	72.60	
101.0000.11.542.64.47.005			3/21/2021	67044-046 03/21/21	PKST 02/17-03/17 10013 GLD SW	193.05	
101.0000.11.542.64.47.005			3/21/2021	67044-047 03/21/21	PKST 02/17-03/17 59th Ave SW &	73.77	
001.0000.11.576.80.47.005			3/21/2021	67044-063 03/21/21	PKFC 02/17-03/17 6002 Fairlawn	125.46	
101.0000.11.542.64.47.005			3/21/2021	67044-064 03/21/21	PKST 02/17-03/17 93rd St SW &	62.17	
101.0000.11.542.64.47.005			3/21/2021	67044-082 03/21/21	PKST 02/17-03/17 GLD & Mt Tac	163.23	
101.0000.11.542.63.47.006			3/21/2021	67044-086 03/21/21	PKST 02/17-03/17 6119 Motor Av	65.91	
101.0000.11.542.63.47.005			3/21/2021	67044-088 03/21/21	PK 02/17-03/17 11950 47th ST S	51.00	
101.0000.11.542.64.47.005			3/28/2021	67044-028 03/28/21	PKST 02/24-03/24 Pac Hwy SW &	70.64	
401.0000.41.531.10.47.005			3/28/2021	67044-037 03/28/21	PWSW 02/24-03/24 Pac Hwy SW	43.78	
101.0000.11.542.64.47.005			3/28/2021	67044-038 03/28/21	PKST 02/24-03/24 BP Way & Pac	70.64	
001.0000.11.576.80.47.005			3/28/2021	67044-041 03/28/21	PKFC 02/24-03/24 4721 127th St	37.43	
101.0000.11.542.64.47.005			3/28/2021	67044-043 03/28/21	PKST 02/24-03/24 BPW SW & San	132.07	
101.0000.11.542.64.47.005			3/28/2021	67044-054 03/28/21	PKST 02/24-03/24 11417 Pac Hwy	72.44	
101.0000.11.542.64.47.005			3/28/2021	67044-055 03/28/21	PKST 02/24-03/24 11424 Pac Hwy	66.71	
101.0000.11.542.64.47.005			3/28/2021	67044-056 03/28/21	PKST 02/24-03/24 11517 Pac Hwy	72.07	
401.0000.41.531.10.47.005			3/28/2021	67044-057 03/28/21	PWSW 02/24-03/24 5118 Seattle	46.28	
101.0000.11.542.64.47.005			4/7/2021	67044-002 04/07/21	PKST 03/03-04/03 Pac Hwy & STW	78.86	
101.0000.11.542.64.47.005			4/7/2021	67044-012 04/07/21	PKST 03/03-04/03 Hwy 512 & STW	111.63	
101.0000.11.542.63.47.006			4/7/2021	67044-014 04/07/21	PKST 03/03-04/03 Hwy 512 & STW	91.54	
101.0000.11.542.64.47.005			4/7/2021	67044-016 04/07/21	PKST 03/03-04/03 40th Ave SW	63.23	
101.0000.11.542.64.47.005			4/7/2021	67044-031 04/07/21	PKST 03/03-04/03 84th St S & S	69.04	
101.0000.11.542.64.47.005			4/7/2021	67044-032 04/07/21	PKST 03/03-04/03 100th ST SW &	88.05	
001.0000.11.576.80.47.005			4/7/2021	67044-048 04/07/21	PKFC 03/03-04/03 2716 84th St	107.65	
101.0000.11.542.64.47.005			4/7/2021	67044-050 04/07/21	PKST03/03-04/03 LKWD Dr SW/Ste	80.56	
101.0000.11.542.64.47.005			4/7/2021	67044-053 04/07/21	PKST 03/03-04/03 4648 Steil Bl	59.75	
101.0000.11.543.50.47.005			4/7/2021	67044-074 04/07/21	PKST 03/03-04/03 9424 Front St	457.73	
001.0000.11.576.80.47.005			4/7/2021	67044-075 04/07/21	PKFC 03/03-04/03 8807 25th Ave	175.59	
101.0000.11.542.64.47.005			4/7/2021	67044-078 04/07/21	PKST 03/03-04/03 100th St SW	106.72	
101.0000.11.542.64.47.005			4/7/2021	67044-079 04/07/21	PKST 03/03-04/03 96th St S & S	230.89	
101.0000.11.542.64.47.005			4/7/2021	67044-080 04/07/21	PKST 03/03-04/03 8802 STW	80.29	
101.0000.11.542.64.47.005			4/7/2021	67044-081 04/07/21	PKST 03/03-04/03 3601 Steil Bl	75.48	
101.0000.11.542.63.47.006			4/7/2021	67044-083 04/07/21	PKST 03/03-04/03 40th & 100th	99.67	
101.0000.11.542.64.47.005			4/7/2021	67044-084 04/07/21	PKST 03/03-04/03 Steil & Lkvw	79.22	
101.0000.11.542.63.47.006			4/7/2021	67044-085 04/07/21	PKST 03/03-04/03 26th & 88th S	46.10	

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101.0000.11.542.63.47.006			4/7/2021	67044-087 04/07/21	PKST 3/03-04/03 123rd & BPW SW	77.70	
101.0000.11.542.63.47.006			4/7/2021	67044-089 04/07/21	PKST 03/03-04/03 9520 Front ST	50.56	
17238	4/15/2021	000288		LAKEWOOD HARDWARE & PAINT INC,			\$648.67
101.0000.11.544.90.31.001			4/5/2021	644906	PKST Trimmer Line, Grease, Sta	108.61	
101.0000.11.544.90.31.001			3/29/2021	644237	PKST Pulley Swiv, Quick Link	41.28	
101.0000.11.544.90.31.001			3/29/2021	644270	PKST Key	13.13	
001.0000.11.576.80.31.001			3/30/2021	644382	PKFC Floor Base	485.65	
17239	4/15/2021	000298		LAKEWOOD TOWING,			\$1,156.15
001.0000.15.521.10.41.070			3/30/2021	226943	PD 03/29	87.92	
001.0000.15.521.10.41.070			3/29/2021	226892	PD 03/28	87.92	
001.0000.15.521.10.41.070			3/29/2021	226897	PD 03/28	87.92	
001.0000.15.521.10.41.070			4/1/2021	217443	PD 07/28/20	87.92	
001.0000.15.521.10.41.070			4/1/2021	218927	PD 08/31	189.03	
001.0000.15.521.10.41.070			4/1/2021	219348	PD 09/09/20	87.92	
001.0000.15.521.10.41.070			4/1/2021	219367	PD 09/09/20	87.92	
001.0000.15.521.10.41.070			4/1/2021	219851	PD 09/21/20	87.92	
001.0000.15.521.10.41.070			4/5/2021	215001	PD 06/09/20	87.92	
001.0000.15.521.10.41.070			4/5/2021	218762	PD 08/27/20	87.92	
001.0000.15.521.10.41.070			4/5/2021	223158	PD 12/09/20	87.92	
001.0000.15.521.10.41.070			4/5/2021	224118	PD 01/07	87.92	
17240	4/15/2021	003008		LARSEN SIGN CO,			\$3,588.25
501.0000.51.548.79.48.005			4/12/2021	28548	PKFL Remove Vehicle Graphics	576.98	
104.0010.01.557.30.41.001			3/4/2021	28366	HM Signs	2,939.83	
001.0000.11.576.81.31.001			1/22/2021	28162	PKFC Panel Decals	71.44	
17241	4/15/2021	012986		LAW OFFICE CYNTHIA MACKLIN,			\$3,250.00
001.0000.02.512.51.41.035			3/25/2021	03/25/21	MC 12/17/20-03/04/21 Public De	3,250.00	
17242	4/15/2021	002296		LEXIS NEXIS,			\$683.58
503.0000.04.518.80.49.004			3/31/2021	3093199539	IT 03/21 LexisNexis	683.58	
17243	4/15/2021	009711		LEXIS NEXIS RISK DATA MGMT INC,			\$59.46
001.0000.15.521.10.41.001			3/31/2021	1226184-20210331	PD 03/21 Person Searches	46.82	
001.0000.15.521.10.41.001			2/28/2021	1226184-0210228	PD 02/21 Person Searches	12.64	
17244	4/15/2021	002185		LOWE'S COMPANIES INC,			\$1,517.03

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
502.0000.17.521.50.31.001			2/26/2021	924341	PKFC Supplies	12.50	
101.0000.11.544.90.31.001			3/4/2021	924229	PKFC Supplies	97.72	
101.0000.11.544.90.31.001			3/4/2021	924253	PKFC Supplies	8.31	
101.0000.11.544.90.31.001			3/1/2021	923108	PKFC Supplies	33.56	
101.0000.11.544.90.31.001			3/1/2021	923198	PKFC Supplies	15.17	
101.0000.11.544.90.31.001			3/1/2021	943872	PKFC Supplies	24.50	
001.0000.11.576.80.31.001			3/8/2021	923193	PKFC Supplies	37.55	
101.0000.11.542.64.31.001			3/8/2021	923903	PKFC Supplies	30.27	
101.0000.11.542.66.31.001			3/9/2021	924210	PKFC Supplies	5.67	
502.0000.17.521.50.31.001			3/18/2021	923449	PKFC Supplies	60.10	
502.0000.17.518.35.31.001			3/15/2021	923356	PKFC Supplies	8.19	
502.0000.17.518.35.31.001			3/15/2021	924994	PKFC supplies	97.02	
502.0000.17.518.35.31.001			3/16/2021	923681	PKFC Supplies	20.87	
001.0000.11.576.81.31.001			3/16/2021	923860	PKFC Supplies	52.05	
502.0000.17.518.30.31.001			1/2/2001	923748	PKFC Supplies	52.19	
101.0000.11.542.64.31.030			3/23/2021	923091	PKFC Supplies	192.24	
101.0000.11.544.90.31.001			3/23/2021	924060	PKFC Supplies	26.02	
101.0000.11.544.90.31.001			3/24/2021	923545	PKFC Supplies	590.99	
001.0000.11.576.81.31.001			3/24/2021	923771	PKFC Supplies	152.11	
17245	4/15/2021	011494		MARTIN, BRIAN			\$178.75
503.0006.04.518.80.41.001			4/2/2021	2108	IT 04/21 COL Speed Measuring D	178.75	
17246	4/15/2021	009130		MATVIYCHUK, IRENE			\$126.16
001.0000.02.512.51.49.009			3/29/2021	03/29/21	MC 03/29 Interpreter	126.16	
17247	4/15/2021	009018		METAL MAGIC NW, INC,			\$495.90
504.0000.09.518.35.48.001			3/24/2021	66464	PDFL Insurance Repairs	495.90	
17248	4/15/2021	009724		MILES RESOURCES LLC,			\$122.34
101.0000.11.542.30.31.030			4/5/2021	319745	PKST Cold Mix	122.34	
17249	4/15/2021	000364		NORTHWEST BUILDING LLC,			\$4,990.92
502.0000.17.521.50.48.009			3/26/2021	2020 Op Exp Recon	PKFC 2020 Operating Exp. Recon	1,189.92	
502.0000.17.521.50.48.009			4/1/2021	2Q/21	PKFC 2Q/21 Common Area Exp. Fo	3,801.00	
17250	4/15/2021	008092		NVL LABORATORIES INC,			\$1,920.40
301.0032.11.594.76.41.001			4/2/2021	2021-0194	Pk Asbestos/Lead Test 12616 47	1,920.40	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
17251	4/15/2021	009317		OPTIC FUSION INC,			\$1,524.28
503.0000.04.518.80.42.001			4/1/2021	95-19049	IT 04/21 Internet Connectivity	1,524.28	
17252	4/15/2021	012500		O'REILLY AUTO PARTS,			\$114.66
501.0000.51.548.79.31.006			3/23/2021	2863-223053	PKFL Bulbs, DEF	114.66	
17253	4/15/2021	009775		PETERSEN BROTHERS INC,			\$22,146.27
504.0000.09.518.38.48.001			12/19/2020	20038031	RM Claim# 2020-0088 37710 100t	3,861.69	
504.0000.09.518.38.48.001			12/19/2020	20038032	RM Claim# 2020-0090 9500 Front	4,884.47	
504.0000.09.518.38.48.001			12/19/2020	20038033	RM Claim# 2020-0091 Chapel St A	7,366.78	
504.0000.09.518.38.48.001			3/6/2021	2100501	RM Claim #2020-0104	6,033.33	
17254	4/15/2021	000407		PIERCE COUNTY,			\$74,596.83
101.0000.11.542.64.41.001			3/31/2021	CI-300804	PKST 02/21 Traffic Ops. Maint.	11,940.23	
401.0000.41.531.10.41.001			4/1/2021	CI-300618	PWSW SWM Svc Charge Billing	59,958.77	
105.0001.07.559.20.41.001			4/5/2021	CI-300692	AB/PWSC/PWSW 03/21 Recording F	1,134.00	
311.0000.01.535.30.41.001			4/5/2021	CI-300692	AB/PWSC/PWSW 03/21 Recording F	39.00	
401.0021.41.531.10.41.001			4/5/2021	CI-300692	AB/PWSC/PWSW 03/21 Recording F	39.00	
001.0000.02.237.10.00.002			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	1,485.83	
17255	4/15/2021	000421		PIERCE COUNTY BUDGET & FINANCE,			\$48,520.31
001.9999.13.594.58.61.007			4/13/2021	2200002221 ED 2021	ED 2021 Prop Tax 14702 Union A	718.09	
401.0000.41.531.10.44.005			4/13/2021	3085002370 PW 2021	PW 2021 Prop Tax Undetermined	10.56	
401.0000.41.531.10.44.005			4/13/2021	3980300020 PW 2021	PW 2021 Prop Tax XXX Butte Dr	10.56	
401.0000.41.531.10.44.005			4/13/2021	4000220210 PW 2021	PW 2021 Prop Tax XXX Lake Loui	10.57	
401.0000.41.531.10.44.005			4/13/2021	4002780210 PW 2021	PW 2021 Prop Tax Tracts	10.57	
401.0000.41.531.10.44.005			4/13/2021	4145240060 PW 2021	PW 2021 Prop Tax XXX 112th St	10.57	
401.0000.41.531.10.44.005			4/13/2021	4550300080 PW 2021	PW 2021 Prop Tax XXX Military	10.57	
401.0000.41.531.10.44.005			4/13/2021	4550320290 PW 2021	PW 2021 Prop Tax XXX 107th St	10.57	
401.0000.41.531.10.44.005			4/13/2021	4776500200 PW 2021	PW 2021 Prop Tax XXX 25th Av S	12.58	
401.0000.41.531.10.44.005			4/13/2021	5000490190 PW 2021	PW 2021 Prop Tax XXX 107th St	10.56	
401.0000.41.531.10.44.005			4/13/2021	5000490200 PW 2021	PW 2021 Prop Tax XXX 107th St	10.56	
101.0000.21.542.70.44.005			4/13/2021	5130000630 PW 2021	PW 2021 Prop Tax XXX Whitman A	10.57	
101.0000.21.542.70.44.005			4/13/2021	5130000800 PW 2021	PW 2021 Prop Tax XXX Fairlawn	10.57	
101.0000.21.542.70.44.005			4/13/2021	5130000820 PW 2021	PW 2021 Prop Tax XXX Fairlawn	10.57	
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401.0000.41.531.10.44.005			4/13/2021	5721500110 PW 2021	PW 2021 Prop Tax XXX 71st St C	10.57	
401.0000.41.531.10.44.005			4/13/2021	6190000030 PW 2021	PW 2021 Prop Tax XXX Woodbine	10.57	
401.0000.41.531.10.44.005			4/13/2021	6580000013 PW 2021	PW 2021 Prop Tax XXX 87th St S	10.57	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
401.0000.41.531.10.44.005			4/13/2021	7107800391 PW 2021	PW 2021 Prop Tax XXX Lake Loui	10.57	
401.0000.41.531.10.44.005			4/13/2021	7765300850 PW 2021	PW 2021 Prop Tax XXX Earley Av	10.56	
401.0000.41.531.10.44.005			4/13/2021	7765301270 PW 2021	PW 2021 Prop Tax XXX Earley Av	10.76	
401.0000.41.531.10.44.005			4/13/2021	8880900340 PW 2021	PW 2021 Prop Tax XXX 25th Av S	10.56	
101.0000.21.542.70.44.005			4/13/2021	0219011105 PW 2021	PW 2021 Prop Tax 3710 100th St	10.56	
101.0000.21.542.70.44.005			4/13/2021	0219011119 PW 2021	PW 2021 Prop Tax XXX S Tacoma	10.57	
401.0000.41.531.10.44.005			4/13/2021	0219043108 PW 2021	PW 2021 Prop Tax 112 SW County	10.57	
401.0000.41.531.10.44.005			4/13/2021	0219096016 PW 2021	PW 2021 Prop Tax XXX 112th St	10.84	
101.0000.21.542.70.44.005			4/13/2021	0219111008 PW 2021	PW 2021 Prop Tax XXX Bridgepor	10.57	
401.0000.41.531.10.44.005			4/13/2021	0219123082 PW 2021	PW 2021 Prop Tax 12502 47th Av	10.91	
311.0000.21.559.30.44.005			4/13/2021	0219226008 PW 2021	PW 2021 Prop Tax XXX Spring St	11.44	
401.0000.41.531.10.44.005			4/13/2021	0220351036 PW 2021	PW 2021 Prop Tax XXX Woodlawn	13.31	
401.0000.41.531.10.44.005			4/13/2021	0220351041 PW 2021	PW 2021 Prop Tax XXX 79th St W	10.73	
401.0000.41.531.10.44.005			4/13/2021	0220352183 PW 2021	PW 2021 Prop Tax XXX 59th Av S	10.56	
401.0000.41.531.10.44.005			4/13/2021	0220355023 PW 2021	PW 2021 Prop Tax 5427 Steilaco	11.78	
401.0000.41.531.10.44.005			4/13/2021	0220355028 PW 2021	PW 2021 Prop Tax XXX Lakewood	10.90	
401.0000.41.531.10.44.005			4/13/2021	0220356007 PW 2021	PW 2021 Prop Tax XXX Bridgepor	10.56	
401.0000.41.531.10.44.005			4/13/2021	0220356008 PW 2021	PW 2021 Prop Tax XXX Bridgepor	10.56	
401.0000.41.531.10.44.005			4/13/2021	0220356009 PW 2021	PW 2021 Prop Tax XXX Bridgepor	10.56	
401.0000.41.531.10.44.005			4/13/2021	0220356010 PW 2021	PW 2021 Prop Tax XXX Bridgepor	10.56	
401.0000.41.531.10.44.005			4/13/2021	0220356011 PW 2021	PW 2021 Prop Tax XXX Bridgepor	10.56	
401.0000.41.531.10.44.005			4/13/2021	0220356012 PW 2021	PW 2021 Prop Tax XXX 59th Av S	10.56	
101.0000.21.542.70.44.005			4/13/2021	0320314090 PW 2021	PW 2021 Prop Tax 9420 Front St	560.72	
401.0000.41.531.10.44.005			4/13/2021	0320314090 PW 2021	PW 2021 Prop Tax 9420 Front St	560.72	
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001.0000.11.576.80.44.005			2/14/2021	0219042008 PK 2021	PK 2021 Prop Tax Undetermined	13.03	
001.0000.11.576.80.44.005			2/14/2021	0219123015 PK 2021	PK 2021 Prop Tax 4713 127th St	10.78	
001.0000.11.576.80.44.005			2/14/2021	0219123016 PK 2021	PK 2021 Prop Tax 4709 127th St	10.56	
001.0000.11.576.80.44.005			2/14/2021	0219123031 PK 2021	PK 2021 Prop Tax 12612 47th Av	10.56	
001.0000.11.576.80.44.005			2/14/2021	0219123032 PK 2021	PK 2021 Prop Tax 12616 47t Ave	133.77	
001.0000.11.576.80.44.005			2/14/2021	0219123038 PK 2021	PK 2021 Prop Tax 12601 Addison	132.55	
001.0000.11.576.80.44.005			2/14/2021	0219123106 PK 2021	PK 2021 Prop Tax 4723 127th St	10.76	
001.0000.11.576.80.44.005			2/14/2021	0219123107 PK 2021	PK 2021 Prop Tax 12601 Addison	11.07	
001.0000.11.576.80.44.005			2/14/2021	0219162008 PK 2021	PK 2021 Prop Tax 9222 Veterans	2,565.07	
001.0000.11.576.80.44.005			2/14/2021	0219164006 PK 2021	PK 2021 Prop Tax 8928 N Thorne	10.57	
001.0000.11.576.80.44.005			2/14/2021	0219164053 PK 2021	PK 2021 Prop Tax 8928 N Thorne	10.75	
001.0000.11.576.80.44.005			2/14/2021	0219164080 PK 2021	PK 2021 Prop Tax 8928 N Thorne	11.01	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
001.0000.11.576.80.44.005			2/14/2021	0219212131 PK 2021	PK 2021 Prop Tax XXX Silcox Dr	10.56	
001.0000.11.576.80.44.005			2/14/2021	0220321023 PK 2021	PK 2021 Prop Tax 8200 87th Ave	20,029.43	
001.0000.11.576.80.44.005			2/14/2021	0320311005 PK 2021	PK 2021 Prop Tax 2716 84th St	11.37	
001.0000.11.576.80.44.005			2/14/2021	0320311006 PK 2021	PK 2021 Prop Tax 2716 84th St	11.33	
001.0000.11.576.80.44.005			2/14/2021	0320311008 PK 2021	PK 2021 Prop Tax 2716 84th St	10.57	
001.0000.11.576.80.44.005			2/14/2021	0320311010 PK 2021	PK 2021 Prop Tax 2716 84th St	10.73	
001.0000.11.576.80.44.005			2/14/2021	0320311012 PK 2021	PK 2021 Prop Tax 2716 84th St	11.07	
001.0000.11.576.80.44.005			2/14/2021	0320311014 PK 2021	PK 2021 Prop Tax 2716 84th St	10.92	
001.0000.11.576.80.44.005			2/14/2021	0320311029 PK 2021	PK 2021 Prop Tax 2716 84th St	11.17	
001.0000.11.576.80.44.005			2/14/2021	0320311035 PK 2021	PK 2021 Prop Tax 2716 84th St	10.57	
001.0000.11.576.80.44.005			2/14/2021	0320314042 PK 2021	PK 2021 Prop Tax 8807 25th Ave	133.77	
001.0000.11.576.80.44.005			2/14/2021	0320314043 PK 2021	PK 2021 Prop Tax 8807 25th Ave	11.11	
001.0000.11.576.80.44.005			2/14/2021	0320315005 PK 2021	PK 2021 Prop Tax XXX 84th St C	10.57	
001.0000.11.576.80.44.005			2/14/2021	0320315006 PK 2021	PK 2021 Prop Tax 2511 88th St	10.57	
001.0000.11.576.80.44.005			2/14/2021	2200000021 PK 2021	PK 2021 Prop Tax 8928 North Th	4,101.20	
001.0000.11.576.80.44.005			2/14/2021	2200000023 PK 2021	PK 2021 Prop Tax 8928 North Th	10.57	
001.0000.11.576.80.44.005			2/14/2021	2335201250 PK 2021	PK 2021 Prop Tax 10506 Russell	11.80	
001.0000.11.576.80.44.005			2/14/2021	3085002360 PK 2021	PK 2021 Prop Tax 9102 Edgewater	10,812.57	
001.0000.11.576.80.44.005			2/14/2021	3935000350 PK 2021	PK 2021 Prop Tax 11528 Militar	11.98	
001.0000.11.576.80.44.005			2/14/2021	4000680320 PK 2021	PK 2021 Prop Tax 9701 Onyx Dr	10.57	
001.0000.11.576.80.44.005			2/14/2021	4000680330 PK 2021	PK 2021 Prop Tax 9701 Onyx Dr	10.57	
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001.0000.11.576.80.44.005			2/14/2021	4000680350 PK 2021	PK 2021 Prop Tax 9701 Onyx Dr	10.57	
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502.0000.17.518.35.44.005			2/14/2021	4001880094 PK 2021	PKFC 2021 Prop Tax 6006 Main S	357.17	
502.0000.17.518.35.44.005			2/14/2021	4001880100 PK 2021	PKFC 2021 Prop Tax 6000 Main S	5,968.14	
502.0000.17.518.35.44.005			2/14/2021	4002220020 PK 2021	PKFC 2021 Prop Tax 9401 Lakewo	1,603.16	
001.0000.11.576.80.44.005			2/14/2021	4145200521 PK 2021	PKFC 2021 Prop Tax 112th St SW	10.78	
001.0000.11.576.80.44.005			2/14/2021	5130001331 PK 2021	PKFC 2021 Prop Tax 6002 Fairla	11.05	
001.0000.11.576.80.44.005			2/14/2021	5900000012 PK 2021	PKFC 2021 Prop Tax Undetermine	11.82	
001.0000.11.576.80.44.005			2/14/2021	5900000013 PK 2021	PKFC 2021 Prop Tax Undetermine	12.51	
001.0000.11.576.80.44.005			2/14/2021	6385000181 PK 2021	PKFC 2021 Prop Tax 9222 Vetera	10.95	
001.0000.11.576.80.44.005			2/14/2021	6385000200 PK 2021	PKFC 2021 Prop Tax 9222 Vetera	10.83	
001.0000.11.576.80.44.005			2/14/2021	7025000161 PK 2021	PKFC 2021 Prop Tax 10 Barlow S	10.57	
17256	4/15/2021	000428		PIERCE COUNTY SEWER,			\$584.25
001.0000.11.576.80.47.004			3/28/2021	01583646 03/28/21	PKFC 01/01-02/28 8807 25th Ave	121.65	
001.0000.11.576.80.47.004			4/1/2021	00936570 04/01/21	PKFC 03/21 6002 Fairlawn DR SW	23.28	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
001.0000.11.576.80.47.004			4/1/2021	01032275 04/01/21	PKFC 03/21 8421 Pine St S	18.21	
502.0000.17.521.50.47.004			4/1/2021	01360914 04/01/21	PKFC 03/21 9401 Lkwd Dr SW	99.28	
001.0000.11.576.81.47.004			4/1/2021	01431285 04/01/21	PKFC 03/21 8200 87th Ave SW	94.22	
101.0000.11.543.50.47.004			4/1/2021	01552201 04/01/21	PKST 03/21 9420 Front St S	52.00	
001.0000.11.576.81.47.004			4/1/2021	02020548 04/01/21	PKFC 03/21 9115 Angle LN SW Sh	38.47	
001.0000.11.576.81.47.001			4/1/2021	02067277 04/01/21	PKFC 03/21 9251 Angle LN SW	73.32	
001.0000.11.576.80.47.004			4/1/2021	00162489 04/01/21	PKFC 03/21 9222 Veterans Dr SW	63.82	
17257	4/15/2021	012320		PIX4D INC,			\$499.00
001.0000.15.521.40.49.001			4/13/2021	202104-I-D-US-003010	PD Pix4Dmapper Desktop (1 Devi	499.00	
17258	4/15/2021	010630		PRINT NW,			\$118.66
101.0000.21.544.20.31.001			4/5/2021	31234001	PWST Business Cards: T. Hill	37.40	
001.0000.07.558.50.31.001			4/5/2021	31234001	CD Business Cards: M. Hobart	37.40	
001.0000.07.558.50.31.001			4/9/2021	31267701	CD Business Cards: R Gindy	43.86	
17259	4/15/2021	009928		PROFAST SUPPLY LLC,			\$1,648.50
501.0000.51.548.79.31.006			3/26/2021	31110	PKFL Bulk Containers	1,648.50	
17260	4/15/2021	009761		PUGET SOUND SPECIALTIES INC,			\$12,026.36
001.0000.11.576.81.31.001			3/30/2021	27169	PKFC Fertilizer & Blend Seed	12,026.36	
17261	4/15/2021	012953		R. L. ALIA COMPANY,			\$497,329.79
302.0015.21.595.30.63.001			3/31/2021	AG 2021-013 PP # 1	PWCP AG 2021-013 03/15-03/31	160,462.48	
302.0000.00.223.40.00.000			3/31/2021	AG 2021-013 PP # 1	PWCP AG 2021-013 Retainage	-8,023.12	
302.0119.21.595.30.63.001			3/31/2021	AG 2021-014 PP # 1	PWCP AG 2021-014 02/22-03/31 L	363,042.56	
302.0000.00.223.40.00.000			3/31/2021	AG 2021-014 PP # 1	PWCP AG 2021-014 Retainage	-18,152.13	
17262	4/15/2021	007505		REDFLEX TRAFFIC SYSTEMS INC,			\$32,240.00
001.0000.15.521.71.41.080			3/31/2021	INVI-3390	PD 03/21 Photo Enforcement	32,240.00	
17263	4/15/2021	010740		RFI ENTERPRISES INC,			\$684.66
101.0000.11.544.90.41.001			4/5/2021	613874	PKST 05/01-07/31 Intrusion Mon	110.98	
101.0000.11.544.90.41.001			3/29/2021	613365	PKST Troubleshoot Panel 9420 F	573.68	
17264	4/15/2021	004775		SAN DIEGO POLICE EQUIP CO,			\$10,843.40
001.0000.15.521.26.31.020			4/1/2021	646863	PD Federal .223 55GR FMJ-BT	5,778.36	
001.0000.15.521.26.31.020			4/1/2021	646863	Sales Tax	572.05	
001.0000.15.521.26.31.020			1/7/2021	645674	PD Speer Lawman 9mm 115GR TMJ	3,201.78	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
001.0000.15.521.26.31.020			1/7/2021	645674	Sales Tax	316.98	
001.0000.15.521.10.31.020			1/22/2021	645830	PD Force On Force 5.56MM Non-M	886.47	
001.0000.15.521.10.31.020			1/22/2021	645830	Sales Tax	87.76	
17265	4/15/2021	002912		SOUND ELECTRONICS,			\$428.61
502.0000.17.542.65.48.001			3/31/2021	511228	PKFC Elevator Stuck In Recall	428.61	
17266	4/15/2021	000066		SOUND UNIFORM SOLUTIONS,			\$1,064.54
001.0000.15.521.30.31.008			3/31/2021	202103SU239	PD Jumpsuit, Badge: K. Devaney	532.27	
001.0000.15.521.22.31.008			4/15/2021	202103SU111	PD Jumpsuit, Badge For: Fraser	532.27	
17267	4/15/2021	010656		SOUTH SOUND 911,			\$156,477.49
001.0000.15.521.10.41.126			4/1/2021	00303	PD 04/21 Communication Svcs	115,910.00	
001.0000.15.521.10.41.126			4/1/2021	00303	PD 04/21 RMS Svcs	24,895.83	
001.0000.15.521.10.41.126			4/1/2021	00303	PD 04/21 Records/Permitting Sv	8,508.33	
001.0000.15.521.10.41.126			4/1/2021	00303	PD 04/21 Warrant Services	7,163.33	
17268	4/15/2021	002881		SPRAGUE PEST SOLUTIONS CO,			\$122.51
502.0000.17.518.35.41.001			4/5/2021	4481420	PKFC 04/05 Pest Control CH	64.81	
502.0000.17.542.65.48.001			4/6/2021	4487843	PKFC 04/06 Pest Control Transi	57.70	
17269	4/15/2021	009493		STAPLES ADVANTAGE,			\$838.72
001.0000.02.512.50.31.001			4/3/2021	3473900733	MC Shredder Oil	22.52	
001.0000.02.512.50.31.001			4/2/2021	3473644068	MC Static Eliminator	13.61	
001.0000.02.512.50.31.001			4/2/2021	3473644069	MC Plaques	127.20	
001.0000.02.512.50.31.001			3/10/2021	3471787026	MC Office Supplies	207.92	
001.0000.02.512.50.31.001			3/10/2021	3471787027	MC Office Supplies	292.42	
001.0000.15.521.10.31.001			3/17/2021	3472268948	PD Office Supplies	175.05	
17270	4/15/2021	009030		STERICYCLE INC,			\$10.36
001.0000.15.521.10.41.001			3/31/2021	3005502847	PD 03/21 On Call Svcs	10.36	
17271	4/15/2021	011544		STOWE DEV AND STRATEGIES,			\$650.00
001.9999.13.558.70.41.001			4/4/2021	51	ED AG 2016-181 03/21 Consultin	650.00	
17272	4/15/2021	006497		SYSTEMS FOR PUBLIC SAFETY,			\$8,677.07
501.0000.51.521.10.48.005			3/31/2021	39431	PDFL Car Maint	630.16	
501.0000.51.521.10.48.005			3/31/2021	39500	PDFL Other	512.73	
501.0000.51.521.10.48.005			3/31/2021	39500	PDFL Electrical	83.80	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
501.0000.51.521.10.48.005			3/31/2021	39534	PDFL Other	1,067.50	
501.0000.51.521.10.48.005			3/29/2021	39351	PDFL Other	1,129.51	
501.0000.51.521.10.48.005			3/29/2021	39436	PDFL Other	1,190.23	
501.0000.51.521.10.48.005			3/29/2021	39442	PDFL Other	667.65	
501.0000.51.521.10.48.005			3/29/2021	39442	PDFL Maint	63.65	
501.0000.51.521.10.48.005			3/29/2021	39472	PDFL Tire	866.64	
501.0000.51.521.10.48.005			3/29/2021	39506	PDFL Battery	98.09	
501.0000.51.521.10.48.005			3/29/2021	39531	PDFL Battery	745.84	
501.0000.51.521.10.48.005			3/29/2021	39531	PDFL Alternator	534.06	
501.0000.51.521.10.48.005			3/29/2021	39540	PDFL Other	24.52	
501.0000.51.521.10.48.005			3/29/2021	39560	PDFL Other	220.69	
501.0000.51.521.10.48.005			3/29/2021	39563	PDFL Wipers	151.85	
501.0000.51.521.10.48.005			3/29/2021	39566	PDFL Other	59.05	
501.0000.51.521.10.48.005			3/29/2021	39567	PDFL Tires	98.09	
501.0000.51.521.10.48.005			3/29/2021	39569	PDFL Diagnostics	49.05	
501.0000.51.521.10.48.005			3/29/2021	39570	PDFL Tire Repair	73.57	
501.0000.51.521.10.48.005			4/2/2021	39572	PDFL Electrical	151.33	
501.0000.51.521.10.48.005			4/2/2021	39576	PDFL Tire Repair	49.05	
501.0000.51.521.10.48.005			4/2/2021	39579	PDFL Tire	210.01	
17273	4/15/2021	000540		TACOMA RUBBER STAMP,			\$18.84
101.0000.21.544.20.31.001			4/2/2021	I-673153-1	PWST Name Plate: T Hill	18.84	
17274	4/15/2021	012922		TIMBER COAST CONSTRUCTION,			\$16,291.71
190.4999.52.559.32.41.001			4/14/2021	2231 Payment # 2	CDBG MHR-177 Russell	20,364.63	
190.0000.00.223.40.00.000			4/14/2021	2231 Payment # 2	CDBG MHR-177 Retainage	-4,072.92	
17275	4/15/2021	011708		TOTAL FILTRATION SERVICES INC,			\$6,163.37
502.0000.17.521.50.31.001			3/18/2021	PSC2129078	PKFC Filters	6,163.37	
17276	4/15/2021	008186		TRCVB,			\$7,557.79
104.0016.01.557.30.41.001			3/31/2021	LW-2021-03	HM AG 2021-087 03/21 Lodging T	7,557.79	
17277	4/15/2021	007885		ULINE, INC,			\$40.09
001.0000.15.521.80.31.001			3/23/2021	131666224	PD Envelopes	40.09	
17278	4/15/2021	009372		VENTEK INTERNATIONAL,			\$90.00
503.0000.04.518.80.42.001			4/1/2021	126158	IT 04/21 CCU Server Hosting, D	90.00	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
17279	4/15/2021	012914		VERIZON COMMUNICATIONS INC,			\$229.09
503.0000.04.518.80.42.001			4/1/2021	334000014023	IT 03/21 GPS	229.09	
17280	4/15/2021	012984		WALL, NOEL			\$2,200.00
190.2003.53.559.32.41.001			4/13/2021	AG 2021-064	CDBG AG 2021-064 Rent Assist:	2,200.00	
17281	4/15/2021	000593		WASHINGTON STATE TREASURER,			\$101,177.69
001.0000.02.237.10.00.002			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	44,229.39	
001.0000.02.237.10.00.001			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	26,329.33	
001.0000.02.237.30.00.000			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	147.97	
001.0000.02.386.89.15.001			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	47.16	
001.0000.02.237.10.00.007			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	3,227.33	
001.0000.02.386.82.00.000			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	17.58	
001.0000.02.386.89.16.001			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	260.56	
001.0000.02.386.89.14.001			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	191.03	
001.0000.02.237.10.00.008			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	6,484.18	
001.0000.02.237.10.00.009			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	2,375.35	
001.0000.02.386.89.12.001			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	99.99	
001.0000.02.386.89.13.001			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	99.99	
001.0000.02.237.10.00.003			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	16,569.46	
001.0000.02.237.10.00.006			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	1,120.86	
001.0000.00.237.25.00.000				03/21 Court Remit	MC 03/21 Court Remit	-22.49	
17282	4/15/2021	009941		WU, THOMAS T			\$172.53
001.0000.02.512.51.49.009			4/2/2021	04/02/21	MC 04/02 Interpreter	172.53	
17283	4/15/2021	008553		ZONES INC,			\$3,653.19
503.0000.04.518.80.48.003			3/31/2021	K16669870101	IT 3yr HDM 17 Enterprise Techn	3,653.19	
94391	3/31/2021	006465		AGRI SHOP,			\$134.96
001.0000.11.576.80.48.001			3/25/2021	240664/2	PK Maint Mower	134.96	
94392	3/31/2021	002293		AHBL INC,			\$8,850.00
001.9999.07.558.65.41.001			2/28/2021	124245	CD 01/26-02/25 Lkwd Energy & C	8,850.00	
94393	3/31/2021	012973		ALLEN REALTORS,			\$1,520.00
190.2003.53.559.32.41.001			3/25/2021	AG 2021-079	CDBG AG 2021-079 Rent Assist:	1,520.00	
94394	3/31/2021	000037		ASPHALT PATCH SYSTEMS INC,			\$9,738.13

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
302.0000.00.223.40.00.000			3/31/2021	AG 2020-101 Ret Rel	PWCP AG 2020-101 Retainage Rel	9,738.13	
94395	3/31/2021	008307		AT&T MOBILITY,			\$16,189.94
180.0000.15.521.21.42.001			2/19/2021	287293165778 02/21	IT/PD Thru 02/19 Phone	400.20	
503.0000.04.518.80.42.001			2/19/2021	287293165778 02/21	IT/PD Thru 02/19 Phone	11,251.08	
503.0000.04.518.80.42.001			2/19/2021	287296255265 02/21	IT Thru 02/19 Phone	4,538.66	
94396	3/31/2021	012902		BUELL RECREATION,			\$70,344.79
301.0025.11.594.76.63.001			3/26/2021	210048	PK AG 2021-028 Ft. Steilacoom	57,508.00	
301.0025.11.594.76.63.001			3/26/2021	210048	freight	6,500.00	
301.0025.11.594.76.63.001			3/26/2021	210048	Sales Tax	6,336.79	
94397	3/31/2021	005965		BUILDERS EXCHANGE OF,			\$264.30
302.0080.21.595.12.44.001			3/8/2021	1068393	PWCP Publish Projects Online	3.05	
302.0077.21.595.12.44.001			3/8/2021	1068393	PWCP Publish Projects Online	261.25	
94398	3/31/2021	012480		CABRAL, NICOLE			\$200.00
195.0021.02.512.53.41.001			3/8/2021	02/21-03/21	MC Feb & Mar BJA Grant VC-BX-0	200.00	
94399	3/31/2021	011030		CASCADE PRINT MEDIA INC,			\$610.97
001.0000.99.518.40.31.001			3/15/2021	2160039	ND #10 Window Envelopes	610.97	
94400	3/31/2021	009191		CITY OF DUPONT,			\$2,358.82
001.0000.02.229.10.00.003			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	2,358.82	
94401	3/31/2021	006613		CITY OF UNIVERSITY PLACE,			\$4,764.39
001.0000.02.229.10.00.001			3/22/2021	02/21 Court Remit	MC 02/21 Court Remit	4,764.39	
94402	3/31/2021	003948		COMCAST CORPORATION,			\$218.25
503.0000.04.518.80.42.001			3/15/2021	8498 35 011 2205662	IT 03/25-04/24 9420 Front St S	218.25	
94403	3/31/2021	008105		DEPARTMENT OF TRANSPORTATION,			\$1,097.15
101.0000.11.544.90.41.001			3/15/2021	RE-313-ATB10315010	PKST/PKSW 02/21 Traffic Mgmt.	731.43	
401.0000.11.531.10.41.001			3/15/2021	RE-313-ATB10315010	PKST/PKSW 02/21 Traffic Mgmt.	365.72	
94404	3/31/2021	004614		DLT SOLUTIONS LLC,			\$12,893.03
503.0000.04.518.80.48.003			2/24/2021	SI510758	IT - 02/22/21-02/21/22 Autocad	11,731.60	
503.0000.04.518.80.48.003			2/24/2021	SI510758	Sales Tax	1,161.43	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94405	3/31/2021	012972		DOUBLE Z INC,			\$1,909.00
190.2003.53.559.32.41.001		3/25/2021		AG 2021-084	CDBG AG 2021-084 Rent Assist:	1,909.00	
94406	3/31/2021	004710		EQUIFAX CREDIT NORTHWEST CORP,			\$109.73
001.0000.15.521.10.41.001		3/17/2021		6268700	MC 03/21	109.73	
94407	3/31/2021	000166		FEDERAL EXPRESS,			\$127.91
001.0000.99.518.40.42.002		3/26/2021		7-319-37464	ND 03/23 Shipping	127.91	
94408	3/31/2021	002662		GENE'S TOWING INC,			\$76.93
001.0000.15.521.10.41.070		3/13/2021		489689	PD 03/13	76.93	
94409	3/31/2021	000196		GOV'T FINANCE OFFICERS ASSOC,			\$612.00
001.0000.04.514.20.31.004		6/24/2020		2968766	FN GAAFR 2020 Edition	477.00	
001.0000.04.514.20.49.003		10/1/2020		2982148	FN Annual Gov't GAAP Update: K	135.00	
94410	3/31/2021	012977		HUNTERS GLEN APARTMENTS,			\$1,000.00
190.2003.53.559.32.41.001		3/29/2021		AG 2021-060	CDBG AG 2021-060 Rent Assist:	1,000.00	
94411	3/31/2021	011961		KELLEY CONNECT,			\$3,254.41
503.0000.04.518.80.45.002		2/26/2021		IN800500	IT 02/21 Copier	98.19	
503.0000.04.518.80.45.002		2/26/2021		IN800500	IT 02/21 Copier	13.15	
503.0000.04.518.80.45.002		2/26/2021		IN800500	IT 02/21 Copier	71.21	
503.0000.04.518.80.45.002		2/26/2021		IN800500	IT 02/21 Copier	18.75	
503.0000.04.518.80.45.002		2/26/2021		IN800500	IT 02/21 Copier	144.42	
503.0000.04.518.80.45.002		2/26/2021		IN800500	IT 02/21 Copier	22.14	
503.0000.04.518.80.45.002		2/26/2021		IN800500	IT 02/21 Copier	381.82	
503.0000.04.518.80.45.002		2/26/2021		IN800500	IT 02/21 Copier	7.77	
503.0000.04.518.80.45.002		2/26/2021		IN800500	IT 02/21 Copier	848.46	
503.0000.04.518.80.48.003		3/11/2021		IN808378	IT 1 Yr Maint/Support PaperCut	1,648.50	
94412	3/31/2021	008414		LAKEWOOD FORD,			\$144.74
501.0000.51.548.79.48.005		12/20/2020		LCCS472183	PKFL Vehicle Maint	42.58	
501.0000.51.548.79.32.001		12/20/2020		LCCS472183	PKFL Vehicle Maint	21.76	
501.0000.51.548.79.31.006		1/4/2021		LCW104466F	PKFL Vehcile Supplies	80.40	
94413	3/31/2021	000300		LAKEWOOD WATER DISTRICT,			\$5,201.00
301.0003.11.594.76.63.001		3/18/2021		03/23/21 Permit	PKFC Permit:8928 N Thorne LN S	5,121.00	
101.0000.11.542.70.47.001		3/16/2021		20229.02 03/16/21	PKST 01/23-03/19 11201 Old Mil	40.00	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
001.0000.11.576.81.47.001			3/23/2021	27555.01 03/23/21	PKFC 01/30-03/26 0 Steil Blvd	40.00	
94414	3/31/2021	009262		LAW OFFICES OF BARBARA BOWDEN,			\$250.00
001.0000.02.512.51.41.035			3/15/2021	026882	MC 01/29 Conflict Public Defen	250.00	
94415	3/31/2021	005685		LEMAY MOBILE SHREDDING,			\$50.00
001.0000.99.518.40.41.001			3/1/2021	4693307	ND 02/21 3rd Floor CH Shreddin	50.00	
94416	3/31/2021	007032		MICRO TEL,			\$1,150.00
503.0000.04.518.80.42.001			3/2/2021	21-0620783	IT 06/21/21-06/22/22 Microcall	1,150.00	
94417	3/31/2021	002474		NATIONAL BARRICADE CO LLC,			\$460.43
501.0000.51.548.79.48.005			3/15/2021	290663	PKFL Repair Wanco Diesel Arrow	137.63	
501.0000.51.548.79.48.005			3/15/2021	290663	PKFL Toggle Switch For Repair	14.52	
501.0000.51.548.79.48.005			3/15/2021	290663	PKFL Controller For Repair Of	264.24	
501.0000.51.548.79.48.005			3/15/2021	290663	PKFL Freight	44.04	
94418	3/31/2021	010743		NISQUALLY INDIAN TRIBE,			\$35.14
001.0000.15.521.10.41.125			2/28/2021	25914	PD 02/21 Pharmacy Reimb	35.14	
94419	3/31/2021	002421		NORTHWEST PLAYGROUND EQUIP,			\$5,343.68
301.0016.11.594.76.63.001			3/10/2021	47779	PK Wards LK Slide Replacement	1,457.10	
301.0016.11.594.76.63.001			3/10/2021	47779	PK Wards LK Slide Replacement	810.00	
301.0016.11.594.76.63.001			3/10/2021	47779	PK Wards LK Slide Replacement	1,215.00	
301.0016.11.594.76.63.001			3/10/2021	47779	PK Wards LK Slide Replacement	405.00	
301.0016.11.594.76.63.001			3/10/2021	47779	PK Wards LK Slide Replacement	202.50	
301.0016.11.594.76.63.001			3/10/2021	47779	PK Wards LK Slide Replacement~	164.70	
301.0016.11.594.76.63.001			3/10/2021	47779	freight	608.01	
301.0016.11.594.76.63.001			3/10/2021	47779	Sales Tax	481.37	
94420	3/31/2021	010633		P & C ROOFING INC,			\$18,204.94
190.4999.52.559.32.41.001			3/29/2021	03/29/21 MHR-180	CDBG MHR-180 Fennell	18,204.94	
94421	3/31/2021	012946		PARK CHASE ASSOC. LP,			\$4,440.92
190.2003.53.559.32.41.001			3/29/2021	AG 2021-056	CDBG AG 2021-056 Rent Assist:	3,111.28	
190.2003.53.559.32.41.001			3/25/2021	AG 2021-085	CDBG AG 2021-085 Rent Assist:	1,329.64	
94422	3/31/2021	000405		PCCFOA,			\$15.00
101.0000.11.544.90.49.001			3/22/2021	2021 Aguon Dues	PKST 2021 PCCFOA Dues: Aguon	15.00	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94423	3/31/2021	006117		PETTY CASH,			\$331.00
001.0000.02.512.51.49.008		3/25/2021	03/21 Jury	MC 03/21 Replenish Jury Petty	331.00		
94424	3/31/2021	006117		PETTY CASH,			\$85.20
190.0005.52.559.31.41.001		3/29/2021	Thru 03/04/21 VH	CDBG Larkin: Office Supplies	46.73		
001.0000.02.512.50.31.001		3/29/2021	Thru 03/04/21 VH	MC Mansfield: Office Supplies	16.47		
401.0000.41.531.10.31.001		3/29/2021	Thru 03/04/21 VH	PWSW Devereaux: Stamps For Sew	22.00		
94425	3/31/2021	012978		POLLARD, CURT			\$4,050.00
105.0001.07.559.20.41.001		3/30/2021	03/31/21 Relocate	AB 7116 146th St SW Relocation	4,050.00		
94426	3/31/2021	011511		POST INDUSTRIAL PRESS,			\$2,056.23
104.0010.01.557.30.31.001		3/30/2021	21000	HM FM T Shirts	2,056.23		
94427	3/31/2021	011112		PROVIDENCE ST PETER HOSPITAL,			\$1,085.00
001.0000.15.521.10.41.125		1/1/2021	33000201144401	PD 12/15/20 Med Svcs For Priso	1,085.00		
94428	3/31/2021	000445		PUGET SOUND ENERGY,			\$26,217.17
502.0000.17.521.50.47.011		3/18/2021	200008745289 3/18/21	PKFC 02/16-03/17 9401 Lkwd Dr	219.41		
001.0000.11.576.81.47.005		3/22/2021	200001527551 3/22/21	PKFC 02/18-03/19 9115 Angle Ln	148.22		
502.0000.17.518.35.47.011		3/19/2021	200018357661 3/19/21	PKFC 02/17-03/18 6000 Main St	849.40		
001.0000.11.576.80.47.005		3/25/2021	220002793168 3/25/21	PKFC 02/23-03/24 8807 25th Ave	130.96		
101.0000.11.542.64.47.005		3/22/2021	300000005037 3/22/21	PKST 01/29-03/101 Gravelly Lk &	356.54		
101.0000.11.542.63.47.006		3/22/2021	300000007165 3/22/21	PKST 02/02-03/02 N of Lk WA Bl	23,565.57		
001.0000.11.576.81.47.005		3/22/2021	300000010896 3/22/21	PKFC 02/18-03/19 Ft Steil Park	363.72		
001.0000.11.576.81.47.005		3/22/2021	300000010938 3/22/21	PKFC 02/18-03/19 8802 Dresden	316.88		
001.0000.11.576.81.47.005		3/24/2021	200001527346 3/24/21	PKFC 02/22-03/23 8714 87th Ave	10.93		
001.0000.11.576.81.47.005		3/24/2021	220017468871 3/24/21	PKFC 02/22-03/23 9107 Angle La	192.69		
001.0000.11.576.81.47.005		3/24/2021	220024933081 3/24/21	PKFC 02/22-03/23 8714 87th Ave	62.85		
94429	3/31/2021	008849		Q & A POLYGRAPH SVCS,			\$600.00
001.0000.15.521.40.41.001		3/5/2021	21-001	PD 03/04 Pre-Emp Polygraph: L.	300.00		
001.0000.15.521.40.41.001		3/14/2021	21-002	PD 03/15 Polygraph: Babcock	300.00		
94430	3/31/2021	005342		RAINIER LIGHTING & ELECTRICAL,			\$354.99
502.0016.17.594.18.63.001		3/18/2021	541022-1	PKFC Red Wing Nut, Hex Key Set	70.79		
502.0000.17.542.65.31.001		3/25/2021	540272-1	PKFC Lights	284.20		

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94431	3/31/2021	012960		RAINIER RENTALS,			\$1,250.00
190.2003.53.559.32.41.001		3/29/2021		AG 2021-057	CDBG AG 2021-057 Rent Assist:	1,250.00	
94432	3/31/2021	012825		READY SET TOW LLC,			\$87.92
001.0000.15.521.10.41.070		3/22/2021		21-07815	PD 03/22	87.92	
94433	3/31/2021	010478		RICOH USA INC,			\$375.85
503.0000.04.518.80.45.002		3/24/2021		104799544	IT 03/18-04/17 Copier	375.85	
94434	3/31/2021	010447		SPECIAL SERVICES GROUP LLC,			\$2,241.96
180.0000.15.521.21.35.001		3/9/2021		15070	PD - Upgrade of active Stelath	995.00	
180.0000.15.521.21.35.001		3/9/2021		15070	PD - Upgrade of active Stelat	995.00	
180.0000.15.521.21.35.001		3/9/2021		15070	freight	50.00	
180.0000.15.521.21.35.001		3/9/2021		15070	Sales Tax	98.50	
180.0000.15.521.21.35.001		3/9/2021		15070	Sales Tax	98.51	
180.0000.15.521.21.35.001		3/9/2021		15070	Sales Tax	4.95	
94435	3/31/2021	005575		SUMNER VETERINARY HOSPITAL,			\$131.44
001.0000.15.521.10.41.001		2/19/2021		760314	PD 02/15 Canine Rock Vet Svcs	131.44	
94436	3/31/2021	002667		TACOMA TOWING LLC,			\$529.72
001.0000.15.521.10.41.070		3/16/2021		244332	PD 03/15	441.80	
001.0000.15.521.10.41.070		3/23/2021		244351	PD 03/19	87.92	
94437	3/31/2021	005831		TOWN OF STEILACOOM,			\$3,806.62
001.0000.02.229.10.00.002		3/22/2021		02/21 Court Remit	MC 02/21 Court Remit	3,806.62	
94438	3/31/2021	012961		TRAN, THAI			\$125.00
105.0002.07.342.40.00.000		3/29/2021		RBL01-1907108334-1	AB RH Refund: RBL01-1907108334	125.00	
94439	3/31/2021	000564		TUCCI & SONS, INC,			\$845.15
101.0000.11.542.30.31.030		3/18/2021		67425	PKST Hot Mix Asphalt	182.69	
101.0000.11.542.63.31.030		3/11/2021		67384	PKST Hot Mix Asphalt	60.50	
101.0000.11.542.63.31.030		3/10/2021		67365	PKST Hot Mix Asphalt	601.96	
94440	3/31/2021	002509		VERIZON WIRELESS,			\$1,503.38
503.0000.04.518.80.42.001		3/16/2021		9875633223	02/17-03/16 Phone	17.63	
503.0000.04.518.80.42.001		3/16/2021		9875633223	02/17-03/16 Phone	170.86	
503.0000.04.518.80.42.001		3/16/2021		9875633223	02/17-03/16 Phone	140.95	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
503.0000.04.518.80.42.001			3/16/2021	9875633223	02/17-03/16 Phone	35.24	
503.0000.04.518.80.42.001			3/16/2021	9875633224	02/17-03/16 Phone	171.20	
503.0000.04.518.80.42.001			3/16/2021	9875633224	02/17-03/16 Phone	176.76	
180.0000.15.521.21.42.001			2/26/2021	9874258163	IT 01/27-02/26 Phone	378.74	
503.0000.04.518.80.42.001			2/26/2021	9874258163	IT 01/27-02/26 Phone	412.00	
94441	3/31/2021	012976		VINCENT AND CAROL NELSON,			\$264.00
105.0002.07.342.40.00.000			3/23/2021	03/23/21 Refund	AB RH Refund Duplicate Pmt	264.00	
94442	3/31/2021	000577		WABO,			\$250.00
001.0000.07.558.50.49.003			2/16/2021	40775	CD 2021 WABO Education Institu	250.00	
94443	3/31/2021	011595		WALTER E NELSON CO,			\$3,961.32
502.0000.17.521.50.31.001			3/25/2021	800937	PKFC Soap, Sanitizer	3,961.32	
94444	3/31/2021	012970		WENGER SALES & MANAGEMENT LLC,			\$650.00
190.2003.53.559.32.41.001			3/25/2021	AG 2021-075	CDBG AG 2021-075 Rent Assist:	650.00	
94445	3/31/2021	009254		WFOA,			\$75.00
001.0000.04.514.20.49.003			3/5/2021	FAW521-032021-0201	FN Federal Award Requirements:	75.00	
94446	3/31/2021	012486		WOODWORTH INDUSTRIAL PARK,			\$1,200.00
192.0007.07.558.60.49.001			2/25/2021	2021-11	SSMP 2021 Dues Lots 19 & 20	1,200.00	
94447	3/31/2021	012444		WSAMA,			\$99.00
001.0000.06.515.30.49.003			3/22/2021	Spring 2021: Wachter	LG WSAMA 2021 Spring Conf Wach	99.00	
94448	4/15/2021	010899		ACCESS INFORMATION MANAGEMENT,			\$2,553.31
001.0000.06.514.30.41.001			3/31/2021	8709232	LG 03/21 Record Retention & Mg	1,725.17	
001.0000.06.514.30.41.001			3/31/2021	8713505	LG Retrieval For Destruction/S	828.14	
94449	4/15/2021	006465		AGRI SHOP,			\$1,855.94
001.0000.11.576.80.48.001			3/12/2021	240583/2	PKFC Gold Level Walk Behind Se	370.50	
001.0000.11.576.80.48.001			3/12/2021	240584/2	PKFC Gold Level Walk Behind Se	198.62	
001.0000.11.576.80.48.001			3/12/2021	240585/2	PKFC Gold Level Walk Behind Se	242.37	
001.0000.11.576.80.48.001			3/12/2021	240586/2	PKFC Gold Level Walk Behind Mo	269.39	
001.0000.11.576.80.48.001			3/12/2021	240588/2	PKFC Service/Tune-UP	446.49	
001.0000.11.576.80.48.001			3/12/2021	240589/2	PKFC Edger Service	328.57	

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94450	4/15/2021	002293		AHBL INC,			\$27,059.94
001.0000.07.558.60.41.001			12/31/2020	123158	CD 11/26/20-12/25/20 Land Use	1,750.00	
192.0009.07.558.60.41.001			3/31/2021	124650	SSMP AG 2021-070 02/26-03/25 J	25,309.94	
94451	4/15/2021	008307		AT&T MOBILITY,			\$16,238.63
180.0000.15.521.21.42.001			3/19/2021	287293165778 03/21	IT/PD Thru 03/19 Phone	400.20	
503.0000.04.518.80.42.001			3/19/2021	287293165778 03/21	IT/PD Thru 03/19 Phone	11,272.57	
503.0000.04.518.80.42.001			3/19/2021	287296255265 03/21	IT Thru 03/19 Phone	4,565.86	
94452	4/15/2021	009770		BRUCE DEES & ASSOCIATES,			\$11,639.00
301.0032.11.594.76.41.001			4/1/2021	6367	PK AG 2020-169 Springbrook Par	11,639.00	
94453	4/15/2021	012982		CERDA, SARA			\$36.00
105.0002.07.342.40.00.000			4/1/2021	04/01/21 Refund	AB RH Refund Dup Pymt For 2 Lo	36.00	
94454	4/15/2021	012025		CHI FRANCISCAN OCCUP HEALTH,			\$865.00
001.0000.09.518.10.41.001			4/1/2021	00010867-00	HR 03/21 Physicals: Babcock, C	865.00	
94455	4/15/2021	000095		CHOUGH, KWANG S			\$376.80
001.0000.02.512.51.49.009			3/2/2021	03/02-03/30/21	MC 03/02-03/30 Interpreter	376.80	
94456	4/15/2021	009191		CITY OF DUPONT,			\$5,684.61
001.0000.02.229.10.00.003			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	5,684.61	
94457	4/15/2021	005022		CITY OF PUYALLUP,			\$3,600.00
001.0000.15.521.21.41.001			3/29/2021	AR114158	PD Forensic Svcs: Video Prep/A	3,600.00	
94458	4/15/2021	006613		CITY OF UNIVERSITY PLACE,			\$7,173.82
001.0000.02.229.10.00.001			4/15/2021	03/21 Court Remit	MC 03/21 Court Remit	7,173.82	
94459	4/15/2021	011584		COBAN TECHNOLOGIES INC,			\$309.92
180.0000.15.521.21.35.010			4/1/2021	39187	PD - 6 MIC- G5 Transmitter Ant	90.00	
180.0000.15.521.21.35.010			4/1/2021	39187	PD - 6 MIC- G5 Transmitter Bat	192.00	
180.0000.15.521.21.35.010			4/1/2021	39187	Sales Tax	8.91	
180.0000.15.521.21.35.010			4/1/2021	39187	Sales Tax	19.01	
94460	4/15/2021	011564		CODE PUBLISHING COMPANY,			\$241.23
001.0000.06.514.30.41.001			4/12/2021	69018	LG Muni Code Web Update, New P	241.23	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94461	4/15/2021	003948		COMCAST CORPORATION,			\$279.50
180.0000.15.521.21.42.001			4/6/2021	8498 30 099 0003937	PD 04/16-05/15 TLSO Modem	279.50	
94462	4/15/2021	002994		CORDANT HEALTH SOLUTIONS,			\$842.65
001.0000.02.523.30.41.001			3/31/2021	TC-42210033121	MC 03/21 UA Fees	431.25	
001.0000.02.523.30.41.001			2/28/2021	TC-42210022821	MC 02/21 UA Fees	411.40	
94463	4/15/2021	000592		DEPARTMENT OF REVENUE,			\$4,214.44
401.0000.00.223.40.00.000			4/14/2021	AG 2018-167 Ret. Rel	PWSW AG 2018-167 DOR Retainage	4,214.44	
94464	4/15/2021	000140		DEPT OF REVENUE-LEASEHOLD,			\$616.32
001.0000.00.237.10.00.000			4/15/2021	Q1/21	FN Q1/21 Leasehold Tax	616.32	
94465	4/15/2021	010998		DIPWEK, MARCUS			\$120.00
001.0000.02.512.51.49.009			3/18/2021	03/18/21	MC 03/18 Interpreter	120.00	
94466	4/15/2021	009472		DISH NETWORK LLC,			\$165.02
503.0000.04.518.80.42.001			4/4/2021	8255 7070 8168 1616	IT 04/16-05/15 PD TV/HD Receiv	165.02	
94467	4/15/2021	000159		EMPLOYMENT SECURITY DEPT,			\$3.50
001.0000.06.515.30.41.001			3/5/2021	21-010567-RDU-C5	LG Henesy Work History Search	3.50	
94468	4/15/2021	000166		FEDERAL EXPRESS,			\$17.38
001.0000.99.518.40.42.002			4/2/2021	7-326-95942	ND 03/24 PD Shipping	17.38	
94469	4/15/2021	001716		FENCE SPECIALISTS INC,			\$796.78
001.0000.11.576.80.41.001			3/31/2021	0047167	PKFC Fence Install: Harry todd	796.78	
94470	4/15/2021	002662		GENE'S TOWING INC,			\$76.93
001.0000.15.521.10.41.070			12/20/2020	487578	PD 12/20/2020	76.93	
94471	4/15/2021	005398		GLOBAL SECURITY &,			\$174.58
101.0000.11.543.50.41.001			4/1/2021	4452410	PKST 2Q/21 Intrusion Monitorin	174.58	
94472	4/15/2021	009728		HSA BANK,			\$78.75
001.0000.09.518.10.41.001			4/5/2021	W301054	HR 03/21 Svc Fee	78.75	
94473	4/15/2021	010885		JOHNSTON GROUP LLC,			\$4,500.00
001.0000.03.513.10.41.001			4/2/2021	1157	CM AG 2020-276 04/21 Fed. Gov.	4,500.00	

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94474	4/15/2021	011961		KELLEY CONNECT,			\$2,110.83
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	210.63	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	0.09	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	14.00	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	85.89	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	14.18	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	234.86	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	14.98	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	11.41	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	385.19	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	10.97	
503.0000.04.518.80.45.002			3/31/2021	IN816511	IT 03/21 Copier	741.23	
503.0000.04.518.80.31.002			4/8/2021	IN821843	IT Ink Cartridges	387.40	
94475	4/15/2021	000292		LAKEWOOD POLICE SPECIAL INVEST,			\$3,500.00
180.0000.15.521.21.31.012			4/6/2021	04/21 Narcotics	PDSZ 04/21Narcotics Replenish	3,500.00	
94476	4/15/2021	000292		LAKEWOOD POLICE SPECIAL INVEST,			\$586.00
181.0000.15.521.21.31.012			4/6/2021	04/21 Investigative	PDSZ 04/21 investigative Reple	586.00	
94477	4/15/2021	000300		LAKEWOOD WATER DISTRICT,			\$863.61
001.0000.11.576.80.47.001			4/6/2021	10152.01 04/06/21	PKFC 02/06-04/09 59th Ave & Fa	40.00	
101.0000.11.542.70.47.001			4/6/2021	10567.02 04/06/21	PKST 02/06-04/09 8902 Meadow R	40.00	
001.0000.11.576.81.47.001			4/6/2021	11535.02 04/06/21	PKFC 02/06-04/098714 87th Ave	447.56	
101.0000.11.542.70.47.001			4/6/2021	12586.02 04/06/21	PKST 02/06-04/09 Traffic Islan	40.00	
001.0000.11.576.81.47.001			4/6/2021	26554.02 04/06/21	PKFC 02/06-04/09 8714 87th Ave	71.14	
101.0000.11.542.70.47.001			4/6/2021	26979.01 04/06/21	PKST 02/06-04/09 0 Steil & GLD	43.12	
001.0000.11.576.81.47.001			4/6/2021	27581.01 04/06/21	PKFC 02/06-04/09 9101 Angle Ln	43.74	
001.0000.11.576.81.47.001			4/6/2021	27583.01 04/06/21	PKFC 02/06-04/09 9115 Angle Ln	40.00	
001.0000.11.576.81.47.001			4/6/2021	27586.01 04/06/21	PKFC 02/06-04/09 9349 Angle LN	54.93	
101.0000.11.542.70.47.001			4/6/2001	12796.02 04/06/21	PKST 02/06-04/09 Traffic Islan	43.12	
94478	4/15/2021	004680		LANGUAGE LINE SERVICES,			\$314.73
001.0000.02.512.51.49.009			2/28/2021	4946707	MC 02/21	94.35	
001.0000.02.512.51.49.009			3/31/2021	4953359	MC 03/21	220.38	
94479	4/15/2021	012985		LAW OFFICE MICHAEL A. CAMPBELL,			\$628.70
001.9999.15.521.21.41.070			4/12/2021	04/12/2021	PD Case 18-216-00911Towing & K	202.70	

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001.9999.15.521.21.41.001			4/12/2021	04/12/2021	PD Case 18-216-00911 Towing & K	426.00	
94480	4/15/2021	011263		LAW OFFICES OF MATTHEW RUSNAK,			\$500.00
001.0000.02.512.51.41.035			4/1/2021	332	MC 03/21	500.00	
94481	4/15/2021	005685		LEMAY MOBILE SHREDDING,			\$166.40
001.0000.15.521.10.41.001			4/1/2021	4694625	PD 03/05 & 03/19	166.40	
94482	4/15/2021	000309		LES SCHWAB TIRE CENTER,			\$244.28
501.0000.51.548.79.31.006			4/7/2021	30500655919	PKFL Battery	183.96	
501.0000.51.548.79.31.006			3/30/2021	30500654591	PKFL Battery	60.32	
94483	4/15/2021	011393		NAVIA BENEFIT SOLUTIONS,			\$232.40
001.0000.09.518.10.41.001			3/31/2021	10342689	HR 03/21 Participant Fee	232.40	
94484	4/15/2021	012983		NGUYEN, GIAC			\$36.00
105.0002.07.342.40.00.000			3/29/2021	RBL66-1901313972	AB RH Refund: Dup Pymt For	36.00	
94485	4/15/2021	010743		NISQUALLY INDIAN TRIBE,			\$26,260.00
001.0000.15.521.10.41.125			3/31/2021	26257	PD 03/21	26,260.00	
94486	4/15/2021	011424		OLBRECHTS & ASSOCIATES PLLC,			\$852.50
001.0000.07.558.60.41.007			4/9/2021	03/21	CD 03/21 Hearing Examiner: Ced	418.50	
001.0000.07.589.00.41.001			4/9/2021	03/21	CD 03/21 Hearing Examiner: Hem	434.00	
001.0000.07.389.00.00.000			4/9/2021	03/21	CD 03/21 Hearing Examiner: Hem	-434.00	
001.0000.07.237.20.00.000			4/9/2021	03/21	CD 03/21 Hearing Examiner: Hem	434.00	
94487	4/15/2021	006117		PETTY CASH,			\$127.77
001.0000.15.521.10.31.001			4/14/2021	03/21 JL	PD C. Bowl: Kick Stand	27.46	
501.0000.51.521.10.32.001			4/14/2021	03/21 JL	PD M. Criss Fuel For Trng	30.00	
501.0000.51.521.10.32.001			4/14/2021	03/21 JL	PD N. Dier Fuel For Training	70.31	
94488	4/15/2021	010064		PINTO, MICHELLE			\$1,628.16
001.0000.02.512.51.49.009			3/2/2021	02/26-03/30/21	MC 02/26-03/30 Interpreter	1,628.16	
94489	4/15/2021	010429		PMAM CORPORATION,			\$1,390.00
001.0000.15.521.10.41.015			4/7/2021	20210422	PD 03/21 Alarm Monitoring	1,390.00	
94490	4/15/2021	010204		PROTECT YOUTH SPORTS,			\$9.95

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001.0000.09.518.10.41.001			3/31/2021	855717	HR 03/21 Basic, Nat'l Combo Se	9.95	
94491	4/15/2021	009152		PUBLIC FINANCE INC,			\$273.00
202.0000.01.514.20.41.001			4/1/2021	0002305	DS 2Q/21 LID Admin: CLID No. 1	91.00	
202.0000.02.514.20.41.001			4/1/2021	0002305	DS 2Q/21 LID Admin: LID No. 11	91.00	
202.0000.02.514.20.41.001			4/1/2021	0002305	DS 2Q/21 LID Admin: LID No. 11	91.00	
94492	4/15/2021	000445		PUGET SOUND ENERGY,			\$24,447.54
001.0000.11.576.80.47.005			4/2/2021	300000000129 4/02/21	PKFC 03/10-03/31 11500 Militar	164.45	
101.0000.11.542.63.47.006			4/2/2021	300000007165 4/02/21	PKST 03/03-03/31 N of Lk WA Bl	23,510.79	
001.0000.11.576.80.47.005			4/2/2021	300000010268 4/02/21	PKFC 03/03-03/31 Woodlawn Ave	123.79	
001.0000.11.576.80.47.005			3/31/2021	200001526637 3/31/21	PKFC 03/01-03/30 9222 Veteran'	54.54	
101.0000.11.542.63.47.006			3/31/2021	200006381095 3/31/21	PKST 03/01-03/30 7819 150th St	22.41	
101.0000.11.542.63.47.006			3/31/2021	220008814687 3/31/21	PKST 03/01-03/30 7000 150th St	87.31	
101.0000.11.542.63.47.006			3/31/2021	220017817689 3/31/21	PKST 03/01-03/30 11521 GLD SW	82.87	
001.0000.11.576.80.47.005			3/31/2021	220018963391 3/31/21	PKFC 03/01-03/30 10365 112th S	59.82	
101.0000.11.542.63.47.005			3/31/2021	220025290614 3/31/21	PKST 03/01-03/30 12702 Vernon	209.46	
101.0000.11.542.63.47.005			3/31/2021	220025290630 3/31/21	PKST 03/01-03/30 8299 Veterans	132.10	
94493	4/15/2021	008849		Q & A POLYGRAPH SVCS,			\$300.00
001.0000.15.521.40.41.001			3/31/2021	21-003	PD 03/29 Polygraph: A. Beard	300.00	
94494	4/15/2021	005342		RAINIER LIGHTING & ELECTRICAL,			\$6,940.65
502.0016.17.594.18.63.001			4/8/2021	540440-1	PKFC 120W LED Wall Pack	384.65	
502.0016.17.594.18.63.001			4/8/2021	541346-1	PKFC Adjustable Mounting Arm,	247.28	
001.0000.11.576.80.31.001			4/8/2021	541982-1	PKFC Aqua/Blue Waterproof Conn	13.74	
502.0016.17.594.18.63.001			3/31/2021	541607-1	PKFC Splice Kit, lrg Blue Wing	69.79	
502.0000.17.518.35.31.001			3/30/2021	541552-1	PKFC Lights	125.29	
502.0000.17.521.50.31.001			3/8/2021	540493-1	PKFC Lights, Sockets, Bi-Pins	210.35	
502.0016.17.594.18.63.001			3/11/2021	539631-1	PKFC Lights, Adjustable Mounti	5,889.55	
94495	4/15/2021	000473		ROBBLEE'S TOTAL SECURITY INC,			\$107.78
101.0000.11.544.90.31.001			3/25/2021	116170	PKST Keys	21.82	
502.0000.17.542.65.31.001			4/9/2021	116375	PKFC Elevator Key Box	85.96	
94496	4/15/2021	011507		SEUI, MICHAEL			\$132.32
001.0000.02.512.51.49.009			3/2/2021	03/02/21	MC 03/02 Interpreter	132.32	
94497	4/15/2021	009925		SHOPE CONCRETE PRODUCTS CO,			\$792.83

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
401.0000.11.531.10.31.030			3/25/2021	10009173	PKSW Covers (Drain)	792.83	
94498	4/15/2021	011271		SITE ONE LANDSCAPE SUPPLY LLC,			\$685.97
101.0000.11.542.70.35.001			4/12/2021	107871824-001	PKST Backpack Sprayer	103.59	
101.0000.11.542.70.31.030			4/12/2021	107889102-001	PKST Herbicide	582.38	
94499	4/15/2021	003919		TACOMA PIERCE COUNTY CHAMBER,			\$550.00
192.0000.00.558.60.49.001			2/28/2021	57981	SSMP Tacoma PC Chamber Standar	550.00	
94500	4/15/2021	012981		TEKS SERVICES INC,			\$5,143.23
001.0000.99.518.40.42.002			4/6/2021	46346	ND Postage - Spring Clean Up P	5,143.23	
94501	4/15/2021	009354		TK ELEVATOR,			\$1,411.81
502.0000.17.518.35.48.001			3/31/2021	3005850069	PKFC 03/21 Elevator Svc	524.72	
502.0000.17.521.50.48.001			3/31/2021	3005850069	PKFC 03/21 Elevator Svc	262.36	
502.0000.17.542.65.48.001			3/31/2021	3005850069	PKFC 03/21 Elevator Svc	624.73	
94502	4/15/2021	005831		TOWN OF STEILACOOM,			\$9,762.50
001.0000.02.229.10.00.002			4/15/2021	03/21 court Remit	MC 03/21 Court Remit	9,762.50	
94503	4/15/2021	010640		TRANSUNION RISK AND,			\$192.55
001.0000.15.521.21.41.001			4/1/2021	212084 03/21	PD 03/21 People Searches	192.55	
94504	4/15/2021	000564		TUCCI & SONS, INC,			\$860.10
101.0000.11.542.30.31.030			3/17/2021	67457	PKST Hot Mix Asphalt	612.13	
101.0000.11.542.30.31.030			3/18/2021	67478	PKST Hot Mix Asphalt	247.97	
94505	4/15/2021	005543		UNDERWATER SPORTS INC,			\$10.99
001.0000.15.521.22.48.001			3/10/2021	50019518	PD Equip Repair	10.99	
94506	4/15/2021	011127		US BANK VOYAGER FLEET SYSTEMS,			\$1,554.63
501.0000.51.521.10.32.001			4/1/2021	8693430122114	PKFL/PDFL 03/21 Fuel	372.15	
180.0000.15.521.21.32.001			4/1/2021	8693430122114	PKFL/PDFL 03/21 Fuel	409.55	
181.0000.15.521.30.32.001			4/1/2021	8693430122114	PKFL/PDFL 03/21 Fuel	79.15	
501.0000.51.548.79.32.001			4/1/2021	8693430122114	PKFL/PDFL 03/21 Fuel	693.78	
94507	4/15/2021	009856		UTILITIES UNDERGROUND LOCATION,			\$438.60
101.0000.11.544.90.41.001			3/31/2021	1030169	PKST/PKSW 03/21 Excavation Not	219.30	
401.0000.11.531.10.41.001			3/31/2021	1030169	PKST/PKSW 03/21 Excavation Not	219.30	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94508	4/15/2021	002509		VERIZON WIRELESS,			\$790.74
180.0000.15.521.21.42.001			3/26/2021	9876408016	IT/PD 02/27-03/26 Phone	378.74	
503.0000.04.518.80.42.001			3/26/2021	9876408016	IT/PD 02/27-03/26 Phone	412.00	
94509	4/15/2021	002509		VERIZON WIRELESS,			\$245.09
503.0000.04.518.80.42.001			11/16/2020	INV21219948	IT 09/20 GPS	245.09	
94510	4/15/2021	011804		VISA - 0281,			\$673.01
001.0000.15.521.21.35.001			3/28/2021	0281/Johnso 03/28/21	PD Video Accessories	65.03	
001.0000.15.521.21.35.001			3/28/2021	0281/Johnso 03/28/21	PD Forensic Testing Kits	607.98	
94511	4/15/2021	011755		VISA - 0349,			\$2,467.75
001.9999.15.525.60.41.001			3/28/2021	0349/Meeks 03/28/21	PD Clean Calibration Machine -	819.66	
001.0000.99.518.40.42.002			3/28/2021	0349/Meeks 03/28/21	ND PD 02/12 Evience Mailing	33.33	
001.0000.15.521.40.43.001			3/28/2021	0349/Meeks 03/28/21	PD IAPE Membership	50.00	
001.0000.15.521.22.31.001			3/28/2021	0349/Meeks 03/28/21	PD Batteries	1,978.20	
001.0000.15.521.22.31.008			3/28/2021	0349/Meeks 03/28/21	PD Uniforms	879.68	
001.0000.99.518.40.42.002			3/28/2021	0349/Meeks 03/28/21	ND PD Evidence Mailings	14.16	
001.0000.15.521.40.43.001			3/28/2021	0349/Meeks 03/28/21	PS IAPE Cert Fee	100.00	
001.0000.15.521.40.43.004			3/28/2021	0349/Meeks 03/28/21	PD 03/16-03/17 Prop Mgmt Trng	170.48	
001.0000.09.518.10.31.001			3/28/2021	0349/Meeks 03/28/21	HR PD Key Chains	347.50	
001.0000.99.518.40.42.002			3/28/2021	0349/Meeks 03/28/21	ND PD Evidence Mailing	78.18	
001.9999.15.525.60.31.001			3/28/2021	0349/Meeks 03/28/21	PD Adapter - Covid	203.32	
001.0000.15.521.22.31.008				0319/Meeks 03/28/21	PD Credit Re: RTN00129724	-17.32	
001.0000.15.521.22.31.008				0349/Meeks 03/28/21	PD Credit Voucher Re: RTNN0012	-1,326.52	
001.0000.15.521.80.31.001				049/Meeks 03/28/21	PD Credit Voucher Re: Invoice	-862.92	
94512	4/15/2021	011749		VISA - 0513,			\$8.78
502.0000.17.518.35.31.001			3/28/2021	0513/Ferm 03/28/21	PKFC Moving Blankets	8.78	
94513	4/15/2021	011958		VISA - 0975,			\$3,493.77
105.0001.07.559.20.42.002			3/28/2021	0975/Gumm 03/28/21	AB Postage	157.50	
190.0005.52.559.31.31.001			3/28/2021	0975/Gumm 03/28/21	AB/CDBG Toner Cartridge	26.83	
105.0002.07.559.20.31.001			3/28/2021	0975/Gumm 03/28/21	AB/CDBG Toner Cartridge	26.83	
105.0001.07.559.20.31.001			3/28/2021	0975/Gumm 03/28/21	AB/CDBG Toner Cartridge	26.84	
190.0005.52.559.31.31.001			3/28/2021	0975/Gumm 03/28/21	AB/CDBG Toner Cartridges	118.79	
105.0002.07.559.20.31.001			3/28/2021	0975/Gumm 03/28/21	AB/CDBG Toner Cartridges	118.79	
105.0001.07.559.20.31.001			3/28/2021	0975/Gumm 03/28/21	AB/CDBG Toner Cartridges	118.80	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
190.6002.52.559.31.41.001			3/28/2021	0975/Gumm 03/28/21	CDBG CV3 Application Translati	981.13	
190.0005.52.559.31.35.001			3/28/2021	0975/Gumm 03/28/21	CDBG CV3 Scanner	438.48	
001.0000.99.518.40.42.002			3/28/2021	0975/Gumm 03/28/21	ND Postage	1.00	
190.0005.52.559.31.35.001			3/28/2021	0975/Gumm 03/28/21	CDBG Printer	439.59	
190.0005.52.559.31.35.001			3/28/2021	0975/Gumm 03/28/21	CDBG Scanner	439.59	
190.0005.52.559.31.49.003			3/28/2021	0975/Gumm 03/28/21	CDBG Fundamentals Online: Holv	545.00	
001.0000.99.518.40.42.002			3/28/2021	0975/Gumm 03/28/21	CD Postage	31.80	
190.0005.52.559.31.41.001			3/28/2021	0975/Gumm 03/28/21	CDBG MHR-180 Fennell	58.46	
190.0005.52.559.31.41.001				0975/Gumm 03/28/21	CDBG MHR-180 Fennell	-35.66	
94514	4/15/2021	012354		VISA - 1105,			\$973.87
001.9999.11.571.10.31.001			3/28/2021	1105/Martin 03/28/21	PKRC Anniv Mugs	848.87	
001.0000.11.571.20.49.001			3/28/2021	1105/Martin 03/28/21	PKRC WFEA Membership	125.00	
94515	4/15/2021	011540		VISA - 1371,			\$1,508.93
001.0000.15.521.70.31.001			3/28/2021	1371/Gil deh 03/28/21	PD Batteries	1,508.93	
94516	4/15/2021	012401		VISA - 3408,			\$410.14
195.0024.15.521.30.31.001			3/28/2021	3408/Carrol 03/28/21	PD Comcast 02/23-03/22	365.90	
195.0024.15.521.30.31.001			3/28/2021	3408/Carrol 03/28/21	PD LPR Mo Subscription	44.24	
94517	4/15/2021	012656		VISA - 4197,			\$609.97
501.0000.51.548.79.31.006			3/28/2021	4197/Fin 6 03/28/21	PKFL Battery	55.99	
001.0000.06.515.30.41.001			3/28/2021	4197/Fin 6 03/28/21	LG Cert. NCO 201063441	38.49	
001.0000.06.515.30.41.001			3/28/2021	4197/Fin 6 03/28/21	LG Cert. Judgement & Sentence	40.49	
001.0000.09.518.10.41.010			3/28/2021	4197/Fin 6 03/28/21	HR The Attendance Maze Webinar	75.00	
001.0000.04.514.20.49.003			3/28/2021	4197/Fin 6 03/28/21	FN WCIA LRI Training: Kapla &	400.00	
94518	4/15/2021	012668		VISA - 4635,			\$8,118.72
101.0000.21.544.20.49.001			3/28/2021	4635/Fin 5 03/28/21	PWST APWA Dues: Bucich, Barron	1,230.00	
401.0000.41.531.10.49.001			3/28/2021	4635/Fin 5 03/28/21	PWSW APWA Dues: Ott, Halar, Ho	820.00	
401.0000.41.531.10.31.001			3/28/2021	4635/Fin 5 03/28/21	PWSW Poster	23.10	
001.0000.04.514.20.49.017			3/28/2021	4635/Fin 5 03/28/21	FN GASB Codification Books	293.00	
001.0000.07.558.60.49.001			3/28/2021	4635/Fin 5 03/28/21	CD 2021 AWCPD Membership: Bugh	100.00	
104.0010.01.557.30.31.001			3/28/2021	4635/Fin 5 03/28/21	HM Farmers Market Note Pads	202.92	
001.9999.11.571.10.31.001			3/28/2021	4635/Fin 5 03/28/21	PKRC 25th Anniversary Glasses	503.78	
001.9999.11.571.10.31.001			3/28/2021	4635/Fin 5 03/28/21	PKRC 25th Anniversary Totes	3,403.52	
001.0000.06.515.30.41.001			3/28/2021	4635/Fin 5 03/28/21	LG Cert. NCO 182001051	11.50	
001.9999.11.571.10.31.001			3/28/2021	4635/Fin 5 03/28/21	PKRC 25th Anniversary Tatoos	273.22	

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001.0000.06.515.30.41.001			3/28/2021	4635/Fin 5 03/28/21	LG Cert. NCO 161033611	6.50	
001.9999.11.571.10.31.001			3/28/2021	4635/Fin 5 03/28/21	PKRC 25th Anniversary Coins	1,113.84	
104.0010.01.557.30.31.001			3/28/2021	4635/Fin 5 03/28/21	HM Money Counter & Locking Bac	137.34	
94519	4/15/2021	012715		VISA - 5244,			\$337.50
001.0000.01.511.60.31.001			3/28/2021	5244/Schuma 03/28/21	CC Photo	4.39	
001.0000.01.511.60.31.001			3/28/2021	5244/Schuma 03/28/21	CC Name Block: Belle	13.19	
001.0000.03.513.10.49.004			3/28/2021	5244/Schuma 03/28/21	CM Annual Smartsheet Subscript	184.63	
001.0000.01.511.60.31.005			3/28/2021	5244/Schuma 03/28/21	CC Retreat Food	33.00	
001.0000.01.511.60.31.005			3/28/2021	5244/Schuma 03/28/21	CC Retreat Food	102.29	
94520	4/15/2021	012864		VISA - 7000,			\$7,066.98
104.0010.01.557.30.31.001			3/28/2021	7000/Fin 1 03/28/21	HM Farmers Market Totes	6,081.61	
104.0010.01.557.30.31.001			3/28/2021	7000/Fin 1 03/28/21	HM/PKRC Magnets	425.32	
001.9999.11.571.10.31.001			3/28/2021	7000/Fin 1 03/28/21	HM/PKRC Magnets	247.28	
001.9999.11.571.10.31.001			3/28/2021	7000/Fin 1 03/28/21	PKRC 25th Anniversary Stickers	246.77	
104.0010.01.557.30.31.001			3/28/2021	7000/Fin 1 03/28/21	HM Farmers Market Tatoos	66.00	
94521	4/15/2021	011659		VISA - 7212,			\$2,374.38
001.0000.11.576.81.31.008			3/28/2021	7212/PWOM 03/28/21	PKFC Work Pants: T Higgins	409.37	
001.0000.11.576.81.31.008			3/28/2021	7212/PWOM 03/28/21	PKFC Work Pants: S Bird	327.50	
001.0000.11.576.81.31.008			3/28/2021	7212/PWOM 03/28/21	PKFC Work Pants: S Turner	327.50	
001.0000.11.576.81.31.008			3/28/2021	7212/PWOM 03/28/21	PKFC Work Pants: O Moreno	327.50	
502.0000.17.518.35.31.008			3/28/2021	7212/PWOM 03/28/21	PKFC Work Pants: J Anderson	327.50	
502.0000.17.518.35.31.008			3/28/2021	7212/PWOM 03/28/21	PKFC Work Pants: S Fermahin &	655.01	
94522	4/15/2021	012484		VISA - 7482,			\$131.82
001.0000.02.523.30.31.001			3/28/2021	7482/Wright 03/28/21	MC Pick Sticks	131.82	
94523	4/15/2021	011136		VISA - 7750,			\$167.89
001.0000.99.518.40.42.002			3/28/2021	7750/Allen 03/28/21	NN PD 03/09 & 03/23 Tox Lab Ma	45.00	
001.0000.15.521.80.31.001			3/28/2021	7750/Allen 03/28/21	PD Supplies: Evidence	122.89	
94524	4/15/2021	011138		VISA - 7776,			\$275.85
001.0000.11.576.81.45.004			3/28/2021	7776/Anders 03/28/21	PKFC Trailer Rental	209.91	
001.0000.11.576.81.31.001			3/28/2021	7776/Anders 03/28/21	PKFC Signs: Memory Plates	65.94	
94525	4/15/2021	011140		VISA - 7800,			\$799.39
501.0000.51.548.79.32.001			3/28/2021	7800/Cummin 03/28/21	PKFL Propane	46.71	


Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
101.0000.11.542.30.31.030			3/28/2021	7800/Cummin 03/28/21	PKST Cat Litter For Fluid Spil	594.42	
501.0000.51.548.79.31.006			3/28/2021	7800/Cummin 03/28/21	PKFL 8x4 Sheet Expanded Metal	158.26	
94526	4/15/2021	012863		VISA - 7935,			\$302.96
503.0000.04.518.80.49.004			3/28/2021	7935/White 03/28/21	IT 02/11-03/11 Fix & Protect	29.99	
503.0000.04.518.80.49.004			3/28/2021	7935/White 03/28/21	IT Int'l Trx Fee on Fix & Prot	0.30	
503.0000.04.518.80.49.004			3/28/2021	7935/White 03/28/21	IT CH Pandora	29.62	
503.0000.04.518.80.49.004			3/28/2021	7935/White 03/28/21	IT Creative Cloud	58.24	
503.0000.04.518.80.49.004			3/28/2021	7935/White 03/28/21	IT Photography Plan	10.98	
503.0000.04.518.80.49.004			3/28/2021	7935/White 03/28/21	IT Monthly MailChimp	96.70	
503.0000.04.518.80.49.004			3/28/2021	7935/White 03/28/21	IT 03/11-04/11 Fix & Protect	29.99	
503.0000.04.518.80.49.004			3/28/2021	7935/White 03/28/21	IT Int'l Trx Fee On Fix & Prot	0.30	
503.0000.04.518.80.49.004			3/28/2021	7935/White 03/28/21	IT InDesign	23.07	
503.0000.04.518.80.35.030			3/28/2021	7935/White 03/28/21	IT Controller	53.99	
503.0000.04.518.80.35.001				7935/White 03/28/21	IT Return Power Adapter Cable	-30.22	
94527	4/15/2021	011158		VISA - 7966,			\$3,698.50
001.0000.15.521.40.49.003			3/28/2021	7966/Pitts 03/28/21	PD Bckgrd Invest Trng: Dier	385.00	
001.0000.15.521.40.49.003			3/28/2021	7966/Pitts 03/28/21	PD Lessons Learned Protest Trn	50.00	
001.0000.15.521.40.49.003			3/28/2021	7966/Pitts 03/28/21	PD Volcanic Mech Course: Lutru	2,935.50	
001.0000.15.521.40.49.003			3/28/2021	7966/Pitts 03/28/21	PD Body Language Trng: Pettit	199.00	
001.0000.15.521.40.49.003			3/28/2021	7966/Pitts 03/28/21	PD Deception Trng: Northcutt	129.00	
94528	4/15/2021	011159		VISA - 7974,			\$21.92
001.0000.11.569.50.31.001			3/28/2021	7974/Scheid 03/28/21	PKSR Supplies	21.92	
94529	4/15/2021	011714		VISA - 8434,			\$1,016.95
001.0000.15.521.10.31.001			3/28/2021	8434/LaVerg 03/28/21	PD Supplies	21.95	
001.0000.15.521.40.49.003			3/28/2021	8434/LaVerg 03/28/21	PD Verbal De-Escalation Reg:J	995.00	
94530	4/15/2021	011177		VISA - 8550,			\$13.21
001.0000.13.558.70.49.004			4/28/2021	8550/Newton 03/28/21	ED 02/28-03/29 Dropbox	13.21	
94531	4/15/2021	012924		VISA - 9329,			\$10,814.18
001.0000.15.521.10.31.001			3/28/2021	9329/PD1 03/28/21	PD Phone Chargers	39.54	
001.0000.99.518.40.42.002			3/28/2021	9329/PD1 03/28/21	ND PD 03/01 Certified Letter	7.20	
001.0000.99.518.40.42.002			3/28/2021	9329/PD1 03/28/21	ND PD 03/01 Certified letter	7.00	
001.0000.15.521.22.35.010			3/28/2021	9329/PD1 03/28/21	PD Loop Handles & Pouches	1,115.00	
001.0000.99.518.40.42.002			3/28/2021	9329/PD1 03/28/21	ND PD 03/03 Certified Letter	7.00	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
001.0000.15.521.10.31.001			3/28/2021	9329/PD1 03/28/21	PD Postage Stamps	55.00	
195.0026.15.525.30.31.001			3/28/2021	9329/PD1 03/28/21	PD Exam Gloves	1,075.00	
001.0000.15.521.21.41.001			3/28/2021	9329/PD1 03/28/21	PD Website Renewal RubMaps	150.49	
195.0026.15.525.30.31.001			3/28/2021	9329/PD1 03/28/21	PD Exam Gloves	1,075.00	
001.0000.99.518.40.42.002			3/28/2021	9329/PD1 03/28/21	ND PD 03/08-03/12 Certified Le	117.85	
001.0000.15.521.10.49.001			3/28/2021	9329/PD1 03/28/21	PD Norcan Membership: D. srtan	30.00	
001.0000.15.521.10.41.001			3/28/2021	9329/PD1 03/28/21	PD Range Filters Install	3,162.90	
180.0000.15.521.21.31.001			3/28/2021	9329/PD1 03/28/21	PD Bungee Cords	126.56	
195.0026.15.525.30.31.001			3/28/2021	9329/PD1 03/28/21	PD Exam Gloves	1,075.00	
195.0026.15.525.30.31.001			3/28/2021	9329/PD1 03/28/21	PD Exam Gloves	1,195.00	
001.0000.15.521.10.31.020			3/28/2021	9329/PD1 03/28/21	PD Weapon Lights	1,575.64	
94532	4/15/2021	000595		WASHINGTON ASSOC OF SHERIFFS,			\$1,108.13
001.0000.02.523.30.41.001			2/28/2021	EM 2021-00215	MC 02/21 Home Monitoring	1,108.13	
# of Checks Issued		287					
Total		\$2,806,854.84					
Voids							
		(144.74)	Ck# 94328 03/26/2021				
		(245.09)	Ck# 93893 04/05/2021				
TOTAL		\$2,806,465.01					



To: Mayor and City Councilmembers

From: Tho Kraus, Assistant City Manager/Chief Finance Officer

Through: John J. Caulfield, City Manager 

Date: May 3, 2021

Subject: Payroll Check Approval

Payroll Period(s): March 16-31, 2021 and April 1-15, 2021

Total Amount: \$2,467,857.81

Checks Issued:

Check Numbers: 114296-114301

Total Amount of Checks Issued: \$20,181.59

Electronic Funds Transfer:

Total Amount of EFT Payments: \$604,598.34

Direct Deposit:

Total Amount of Direct Deposit Payments: \$1,638,454.18

Federal Tax Deposit:

Total Amount of Deposit: \$204,623.70

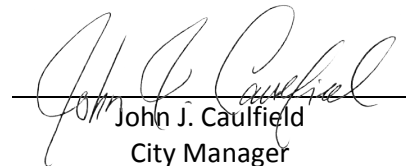
I, the undersigned, do hereby certify under penalty of perjury that the materials have been furnished, the services rendered, or the labor performed as described herein and that the claims are just and due obligations against the City of Lakewood, Washington, and that I am authorized to authenticate and certify said liens.



Dana Kapla
Finance Supervisor



Tho Kraus
Assistant City Manager
Chief Finance Officer



John J. Caulfield
City Manager

Payroll Distribution**City of Lakewood****Pay Period ending 03-16-2021 thru 04-15-2021****Direct Deposit and ACH in the amount of : \$2,447,676.22****Payroll Ck#'s 114296-114301 in the amount of : \$20,181.59****Total Payroll Distribution: \$2,467,857.81****Employee Pay Total by Fund:****Fund 001 - General**

	Amount
City Council	\$ 10,100.00
Municipal Court	\$ 62,221.35
City Manager	\$ 23,033.34
Administrative Services	\$ 93,944.00
Legal	\$ 66,591.19
Community and Economic Development	\$ 97,333.49
Parks, Recreation and Community Services	\$ 91,885.50
Police	\$ 985,738.03
Non-Departmental	\$ -
General Fund Total	\$ 1,430,846.90

Fund 101 - Street	\$ 47,407.71
Fund 102 - Real Estate Excise	\$ -
Fund 104 - Hotel / Motel Lodging Tax	\$ -
Fund 105 - Property Abatement/Rental Housing Safety Program	\$ 14,641.28
Fund 180 - Narcotics Seizure	\$ 784.67
Fund 181 - Felony Seizure	\$ -
Fund 182 - Federal Seizure	\$ -
Fund 190 - CDBG Grants	\$ 17,771.43
Fund 191 - Neighborhood Stabilization Program	\$ 1,197.39
Fund 192 - Office of Economic Adjustment/SSMCP	\$ 9,507.85
Fund 195 - Public Safety Grants	\$ 32,825.96
Fund 301 - Parks CIP	\$ 1,485.00
Fund 302 - Transportation CIP	\$ 73,811.54
Fund 311 - Sewer Capital Project	\$ 3,148.44
Fund 401 - Surface Water Management	\$ 44,033.29
Fund 502 - Property Management	\$ 11,946.38
Fund 503 - Information Technology	\$ 34,325.00
Fund 504 - Risk Management	\$ 140.95
Other Funds Total	\$ 293,026.89

Employee Gross Pay Total	\$ 1,723,873.79
Benefits and Deductions:	\$ 743,984.02
Grand Total	\$ 2,467,857.81

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED:

May 3, 2021

TITLE: Harry Todd
Playground Replacement

TYPE OF ACTION:

— ORDINANCE NO.

REVIEW:

May 3, 2021

ATTACHMENTS:
Playground Concept

— RESOLUTION NO.

X MOTION NO. 2021-25

— OTHER

SUBMITTED BY: Mary Dodsworth, Parks, Recreation and Community Services Director

RECOMMENDATION: It is recommended that City Council authorize the City manager to execute a contract with Landscape Structures, Inc. and approve additional expenditures in an amount not to exceed \$110,000 for the installation of new playground equipment, surfacing, permitting and site work as needed to complete the project at Harry Todd Park.

DISCUSSION: The City is making major site improvements at Harry Todd Park. The project includes ADA access to the shoreline, a new bulkhead, docks, fishing pier, restroom building and playground structure. The original park playground was installed in 2000. A playground has a lifecycle of 10 – 15 years and several elements of the playground had been damaged and removed for safety. The entire playground was removed during construction to make way for the new restroom, playground and parking areas. Council approved the Harry Todd Park capital improvement project as part of the 2019/20 Parks CIP budget. Since waterfront contractors are not typically playground installers, the City saved approximately \$12,000 by purchasing and installing the playground as a separate project element.

A Request for Proposal was sent to many different playground companies, various proposals were received and the City selected a playground structure for Harry Todd Park. Criteria used to select the structure(s) was playability, variety of experiences and provisions for multiple age group participation, safety, creativity, overall aesthetics, use of space, and compatibility with other park surroundings, cost and timing of delivery and installation of the equipment and material. Cost for equipment, surfacing and installation is \$97,612.72. A contingency is being included to support building permits, equipment needs, site access work and additional surfacing as needed to complete the project.

ALTERNATIVE(S): Council could request new proposals for equipment purchase which would delay the installation of the new playground at the park.


FISCAL IMPACT: \$110,000

Mary Dodsworth

Prepared by

Mary Dodsworth

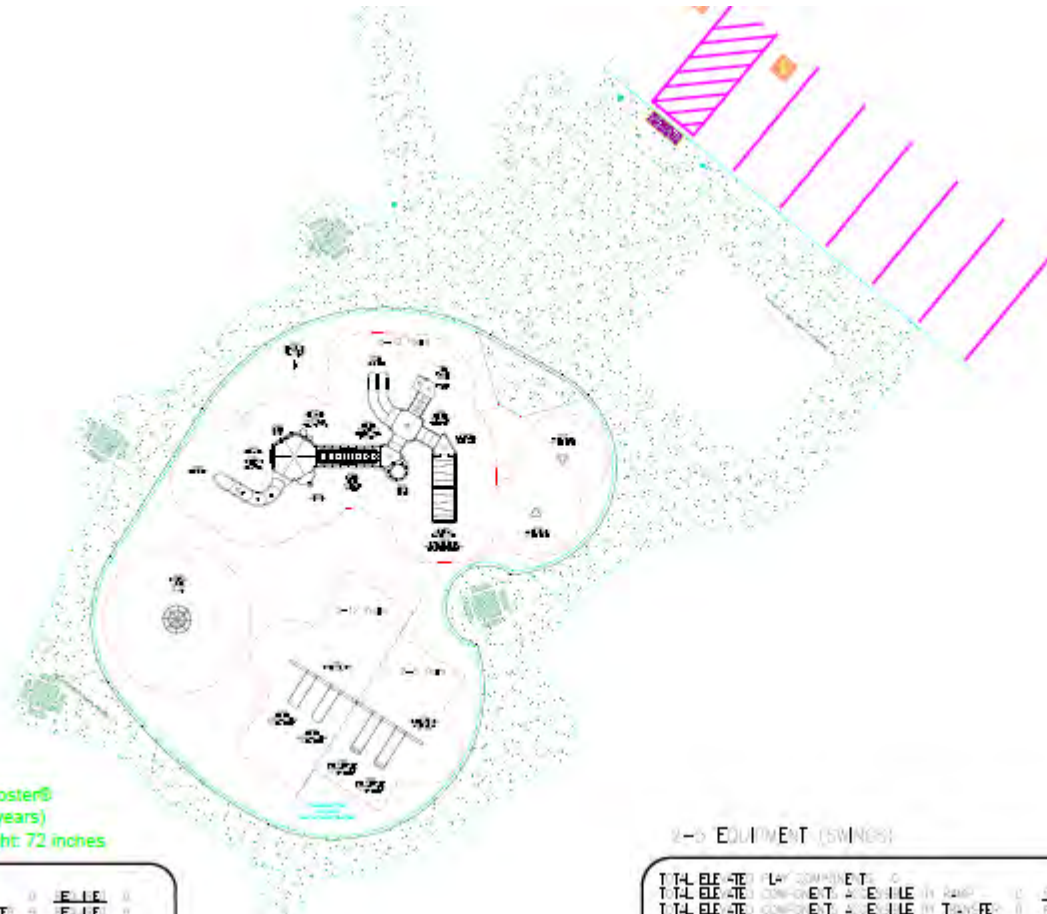
Department Director


City Manager Review

Attachment - Playground Concepts



Multi-age play structure, swings, spinner and saddle spinners.



PlayBooster®
(5-12 years)

5-12 EQUIPMENT Max Fall Height: 72 inches

TOTAL DECK PLAY COMPONENTS:	2
TOTAL DECK COMPONENTS ACCESSIBLE BY RAMP:	0
TOTAL DECK COMPONENTS ACCESSIBLE BY TRANSFER:	0

2-5 EQUIPMENT (SWINGS)

TOTAL DECK PLAY COMPONENTS:	0
TOTAL DECK COMPONENTS ACCESSIBLE BY RAMP:	0
TOTAL DECK COMPONENTS ACCESSIBLE BY TRANSFER:	0

landscape
structures



IPMAA CERTIFIED
100% COMPLIANT
WITH THE 2010 ADA
STANDARDS FOR
ACCESSIBILITY
AND PROGRAM
ACCESSIBILITY

WE HAVE THE BEST
DESIGN - BUILD
TEAM

THE PLAYERS OF THE
FUTURE ARE HERE
AND THEY ARE
READY TO PLAY

WE HAVE THE BEST
DESIGN - BUILD
TEAM
AND THE BEST
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FOR THE FUTURE

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FOR THE FUTURE

EXHIBIT A



**Harry Todd Park Playground
Worksheet #1811-10149-4**

To:
City of Lakewood Parks and Recreation Dept.
Douglas Fraser
6000 Main Street SW
Lakewood WA 98409
253-983-7852 | dfraser@cityoflakewood.us

Owner:
City of Lakewood Parks and Recreation Dept.
6000 Main Street SW
Lakewood WA 98409
253-983-7852 | dfraser@cityoflakewood.us

Project Location:
Harry Todd Park
8928 N. Thorne Ln. SW
Lakewood WA 98409

Ship To:
Harry Todd Park
8928 N. Thorne Ln. SW
Lakewood WA 98409

Date	Lead Time	Terms	Quoted By
April 22, 2021	4 to 6 weeks	see attached	Chris Donahue 253.691.6847

Quantity	Drawing / Model #	Description	per unit	Total
1	1762020-2	PlayBooster 5-12 Age by Landscape Structures per attached Itemized Quote and Drawing 1762020-2		\$ 55,215.00
1	Installation	Complete Equipment Installation by Factory Trained And Certified Installation Crew, Locally Based		\$ 22,086.00
225	sq. yds.	Woodcrafter, Engineered Wood Fiber Playground Safety Surfacing, ASTM F2075-10a compliant.	\$ 29.54	\$ 6,645.38
225	sq. yds.	Blown-In Installation of Engineered Wood Fiber, 4010 sq. ft. at 12" Compacted Depth, Includes Fiber Fabric.	\$ 15.13	\$ 3,403.13
		Existing Equipment / Surfacing Removal and Site Preparation done by others.		

PlayCreation Contribution \$ (1,104.30)

Sourcewell Contract Pricing #030117-LSI \$ (4,417.20)

Include PU/Waste/Debris Fee

Landscape Structures, Inc.
attention: Misty Link
801 - 7th Street South
Delano, MN 55328-0188
mistylink@playall.com
763.972.5591



Send Purchase Order for processing to:

PlayCreation, Inc.
attention: Chris Donahue
2104 SW 152nd Street, ste 1
Burien, WA 98166
Chris@PlayCreation.com
206.952.6366

Freight	\$ 6,991.58
Project Total	\$ 88,819.58
Tax 9.9%	\$ 8,793.14
Sub Total	\$ 97,612.72

**Grand Total
97,612.72**

APPROVAL

Signature

Date

PO#

Quote Based on Current 2021 pricing | Pricing Valid thru 11-30-2021 | 50% Deposit due at Order Entry

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: May 3, 2021	TITLE: 2020 Transportation Benefit District Annual Report	TYPE OF ACTION: ORDINANCE NO. RESOLUTION NO. <u>X</u> MOTION 2021-26 OTHER
REVIEW: April 19, 2021	ATTACHMENTS: <ul style="list-style-type: none">• 2020 TBD Annual Report	

SUBMITTED BY: Tho Kraus, Assistant City Manager/Chief Financial Officer

RECOMMENDATION:

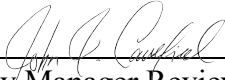
It is recommended that the City Council approve the 2020 Transportation Benefit District Annual Report.

DISCUSSION:

RCW 36.73.160(2) requires Transportation Benefit Districts to issue a separate annual transportation improvement report detailing the district revenues, expenditures and status of all projects, including cost and construction schedules. The report must be made to the public and newspapers of record in the district. The attached report satisfies the requirements of state law.

ALTERNATIVE(S): The City Council may deny approval and provide direction for changes to the report.

FISCAL IMPACT: N/A

Tho Kraus	
Department Director	City Manager Review



To: Mayor and City Councilmembers
 From: Tho Kraus, Assistant City Manager/Chief Financial Officer
 Through: John J. Caulfield, City Manager *John J. Caulfield*
 Date: May 3, 2021
 Subject: City of Lakewood Transportation Benefit District 2020 Annual Report

Report Purpose

This report provides information on the status of the City of Lakewood Transportation Benefit District ("District") and fulfills the requirements of the State of Washington and the District for an annual report. Below are excerpts from the relevant state law and local documents.

RCW 36.73.160(2): A district shall issue an annual report, indicating the status of transportation improvement costs, transportation improvement expenditures, revenues, and construction schedules, to the public and to newspapers of record in the district.

Action taken by the City Council

- Adopted the TBD Annual Report for FY 2019 on May 4, 2020 (Reviewed at April 27, 2020 Study Session)

Transportation Benefit District Financial Summary

The following table provides a summary of the Transportation Benefit District Fund for fiscal year 2020.

Fund 103 Transportation Benefit District	2020 Budget	2020 Actual
Revenues:		
\$20 Vehicle License Fee	\$ 835,000	\$ 881,849
Interest Earnings	-	904
Total Revenues	\$ 835,000	\$ 882,753
Expenditures (Transfers to Transportation CIP Fund):		
302.0005 Chip Seal Program - Local Access Roads	52,456	52,456
302.0135 JBLM North Access Improvements	195,000	195,000
Total Expenditures	\$ 247,456	\$ 247,456
Beginning Fund Balance, January 1	\$ 52,456	\$ 52,457
Ending Fund Balance, December 31	\$ 640,000	\$ 687,754

Note – The Washington State Department of Licensing (DOL) collects the fees and distributes the net proceeds to the City on a monthly basis. DOL automatically deducts one percent (1%) of the \$20 fee at the time of collection for administration and collections expenses incurred. The administration fee is the maximum amount permitted by RCW 82.80.140.

TBD Funded Projects – Revenues, Expenditures & Construction Schedules

The following is an update of TBD funded projects in 2020.

302.0005 Chip Seal Program – Local Access Roads

302.0005 Chip Seal Program - Local Access Roads	2019 Actual	2020 Actual
Revenues:		
Real Estate Excise Tax	\$ 114,000	\$ 307,543
TBD \$20 Vehicle License Fee	246,000	52,457
Other Project Savings	-	1,439
Total Revenues	\$ 360,000	\$ 361,439
Expenditure:		
Capital	342,363	446,020
302.0135 JBLM North Access Improvements	\$ 342,363	\$ 446,020
Beginning Fund Balance, January 1	\$ 66,944	\$ 84,581
Ending Fund Balance, December 31	\$ 84,581	\$ -

Note: 2019 balance from TBD funds totaling \$84,581 rolled over to 2020 Chip Seal Program.

The focus of the 2020 chip seal program was on the area east of Clover Park High School, north of 112th Street, west of Pac Highway and primarily south of 108th. Some streets were chip sealed north of 108th in the vicinity of Douglas Drive, Filbert Lane, and just west of Bridgeport Way.

The focus of the 2021 chip seal program will be the area bounded by Veteran's Drive to the north and Gravelly Lake Drive to the east and American Lake to the south.

109th Street Before:



109th Street After:



Montgrove Drive Before:



Montgrove Drive After:



302.0135 JBLM North Access Improvements

302.0135 JBLM North Access Improvements	2020 Actual
Revenues:	
TBD \$20 Vehicle License Fee	\$ 195,000
General Obligation Bonds	659,570
Real Estate Excise Tax	548,722
SWM Funds	218,900
Interest Earnings	251
Total Revenues	\$ 1,622,443
302.0135 JBLM North Access Improvements	
Expenditure:	
Capital	1,579,661
Total Expenditures	\$ 1,579,661
Beginning Fund Balance, January 1	\$ 167,768
Ending Fund Balance, December 31	\$ 210,550

In 2020, TBD funds were used to assist in the design of the JBLM North Access Improvements Project. Phase I of the construction will be constructed starting in May of 2021. The first phase of construction is expected to take 18 months to complete.

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: May 3, 2021	TITLE: Authorizing the execution of a contract with Greater Lakes Mental Health for two Mental Health Professionals.	TYPE OF ACTION: — ORDINANCE — RESOLUTION <u>X</u> MOTION NO. 2021-27 — OTHER
REVIEW:	ATTACHMENTS: Contract	

SUBMITTED BY: Michael Zaro, Police Chief

RECOMMENDATION: It is recommended that the City Council enter into an agreement with Greater Lakes Mental Health (GLMH) to continue to receive services from a Mental Health Professional (MHP) imbedded within the Police Department and an additional MHP working with both the Police Department and the Rental Housing Safety Program.

DISCUSSION: Police calls for service involving subjects experiencing mental illness account for a significant volume of calls for the Police Department's Patrol Division. Officers are typically limited to the hospital or jail as their primary options for resolution. These options are not necessarily the most appropriate for the person experiencing crisis and can often tie up patrol resources for an extended period of time. Additionally, officers repeatedly deal with the same subjects who lack any structured follow up on a treatment plan. Through this contract, GLMH will continue to provide a dedicated MHP working out of the police station who will respond with officers to calls for service involving mental crisis and identify the most appropriate resolution. He or she would also provide much needed follow-up to ensure the patients are following their prescribed treatment plan. The third service the MHP will provide is in-service training for our officers on handling persons in crisis and the options available. While this agreement has been in place since 2015, a need was recently identified in our Rental Housing Safety Program (RHSP) for the same services to help tenants they frequently come in contact with. The second MHP identified in this agreement will work part time with the RHSP and part time with the Police Department supporting the existing MHP.

ALTERNATIVE(S): Reject the contract and end the program.

FISCAL IMPACT: Contract costs for both MHPs will not exceed \$160,862 in 2021 or \$226,683 in 2022.

Mike Zaro
Prepared by

Department Director


City Manager Review

**CONTRACT BETWEEN THE CITY OF LAKEWOOD AND GREATER LAKES
MENTAL HEALTHCARE**

THIS AGREEMENT is entered into this _____ day of _____ by and between the City of Lakewood, Washington, a code city duly organized and existing pursuant to laws of the State of Washington, hereinafter referred to as the "City" and Greater Lakes Mental Health Foundation, doing business as Greater Lakes Mental Healthcare, hereinafter referred to as the "Agency".

WHEREAS, the City has determined the need to have certain human services performed for its citizens; and,

WHEREAS, the City desires to have the Agency perform such services pursuant to certain terms and conditions;

NOW THEREFORE, IN CONSIDERATION OF the mutual benefits and conditions hereinafter contained, the parties hereto agree as follows:

- I. **Scope of Services to be performed by Agency.** The Agency shall provide the services of a Mental Healthcare Professional (MHP). This individual shall work in the field with Neighborhood Police Officers on calls involving mentally unstable persons.

The MHP will work in the field to assess, stabilize, and provide alternatives, if possible, to jailing or hospitalizing a person with mental illness that is the subject of a call for police service. The **MHP** and/or GLMHC will provide training to police officers, first responders and managers of hotel/motel group.

In performing such services, the Agency shall at all times comply with all Federal, State, and local statutes, ordinances and rules applicable to the performance of such services and the handling of any funds used in connection therewith. The Agency shall request, in writing, prior approval from the City whenever the Agency desires to amend the scope of services.

2. **Compensation and Methods of Payment.** The City shall pay the Agency for services rendered. The total amount to be paid for the first MHP shall not exceed \$100,862. in 2021 and \$105,556 in 2022. The total amount to be paid for a second MHP shall not exceed \$60,000 in 2021 and \$121,127.00 in 2022.

Payment will be made on a reimbursement basis no more than monthly and no less than quarterly for the duration of this project. The Agency will provide proof of hours worked and report data according to the list of deliverables listed in Exhibit A.

The requests for such payment shall be processed for payment by the City in the normal course after that date, in accordance with the terms hereof.

3. **Agency Budget.** The Agency shall apply the funds received under this Agreement with

The maximum limits set forth in this Contract solely to the services specified in Paragraph I, above, and according to the approved budget of the Agency. Prior approval from the City is required whenever the Agency desires to amend its budget by transferring funds among the budget categories.

4. Duration of Contract. This Contract shall be in full force and effect for a period commencing on the 1st day of January, 2021 and ending on the 31st day of December, 2022, unless sooner terminated under the provisions hereinafter specified.
5. Independent Contractor. The Agency and City agree that the Agency is an independent contractor with respect to the services provided pursuant to this Contract. Nothing in this Contract shall be considered to create the relationship of employer and employee between the parties hereto. Neither the Agency nor any employee of the Agency shall be entitled to any benefits accorded City employees by virtue of the services provided under this Contract. The City shall not be responsible for withholding or otherwise deducting federal income tax or social security payments or contributing to the State Industrial Insurance Program, or otherwise assuming the duties of an employer with respect to the Agency, or any other employee of the Agency.
6. Indemnification and Defense. The Agency shall defend and indemnify, and save harmless the City, its agents and employees, from and against any and all liability arising from injury or death to persons or omissions of the Agency, its agents, servants, officers or employees, irrespective of whether in connection with such act or omission it is alleged or claimed that an act of the City, or its agents or employees caused or contributed thereto. In the event that the City shall elect to defend itself against any claim or suit arising from such injury, death or damage, the Agency shall, in addition to indemnifying and holding the City harmless from any liability, indemnify the City for any and all expense incurred by the City in defending such claim or suit, including attorney's fees.
7. Insurance.
 - A. The Agency shall procure and maintain in full force throughout the duration of the Contract commercial comprehensive general liability insurance with a minimum coverage of \$1,000,000.00 per occurrence combined single limit and \$2,000,000.00 in the aggregate for personal injury and property damage and non-owned automobile. The said policy shall name the City as an additional named insured on the insurance policies, and **A COPY OF THE ENDORSEMENT NAMING THE CITY AS AN ADDITIONAL INSURED SHALL BE ATTACHED TO THE CERTIFICATE OF INSURANCE.**
 - B. In addition to the insurance provided for in Paragraph A above, the Agency shall maintain or insure that its professional employees or contractors maintain professional liability insurance in the event that services delivered pursuant to this Contract, either directly or indirectly, involve providing professional services. Such professional liability insurance shall be maintained in an amount not less than \$500,000.00 combined single limit per claim and in the aggregate. For the purposes of this paragraph "professional service" shall mean services provided by a physician, licensed psychologist, or other licensed professional.

- C. Certificates of coverage as required by Paragraph A and B above shall be delivered to the City within fifteen (15) days of execution of this contract. Further, it is the responsibility of the Agency to ensure a valid certificate of insurance is in effect at all times throughout the course of this contract. Requests for reimbursement under this contract may be withheld until such time as a valid certificate of insurance is provided to the City.
8. Record Keeping and Reporting.
- A. The Agency shall maintain accounts and records, including personnel, property, financial and programmatic records which sufficiently and properly reflect all direct and indirect costs of any nature expended and services performed in the performance of this Contract and other such records as may be deemed necessary to the City to ensure proper accounting for all funds contributed by the City for the performance of this Contract and compliance with this Contract. The Agency shall notify the City within ten (10) days of any change in program personnel.
 - B. These records shall be maintained for a period of seven (7) years after termination hereof unless permission to destroy them is granted by the office of the archivist in accordance with Chapter 40.14 RCW and by the City.
 - C. The Agency shall provide quarterly activity reports to the City containing actual outcomes, indicators and an evaluation of the program. The goals, activities to accomplish those goals and outcome-based metrics shall be mutually agreed upon by the parties and are outlined in Exhibit A.
 - D. The Agency, at the request of the City, shall make public presentations regarding the program funded by the City. Such presentation shall be prepared in advance and approved by the City.
 - E. The City of Lakewood places a high priority on collaboration. As such, the Agency shall provide representation at the monthly Collaboration Meeting.
9. Audits and Inspections. The records and documents with respect to all matters covered by this Contract shall be subject at all times to inspection, review or audit during the performance of this Contract. The City shall have the right to an annual audit of the Agency's financial statements and condition. In addition, the Agency is subject to an Annual site monitor of the systems supporting Outcomes Based Evaluation. The City shall have the right to an annual inspection of the Agency's data systems for tracking outcome achievement. Areas of default noted during the annual inspection may demand additional site monitoring(s).
10. Termination. The City of Lakewood may suspend or terminate this Agreement in whole or in part for convenience, upon 15 days written notice to the Agency. If the Agency's insurance coverage is canceled for any reason, the City shall have the right to terminate this Contract immediately. If for any reason an agency does not comply with all aspects of this contract, including mandatory reports, such non-compliance may jeopardize the agency's ability to receive future funding.

Further: This Contract may be terminated upon evidence of the following conditions:

- I. Agency is no longer operating. The Contract shall be terminated within 10 days of notification that the Agency is no longer operating and performing the duties identified in "Exhibit A" Scope of Services.
- L Change in Scope of Services: Should the Agency no longer provide services identified in "Exhibit A" Scope of Services, the contract may be terminated for non- performance.
11. Discrimination Prohibited. The Agency shall not discriminate against any employee, applicant for employment, or any person seeking the services of the Agency to be provided under this Contract on the basis of race, color, religion, creed, sex, age, national origin, marital status or presence of any sensory, mental or physical handicap.
12. Assignment and Subcontract. The Agency shall not assign or subcontract any portion of the services contemplated by this Contract without the written consent of the City.
13. Entire Agreement. This Contract contains the entire agreement between the parties hereto and no other agreements, oral or otherwise, regarding the subject matter of this Contract shall be deemed to exist or bind any of the parties hereto. Either party may request changes in the Contract. Proposed changes which are mutually agreed upon shall be incorporated by written amendments to this Contract.
14. CONTRACTOR'S EMPLOYEES EMPLOYMENT ELIGIBILITY REQUIREMENTS. The Agency and any subcontractors shall comply with E-Verify as set forth in Lakewood Municipal Code Chapter 1.42. E-Verify is an Internet-based system operated by United States Citizenship and Immigration Services in partnership with the Social Security Administration. E-Verify is free to employers and is available in all 50 states. E-Verify provides -an automated link to federal databases to help employers determine -employment-eligibility-of new hires and the validity of their Social Security numbers. The Contractor shall enroll in, participate in and document use of E-Verify as a condition of the award of this contract. The Contractor shall continue participation in E-Verify throughout the course of the Contractor's contractual relationship with the City. If the Contractor uses or employs any subcontractor in the performance of work under this contract, or any subsequent renewals, modifications or extension of this contract, the subcontractor shall register in and participate in E-Verify and certify such participation to the Contractor. The Contractor shall show proof of compliance with this section, and/or proof of subcontractor compliance with this section, within three (3) working days of the date of the City's request for such proof.
15. Notices. Notices required by terms of this Contract shall be sent to the other party at the following addresses, unless otherwise requested, in writing, by one of the parties hereto:

TO THE CITY
City of Lakewood
Attn: Mike Zaro
Police Department
6000 Main St SW
Lakewood, Washington 98499

TO THE AGENCY:
Terri Card
President and CEO
Greater Lakes Mental Healthcare
(253) 620-5135
Terrie@glmhc.org

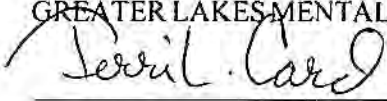
16. Applicable Law, Venue, Attorney's Fees. This Contract shall be Governed by and construed in accordance with the laws of the State of Washington. In the event any suit, arbitration, or other proceeding is instituted to enforce any term of this Contract, the parties specifically understand and agree that venue shall be properly laid in Pierce County, Washington. The prevailing party in any such action shall be entitled to its attorney's fees and costs of suit.

IN WITNESS WHEREOF THE PARTIES HERETO EXECUTED THIS CONTRACT AS OF THE DATE AND YEAR FIRST ABOVE WRITTEN.

CITY OF LAKEWOOD

GREATER LAKES MENTAL HEALTH FOUNDATION

John J. Caulfield
City Manager



Terri Card
President and CEO

Attest:

Briana Schumacher, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney

Exhibit A

Goal	Activity	Immediate/Short Term Disposition
Provide Alternatives to use of Emergency Room and Jails	MHP will provide structured assistance to persons in crisis	Client's remains at home Client referred to shelter Client referred to crisis bed Client referred to Recovery Innovations Client arrested/jail Client taken to ED Client referred to current Mental Health Center/Case Manager Client detained to Evaluation and Treatment Center Other
Goal	Activity	Follow-Up/Long Term Disposition
Improved Treatment for residents with mental illness	MHP will provide follow up services and document outcome status	Client contacted by MHP Client could not be reached/located Client declined assistance Client enrolled in mental health services Client provided with housing referrals Other
Goal	Activity	Outcome
More effective interaction between police and citizens with mental illness	Mental Health Training & Education	Who provided the training What was the focus of the training How many attended Duration



COMMUNITY SERVICES ADVISORY BOARD
Regular Meeting
Wednesday, March 17, 2021
Via Zoom

CALL TO ORDER

Sarah Yamamoto called the meeting to order at 5:35 p.m.

ATTENDANCE VIA ZOOM

Board Members Present: Sarah Yamamoto, Michael Lacadie, Edith Owen-Wallace, John Mayfield, and Dirk Wooten

Board Members Excused: Laurie Maus

Board Members Absent: Elisapeta Scanlan

Youth Council Representatives Present: None

City Council Liaison: Linda Farmer

Staff Members Present: Jeff Gumm, Martha Larkin

PUBLIC COMMENT

There was no one present who wished to comment.

OTHER BUSINESS

- **Introductions**
New Board member, Dirk Wooten introduced himself and shared his background. Members and staff then introduced themselves and welcomed Mr. Wooten.
- **Election of Vice-Chair**
Michael Lacadie nominated Edith Owen-Wallace and she agreed to serve. Voice vote was taken and Ms. Owen-Wallace was unanimously elected Vice-Chair.
- **Review of CDBG FY 2021 Annual Action Plan**
Jeff Gumm, Program Manager reviewed the CDBG FY 2021 Annual Action Plan. He gave detailed information on proposed programs and project funding. He answered questions about the Council's priorities and explained the pros and cons of funding housing, sidewalks and streetlights. Discussion ensued.
- **Review of CSAB Human Services Funding Process/Timeline**
Mr. Gumm reviewed the timeline for the upcoming Human Services process.

ADJOURNMENT

There being no other business, Ms. Yamamoto adjourned the meeting at 7:00 p.m.

Sarah Yamamoto (Digitally signed) 4/21/2021
Sarah Yamamoto, Chair Date



**PLANNING COMMISSION
REGULAR MEETING MINUTES
April 7, 2021
Zoom Meeting
6000 Main Street SW
Lakewood, WA 98499**

Call to Order

Mr. Don Daniels, Chair called the ZOOM meeting to order at 6:30 p.m.

Roll Call

Planning Commission Members Present: Don Daniels, Chair; Ryan Pearson, Paul Wagemann, James Guerrero, Phillip Combs and Linn Larsen (Connie Coleman-Lacadie arrived after approving vote on minutes)

Planning Commission Members Excused: None.

Commission Members Absent: None

Staff Present: David Bugher, Assistant City Manager for Development Services; Tiffany Speir, Long Range & Strategic Planning Manager; and Karen Devereaux, Administrative Assistant

Council Liaison: None

Approval of Minutes

The minutes of the meeting held on March 24, 2021 were approved as written by voice vote M/S/C Wagemann/Guerrero. The motion passed unanimously, 5 - 0.

Agenda Updates

None

Public Comments

This meeting was held virtually to comply with Governor Inslee's Emergency Proclamations 20-28 and its addendums. Citizens were encouraged to virtually attend and to provide written comments prior to the meeting. No public comments were received.

Public Hearings

None

Unfinished Business

None

New Business

Ms. Tiffany Speir discussed the staff recommendations on each of the following Comprehensive Plan Amendments 2021-01, -04, -06, -08, -10 and -11 as listed below and answered Commissioner's questions.

2021-01 Replacement of Comprehensive Plan Sustainability Chapter with an Energy and Climate Change Chapter

CEDD Recommendation: Approval

2021-04 Updates related to allowing and/or encouraging various housing types (e.g., transitory accommodations, accessory dwelling units, and "missing middle) housing.)

CEDD Recommendation: Approval

2021-06 Updates reflecting adoption of the Downtown Subarea Plan and the Lakewood Station District Subarea (LSDS) Plan.

CEDD Recommendation: Approval

2021-08 per Council action and analysis of affected areas:

I. *Parcels in Proximity to Berkeley Interchange* – Redesignate/rezone parcels 2200000172, -173, -192, -193, -210, -240, -250, -260, -270, -941, -942, and -950 from Single Family (SF) /Residential 3 (R3) to Neighborhood Business District (NBD)/Neighborhood Commercial 2 (NC2)

II. *Parcels included within pending Habitat for Humanity Project* – Redesignate/rezone parcels 0219212116, -0017, -056, -063 from Single Family (SF)/Residential 3 (R#) to Mixed Residential (MR)/Mixed Residential 2 (MR2)

III. Conduct review of the 2011 Tillicum Neighborhood Plan, the Tillicum Center of Local Importance (CoLI), and the text in Comprehensive Plan Sections 1.5, 2.5.1, and 4.5.3 and Goal LU-52, with the appropriate public outreach and participation, for potential updates and amendments as part of the 2022 or 2023 Comprehensive Plan amendment cycle.

CEDD Recommendation: Approval

2021-10 Text amendments to LMC Chapter 18A.40 expanding the list of water supply related facilities (water wells, culverts, water tanks) and sewer or pumping station facilities in the Lakewood development code.

CEDD Recommendation: Approval

2021-11 Text and Map amendments regarding Transitory Accommodations in response to 2020 ESSB 1754, adding "Religious Organizations; Hosting of the Homeless" to the Comprehensive Plan and LMC Title 18A.

Recommendation: Approval

The Commission would hold a virtual public hearing regarding the 2021 Comprehensive Plan Amendments on April 21, 2021. Consideration of the amendments and action on the 2021 CPAs would occur on May 5, 2021.

2020 Shoreline Restoration Report

Ms. Tiffany Speir introduced restoration committee speakers Mr. Al Schmauder, Ms. Janet Spingath, Ms. Rikki McGee, and Mr. Paul Bucich, City of Lakewood Public Works Engineering Director. All spoke on different segments of the work completed over the last year. Reports were presented on American Lake shoreline and invasive aquatic weeds, clams and snails; Waughop Lake treatments and non-chemical alternatives; Clover Creek stream management programs and flood plain zoning evaluations.

Committee members requested the commissioners to view the City of Bellingham's program presentation regarding successful prevention of zebra mussel infestations on their lakes and consider enforcing a boat launch fee collection at American Lake Park ramps to finance a boat inspection program. Commissioners requested the committee provide a cost breakdown of the suggested programs for consideration.

Report from Council Liaison

Councilmember Mr. Paul Bocchi updated commissioners on the following topics:

City Council held a public hearing on the Lakewood Station District Subarea Plan (LSDS) at Monday evening's meeting and would continue working on the adoption of the packet.

Reports from Commission Members and Staff

Future Planning Commission Agenda Topics

May 5:

- 2021 Comprehensive Plan Amendment Package Public Hearing
- Six Year Transportation Improvement Program 2022-2027

Regional Planning Land Use Updates

None

Other


Mr. Don Daniels, Chair, welcomed newest commission member, Mr. Linn Larsen, to the group.

Next Regular Meeting: The next regular meeting would be held on April 21, 2021.

Meeting Adjourned at 8:16 p.m.


Don Daniels, Chair
Planning Commission 04/21/2021
Karen Devereaux, Recording Secretary
Planning Commission 04/21/2021



To: Mayor and City Councilmembers
From: Tho Kraus, Assistant City Manager/Chief Financial Officer
Through: John J. Caulfield, City Manager 
Date: May 3, 2021
Subject: 2021 Carry Forward Budget Adjustment – Public Hearing

BACKGROUND

The proposed budget adjustment makes the following types of modification to the current biennium:

- Revise the estimated beginning fund balance to reflect the final 2020 ending fund balance
- Adjustments to incorporate items previously approved by the City Council;
- Appropriate projects funded by grants and contributions;
- New allocations on an exception basis.

Note: Changes since the proposed budget was presented at the April 26, 2021 City Council Study Session:

- *Fund 001 General Fund. Regarding Washington State Department of Commerce Grant, changed project name from “Lakeview Transit Area Subarea Plan” to “Lakewood Station Subarea Plan”.*
- *Fund 106 Public Art Fund. Increased funding by \$4,000 for a total of \$10,000 for four (4) signal box wraps. \$95,000 is carried over for the Colonial Plaza art, leaving \$32,500 available for major public art to be determined by the City Council.00.*
- *Fund 401 Surface Water Management Fund. Reduced carry forward for Waughop Lake Treatment from \$68,177 to \$20,000. The project is complete, however, the City is having volunteers continue to do monitoring and have a billing a two left for consultant pay. The project will be closed out completely at the end of 2021.*

The proposed budget adjustment continues following the City Manager’s directives and action strategies below (through 2020 and continue indefinitely through 2021/2022) to mitigate the impact of the COVID-19 pandemic, it is expected that by year-end, we will meet or exceed the 2022 adopted estimated General Fund ending fund balance.

- All current and future vacant positions will not be filled in the foreseeable future, with exceptions given funding sources, legal mandates and/or reporting requirements.
- All discretionary spending are on hold indefinitely. Only those expenditures related to basic services are authorized.

- Anything not covered above shall be approved in advance after review by Assistant City Manager/Chief Financial Officer and City Manager.
- Fleet & equipment, information technology, and property management projects and replacements shall only include essential items as well as those completed by December 31, 2020.
- Transportation, parks and sewer improvement projects will be continued as planned for 2020 given these projects are funded with very limited general government funds.
- Excess REET will be set aside to offset any decrease in 2020 REET collections and to ensure payment of debt service in 2020 and 2021.

The proposed budget adjustment does not include the following items which will be considered and potentially addressed as part of the 2021/2022 mid-biennial review:

- **ARPA (American Rescue Plan Act).** Allocate funds once the City Council has had the opportunity to further discuss and provide direction on the allocation.
- **Municipal Court Video and Audio Equipment.** Municipal court submitted an application in the amount of \$119,438 to AOC (Administration of Court) for CARES funding for audio/video replacement. In summary, the request is to: replace existing audio solution with new technology to include wireless microphones, handheld & lapel microphones; implement wireless content sharing providing real time collaboration during court proceedings utilizing an AirMedia presentation gateway solution; replace existing overhead speakers with new units to eliminate any overhead feedback or squeal; replace existing amplifiers, signal processors and mixing hardware; build the system around digital audio recordings software (currently For the Record 'FTR'); implement new video solution to include cameras, digital display screens, remote streaming capability along with local recording options; implement new video solution which provides enhanced streaming capability, numerous recording and archival options for long term storage of court proceedings & provide online; remote services and court proceedings for the public; add assistive listening technology; and comply with all state and federal guidelines pertaining to Covid-19.
- **Workforce Development.** The City was waiting to see if a competitive workforce partner would score high enough during the regular human services process to fill this role. None did so this is something Lakewood's Promise will speak with Council about during a joint session with the City Council. The estimated cost is \$50,000.
- **Volunteer Graffiti Program.** Create a volunteer graffiti program to include graffiti supplies and appropriate supervision to coordinate and manage the program. The city's maintenance and operations team regularly and proactively addresses the graffiti tagging citywide. When graffiti is identified on private property, the City's code enforcement team reaches out to have them address. In general, private property owners comply and clean their property. The biggest challenge is not cleaning the graffiti. Once graffiti is cleaned up, those who are doing this come back the next day or within days with more tagging. For the most part, the tagging is not gang related, it is a handful or even less number of members in the community who are doing this.
- **Opportunity Fund.** Create a fund for the purpose of accumulating and setting aside funds for future economic development and capital improvement purposes.

- **Transportation Benefit District.** Allocate the current biennium's \$20 vehicle licensing fee revenue in accordance with City Council direction, including the potential of a bond issue in support of transportation capital projects. As part of this process, update the list of TBD projects completed and eligible.
- **Transportation Mitigation Fees.** Life-to-date through December 2020 traffic mitigation fees received total \$103,505 and is expected to grow to \$200,000 in the near future according to the City's Assistant City Manager for Community and Economic Development Services. Plan for the use of these moneys is to first to construct a new traffic signal at Avondale and Gravelly Lake Drive to include signal coordination along the Gravelly Lake Drive Corridor. The estimated cost for a new traffic signal in 2020 dollars is roughly \$700,000 and is anticipated to be covered 50% by traffic mitigation funds matched by 50% City funds.

Abatement Funds. Demolition of the former QFC building that is under an abatement order and has been declared unsafe by the City's hearing examiner that will require additional abatement funds unless the property owner moves forward with securing a demolition permit to abate. The current estimated cost for the demolition including asbestos removal is \$315,000 and would likely be funded with a combination of available abatement funds and a financial contribution from the General Fund. Once completed, a lien would be placed on the property to ensure the owner reimburses the City of the abatement costs.

NEXT STEPS

- Adoption – May 17, 2021 Regular Council Meeting

PROPOSED BUDGET ADJUSTMENT SUMMARY

Year 2021:

- Increases beginning fund balance by \$25.37M, resulting in a revised estimate of \$41.94M;
- Increases revenues by \$22.56M, resulting in a revised estimate of \$97.48M;
- Increases expenditures by \$38.09M, resulting in a revised estimate of \$114.92M; and
- Increases ending fund balance by \$9.84M, resulting in a revised estimate of \$24.50M.

Year 2022:

- Increases beginning fund balance by \$9.84M, resulting in a revised estimate of \$24.50M;
- Increases revenues by \$0.71M, resulting in a revised estimate of \$77.07M;
- Increases expenditures by \$1.47M, resulting in a revised estimate of \$76.98M; and
- Increases ending fund balance by \$9.08M, resulting in a revised estimate of \$24.60M.

The table below provides a breakdown of the proposed budget adjustment (\$ in millions):

Fund Group	Beginning Fund Balance			Revenue			Expenditure			Ending Fund Balance		
	Current Budget	Prop Adj	Proposed Revised Budget	Current Budget	Prop Adj	Proposed Revised Budget	Current Budget	Prop Adj	Proposed Revised Budget	Current Budget	Prop Adj	Proposed Revised Budget
Total Year 2021	\$ 16.57	\$ 25.37	\$ 41.94	\$ 74.92	\$ 22.56	\$ 97.48	\$ 76.83	\$ 38.09	\$ 114.92	\$ 14.67	\$ 9.84	\$ 24.50
General	\$ 5.77	\$ 7.81	\$ 13.57	\$ 38.40	\$ 0.28	\$ 38.68	\$ 39.36	\$ 1.84	\$ 41.20	\$ 4.80	\$ 6.25	\$ 11.05
Special Revenue	\$ 2.67	\$ 4.83	\$ 7.50	\$ 7.29	\$ 11.70	\$ 18.99	\$ 6.84	\$ 14.63	\$ 21.47	\$ 3.11	\$ 1.91	\$ 5.02
Debt Service	\$ 0.71	\$ 0.28	\$ 0.99	\$ 2.76	\$ -	\$ 2.76	\$ 2.63	\$ 0.25	\$ 2.88	\$ 0.84	\$ 0.03	\$ 0.87
Capital Projects	\$ 1.39	\$ 7.61	\$ 9.01	\$ 15.91	\$ 10.14	\$ 26.05	\$ 17.10	\$ 17.25	\$ 34.35	\$ 0.21	\$ 0.51	\$ 0.71
Enterprise	\$ 1.50	\$ 4.32	\$ 5.82	\$ 5.45	\$ 0.00	\$ 5.45	\$ 5.07	\$ 3.44	\$ 8.51	\$ 1.88	\$ 0.89	\$ 2.77
Internal Service	\$ 4.54	\$ 0.52	\$ 5.05	\$ 5.11	\$ 0.43	\$ 5.54	\$ 5.82	\$ 0.70	\$ 6.52	\$ 3.82	\$ 0.25	\$ 4.07
Total Year 2022	\$ 14.67	\$ 9.84	\$ 24.50	\$ 76.36	\$ 0.71	\$ 77.07	\$ 75.51	\$ 1.47	\$ 76.98	\$ 15.52	\$ 9.08	\$ 24.60
General	\$ 4.80	\$ 6.25	\$ 11.05	\$ 40.23	\$ -	\$ 40.23	\$ 40.09	\$ 0.36	\$ 40.46	\$ 4.94	\$ 5.89	\$ 10.82
Special Revenue	\$ 3.11	\$ 1.91	\$ 5.02	\$ 7.45	\$ 0.00	\$ 7.46	\$ 7.13	\$ 0.63	\$ 7.75	\$ 3.44	\$ 1.29	\$ 4.73
Debt Service	\$ 0.84	\$ 0.03	\$ 0.87	\$ 2.77	\$ -	\$ 2.77	\$ 2.37	\$ 0.09	\$ 2.47	\$ 1.24	\$ (0.06)	\$ 1.18
Capital Projects	\$ 0.21	\$ 0.51	\$ 0.71	\$ 13.02	\$ 0.70	\$ 13.72	\$ 12.90	\$ 0.38	\$ 13.27	\$ 0.33	\$ 0.83	\$ 1.16
Enterprise	\$ 1.88	\$ 0.89	\$ 2.77	\$ 7.57	\$ -	\$ 7.57	\$ 7.60	\$ 0.00	\$ 7.60	\$ 1.85	\$ 0.89	\$ 2.73
Internal Service	\$ 3.82	\$ 0.25	\$ 4.07	\$ 5.33	\$ 0.01	\$ 5.34	\$ 5.42	\$ 0.01	\$ 5.43	\$ 3.73	\$ 0.25	\$ 3.98

GENERAL FUND ENDING FUND BALANCE

In support of the City's financial integrity, the City Council originally adopted on September 15, 2014, a set of financial policies including fund balance reserves totaling 12% of General/Street O&M Funds operating revenues as follows:

- 2% General Fund Contingency Reserves: The purpose of this reserve is to accommodate unexpected operational changes, legislative impacts, or other economic events affecting the City's operations which could not have been reasonably anticipated at the time the original budget was prepared.
- 5% General Fund Ending Fund Balance Reserves: The purpose of this reserve is to provide financial stability, cash flow for operations and the assurance that the City will be able to respond to revenue shortfalls with fiscal strength.
- 5% Strategic Reserves: The purpose of this reserve is to provide some fiscal means for the City to respond to potential adversities such as public emergencies, natural disasters or similarly major, unanticipated events.

With this budget adjustment, 2022 estimated General/Street O&M Funds ending fund balance of \$10.8M equates to 26% of General/Street O&M Funds operating revenues and is in alignment with the City's financial policies. The composition of the \$10.8M consists of \$4.9M or 12% allocated to General Fund Ending Fund Balance Reserves and \$5.9M Unreserved/Undesignated.

\$823K for 2% General Fund Contingency Reserves

\$2.06M for 5% General Fund Ending Fund Balance Reserves

\$2.06M for 5% Strategic Reserves

\$4.9M Total Reserves

\$5.9M Unreserved

\$10.8 Total Estimated Ending Fund Balance at 12/31/2022

PROPOSED BUDGET ADJUSTMENT DETAILS

The narrative below provides detailed information on the proposed budget adjustments. A summarized list is included as an attachment to this memo.

Fund 001 General

CD – Build Your Better Marketing Campaign, New/1-Time

Add \$141,500 in expenditures for this marketing campaign that began in 2018 with the purpose of increasing favorable perceptions of Lakewood as a great place to live, work, learn, recreate, and do business. The City Council approved a budget for 2019/2020 in the amount of \$179,250. Through video, social media, print, radio, and website the City was able to achieve over 2 million views, 38,913 clicks through to the website, 2,345 City Facebook page likes, 39,605 page views within the website, 1,226 “Better Business” PDF downloads, and 374 “Better Living” click through searches for homes in Lakewood. Survey performance indicates that the net promoter score increased by +10 for residents, and +2 for non-residents, meaning people were more likely to recommend Lakewood over time. The campaign concluded at the end of December 2020. In January 2021 engagement dropped off significantly. Brand awareness campaigns are measured in exposure over time. Therefore, a refresh of the brand assets with continued outreach to Builders and Warriors intensified outreach to Starters, and strengthening our relationship with Long-termers is recommended to build on the momentum we have gained, and to keep Lakewood, WA top of minds for residents, non-residents, businesses, and builders. The funds will be used for: \$20,000 refresh video + photo assets; \$2,500 digital ad design, landing pages; \$2,500 print add design, landing pages; \$2,500 brand awareness survey (x2); and \$114,000 digital, print, and radio spend.

CD – Washington State Department of Commerce Grant, Continuation / 1-Time

Carry forward \$48,000 in revenue for Washington State Department of Commerce interagency agreement with the City of Lakewood through Growth Management Services for E2SHB 1923 Grant Program selected actions to increase residential building capacity. Funds will be used to develop the Lakewood Transit Area Subarea Plan. The scope of work includes the following selected actions to increase residential building capacity under RCW 36.70A.600(1): adopting a form-based code in one or more zoning districts that permit residential uses; adopting a subarea plan pursuant to RCW 43.21C.420; and adopting a planned action pursuant to RCW 43.21C.440(1)(b)(22), except that an environmental impact statement pursuant to 43.21C.030 is not required for such an action.

CD - Comprehensive Plan Energy and Climate Change Grant, Continuation/1-Time

Carry forward \$20,000 in revenues and expenditures for the balance of the grant received from Washington State Department of Commerce Growth Management Services for 2020/2021 Climate Change Grant. The purpose of this grant is to assist the City with development of a new, separate Comprehensive Plan Energy and Climate change Chapter that includes goals, policies and objectives and replaces the current sustainability chapter.

CD - WSDOT Relocation / Pacific Highway Redevelopment – Continuation / 1-Time

Carry forward balance of \$32,109 for Pacific Highway conceptual design and critical pathway development.

CD - Oak Tree Mitigation, Continuation / 1-Time

Appropriate the balance of \$55,446 for projects that meet certain program criteria.

PK – Human Services Programs – Lakewood’s Promise, Continuation/1-Time

Carry forward \$15,000 in expenditures for Lakewood’s Promise for Youth Council (\$5,000) and Work Force Development (\$10,000). In addition to supplementing the Human Services Coordinator’s salary and benefits, the Lakewood’s Promise budget supports four activities: 1) supplements the Youth Summit and other Youth Council activities; 2) Family Workforce Development initiatives, 3) Youth Mental Health initiatives, and 4) Lakewood’s Promise Advisory Board’s communications capacity. All these activities were delayed in 2020 due to COVID-19. The Youth Summit was cancelled weeks before implementation and remaining human services staff capacity was focused on supporting expenditures related to CARES Act funding instead of further developing the workforce development initiative started earlier in 2020.

PK – Human Services Programs – Living Access Support Alliance (LASA), Continuation/1-Time

Carry forward \$15,000 in expenditures for LASA. The City contracted with LASA to provide assistance to Lakewood residents. The City received CARES funds and contracted with LASA to provide rental assistance to Lakewood residents. They did not have the capacity to continue with the utility assistance program. This program was not funded in 2021 so the remaining contract of \$15,000 will be used to provide this important service to Lakewood residents.

PK – Human Services Programs – Lakewood’s CHOICE Contract Increase, Continuation/1-Time

Add \$20,000 for amendment received from Washington State Health Care Authority. Carry forward \$70,721 in revenues and expenses for the balance of the grant. This grant funds programs for the substance abuse prevention of the youths of Lakewood.

PK – Fort Steilacoom Park Cemetery Grant, Continuation/1-Time

Add \$907 in expenditures for the remaining grant balance for cemetery sign replacement at Fort Steilacoom Park, funded by a state grant from the Department of Archaeology and Historic Preservation. The purpose of the grant is to provide support for the preservation of the historic cemetery in Lakewood and Pierce County.

LG – STOP Grant, New/1-Time

Add \$30,313 revenue and expenditures in 2021 for FY 2019 grant award addendum from the US Department of Justice through the Washington State Department of Commerce. These funds are provided for a part-time office assistant position to provide support on STOP activities that will enhance prosecution’s response to adult or teen victims of domestic violence. The effective date is January 1, 2021 through December 31, 2021.

MC – Office of Public Defense Grant, Continuation / 1-Time

Carry forward \$16,875 from the 2020 grant balance and add \$37,000 in revenues and expenditures for the 2021 grant received from Washington State Office of Public Defense. The funds must be used according to the City’s grant agreement GRT20018, to make and sustain appreciable demonstrable improvements in public defense as required by state law. The purpose of this grant is for social work services for the public defense team, interpreter services for attorney-client communications, public defense training, investigation services, and expert witnesses.

MC – 2020 Court Cost True Up, Continuation/Ongoing

Reduce municipal court contract revenues by \$72,555 for 2021 based on 2020 court costs true up. The original budgeted and new revenue estimates are as follows:

Jurisdiction	Original Estimate	Revised Estimate	Change
University Place	\$20,000	\$6,000	(\$14,000)
Steilacoom	\$103,000	\$63,916	(\$39,084)
DuPont	\$87,550	\$68,079	(\$19,471)
Total	\$210,550	\$137,995	(\$72,555)

PD – Ammunition, Continuation/1-Time

Carry forward \$9,870 in expenditures for ammunition ordered in July 2020, however, will not be delivered until Q1 2021. Ammunition has become a critical commodity with delivery times of six-months or more, which was not known when the order was originally placed. The current annual ongoing ammunition budget is \$68,000.

PD – City of Tacoma Radio Fees, New/Ongoing

Add \$28,000 in each year of 2021 and 2022 for City of Tacoma's radio system access fee. The fee is based on City of Tacoma's annual net operating costs divided by total quantity of radios using the system. Net operating costs are calculated by deducting total external revenue from total operating costs. Since 2015 a portion of annual external revenue has been approximately \$440,000 in radio system fees from SS911. This amount includes Tacoma's cost for supporting and upgrading or replacing their dispatch consoles, system management costs, radio system access fees for SS911 dispatch consoles, etc. Traditionally, SS911 has paid this fee from cash reserves. By doing so they did not pass those costs on to law enforcement and fire agencies as part of their dispatch fees. Facing a 2021 COVID related sales tax revenue decline, SS911 decided to use cash reserves for their other operating costs and stop using reserves to pay for radio system access fees. The current annual budget of \$117,560 increases to \$145,560 with this adjustment.

PD – Jail Services, New/Ongoing

Add \$250,000 per year for jail services due to potential changes/elimination of the Nisqually Jail contract. The current annual budget of \$700,000 increases to \$950,000 with this budget adjustment.

PD – SRT Equipment, Continuation/1-Time

Carry forward \$40,000 to complete the purchase of the SRT equipment.

PD – Sniperweek Training, Continuation/1-Time

Carry forward \$1,507 for two police officers to attend Sniperweek training. Registration and lodging expenses were paid and subsequently refunded last year. However, airfare was retained by the airline to be credited on future flights booked by two authorized police officers.

PD – Criminal Justice Training Commission Grant, Continuation/1-Time

Add \$26,266 in revenue and expenses for wages and benefits of having a training officer in administering, conducting, and presenting this state's Basic Law Enforcement Academy program.

PD – FBI Innocence Lost Grant, Continuation/1-Time

Add \$28,000 in revenue for overtime work of two officers with the FBI in targeting the prosecution of organized crime groups responsible for the promotion of prostitution, specifically juvenile prostitution, interstate, or through the use of interstate commerce, drug trafficking, money laundering and alien smuggling.

PD – Emergency Management Performance Grant COVID, New/1-Time

Add \$2,689 in revenue and expenditures for wages and benefits of Assistant Emergency Management Performance Coordinator position. These funds support the collection of data and ongoing communications of community lifelines related to the response to COVID. These funds increase the original grant from the Washington State Military Department from \$11,210 to \$13,899. The end date for this grant is 12/31/2021. The total match amount is increased to \$13,899 which is budgeted in the City of Lakewood's police department budget.

Internal Service Charges, Continuation/1-Time

See internal services funds for additional information.

- \$5,000 Fleet & Equipment
- \$32,500 Property Management
- \$314,863 Information Technology

Internal Service Charges, Continuation/Ongoing

See internal services funds for additional information.

- \$9,463/Year Risk Management

Transfers to Property Abatement Fund, New/1-Time

See Rental Housing Safety Program for additional information.

- \$24,287 to cover 2020 deficit

Transfers to Parks CIP, New/1-Time

Total \$638,750 - see Parks CIP for additional information.

- \$75,000 New Gateways
- \$473,750 American Lake Park – North Parking Lot
- \$65,000 Springbrook Park Property Acquisition
- \$25,000 FSP Barn Demolition

General/Street Fund Subsidy, New/Ongoing

See Street Fund for additional information.

- \$896/Year Risk Management

General/Street Fund Subsidy, Continuation/1-Time

See Street Fund for additional information.

- \$20,139 Non-Motorized Transportation Plan Update
- \$27,400 Fleet & Equipment
- \$27,921 Information Technology

Fund 101 Streets O&M

Non-Motorized Plan Update, Continuation/1-Time

Add \$20,139 funded by project balance. As part of the 2019/2020 biennial budget, in support of the City Council goal of Dependable Infrastructure (increase connectivity and accessibility), the City Council allocated \$50,000 to update the current non-motorized transportation plan that was last updated in 2009. The updates needed include relevant policy updates and capital improvement plan. The plan is scheduled to be completed in 2022.

Internal Service Charges, Continuation/1-Time

See Street Fund for additional information.

- \$27,921 Information Technology
- \$27,400 Property Management

Internal Service Charges, New/Ongoing

See Street Fund for additional information.

- \$896/Year Risk Management

Fund 102 Real Estate Excise Tax

Transfers to Transportation CIP, New/1-Time

Total \$763,200 - See Transportation CIP for additional Information.

- \$139,000 to 302.0024 Steilacoom Blvd-Farwest Drive to Phillips Road (Design & ROW)
- \$406,200 to 302.0024 Steilacoom Blvd-Farwest Drive to Phillips Road (Construction)
- \$118,000 to 302.0151 South Tacoma Way Between 96th St South & Steilacoom Blvd
- \$100,000 to 302.0164 Sidewalks: Farwest Drive from 112th to Lakes HS & 100th to Steilacoom Blvd

Fund 104 Hotel-Motel Lodging Tax

Prior Year Capital Grant Balances, Continuation/1-Time

Add \$140,941 to transfer prior year lodging tax revenues in accordance with LTAC grant award for capital purposes for Parks CIP 301.0006 Gateways (at Berkley & Union and Woodbrook Business Park).

Fund 105 Property Abatement/Rental Housing Safety Program /1406 Affordable Housing

Property Abatement, Continuation/1-Time

Add \$610,386 in expenditures funded by program balance. The Property Abatement portion of this fund accounts for projects that the City has identified and processed through the abatement program. All revenue and the rightful recovery of those project expenses, along with all revenues from fees, fines, and interest, and other rightful recoveries from those projects are deposited into the program for the purpose of funding additional abatement projects.

Rental Housing Safety Program, Continuation/1-Time

Add \$24,287 transfer in from General Fund to cover 2020 program deficit. 2020 actual revenues comprising of program fees \$179,398 and General Fund \$25,000 totaled \$204,398 while expenditures totaled \$263,719. On August 1, 2016, the Lakewood City Council approved Ordinance No. 644 creating a Rental Housing Safety Program (RHSP). The program requires all residential rental properties (apartments, single family homes, duplexes, etc.) within Lakewood city limits to be registered. The program is designed to ensure that all rental housing units comply with specific life and safety standards and are providing a safe place for tenants to live. As of October 4, 2017, all rental properties owners will be required to register their property with the City every year and have the property inspected once every five years.

1406 Affordable Housing, Continuation/1-Time

Add \$72,315 in expenditures funded by program balance. On March 2, 2020, the City Council approved Ordinance 731 relating to local sales and use tax, authorizing the maximum capacity of the tax authorized under the provisions of Substitute House Bill 1406 for affordable and supporting housing. The revenue to the City is a credit of the state's sales tax. With adoption of this ordinance, the City is able to impose the rate of 0.0073%. According to the Department of Revenue (DOR), the maximum amount the City may receive is \$98K per state fiscal year for twenty years totaling an estimated \$1.95M. The City notified DOR on March 13, 2020 and DOR began imposing the tax effective May 1, 2020. The City received its first full distribution amount in July 2020 with some funds trickling through in May and June due to early returns filed. The direction from the Lakewood City Council is to use the funds in conjunction with the City's CDBG Major Home Repair Program, CDBG Major Home Repair and Sewer Loan Program, and HOME Housing Rehabilitation Loan Program given that there is a high demand for home repair and rehabilitation loans in the City. The City anticipates using the 1406 Affordable Housing Program funds for CDBG Major Home Repair Program in 2021.

Fund 106 Public Art

Arts Commission, Continuation & New/1-Time

Add \$4,000 for Arts Commission funded by project balance and add new funds of \$4,000 for a total increase of \$8,000 resulting in a project budget of \$10,000. The Arts Commission received allocations of \$2,000 in 2019 and \$2,000 in 2020. The 2019 funds were not spent and rolled carried over to 2020 and due to the COVID-19 pandemic a number of activities and events were cancelled. The Arts Commission requested the funds to be carried forward to add to the existing 2021 budget to support the signal box wrap art program. A call to artists will occur in 2021 to design artwork to wrap various signal boxes around the city. More boxes will be added as sponsorships are found to expand the program. Cost per signal box is approximately \$2,500 per box.

Colonial Plaza Art, Continuation/1-Time

Add \$95,000 for the Colonial Plaza art funded by project balance.

Major Public Art, Continuation/1-Time

Continue the practice of earmarking \$32,500 for Arts Commission funded by program balance for major public art as determined by the City Council.

Fund 180 Narcotics Seizure

Add \$106,196 funded by 2020 ending fund for balance eligible narcotics seizure related activity. The purpose of this fund is to track assets seized as a result of involvement with the illegal sale, possession, or distribution of drugs or controlled substances, and for the purchase of controlled substances or drugs by law enforcement officers or agents, as well as other expenses to enhance and improve law enforcement activities having a close and demonstrable relationship to enforce enforcement of controlled substances. Funds may not be used to supplant existing funding sources.

Fund 181 Felony Seizure

Felony Seizure, New/1-Time

Add \$47,837 in expenditures funded by felony seizure related activity. The purpose of this fund is for tracking assets seized under RCW 10.105.101 and the related expenditures. The state statute authorizes the seizure of assets that have been or was actually employed as an instrumentality in the commission or in the aiding or abetting in the commission of any felony, or which was furnished or was intended to be furnished by any person in the commission of, as a result of, or as a compensation for the commission of, any felony, or which was acquired in whole or in part with the proceeds traceable to the commission of a felony. Funds shall be used exclusively by the City in the expansion and improvement of law enforcement activity; however may not be used to supplant existing funding sources.

Fund 182 Federal Seizure

Federal Seizure, Continuation/1-Time

Add \$40,906 for purchase of replacement boat offset by a reduction in the current existing budget, funded by federal seizure related revenue. The purpose of this fund is to track the revenues associated with assets seized as a result the Police Department working in conjunction with federal law enforcement.

The Federal Equity Sharing Guidelines lists the following (funds shall be used to increase or supplement and not be used to replace or supplant):

Permissible Uses: law enforcement investigations; law enforcement training, law enforcement and detention facilities; law enforcement equipment; law enforcement travel and transportation; law enforcement awards and memorials; drug and gang education awareness programs; matching fund for grants; pro rata funding of the law enforcement agency's percentage of costs associated with supporting multi-agency items or facilities; asset accounting and tracking of expenditures of federally shared funds; language assistance services in connection with law enforcement activity; transfers of cash to other law enforcement agencies; support of community-based programs (cash transfers to community-based programs are not permitted); and windfall situations to provide additional support to community-based programs.

Impermissible Uses: Salaries and benefits of permanent law enforcement personnel, except in limited circumstances (i.e. express statutory authorization, overtime of officers and investigators, new positions and temporary or not-to-exceed one year appointments and salary of an officer hired to replace an officer assigned to a task force, specialized programs that generally to not involve traditional law enforcement functions); use of forfeited property by non-law enforcement personnel; payment of education-related costs; uses contrary to the laws of the state or local jurisdiction; non-official government use of shared assets; purchase of food and beverage (except for conference and meals during local operations); extravagant expenditures

Fund 190 Community Development Block Grant

2021 CDBG Grant Award Allocation, Continuation/1-Time

- Add 2021 CDBG award totaling \$573,352 as follows:
 - \$264,107 for Housing Program - Major
 - \$114,670 for Housing Program – Administration
 - \$15,000 for Housing Program – Administration of HOME
 - \$50,000 for Public Service – Counseling Foreclosure
 - \$129,575 for Physical Improvements 302.0024 Phillips Road Sidewalks
- Add 2021 CDBG reprogrammed funds totaling \$199,133 as follows:
 - \$170,425 for Physical Improvements 302.0024 Phillips Road Sidewalks
 - \$28,708 for Housing – Emergency Assistance for Displaced Residents (EADR)

2021 HOME Funds, Continuation/1-Time

Add \$321,730 for Affordable Housing & Program Administration.

Prior Years CDBG Funds, Continuation/1-Time

Carry forward \$829,254 for prior years CDBG Funds as follows:

- \$77,063 Housing Programs
- \$752,191 Major Home & Sewer Repair Program

Prior Years HOME Funds, Continuation/1-Time

Carry forward \$929,468 for prior years HOME Funds as follows:

- \$684,748 for Affordable Housing Program
- \$253 for Down Payment Assistance Program
- \$244,466 for Housing Rehabilitation Program

CDBG CARES (Coronavirus Aid, Relief, and Economic Security Act) – CV2 (Round 1), Continuation /1-Time

Allocate the remaining balance of \$287,245 (original amount was \$350,611) as follows:

- \$246,701 reallocated from economic development assistance for small business CV3 Mortgage and Rental Assistance.
- \$40,544 for the purpose of providing emergency economic development assistance for small businesses

CDBG CARES (Coronavirus Aid, Relief, and Economic Security Act) – CV3 (Round 3), Continuation /1-Time

Add \$456,726 in revenues and expenditures for CDBG CARES Act funding for the purpose of providing emergency mortgage assistance to private mortgage holders.

CDBG CARES (Coronavirus Aid, Relief, and Economic Security Act) – CV2 (Round 2)– New/1-Time

The City of Lakewood has been notified by the Washington State Department of Commerce of its eligibility to receive \$136,706 in CDBG-CV2 funding through its status as a CDBG entitlement community. These funds are authorized under the Coronavirus Aid, Relief, and Economic Security Act (CARES Act), Public Law 116-136. CDBG-CV funding is to be used to prevent, prepare for, and respond to the social and economic impacts of the coronavirus outbreak. Eligible activities include: 1) Public service activities; 2) Housing-related activities; 3) Public improvements and facilities; 4) Real property acquisition; 5) Economic development activities; and 6) General administrative and planning activities. All CDBG funded activities must benefit low-and moderate-income persons or meet the CDBG urgent need national objective criteria. Application to receive funds must be received by Commerce no later than April 30, 2021.

CDBG – Restricted Funds, Continuation/1-Time

Earmark \$1,382,124 restricted funds for future loan programs. The required accounting does not recognize revenues or expenditures since these are revolving funds.

Fund 191 Neighborhood Stabilization Program

NSP1 (Neighborhood Stabilization Program 1) Continuation/1-Time

Eliminate the original budget estimates for revenues and expenditures of \$78,000 and replace with current revenue estimate of \$42,000 and expenditure estimate of \$297,114 (2020 program balance \$255,144 and abatement program income and interest \$42,000). The program provides funding to purchase and redevelop low income, foreclosed and abandoned residential properties and/or structures. Revolving funds are used to pay for program costs. In January 2020, CED met with the Washington State Department of Commerce to close out NSP1 funds. Upon formal written notification of the close-out, if the City has any remaining NSP1 funds after five years of close out date, the City reprogram unspent funds with no restrictions, meaning the City may use the funds for example, for abatements, set up a housing loan program similar to CDBG/HOME but without all of the federal requirements or even put it back to the General Fund.

Fund 192 Office of Economic Adjustment/South Sound Military Communities Partnership

SSMCP Operations, Continuation / Ongoing

Increase revenue estimate by \$8,500 to \$236,000 and increase expenditures by \$26,345 to \$253,845. The operations budget includes personnel costs for the SSMCP/OEA Program Manager and Program Coordinator positions.

Office of Economic Adjustment – Growth Management, Grant/1-Time

Add \$643,500 in revenue and expenditures received from the Department of Defense for the Growth Management grant project at Joint Base Lewis McChord. The grant period is from January 1, 2021 through April 1, 2022. The grant requires an in-kind match of \$71,500. This match is comprised of \$70,550 in SSMCP personnel costs and \$950 in local mileage. The grant duration is for 15 months.

North Clear Zone Property Purchase - Tactical Tailor Building

Add \$6,865,728 for the North Clear Zone Property Purchase (Tactical Tailor Building).

The budget adjustment results in a life-to-date through 2020 cost estimate of \$6,865,728 funded by:

- \$3,517,595 United States Air Force (Amount carried forward from 2020)
- \$1,500,000 Department of Defense REPI (Readiness & Environmental Protection Integration) Funds
- \$768,133 WA State Department of Commerce
- \$580,000 Pierce County 2020 Budget Funds
- \$500,000 WA State Capital Budget Funds
- \$6,865,728 Total Sources**

Fund 195 Public Safety Grants

Washington Traffic Safety Commission – Impaired Driving Emphasis, Continuation/1-Time

Add a total of \$7,400 for WTSC Impaired Driving Emphasis grant. No local match is required. The grant provides funding for overtime wages and related benefits for law enforcement personnel to participate in scheduled local and multi-jurisdictional DUI patrols.

Washington Traffic Safety Commission – Motorcycle, Continuation/1-Time

Add \$2,400 for remaining grant balance for 2021. No local match is required. The grant provides funding for overtime wages and related benefits for law enforcement personnel to participate in scheduled local motorcycle patrols.

Emergency Management Planning, Continuation/1-Time

Add \$25,439 for remaining grant balance for period of January 1, 2021 through August 31, 2021. The grant is for the purpose of paying the salary and benefits of an emergency management coordinator in cooperation with West Pierce Fire and Rescue. This grant requires a match which is already budgeted in the General Fund Emergency Management budget in addition to in-kind Assistant Police Chief personnel cost and balance of coordinator's salary.

Bulletproof Vest Partnership, Continuation /1-Time

Add \$12,400 in revenue and expenditures for FY-2020 new award for vests purchased from April 1, 2020 through August 31, 2022. Add carry forward of \$6,961 for remaining grant balance for January 1, 2021 through August 31, 2021. This funding is for armored vests that meet the National Institute of Justice applicable ballistic and stab resistant standards for officers. There is a 50% local match required. This match is already budgeted in the General Fund.

Washington State Parks Recreational Boating Safety, New/1-Time

Add \$11,867 for new grant period March 1, 2021 –September 30, 2021. This grant provides funding for officer overtime for emphasis patrol, vessel safety inspections, and educational boater safety related activities. This grant has in-kind (such as personnel, maintenance of boats, fuel, trailers, boat house) matching requirements of \$6,435.

Pierce County – Phlebotomy – Continuation/1-Time

Add a total of \$2,721 for grant. The purpose of this funding is for the purchase of equipment and supplies to establish a regional phlebotomy site for blood draws.

Washington State Patrol- Mobile Impaired Driving Unit (MIDU), New/1-Time

Add \$20,000 for mobile impaired driving unit grant. These funds are for providing a certified Medical Assistant Phlebotomist to work in conjunction with the WSP various locations around the State of Washington.

US DOJ Justice Assistance Grant – Rifle Noise Suppressors, Continuation/1-Time

Add \$2,069 grant for period 1/1/2020-9/30/2022. This grant is for the purpose of purchasing rifle noise suppressors for patrol rifles.

US DOJ Justice Assistance Grant - COVID-19 Supplemental, Continuation/1-Time

Add \$40,541 in revenues and expenditures for grant received from Department of Justice (JAG) COVID-19 Supplemental. The Coronavirus Emergency Supplemental Funding (CESF) Program allows local governments to support a broad range of activities to prevent, prepare for, and respond to the coronavirus. The City of Lakewood will use these funds from equipment and supplies.

US DOJ Justice Assistance Grant - 2020 Bicycles, Continuation/1-Time

Add \$36,959 in revenues and expenditures for grant received from Department of Justice (JAG) for 2020. The City of Lakewood will use these funds to purchase bicycles for the Lakewood Police Department.

Washington Auto Theft Prevention Authority (WAPTA), Continuation/1-Time

Add \$124,500 in revenues and expenditures for grant received from Washington Auto Theft Task Prevention Authority. This funding through the City of Federal Way provides for the position of an Auto Crimes Enforcement Task Force Investigators who are assigned to the WAPTA program. There is no local match required. The mission of the WAPTA is to prevent and reduce motor thefts in the State

Office of Justice Programs Veterans Court, Continuation /1-Time

Add \$92,491 remaining grant balance from prior years. This is an enhancement grant for the Veterans Treatment Court and serves Type a non-violent offenders. The total grant amount is \$500,000 and runs through December 31, 2022. In addition to personnel costs, the grant includes travel and training, supplies, and subaward for prosecution, consultant evaluator/information manager, and public defense.

Fund 202 LID Debt Service

Bond Redemption, Continuation/1-Time

Add \$248,039 in 2021 and \$92,161 for redemption of LID Bonds.

Fund 301 Parks Capital

301.0003 Harry Todd Park Phase 1 & 2, Continuation/1-Time

Add \$1,415,918 in project expenditures funded by project balance \$878,634, grants \$527,284 and contribution from Commencement Bay Rowing Club \$10,000.

This budget adjustment results in a life-to-date 2022 cost estimate of \$3,596,435 funded by:

- \$1,080,000 RCO Grant
- \$1,126,718 ALEA Grant
- \$10,000 Contributions/Donations
- \$404,474 Sale of Lakeland Property
- \$107,129 General Fund
- \$618,114 REET
- \$200,000 LTAC
- \$50,000 SWM
- \$3,596,435 Total Sources**

301.0005 Chambers Creek Trail, Continuation/1-Time

Add \$236,000 in project expenditures funded by project balance \$136,000 and transfer in from SWM for storm related elements of the project.

This budget adjustment results in a life-to-date 2022 cost estimate of \$325,000 funded by:

- \$79,591 General Fund
- \$100,000 REET
- \$20,409 Paths & Trails
- \$125,000 SWM
- \$325,000 Total Sources**

301.0006 Gateways, Continuation/1-Time

Add \$190,941 in project expenditures funded by project balance \$50,000 and LTAC \$140,941. Funds will be used to construct two gateways in conjunction with the WSDOT JBLM project, one at Berkley & Union and one in the area of the Woodbrook Business Park.

301.0006 Gateways, New/1-Time

Add \$75,000 in project expenditures funded by General Fund. Funds will be used to construct a new gateways at 84th & Tacoma Mall Boulevard. This estimate is based on standard costs and does not include costs of potential easements (unknown at this time).

301.0014 Fort Steilacoom Park/South Angle Lane, Continuation/1-Time

Add \$353,241 in project expenditures funded by project balance \$303,241 and WWRP grant \$50,000.

This budget adjustments results in a life-to-date through 2022 cost estimate of \$1,522,536 funded by:

- \$499,998 General Fund
- \$21,473 Contributions/Donations
- \$350,000 General Fund
- \$390,130 REET
- \$209,870 LTAC
- \$51,064 Project Savings from North Angel Lane Improvements
- \$1,522,536 Total Sources**

301.0016 Park Equipment Replacement, Continuation/1-Time

Add \$47,484 in project expenditures funded by project balance (REET & General Fund) increasing the total amount available in 2021 to \$67,484. An additional \$20,000 from General Fund was approved during the adopted budget process for 2021 annual program.

301.0017 Park Playground Resurfacing, Continuation/1-Time

Add \$6,342 in project expenditures funded by project balance (REET & General Fund) increasing the total amount available in 2021 to \$16,342. An additional \$10,000 from General Fund was approved during the adopted budget process for 2021 annual program.

301.0018 Project Support, Continuation/1-Time

Add \$110,209 in project expenditures funded by project balance (primarily General Fund) increasing the total amount available in 2021 to \$160,209. An additional \$50,000 from General Fund was approved during the adopted budget process in support in parks capital projects.

301.0019 Edgewater Dock, Continuation/1-Time

Add \$29,156 in project expenditures funded by project balance. This budget adjustments results in a life-to-date through 2022 cost estimate of \$50,000 funded by the General Fund.

301.0020 Wards Lake Improvements, Continuation/1-Time

Add \$112,938 in project expenditures funded by project balance \$56,661 and transfer in from SWM \$56,277 for storm drainage element of the project.

This budget adjustments results in a life-to-date through 2022 cost estimate of \$2,165,000 funded by:

- \$1,600,000 Funds Anticipated (Recreation Conservation Office/Land & Water Conservation Futures)
- \$217,500 General Fund
- \$247,500 REET
- \$100,000 SWM
- \$2,165,000 Total Sources**

301.0022 Banners & Brackets, Continuation/1-Time

Add \$8,789 in project expenditures funded by project balance. This budget adjustments results in a life-to-date through 2022 cost estimate of \$50,000 funded by the General Fund.

301.0024 Fort Steilacoom Park Barn Feasibility Study, Continuation/1-Time

Add \$20,000 in project expenditures funded by project balance. Partners for Parks has committed \$25,000 forwards this study and because of the public benefit of this project, they requested \$20,000 from the City to initiate and support this public/private partnership research project.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$20,000 funded by:

- \$10,000 unallocated CIP funds (from various project savings, interest earnings, miscellaneous revenue)
- \$10,000 and General Fund
- \$20,000 Total Sources**

301.0025 Fort Steilacoom Park ADA/Sensory All Abilities Playground, Continuation/1-Time

Add \$150,000 in project expenditures funded by project balance. This budget adjustments results in a life-to-date through 2022 cost estimate of \$150,000 funded by the General Fund.

301.0027 American Lake Improvements, Continuation/1-Time

Add \$197,955 in project expenditures funded by project balance.

This budget adjustments results in a life-to-date through 2022 cost estimate of \$1,788,500 funded by:

\$1,250,000 Funds Anticipated (Recreation Conservation Office/Aquatics Land Enhancement Account)

\$35,000 Pierce County Grant

\$3,500 Contributions/Donations

\$500,000 REET

\$1,788,500 Total Sources

301.0031 Fort Steilacoom Parks Turf Infields, Continuation/1-Time

Add \$1,259,340 in project expenditures funded by project balance \$264,640 and Department of Commerce grant \$994,700.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$1,260,000 funded by:

\$994,700 Department of Commerce Grant

\$15,300 General Fund

\$250,000 REET

\$1,260,000 Total Sources

301.0032 Springbrook Park Expansion Phase V, Continuation & New/1-Time

Add \$879,073 in project expenditures funded by project balance \$108,508, Department of Commerce grant \$705,565 and General Fund \$65,000 (new). The new portion of this adjustment is for legal fees associated with the Louwien property \$25,000 for legal fees and \$40,000 for removal of the house.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$1,166,516 funded by:

\$757,540 Department of Commerce Grant

\$10,000 Tacoma Pierce County Health Department Grant

\$277,000 General Fund

\$121,616 Springbrook Park Acquisition Phase III Project Savings

\$1,166,516 Total Sources

301.0035 Fort Steilacoom Park Pavilion Restroom, Continuation/1-Time

Add \$129,304 in project expenditures funded by project balance.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$152,636 funded by:

\$70,000 General Fund

\$82,636 Project Savings from Fort Steilacoom Park Pavilion Phase I

\$152,636 Total Sources

301.0036 Gateways – Service Club Signs Continuation/1-Time

Add \$22,510 in project expenditures funded by project balance. This budget adjustments results in a life-to-date through 2022 cost estimate of \$25,000 funded by the General Fund.

301.0037 Seeley Lake Improvement Project, Continuation/1-Time

Add \$115,700 in project expenditures funded by project balance \$65,700 and transfer in from SWM \$50,000 for the storm drainage element of the project.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$150,000 funded by:

\$100,000 Pierce County Grant

\$50,000 SWM

\$150,000 Total Sources

301.0039 American Lake North Parking Lot, New/1-Time

Add \$470,000 in project expenditures funded by the General Fund. This budget adjustment results in a life-to-date through 2022 cost estimate of \$473,750 funded by the General Fund (to cover some charges incurred in 2020).

301.xxxx Barn Demolition, New/1-Time

Add \$25,000 in project expenditures funded by the General Fund. In 2020, during a wind storm, a tree fell on a barn structure, partially collapsing it. This was a smaller structure only used for storage. Since the storm, the barn has been sitting with a temp fence around it. We need to do abatement work first before we can demo and disposal of the structure. There was lead paint on the previous barn that fell. This one is much smaller and we would leave the historical footprint.

Fund 302 Transportation Capital

302.0002 New LED Street Light, Continuation / 1-Time

Add project expenditures of \$165,836 funded by project balance of \$100,453 and unallocated Transportation CIP funds \$70,423 (from various project savings, interest earnings and miscellaneous revenue). This budget adjustments results in an annual budget of \$340,836 for 2021 and \$175,000 for 2022.

302.0003 Safety: Neighborhood Traffic Safety/Traffic Calming, Continuation / 1-Time

Add \$23,248 in project expenditures funded by project balance. This budget adjustment results in an annual budget of \$50,247 in 2021 funded by General Fund. The annual budget for 2022 remains unchanged at \$27,000 and is also funded by General Fund.

302.0004 Minor Capital & Major Maintenance, Continuation/ 1-Time

Add \$60,113 in project expenditures funded by project balance of \$60,113. This budget adjustment results in an annual budget of \$320,113 in 2021 funded by General Fund \$255,973, REET \$4,027 and motor vehicle excise tax \$60,113. The annual budget for 2022 remains unchanged at \$260,000.

302.0005 Chip Seal Program – Local Access Roads, Continuation & New / 1-Time

Add \$200,000 in project expenditures funded by accumulated projects savings, interest earnings, and miscellaneous revenue. This budget adjustment results in an annual budget of \$380,000 in 2021 (no change) and \$580,000 in 2022.

The \$200,000 increase in 2022 is a new request for 112th between South Tacoma Way and Steel Street South and is based on an assessment for potential pavement patching. The roadway is in poor enough condition requiring repairs to bring the roadway back up to serviceable condition. It is likely that a much more expansive and expensive project within 10 years may be needed. The work will entail pavement patching and double chip seal of the roadway.

302.0015 Streets: 112th/111th Bridgeport Way to Kendrick, Continuation/1-Time

Add \$1,316,132 in project expenditures funded by project fund balance \$28,916, Sound Transit grant \$1,019,856 and transfer in from SWM fund \$267,360 for storm related elements.

This budget adjustment results in a life-to-date through 2020 cost estimate of \$1,417,238 funded by:

\$13,414 Port of Tacoma Grant

\$1,040,000 Sound Transit Grant

\$19,779 Donations & Contributions

\$3,440 General Fund

\$12,000 REET

\$270,000 SWM

\$58,606 Unallocated CIP (from various project savings, interest earnings, miscellaneous revenue)

\$1,417,238 Total Sources

302.0024 Streets: Steilacoom Blvd – Farwest to Phillips Road (Design Only), Continuation & New / 1-Time

Add \$465,669 in project expenditures funded by project fund balance \$106,201, Federal WSDOT \$259,468 and REET \$100,000. The \$100,000 new request is to cover geotechnical services and right-of-way acquisition agent services due to refined design needs between Phillips Road and 87th Ave SW.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$1,633,115 funded by:

- \$1,102,809 Federal WSDOT Grant
- \$134,052 Other/Town of Steilacoom
- \$163,190 General Fund
- \$120,100 REET
- \$15,964 SWM
- \$97,000 Unallocated CIP (from various project savings, interest earnings, miscellaneous revenue)
- \$1,633,115 Total Sources**

302.0024 Streets: Steilacoom Blvd – Farwest to Phillips Road (Construction Only), New / 1-Time

Add \$406,200 transfer in from REET to provide for grant match for new grant funds awarded for 2023/2024 for construction of the project segment between 83rd and 87th Ave SW. The project will relocate or increase offset of utility poles and remove or relocate roadside objects; improve street lighting; resurface pavement; increase pavement parking reflectivity; change driveway type; install flashing yellow arrow operation, audible pedestrian push buttons, countdown pedestrian signals; re-channelize roadway to include bike lanes and narrower travel lanes; change traffic signal from span wires to mast arms at 83rd and 87th Ave SW. Construction will occur in 2023/2024 and grant funds will be available to draw upon beginning 2023. The total estimated cost is \$2,708,000 funded by PSRC Grant \$2,301,000 and REET \$406,200.

302.0060 Traffic Signal Replacement: 100th & Lakewood Drive, Continuation / 1-Time

Add \$875,001 in project expenditures funded by project balance \$792,445 and transfer in from SWM for storm related elements \$82,556.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$1,030,860 funded by:

- \$1,544 Developer Contributions
- \$317,465 REET
- \$94,000 SWM
- \$617,851 Unallocated CIP (from various project savings, interest earnings, miscellaneous revenue)
- \$1,030,860 Total Sources**

302.0068 Overlay: Pacific Highway – 108th to SR512, Continuation/1-Time

Add \$6,861 in project expenditures funded by project balance.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$717,861 funded by:

- \$612,000 PSRC Grants Secured
- \$6,861 Developer Contributions
- \$94,400 General Fund
- \$4,600 REET
- \$717,861 Total Sources**

302.0071 Safety: SRTS Phillips Road Sidewalks & Bike Lanes (Steilacoom to Onyx), New/1-Time

Add \$452,000 in project expenditures funded by unallocated Transportation CIP funds \$72,000 (from various project savings, interest earnings and miscellaneous revenue), CDBG \$300,000 and transfer in from SWM for storm related elements \$80,000. This project will construct curb, gutter and sidewalks between Agate Drive SW and Onyx Drive SW on Phillips Road SW.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$452,000 funded by:

\$300,000 CDBG

\$80,000 SWM

\$72,000 Unallocated CIP (from various project savings, interest earnings, miscellaneous revenue)

\$452,000 Total Sources

302.0074 Streets: South Tacoma Way – 88th to 80th Street, Continuation/1-Time

Add \$6,685 in project expenditures funded by project balance.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$521,685 funded by:

\$375,000 PSRC Grant

\$6,685 Contributions

\$140,000 REET

\$521,685 Total Sources

302.0080 Overlay: 108th Street – Bridgeport Way to Pacific Highway, Continuation/1-Time

Decrease \$42,381 in project expenditures for a portion of design work performed in 2020.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$754,000 funded by:

\$244,685 REET

\$509,315 Unallocated CIP (from various project savings, interest earnings, miscellaneous revenue)

\$754,000 Total Sources

302.0119 Sidewalks: Lakewood Drive – Steilacoom Blvd to Flett Creek, Continuation/1-Time

Add \$993,933 in project expenditures funded by project balance \$214,545, state TIB grant balance \$515,012 transfer in from SWM for storm related elements \$264,376.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$1,040,000 funded by:

\$540,000 State TIB Grant

\$270,500 SWM

\$229,500 Unallocated CIP (from various project savings, interest earnings, miscellaneous revenue)

\$1,040,000 Total Sources

302.0134 Sidewalks: Veterans Drive – Gravelly Lake Drive to American Lake Park, Continuation / 1-Time

Add \$1,998,341 in project expenditures funded by project balance \$517,620 and transfer in from SWM for storm drainage element \$966,988.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$7,668,394 funded by:

\$2,000,000 Federal WSDOT Grant

\$4,361,300 General Obligation Bonds

\$154,639 Developer Contributions/Donations

\$12,700 General Fund

\$1,086,666 SWM

\$53,089 Interest Earnings

\$7,668,394 Total Sources

302.0135 Streets: WA Blvd & Edgewood Drive (North Fort to Gravelly Lake Drive, Continuation/1-Time

Add \$384,975 in project expenditures funded by project balance \$159,945 and transfer in from SWM for storm related elements \$225,030. The accounting below includes other projects that were merged with this project.

The budget adjustment results in a life-to-date through 2022 cost estimate of \$18,700,838 funded by:

- \$5,484,970 General Obligation Bonds
- \$4,273,998 Grants Anticipated
- \$3,000,000 TIB Grant
- \$652,638 REET
- \$195,000 TBD
- \$150,302 Contributions
- \$4,943,930 SWM
- \$18,700,838 Total Sources**

302.0137 Streets: Steilacoom Boulevard/88th (Weller to Custer Road), Continuation/1-Time

Add \$1,431,566 in project expenditures funded by project balance. The accounting below includes other projects that were merged with this project.

The budget adjustment results in a life-to-date through 2022 cost estimate of \$5,351,431 funded by:

- \$1,197,000 Federal WSDOT Grant
- \$2,981,730 General Obligation Bonds
- \$75,000 General Fund
- \$405,194 REET
- \$461,506 Transportation Benefit District
- \$231,000 SWM
- \$5,351,431 Total Sources**

302.0138 Sidewalks: Onyx Drive SW – 89th to 97th and Garnet to Phillips, Continuation/1-Time

Add \$786,622 in project expenditures funded by project balance \$121,777 and transfer in from SWM for storm drainage element of project.

The budget adjustment results in a life-to-date through 2022 cost estimate of \$5,607,572 funded by:

- \$4,492,000 General Obligation Bonds
- \$41,710 Interest Earnings
- \$307,000 REET
- \$752,529 SWM
- \$14,334 Unallocated CIP (from various project savings, interest earnings, miscellaneous revenue)
- \$5,607,572 Total Sources**

302.0144 146th Street SW to Woodbrook Drive SW (Design) – New/1-Time

Add \$221,620 in project expenditures funded by Port of Tacoma \$50,000 and transfer in from SWM for storm drainage element of project. \$18,380 was spent in 2020 in advance of the contract/award. Public Works Engineering has determined these costs will be covered within the existing Transportation CIP.

This budget adjustment results in a life-to-date through 2022 cost estimate of \$240,000 funded by:

- \$50,000 Port of Tacoma
- \$190,000 SWM
- \$240,000 Total Sources**

302.0151 South Tacoma Way Between 96th St South & Steilacoom Boulevard, New/1-Time

Add \$118,000 transfer in from REET to provide for grant match for new grant funds awarded for 2023/2024. The project is a grind and 2" overlay and the grant is from PRSC in the amount of \$750,000. The City's match is \$118,000 split with \$7,500 for design in 2023 and \$110,500 for construction in 2024. The grant is split \$45,000 in 2023 and \$705,000 in 2024.

302.0164 Sidewalk Fill-in on Farwest Dr from 112th to Lakes HS, & 100th St Ct SW to Steilacoom, Blvd New/1-Time

Add \$1,475,000 in project expenditures funded by grants anticipated \$1,336,000 and REET \$139,000.

The budget adjustment results in a life-to-date through 2022 cost estimate of \$1,475,000 funded by:

\$1,336,000 Grants Anticipated – Safe Routes to Schools (pending legislative approval)

\$139,000 REET

\$1,475,000 Total Sources

Farwest Drive sidewalk improvement project request is based on the City's notification of an impending grant from the state Safe Routes to Schools grant program application. We would design the missing sidewalks on Farwest from approximately 101st St to Steilacoom Blvd on the west side closing the gap starting in January of 2022.

Fund 311 Sewer Capital Projects

311.0002 Side Sewers, Continuation/1-Time

Add \$200,419 in project expenditures funded by project balance. This budget adjustment increases the available funds from \$50,000 to \$250,419 in 2021 funded by the 4.75% sewer surcharge. An additional \$50,000 was approved for 2022 as part of the adopted biennial budget process.

311.0005 Maple Street Sewer Extension, Continuation/1-Time

Add \$521,042 in project expenditures funded by project balance.

The budget adjustment results in a life-to-date through 2022 cost estimate of \$1,316,905 funded by:

\$1,141,905 Sewer Availability

\$27,000 General Fund (for transportation CIP element of project)

\$140,000 Transfer In From Fund 204 (4.75% Sewer Surcharge)

\$8,000 SWM (for storm drainage element of project)

\$1,316,905 Total Sources

311.0013 Fort Steilacoom Park Sewer Extension, Continuation/1-Time

Add \$152,000 in project expenditures funded by project balance.

The budget adjustment results in a life-to-date through 2022 cost estimate of \$227,000 funded by:

\$75,000 Pierce County Sewer Program

\$152,000 und (for transportation CIP element of project)

\$227,000 Total Sources

Fund 401 Surface Water Management

401.0008 Outfall Retrofit, Continuation/1-Time

Add \$124,184 in project expenditures funded by project balance. The budget adjustment results in a life-to-date through 2022 cost estimate of \$620,000 funded by SWM.

401.0012 Outfall Retrofit Feasibility, Continuation/1-Time

Add \$60,000 in project expenditures funded by project balance. The budget adjustment results in a life-to-date through 2022 cost estimate of \$60,000 funded by SWM.

401.0014 Water Quality Improvements 2021, Continuation/1-Time

Add \$20,000 in project expenditures funded by project balance. The budget adjustment results in a life-to-date through 2022 cost estimate of \$245,000 funded by SWM.

401.0015 Oakbrook Outfall Retrofits, Continuation/1-Time

Add \$219,577 in project expenditures funded by project balance. The budget adjustment results in a life-to-date through 2022 cost estimate of \$250,000 funded by SWM.

401.0018 Waughop Lake Treatment, Continuation/1-Time

Add \$20,000 in project expenditures funded by project balance. The project is complete, however, the City is having volunteers continue to do monitoring and have a billing or two left for consultant pay. The project will be closed out completely at the end of 2021.

The budget adjustment results in a life-to-date through 2022 cost estimate of \$402,085 funded by:

\$51,883 SWM

\$50,000 Department of Ecology Grant

\$300,202 Pierce County Flood Control

\$402,085 Total Sources

401.0021 American Lake Management District, Continuation/1-Time

Add \$31,235 in project expenditures funded by project balance from special assessments.

Transfers to Parks CIP, Continuation/1-Time

Add \$206,277 in transfers to Parks CIP for storm drainage related elements of projects as follows:

- \$100,000 to 301.0005 Chambers Creek Trail
- \$56,277 to 301.0020 Wards Lake Improvements
- \$50,000 to Seeley Lake Improvements

Transfers to Transportation CIP, Continuation/1-Time

Add \$2,741,155 in transfers to Transportation CIP for storm drainage related element of projects as follows:

- \$267,360 to 302.0015 111th/112th (Bridgeport to Kendrick)
- \$82,556 to 302.0060 Traffic Signal at 100th & Lakewood Drive
- \$80,000 to 302.0071 Safety: SRTS - Phillips Road Sidewalks & Bike Lanes
- \$264,376 to 302.0119 Sidewalks: Lakewood Drive – Gravelly Lake Drive to American Lake Park
- \$966,988 to 302.0134 Sidewalks: Veterans Drive – Gravelly Lake Drive to American Lake Park
- \$225,030 to 302.0135 Streets: WA Blvd and Edgewood Drive (North Fort to Gravelly Lake Drive)
- \$664,845 to 302.0138 Sidewalks: Onyx Dr. SW – 89th to 97th and Garnet to Phillips
- \$190,000 to 302.0144 146th St: Murray Road SW to Woodbrook Drive SW

Internal Service Charges, Continuation/1-Time

See internal services funds for additional information.

- \$14,122 Information Technology

Internal Service Charges, Continuation/Ongoing

See internal services funds for additional information.

- \$407/Year Risk Management

Fund 501 Fleet & Equipment

PD Vehicle #40242, Continuation/1-Time

Add \$16,418 to complete build for police vehicle #40242. This was previously approved in 2020 but was not purchased in 2020.

PD PIT Equipment for Training Vehicle #40930, Continuation/1-Time

Add \$5,000 for the purchase and installation PIT (Pursuit Immobilization Technique) equipment, funded by General Fund. The current sedan training vehicle will be replaced with a newer one that was scheduled to be surplus. Vehicles that are scheduled for surplus may be used to replace older vehicles (with the older vehicle then being the vehicle that gets surplus). This was previously approved in 2020 but not converted in 2020.

PD Vehicle #40851, Continuation/1-Time

Add \$11,143 to complete build for police vehicle #40851. This was previously approved in 2019/2020 but was not purchased in 2020.

PD Vehicle #40871, Continuation/1-Time

Add \$4,518 to complete build for police vehicle #40871. This was previously approved in 2019/2020 but was not purchased in 2020.

PD Vehicle #40881, Continuation/1-Time

Add \$4,838 to complete build for police vehicle #40881. This was previously approved in 2019/2020 but was not purchased in 2020.

PD Vehicle #40891, Continuation/1-Time

Add \$4,838 to complete build for police vehicle #40891. This was previously approved in 2019/2020 but was not purchased in 2020.

PD Vehicle #40901, Continuation/1-Time

Add \$4,843 to complete build for police vehicle #40901. This was previously approved in 2019/2020 but was not purchased in 2020.

PD Vehicle #40931, Continuation/1-Time

Add \$15,275 to complete build for police vehicle #40931. This was previously approved in 2019/2020 but was not purchased in 2020.

PD Vehicle #41041, Continuation/1-Time

Add \$4,946 to complete build for police vehicle #41041. This was previously approved in 2019/2020 but was not purchased in 2020.

PD Vehicle #41121, Continuation/1-Time

Add \$9,638 to complete build for police vehicle #41121. This was previously approved in 2019/2020 but was not purchased in 2020.

PD Convert to Specialty Vehicle #41411, Continuation/1-Time

Add \$25,000 to convert a 2016 Dodge Ram to a specialty unit for CIU, funded by replacement reserves. This was previously approved in 2020 but not converted in 2020.

PD Vehicle #40611 Continuation / 1-Time

Add \$25,000 to complete build for police vehicle #40611. This was previously approved in 2019/2020 but was not purchased in 2020.

PD Vehicle #40851, Continuation/1-Time

Add \$24,000 for the replacement of this 2008 Ford ½ ton truck, funded by replacement reserves. This was previously approved in 2020 but was not purchased in 2020.

PK Field Rake #43120, Continuation/1-Time

Add \$9,000 in 2020 for the replacement of a John Deere Field Rake #43120. This was previously approved in 2019/2020 but was not purchased in 2020.

PK Salt Spreader #42890, Continuation / 1-Time

PK Broom with Collection Bin #43260 was previously approved in 2019/2020 but was not purchased in 2020. Due to replacing dirt infields with synthetic turf, this PK Broom with Collection Bin will no longer be needed, therefore we request to exchange this with one of two salt spreaders needed that are no longer working. Add this \$7,500 to support operation and maintenance services.

PW Speed Radar Trailers, Continuation/1-Time

Add \$14,200 to replace two existing radar speed trailers #42990 and #40180 at a cost of \$14,200 funded replacement reserves. In addition, add \$17,400 for three new radar speed trailers at a total cost of \$17,400 funded by contributed capital. These was previously approved in 2020 but was not purchased in 2020.

Fund 502 Property Management

502.0008 Exterior Beam Painting, Continuation/1-Time

Add \$75,000 to seal and protect the exposed exterior beams on City Hall funded by replacement reserves. This was previously approved in 2019/2020 but was not completed in 2020.

502.0025 FSC Caretaker House Repairs, Continuation/1-Time

Add \$10,000 for caretaker house repairs funded by replacement reserves. Other than painting the exterior of the house in 2006 when the City took over maintenance and operations from Pierce County, there hasn't been any major repairs. The \$10,000 is for a new roof and gutters necessary to keep the property in good useable condition and ensure a safe living environment is maintained for the tenant at this site.

502.0029 Salt Bin Cover, Continuation/1-Time

Add \$10,000 to store 1,000 to protect salt bin by contributed capital. This was previously approved in 2019/2020 but was not completed in 2020.

502.xxxx City Hall Space Facility Evaluation, New/1-Time

Add \$30,000 for consultant services to perform a needs and space assessment of City hall for post COVID-19 return to work/telework and the potential to reduce the footprint at City hall. The current thought is lease out of a portion of City Hall,

Fund 503 Information Technology

503.0005 Website Update/Redesign, Continuation/1-Time

Add \$7,499 to continue updating and maintaining the City's website.

503.0006 Disaster Recovery & Business Continuity Plan, Continuation/1-Time

Add \$20,000 for creation of comprehensive documentation, policies and procedures in regards to disaster recovery processes related to information technology.

503.0009 Video Surveillance, Continuation/1-Time

Add \$20,000 for cameras to provide law enforcement visibility at locations such as parking lots, high crime areas and city parks such as the boat launch at American Lake Parke and Harry Todd Park.

503.0012 Disaster Recovery/Co-Location Police, Continuation/1-Time

Add \$3,000 to continue incorporating both the primary and secondary data centers.

503.0018 Document Management System, Continuation/1-Time

Add \$296,406 to implement project. In January of 2019, the Information Technology Division received numerous responses to the document management RFP posted on the city's website. Staff have reviewed submissions to determine which proposals meet the needs of the city and were ready to schedule demos and presentations by the vendors. Due to the critical nature of the PALS permitting system project, this was placed on hold until 2020, then COVID-19 pandemic hit which pushed the project out to 2021.

503.0025 Redundant Voice/Data at Police Station, Continuation/1-Time

Add \$10,000 to continue implementation. This project is dependent on co-location as the plan will incorporate both the primary and secondary data centers. Switches need to be installed at the police station which will allow the IT Division to look at third party solutions for redundant voice/data circuits.

Fund 504 Risk Management

WCIA Assessment, New/Ongoing

Add \$9,870 for WCIA assessment to account for increases primarily in general liability.

**2021 Carry Forward Budget Adjustment
Summary of Proposed Requests**

	Adjustment Type	Ongoing/ 1-Time	Year 2021		Year 2022	
			Revenue	Expenditure	Revenue	Expenditure
Grand Total - All Funds			\$ 22,563,205	\$ 38,094,534	\$ 709,966	\$ 1,465,893
Total - Fund 001 General			\$ 283,662	\$ 1,839,422	\$ -	\$ 363,359
CD - Build Your Better Marketing Campaign	New	1-Time	-	141,500	-	-
CD - DOC Growth Management Lakewood Station	Continuation	1-Time	48,000	-	-	-
CD - DOC Comprehensive Plan Climate Change	Continuation	1-Time	20,000	20,000	-	-
CD - WSDOT Relocation	Continuation	1-Time	-	32,109	-	-
CD - Oak Tree Mitigation	Continuation	1-Time	55,446	55,446	-	-
PK - Human Services - Lakewood's Promise - Youth Council	Continuation	1-Time	-	5,000	-	-
PK - Human Services - Lakewood's Promise - Work Force Development	Continuation	1-Time	-	10,000	-	-
PK - Human Services - LASA - Utility Assistance	Continuation	1-Time	-	15,000	-	-
PK - Human Services - Lakewood's Choice	Continuation	1-Time	90,721	90,721	-	-
PK - Cemetery Grant	Continuation	1-Time	907	907	-	-
LG - STOP VAWA 2021 Grant	New	1-Time	30,313	30,313	-	-
MC - 2020 OPD (Office of Public Defense) Grant	Continuation	1-Time	16,875	16,875	-	-
MC - 2021 OPD (Office of Public Defense) Grant	Continuation	1-Time	37,000	37,000	-	-
MC - 2020 Court Cost True Up	Continuation	Ongoing	(72,555)	-	-	-
PD - Ammunition	Continuation	1-Time	-	9,870	-	-
PD - City of Tacoma Radio User Fee	New	Ongoing	-	28,000	-	28,000
PD - Jail Services	New	1-Time	-	250,000	-	250,000
PD - SRT Equipment	Continuation	1-Time	-	40,000	-	-
PD - Sniperweek Training	Continuation	1-Time	-	1,507	-	-
PD - Criminal Justice Training Commission Grant	Continuation	1-Time	26,266	26,266	-	-
PD - FBI Innocence Lost Grant	Continuation	1-Time	28,000	-	-	-
PD - Emergency Management Performance Grant - WA State Military	Continuation	1-Time	2,689	2,689	-	-
I/S Charge - Info Tech - Website Update/Redesign	Continuation	1-Time	-	7,499	-	-
I/S Charge - Info Tech - Disaster Recovery	Continuation	1-Time	-	17,523	-	-
I/S Charge - Info Tech -Disaster Recover/Co-Location	Continuation	1-Time	-	2,628	-	-
I/S Charge - Info Tech -Video Surveillance	Continuation	1-Time	-	17,523	-	-
I/S Charge - Info Tech -Document Management System	Continuation	1-Time	-	259,690	-	-
I/S Charge - Info Tech - Redundant Voice/Data at Police Station	Continuation	1-Time	-	10,000	-	-
I/S Charge - Fleet & Equip - PD Veh 40930 2013 Ford Interceptor	Continuation	1-Time	-	5,000	-	-
I/S Charge - Property Mgmt - PRCS Salt Spreader	Continuation	1-Time	-	2,500	-	-
I/S Charge - Property Mgmt - City Hall Facility Space Evaluation	New	1-Time	-	30,000	-	-
I/S Charge -Risk Mgmt - WCIA Assessment	New	Ongoing	-	9,463	-	9,463
Transfer to Rental Housing Safety Program (Cover 2020 Deficit)	New	1-Time	-	24,287	-	-
Transfer to Parks CIP 301.0006 New Gateway at 84th & Tacoma Mall Blvd	New	1-Time	-	-	-	75,000
Transfer to Parks CIP 301.0039 American Lake Park - North Parking Lot	New	1-Time	-	473,750	-	-
Transfer to Parks CIP 301.0032 Springbrook Property Acquisition - Legal Fees \$25K & Remove House \$40K	New	1-Time	-	65,000	-	-
Transfer to Parks CIP 301.xxxx FSP Barn Demolition	New	1-Time	-	25,000	-	-
General/Street Fund Subsidy	New	Ongoing	-	896	-	896
General/Street Fund Subsidy	Continuation	1-Time	-	75,460	-	-

**2021 Carry Forward Budget Adjustment
Summary of Proposed Requests**

			Adjustment Type	Ongoing/ 1-Time	Year 2021		Year 2022	
					Revenue	Expenditure	Revenue	Expenditure
Total - Special Revenue Funds					\$ 11,701,733	\$ 14,625,210	\$ 896	\$ 625,096
Total - Fund 101 Street O&M					76,356	76,356	896	896
Non-Motorized Plan Update	Continuation	1-Time	-	20,139				
I/S Charge - Info Tech - Disaster Recovery	Continuation	1-Time	-	3,538	-	-		
I/S Charge - Info Tech - Document Management System	Continuation	1-Time	-	24,383	-	-		
I/S Charge - Fleet & Equip - Purchase 3 Radars	Continuation	1-Time	-	17,400	-	-		
I/S Charge - Fleet & Equip - Salt Bin Cover	Continuation	1-Time	-	10,000	-	-		
I/S Charge - Risk Mgmt - WCIA Assessment	New	Ongoing	-	896	-	896		
General/Street Fund Subsidy	New	Ongoing	896	-	896	-		
General/Street Fund Subsidy	Continuation	1-Time	75,460	-	-	-		
Total - Fund 102 Real Estate Excise Tax					\$ -	\$ 139,000	\$ -	\$ 624,200
Transfer to Transportation CIP 302.0024 Steilacoom Blvd-Farwest to Phillips Road (Design & ROW)	New	1-Time	-	139,000	-	-		
Transfer to Transportation CIP 302.0024 Steilacoom Blvd-Farwest to Phillips Road (Construction)	New	1-Time	-	-	-	406,200		
Transfer to Transportation CIP 302.0024 Steilacoom Blvd-Farwest to Phillips Road (Construction)	New	1-Time	-	-	-	100,000		
Transfer to Transportation CIP 302.0151 South Tacoma Way Between 96th St South & Steilacoom Blvd	New	1-Time	-	-	-	118,000		
Total - Fund 104 Hotel/Motel Lodging Tax					\$ -	\$ 140,941	\$ -	\$ -
Transfer to Parks CIP - Gateways	Continuation	1-Time	-	140,941	-	-		
Total - Fund 105 Property Abatement					\$ 24,287	\$ 682,701	\$ -	\$ -
Property Abatement	Continuation	1-Time	-	610,386	-	-		
Rental Housing Safety Program (Cover 2020 Deficit)	New	1-Time	24,287	-	-	-		
1406 Affordable Housing Program	Continuation	1-Time	-	72,315	-	-		
Total - Fund 106 Public Art					\$ -	\$ 135,500	\$ -	\$ -
Arts Commission - 2019 & 2020 Unspent Funds for Signal Box Wrap	Continuation	1-Time	-	4,000	-	-		
Arts Commission - Additional Funds Needed for Signal Box Wrap	New	1-Time	-	4,000	-	-		
Colonial Plaza Art	Continuation	1-Time	-	95,000	-	-		
Major Public Art - To Be Determined	Continuation	1-Time	-	32,500	-	-		
Total - Fund 180 Narcotics Seizure					\$ -	\$ 106,196	\$ -	\$ -
Narcotics Seizure Balance	Continuation	1-Time	-	106,196	-	-		
Total - Fund 181 Felony Seizure					\$ -	\$ 47,837	\$ -	\$ -
Felony Seizure Balance	Continuation	1-Time	-	47,837	-	-		
Total - Fund 182 Federal Seizure					\$ -	\$ 40,906	\$ -	\$ -
Federal Seizure Balance	Continuation	1-Time	-	40,906	-	-		

**2021 Carry Forward Budget Adjustment
Summary of Proposed Requests**

	Adjustment Type	Ongoing/ 1-Time	Year 2021		Year 2022	
			Revenue	Expenditure	Revenue	Expenditure
Total - Fund 190 CDBG			\$ 3,733,614	\$ 5,115,338	\$ -	\$ -
2021 CDBG Entitlement Allocation	Continuation	1-Time	573,352	573,352	-	-
2021 CDBG Entitlement Reallocation	Continuation	1-Time	199,133	199,133	-	-
2021 CDBG CARES COVID 3 Reallocation from COVID 1	Continuation	1-Time	246,701	246,701	-	-
2021 CDBG CARES COVID 3	Continuation	1-Time	456,726	456,726	-	-
2021 CDBG CARES COVID 2	New	1-Time	136,706	136,706		
2020 Balance - Housing Programs	Continuation	1-Time	55,000	55,000	-	-
2020 Balance - Major Home & Sewer Repair	Continuation	1-Time	610,417	610,417	-	-
2020 Balance - CDBG CARES COVID 1	Continuation	1-Time	40,544	40,544	-	-
2019 Balance - Major Home & Sewer Repair	Continuation	1-Time	141,774	141,774	-	-
2019 Balance - Housing Programs	Continuation	1-Time	22,063	22,063	-	-
2021 HOME Allocation	Continuation	1-Time	321,730	321,730	-	-
2020 Balance - HOME Affordable Housing	Continuation	1-Time	222,662	222,662	-	-
2020 Balance - HOME Down Payment Assistance	Continuation	1-Time	253	253	-	-
2020 Balance - HOME Housing Rehabilitation	Continuation	1-Time	50,000	50,000	-	-
2019 Balance - HOME Affordable Housing	Continuation	1-Time	462,086	462,086	-	-
2019 Balance - HOME Housing Rehabilitation	Continuation	1-Time	194,467	194,467	-	-
Earmark CDBG Restricted Funds for Future Loan Programs	Continuation	1-Time	-	1,381,724	-	-
Total - Fund 191 Neighborhood Stabilization Program (NSP)			\$ (36,000)	\$ 219,114	\$ -	\$ -
Eliminate Estimate	Continuation	1-Time	(78,000)	(78,000)		
NSP 1 Abatement	Continuation	1-Time	42,000	297,114		
Total - Fund 192 Office of Economic Adjustment (OEA)			\$ 7,517,728	\$ 7,535,573	\$ -	\$ -
SSMCP Eliminate Estimate	Continuation	Ongoing	(227,500)	(227,500)	-	-
SSMCP Operations	Continuation	Ongoing	236,000	253,845	-	-
North Clear Zone Property Purchase - Tactical Tailor Building	Continuation	1-Time	6,865,728	6,865,728	-	-
Department of Defense - OEA Growth Management	Continuation	1-Time	643,500	643,500	-	-
Total - Fund 195 Public Safety Grants			\$ 385,748	\$ 385,748	\$ -	\$ -
Washington Traffic Safety Commission - Impaired Driving Emphasis	Continuation	1-Time	7,400	7,400	-	-
Washington Traffic Safety Commission - Motorcycle Patrol	Continuation	1-Time	2,400	2,400	-	-
Emergency Management Planning	Continuation	1-Time	25,439	25,439	-	-
Bullet Proof Vest Partnership	Continuation	1-Time	19,361	19,361	-	-
Washington State Parks Recreational Boating Safety	New	1-Time	11,867	11,867	-	-
Pierce County Phlebotomy	Continuation	1-Time	2,721	2,721	-	-
Washington State Patrol - MIDU (Mobile Impaired Driving Unit)	New	1-Time	20,000	20,000	-	-
US Department of Justice Assistance - Rifle Noise Suppressors	Continuation	1-Time	2,069	2,069	-	-
US Department of Justice Assistance - COVID	Continuation	1-Time	40,541	40,541	-	-
US Department of Justice Assistance - 2020 Bicycles	Continuation	1-Time	36,959	36,959	-	-
Washington Auto Theft Task Force	Continuation	1-Time	124,500	124,500	-	-
Office of Justice Programs Veterans Court Grant	Continuation	1-Time	92,491	92,491	-	-
Grand Total - Debt Service Funds			\$ -	\$ 248,039	\$ -	\$ 92,161
Total - Fund 202 LID Debt Service			\$ -	\$ 248,039	\$ -	\$ 92,161
Bond Redemption	Continuation	1-Time	-	248,039	-	92,161

**2021 Carry Forward Budget Adjustment
Summary of Proposed Requests**

	Adjustment Type	Ongoing/ 1-Time	Year 2021		Year 2022	
			Revenue	Expenditure	Revenue	Expenditure
Total - Capital Improvement Project Funds			\$ 10,142,700	\$ 17,249,579	\$ 699,200	\$ 375,000
Total - Fund 301 Parks CIP			\$ 3,198,517	\$ 5,779,899	\$ 75,000	\$ 75,000
301.0003 Harry Todd Park Phase I & II - Project Balance	Continuation	1-Time	527,284	1,405,918	-	-
301.0003 Harry Todd Park Phase I & II - Contribution/Donation	New	1-Time	10,000	10,000		
301.0005 Chambers Creek Trail	Continuation	1-Time	100,000	236,000	-	-
301.0006 Gateways (Berkeley & Union and Woodbrook Business Park)	Continuation	1-Time	140,941	190,941	-	-
301.0006 New Gateway at 84th & Tacoma Mall Blvd	New	1-Time	-	-	75,000	75,000
301.0014 FSP South Angle Lane Parking & Trail Improvement	Continuation	1-Time	50,000	353,241	-	-
301.0016 Park Playground Replacement	Continuation	1-Time	-	47,483	-	-
301.0017 Park Playground Resurfacing - Project Balance	Continuation	1-Time	-	6,342	-	-
301.0018 Project Support - Project Balance	Continuation	1-Time	-	110,209	-	-
301.0019 Edgewater Dock - Project Balance	Continuation	1-Time	-	29,156	-	-
301.0020 Wards Lake Improvements - Project Balance	Continuation	1-Time	56,277	112,938	-	-
301.0022 Banners & Brackets Phase II - Project Balance	Continuation	1-Time	-	8,789	-	-
301.0024 FSP Barn Restoration Feasibility Study - Project Balance	Continuation	1-Time	-	20,000	-	-
301.0025 FSP ADA/Sensory All Abilities Playground - Project Balance	Continuation	1-Time	-	150,000	-	-
301.0027 American Lake Improvements - Project Balance	Continuation	1-Time	-	197,955	-	-
301.0031 FSP Turf Infields - Project Balance	Continuation	1-Time	994,700	1,259,340	-	-
301.0032 Springbrook Park Expansion Phase V - Project Balance	Continuation	1-Time	705,565	814,073	-	-
301.0032 Springbrook Park Expansion Phase V - Legal Fees	New	1-Time	25,000	25,000	-	-
301.0032 Springbrook Park Expansion Phase V - Removal of House	New	1-Time	40,000	40,000	-	-
301.0035 FSP Pavilion Restroom Improvements - Project Balance	Continuation	1-Time	-	129,304	-	-
301.0036 Gateways - Service Club Signs - Project Balance	Continuation	1-Time	-	22,510	-	-
301.0037 Seeley Lake Improvements - Project Balance	Continuation	1-Time	50,000	115,700	-	-
301.0039 American Lake Park - North Parking Lot - Property Purchase	New	1-Time	223,750	220,000	-	-
301.0039 American Lake Park - North Parking Lot - Improvements	New	1-Time	250,000	250,000	-	-
301.xxxx Barn Demolition	New	1-time	25,000	25,000	-	-

**2021 Carry Forward Budget Adjustment
Summary of Proposed Requests**

	Adjustment Type	Ongoing/ 1-Time	Year 2021		Year 2022	
			Revenue	Expenditure	Revenue	Expenditure
Total - Fund 302 Transportation CIP			\$ 6,869,183	\$ 10,521,219	\$ 624,200	\$ 300,000
302.0002 New LED Street Lights	Continuation	1-Time	(5,040)	165,836	-	-
302.0003 Safety: Neighborhood Traffic Safety/Traffic Calming	Continuation	1-Time	-	23,248	-	-
302.0004 Minor Capital & Major Maintenance	Continuation	1-Time	-	60,113	-	-
302.0005 Chip Seal Program – Local Access Roads	New	1-Time	-	-	-	200,000
302.0015 111th/112th (Bridgeport to Kendrick)	Continuation	1-Time	1,287,216	1,316,132	-	-
302.0024 Steilacoom Blvd-Farwest to Phillips Road (Design & ROW)	Continuation	1-Time	259,467	365,668	-	-
302.0024 Steilacoom Blvd-Farwest to Phillips Road (Construction) Match for 2023/2024 Grant Awarded	New	1-Time	-	-	406,200	-
302.0024 Steilacoom Blvd-Farwest to Phillips Road	New	1-Time			100,000	100,000
302.0060 Traffic Signal at 100th	Continuation	1-Time	82,556	875,001	-	-
302.0068 Overlay: Pacific Hwy – 108th to SR512	Continuation	1-Time	-	6,861	-	-
302.0071 Safety: SRTS - Phillips Road Sidewalks & Bike Lanes	Continuation	1-Time	80,000	152,000	-	-
302.0071 Safety: SRTS - Phillips Road Sidewalks & Bike Lanes	New	1-time	300,000	300,000	-	-
302.0074 Streets: S Tacoma Way - 88th to 80th St	Continuation	1-Time	-	6,685	-	-
302.0080 Overlay: 108th Street - BW to Pac Hwy	Continuation	1-Time	-	(42,381)	-	-
302.0119 Sidewalks: Lakewood Drive - Steilacoom Blvd. to Flett Creek	Continuation	1-Time	779,388	993,933	-	-
302.0134 Sidewalks: Veterans Drive - GLD to American Lake Park	Continuation	1-Time	1,480,721	1,998,341	-	-
302.0135 Streets: WA Blvd and Edgewood Drive (North Fort to GLD.)	Continuation	1-Time	225,030	384,975	-	-
302.0137 Streets: Steilacoom Blvd/88th (Weller to Custer Rd.)	Continuation	1-Time	-	1,431,565	-	-
302.0138 Sidewalks: Onyx Dr. SW – 89th to 97th and Garnet to Phillips	Continuation	1-Time	664,845	786,622	-	-
302.0144 146th ST: Murray Rd SW to Woodbrook Dr SW	Continuation	1-Time	240,000	221,620	-	-
302.0151 South Tacoma Way Between 96th St South & Steilacoom Blvd Match for 2023/2024 Grant Awarded	New	1-Time	-	-	118,000	-
302.0164 Sidewalks: Farwest Dr from 112th to Lakes HS & 100th to STL Blvd Grants Anticipated \$1,336,000 and REET \$139,000	New	1-Time	1,475,000	1,475,000	-	-
Total - Fund 311 Sewer Project CIP			\$ 75,000	\$ 948,461	\$ -	\$ -
311.0002 Side Sewer CIPs	Continuation	1-Time	-	200,419	-	-
311.0005 Maple Street Sewer Extension	Continuation	1-Time	-	521,042	-	-
311.0013 Fort Steilacoom Park Sewer Extension	Continuation	1-Time	75,000	227,000	-	-

**2021 Carry Forward Budget Adjustment
Summary of Proposed Requests**

	Adjustment Type	Ongoing/ 1-Time	Year 2021		Year 2022	
			Revenue	Expenditure	Revenue	Expenditure
Total - Enterprise Funds			\$ 3,435	\$ 3,436,957	\$ -	\$ 407
Total - Fund 401 Surface Water Management			\$ 3,435	\$ 3,436,957	\$ -	\$ 407
401.0008 Outfall Retrofit	Continuation	1-Time	-	124,184	-	-
401.0012 Outfall Retrofit Feasibility Project	Continuation	1-Time	-	60,000	-	-
401.0014 Water Quality Improvements 2021	Continuation	1-Time	-	20,000	-	-
401.0015 Oakbrook Outfall Retrofits	Continuation	1-Time	-	219,577	-	-
401.0018 Waughop Lake Treatment	Continuation	1-Time	3,435	20,000	-	-
401.0021 American Lake Management District	Continuation	1-Time	-	31,235	-	-
Transfer to Parks CIP 301.0005 Chambers Creek Trail	Continuation	1-Time	-	100,000	-	-
Transfer to Parks CIP 301.0020 Ward Lake Improvements	Continuation	1-Time	-	56,277	-	-
Transfer to Parks CIP 301.0037 Seeley Lake Improvements	Continuation	1-Time	-	50,000	-	-
Transfer to Transportation CIP 302.0015 111th/112th (Bridgeport to Kendrick)	Continuation	1-Time	-	267,360	-	-
Transfer to Transportation CIP 302.0060 Traffic Signal at 100th & Lakewood Drive	Continuation	1-Time	-	82,556	-	-
Transfer to Transportation CIP 302.0071 Safety: SRTS - Phillips Road Sidewalks & Bike Lanes	Continuation	1-Time	-	80,000	-	-
Transfer to Transportation CIP 302.0119 Sidewalks: Lakewood Dr - GLD to American Lake Park	Continuation	1-Time	-	264,376	-	-
Transfer to Transportation CIP 302.0134 Sidewalks: Veterans Drive - GLD to American Lake Park	Continuation	1-Time	-	966,988	-	-
Transfer to Transportation CIP 302.0135 Streets: WA Blvd and Edgewood Drive (North Fort to GLD.)	Continuation	1-Time	-	225,030	-	-
Transfer to Transportation CIP 302.0138 Sidewalks: Onyx Dr. SW – 89th to 97th and Garnet to Phillips	Continuation	1-Time	-	664,845	-	-
Transfer to Transportation CIP 302.0144 146th ST: Murray Rd SW to Woodbrook Dr SW	Continuation	1-Time	-	190,000	-	-
I/S Charge - Info Tech - Disaster Recovery	Continuation	1-Time	-	832	-	-
I/S Charge - Info Tech - Disaster Recover/Co-Location	Continuation	1-Time	-	125	-	-
I/S Charge - Info Tech - Video Surveillance	Continuation	1-Time	-	832	-	-
I/S Charge - Info Tech - Document Management System	Continuation	1-Time	-	12,333	-	-
I/S Charge - Risk Mgmt - WCIA Assessment	New	Ongoing	-	407	-	407

**2021 Carry Forward Budget Adjustment
Summary of Proposed Requests**

			Adjustment Type	Ongoing/ 1-Time	Year 2021		Year 2022					
					Revenue	Expenditure	Revenue	Expenditure				
Total - Internal Service Funds					\$	431,675	\$	695,327	\$	9,870	\$	9,870
Total - Fund 501 Fleet & Equipment					\$	24,900	\$	203,552	\$	-	\$	-
PD Veh 40242 2020 Ford Explorer AWD - Funded by Replacement Reserves	Continuation	1-Time		-		16,418		-		-		
PD Veh 40930 2013 Ford Interceptor - Funded by Capital Contribution	Continuation	1-Time		5,000		5,000		-		-		
PD Veh 40851 2020 Ford K8A Police Utility AWD - Funded by Replacement Reserves	Continuation	1-Time		-		11,143		-		-		
PD Veh 40871 2020 Ford K8A Police Utility AWD - Funded by Replacement Reserves	Continuation	1-Time		-		4,518		-		-		
PD Veh 40881 2020 Ford Explorer AWD - Funded by Replacement Reserves	Continuation	1-Time		-		4,838		-		-		
PD Veh 40891 2020 Ford Explorer AWD - Funded by Replacement Reserves	Continuation	1-Time		-		4,838		-		-		
PD Veh 40901 2020 Ford Explorer AWD - Funded by Replacement Reserves	Continuation	1-Time		-		4,838		-		-		
PD Veh 40931 2013 Ford Interceptor - Funded by Replacement Reserves	Continuation	1-Time		-		15,275		-		-		
PD Veh 41041 PD 2021 Dodge Durango - Funded by Replacement Reserves	Continuation	1-Time		-		4,946		-		-		
PD Veh 41121 PD 2020 Ford K8A Utility AWD - Funded by Replacement Reserves	Continuation	1-Time		-		9,638		-		-		
PD Veh 41411 2016 Dodge Ram 1500 P/U Ext Cab - Funded by Replacement Reserves	Continuation	1-Time		-		25,000		-		-		
PD Veh 40611 2020 Ford R3X Transit Cargo HR Van - Funded by Replacement Reserves & Reserves from Veh 40630 (to be replaced)	Continuation	1-Time		-		25,000		-		-		
PD Veh 40430 2008 F150 - Funded by Replacement Reserves	Continuation	1-Time		-		24,000		-		-		
PRCS-Vehicle #43120 replaced by 43320 Salt Spreader - Funded by Replacement Reserves \$6,500 and Capital Contributions \$2,500	Continuation	1-Time		2,500		9,000		-		-		
PRCS-Vehicle #43290 replaced by 43310 Salt Spreader - Funded by Replacement Reserves	Continuation	1-Time		-		7,500		-		-		
PW - Replace 1999 Radar Speed Trailer #42990 - Funded by Replacement Reserves	Continuation	1-Time		-		7,100		-		-		
PW - Replace 2004 Radar Speed Trailer #40180 - Funded by Replacement Reserves	Continuation	1-Time		-		7,100		-		-		
PW - Three New Radar Speed Trailers - Funded by Capital Contribution	Continuation	1-Time		17,400		17,400		-		-		
Total - Fund 502 Property Management					\$	40,000	\$	125,000	\$	-	\$	-
502.0008 City Hall Exterior Beam Painting Funded by Management Reserves	Continuation	1-Time		-		75,000		-		-		
502.0025 FSP Caretakers House Funded by Replacement Reserves	Continuation	1-Time		-		10,000		-		-		
502.0029 Salt Bin Cover Funded by Contributed Capital	Continuation	1-Time		10,000		10,000		-		-		
502.xxxx City Hall Facility Space Evaluation Funded by 1-Time M&O Funds	New	1-Time		30,000		30,000		-		-		

**2021 Carry Forward Budget Adjustment
Summary of Proposed Requests**

		Adjustment Type	Ongoing/ 1-Time	Year 2021		Year 2022	
				Revenue	Expenditure	Revenue	Expenditure
Total - Fund 503 Information Technology				356,905	356,905	-	-
503.0005 Website Update/Redesign Funded by 1-Time M&O Funds		Continuation	1-Time	7,499	7,499	-	-
503.0006 Disaster Recovery & Business Continuity Plan Funded by 1-Time M&O Funds		Continuation	1-Time	20,000	20,000	-	-
503.0009 Video Surveillance Funded by 1-Time M&O Funds		Continuation	1-Time	20,000	20,000	-	-
503.0012 Disaster Recover/Co-Location Funded by 1-Time M&O Funds		Continuation	1-Time	3,000	3,000	-	-
503.0018 - Document Management System Funded by 1-Time Contributed Capital		Continuation	1-Time	296,406	296,406	-	-
503.0025 Redundant Voice/Data at Police Station Funded by 1-Time M&O Funds		Continuation	1-Time	10,000	10,000	-	-
Total - Fund 504 Risk Management				\$ 9,870	\$ 9,870	\$ 9,870	\$ 9,870
WCIA Assessment		New	Ongoing	9,870	9,870	9,870	9,870

PROPOSED REVISED BUDGET BY FUND - YEAR 2021

Fund	Beginning Fund Balance			Revenue			Expenditure			Ending Fund Balance
	Prior Amount	Adjustment	Revised	Prior Amount	Adjustment	Revised	Prior Amount	Adjustment	Revised	
General Government Funds:	9,145,661	12,917,784	22,063,445	48,449,854	11,985,395	60,435,249	48,838,381	16,712,669	65,551,050	16,947,643
001 General	\$ 5,767,631	\$ 7,805,491	\$ 13,573,122	\$ 38,400,491	\$ 283,662	\$ 38,684,153	\$ 39,363,968	\$ 1,839,422	\$ 41,203,390	\$ 11,053,885
101 Street	-	-	-	2,343,813	76,356	2,420,169	2,343,813	76,356	2,420,169	-
102 Real Estate Excise Tax	540,095	1,731,415	2,271,510	1,800,000	-	1,800,000	2,078,195	139,000	2,217,195	1,854,315
103 Transportation Benefit District	640,000	47,753	687,753	835,000	-	835,000	-	-	-	1,522,753
104 Hotel/Motel Lodging Tax Fund	1,247,353	411,680	1,659,033	800,000	-	800,000	672,250	140,941	813,191	1,645,842
105 Property Abatement/RHSP/1406	-	658,414	658,414	470,000	24,287	494,287	470,000	682,701	1,152,701	-
106 Public Art	-	135,500	135,500	7,500	-	7,500	7,500	135,500	143,000	-
180 Narcotics Seizure	120,000	106,196	226,196	-	-	-	120,000	106,196	226,196	-
181 Felony Seizure	-	47,837	47,837	-	-	-	-	47,837	47,837	-
182 Federal Seizure	120,000	40,906	160,906	-	-	-	120,000	40,906	160,906	-
190 CDBG	-	1,381,724	1,381,724	595,000	3,733,614	4,328,614	595,000	5,115,338	5,710,338	-
191 Neighborhood Stabilization Prog	-	255,114	255,114	78,000	(36,000)	42,000	78,000	219,114	297,114	-
192 OEA Grant/SSMCP	-	18,017	18,017	227,500	7,517,728	7,745,228	227,500	7,535,573	7,763,073	172
195 Public Safety Grants	-	-	-	132,328	385,748	518,076	132,328	385,748	518,076	-
201 GO Bond Debt Service	-	-	-	1,690,821	-	1,690,821	1,690,821	-	1,690,821	-
202 LID Debt Service	-	248,038	248,038	268,920	-	268,920	268,920	248,038	516,958	-
204 Sewer Project Debt	578,288	29,025	607,313	800,481	-	800,481	670,086	-	670,086	737,708
251 LID Guaranty	132,294	674	132,968	-	-	-	-	-	-	132,968
Capital Project Funds:	1,392,832	7,614,504	9,007,336	15,911,002	10,142,700	26,053,702	17,097,002	17,249,580	34,346,582	714,456
301 Parks CIP	-	2,605,500	2,605,500	570,000	3,198,517	3,768,517	570,000	5,779,899	6,349,899	24,118
302 Transportation CIP	927,246	3,942,672	4,869,918	14,986,002	6,869,183	21,855,185	15,732,002	10,521,220	26,253,222	471,881
311 Sewer Project CIP	465,586	1,066,332	1,531,918	355,000	75,000	430,000	795,000	948,461	1,743,461	218,457
Enterprise Fund:	1,500,405	4,320,613	5,821,018	5,450,385	3,435	5,453,820	5,069,475	3,436,957	8,506,432	2,768,406
401 Surface Water Management	1,500,405	4,320,613	5,821,018	5,450,385	3,435	5,453,820	5,069,475	3,436,957	8,506,432	2,768,406
Internal Service Funds:	4,535,425	515,705	5,051,130	5,107,682	431,675	5,539,357	5,821,682	695,327	6,517,009	4,073,478
501 Fleet & Equipment	3,940,779	320,528	4,261,307	755,720	24,900	780,620	1,239,720	203,552	1,443,272	3,598,655
502 Property Management	389,124	195,176	584,300	695,603	40,000	735,603	925,603	125,000	1,050,603	269,300
503 Information Technology	205,522	1	205,523	2,059,879	356,905	2,416,784	2,059,879	356,905	2,416,784	205,523
504 Risk Management	-	-	-	1,596,480	9,870	1,606,350	1,596,480	9,870	1,606,350	-
Total All Funds	16,574,323	\$ 25,368,606	\$41,942,929	\$ 74,918,923	\$ 22,563,205	\$97,482,128	\$ 76,826,540	\$ 38,094,533	\$114,921,073	\$ 24,503,984

PROPOSED REVISED BUDGET BY FUND - YEAR 2022

Fund	Beginning Fund Balance			Revenue			Expenditure			Ending Fund Balance
	Prior Amount	Adjustment	Revised	Prior Amount	Adjustment	Revised	Prior Amount	Adjustment	Revised	
General Government Funds:	\$ 8,757,134	\$ 8,190,509	\$ 16,947,643	\$ 50,452,982	\$ 896	\$ 50,453,878	\$ 49,594,172	\$ 1,080,616	\$ 50,674,788	\$ 16,726,733
001 General	4,804,154	6,249,731	11,053,885	40,226,201	-	40,226,201	40,094,292	363,359	40,457,651	10,822,435
101 Street	-	-	-	2,398,483	896	2,399,379	2,398,483	896	2,399,379	-
102 Real Estate Excise Tax	261,900	1,592,415	1,854,315	1,800,000	-	1,800,000	1,866,900	624,200	2,491,100	1,163,215
103 Transportation Benefit District	1,475,000	47,753	1,522,753	835,000	-	835,000	640,000	-	640,000	1,717,753
104 Hotel/Motel Lodging Tax Fund	1,375,103	270,739	1,645,842	1,000,000	-	1,000,000	800,000	-	800,000	1,845,842
105 Property Abatement/RHSP/1406	-	-	-	409,500	-	409,500	409,500	-	409,500	-
106 Public Art	-	-	-	15,000	-	15,000	15,000	-	15,000	-
180 Narcotics Seizure	-	-	-	-	-	-	-	-	-	-
181 Felony Seizure	-	-	-	-	-	-	-	-	-	-
182 Federal Seizure	-	-	-	-	-	-	-	-	-	-
190 CDBG	-	-	-	595,000	-	595,000	595,000	-	595,000	-
191 Neighborhood Stabilization Prog	-	-	-	42,000	-	42,000	42,000	-	42,000	-
192 OEA Grant/SSMCP	-	172	172	227,500	-	227,500	227,500	-	227,500	172
195 Public Safety Grants	-	-	-	132,328	-	132,328	132,328	-	132,328	-
201 GO Bond Debt Service	-	-	-	1,689,938	-	1,689,938	1,689,938	-	1,689,938	-
202 LID Debt Service	-	-	-	247,774	-	247,774	155,613	92,161	247,774	-
204 Sewer Project Debt	708,683	29,025	737,708	834,258	-	834,258	527,618	-	527,618	1,044,348
251 LID Guaranty	132,294	674	132,968	-	-	-	-	-	-	132,968
Capital Project Funds:	\$ 206,832	\$ 507,624	\$ 714,456	\$ 13,016,998	\$ 699,200	\$ 13,716,198	\$ 12,896,998	\$ 375,000	\$ 13,271,998	\$ 1,158,656
301 Parks CIP	-	24,118	24,118	3,355,000	75,000	3,430,000	3,355,000	75,000	3,430,000	24,118
302 Transportation CIP	181,246	290,635	471,881	9,411,998	624,200	10,036,198	9,411,998	300,000	9,711,998	796,081
311 Sewer Project CIP	25,586	192,871	218,457	250,000	-	250,000	130,000	-	130,000	338,457
Enterprise Fund:	\$ 1,881,316	\$ 887,091	\$ 2,768,407	\$ 7,565,476	\$ -	\$ 7,565,476	\$ 7,600,625	\$ 407	\$ 7,601,032	\$ 2,732,851
401 Surface Water Management	1,881,316	887,091	2,768,407	7,565,476	-	7,565,476	7,600,625	407	7,601,032	2,732,851
Internal Service Funds:	\$ 3,821,425	\$ 252,053	\$ 4,073,478	\$ 5,328,656	\$ 9,870	\$ 5,338,526	\$ 5,418,900	\$ 9,870	\$ 5,428,770	\$ 3,983,234
501 Fleet & Equipment	3,456,779	141,876	3,598,655	755,720	-	755,720	1,007,720	-	1,007,720	3,346,655
502 Property Management	159,124	110,176	269,300	798,917	-	798,917	703,917	-	703,917	364,300
503 Information Technology	205,522	1	205,523	2,177,539	-	2,177,539	2,110,783	-	2,110,783	272,279
504 Risk Management	-	-	-	1,596,480	9,870	1,606,350	1,596,480	9,870	1,606,350	-
Total All Funds	14,666,707	\$ 9,837,277	\$24,503,984	\$ 76,364,112	\$ 709,966	\$77,074,078	\$ 75,510,695	\$ 1,465,893	\$76,976,588	\$ 24,601,474

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
(001) GENERAL FUND							
REVENUES:							
Taxes	\$28,546,334	\$27,259,500	\$0	\$27,259,500	\$28,853,140	\$0	\$28,853,140
Property Tax	7,259,756	7,412,100	-	7,412,100	7,508,500	-	7,508,500
Local Sales & Use Tax	11,946,044	10,060,000	-	10,060,000	10,724,000	-	10,724,000
Sales/Parks	671,080	575,000	-	575,000	670,000	-	670,000
Brokered Natural Gas Use Tax	39,494	34,000	-	34,000	45,000	-	45,000
Criminal Justice Sales Tax	1,213,087	1,043,000	-	1,043,000	1,188,900	-	1,188,900
Admissions Tax	96,599	200,000	-	200,000	275,000	-	275,000
Utility Tax	5,402,943	5,479,100	-	5,479,100	5,556,700	-	5,556,700
Leasehold Tax	6,903	4,300	-	4,300	5,000	-	5,000
Gambling Tax	1,910,429	2,452,000	-	2,452,000	2,880,040	-	2,880,040
Franchise Fees	4,289,904	4,269,000	-	4,269,000	4,397,000	-	4,397,000
Cable, Water, Sewer, Solid Waste	3,082,339	3,029,000	-	3,029,000	3,119,800	-	3,119,800
Tacoma Power	1,204,366	1,240,000	-	1,240,000	1,277,200	-	1,277,200
Small Cell	3,200	-	-	-	-	-	-
Development Service Fees	2,252,765	1,755,200	-	1,755,200	1,772,600	-	1,772,600
Building Permits	992,686	735,600	-	735,600	743,000	-	743,000
Other Building Permit Fees	273,605	297,700	-	297,700	300,600	-	300,600
Plan Review/Plan Check Fees	810,634	581,400	-	581,400	587,200	-	587,200
Other Zoning/Development Fees	175,840	140,500	-	140,500	141,800	-	141,800
Licenses & Permits	354,013	382,525	-	382,525	382,500	-	382,500
Business License	254,104	276,525	-	276,525	276,500	-	276,500
Alarm Permits & Fees	63,533	70,000	-	70,000	70,000	-	70,000
Animal Licenses	36,376	36,000	-	36,000	36,000	-	36,000
State Shared Revenues	1,479,167	1,236,695	-	1,236,695	1,246,265	-	1,246,265
Sales Tax Mitigation	-	-	-	-	-	-	-
Criminal Justice	179,221	173,235	-	173,235	176,235	-	176,235
Criminal Justice High Crime	427,878	249,450	-	249,450	249,500	-	249,500
Liquor Excise Tax	393,090	339,770	-	339,770	334,600	-	334,600
Liquor Board Profits	478,969	474,240	-	474,240	485,930	-	485,930
Marijuana Enforcement/Excise Tax	8	-	-	-	-	-	-
Intergovernmental	453,830	288,665	(72,555)	216,110	275,274	-	275,274
Police FBI & Other Misc	12,870	12,000	-	12,000	12,000	-	12,000
Police-Animal Svcs-Steilacoom	15,630	16,601	-	16,601	16,843	-	16,843
Police-Animal Svcs-Dupont	33,917	34,514	-	34,514	35,164	-	35,164
Police-South Sound 911 Background Investigations	17,298	15,000	-	15,000	15,000	-	15,000
Muni Court-University Place Contract	251,187	20,000	(14,000)	6,000	-	-	-
Muni Court-Town of Steilacoom Contract	87,364	103,000	(39,084)	63,916	106,090	-	106,090
Muni Court-City of Dupont	35,565	87,550	(19,471)	68,079	90,177	-	90,177

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2021 Prop Adjustment	2021 Prop Revised Budget
(001) GENERAL FUND-continued							
Charges for Services & Fees	1,257,038	1,331,550	-	1,331,550	1,414,300	-	1,414,300
Parks & Recreation Fees	127,720	191,250	-	191,250	274,000	-	274,000
Police - Various Contracts	6,851	7,000	-	7,000	7,000	-	7,000
Police - Towing Impound Fees	-	1,000	-	1,000	1,000	-	1,000
Police - Extra Duty	766,949	775,000	-	775,000	775,000	-	775,000
Police - Western State Hospital Community Policing	355,500	355,500	-	355,500	355,500	-	355,500
Other	18	1,800	-	1,800	1,800	-	1,800
Fines & Forfeitures	1,243,163	1,363,205	-	1,363,205	1,363,205	-	1,363,205
Municipal Court	608,159	663,205	-	663,205	663,205	-	663,205
Photo Infraction	635,004	700,000	-	700,000	700,000	-	700,000
Miscellaneous/Interest/Other	161,833	129,201	-	129,201	136,967	-	136,967
Interest Earnings	52,458	67,930	-	67,930	67,930	-	67,930
Penalties & Interest - Taxes	34,294	7,500	-	7,500	7,500	-	7,500
Miscellaneous/Other	75,081	53,771	-	53,771	61,537	-	61,537
Interfund Transfers	284,700	284,700	-	284,700	284,700	-	284,700
Transfers In - Fund 401 SWM	284,700	284,700	-	284,700	284,700	-	284,700
Subtotal Operating Revenues	\$40,322,748	\$38,300,241	(\$72,555)	\$38,227,686	\$40,125,951	\$0	\$40,125,951
<i>EXPENDITURES:</i>							
City Council	134,101	148,287	-	148,287	148,304	-	148,304
Legislative	134,101	144,837	-	144,837	144,854	-	144,854
Sister City	-	3,450	-	3,450	3,450	-	3,450
City Manager	636,362	709,664	-	709,664	786,062	-	786,062
Executive	551,617	590,909	-	590,909	636,217	-	636,217
Communications	84,745	118,755	-	118,755	149,845	-	149,845
Municipal Court	1,853,556	1,990,524	-	1,990,524	2,009,513	-	2,009,513
Judicial Services	1,045,965	1,076,121	-	1,076,121	1,072,114	-	1,072,114
Professional Services	562,198	572,000	-	572,000	588,000	-	588,000
Probation & Detention	245,393	342,403	-	342,403	349,399	-	349,399
Administrative Services	1,840,554	1,911,795	-	1,911,795	1,976,091	-	1,976,091
Finance	1,265,348	1,285,342	-	1,285,342	1,323,536	-	1,323,536
Human Resources	575,206	626,453	-	626,453	652,555	-	652,555
Legal	1,430,939	1,623,752	-	1,623,752	1,631,231	-	1,631,231
Civil Legal Services	956,930	1,024,671	-	1,024,671	1,069,919	-	1,069,919
Criminal Prosecution Services	164,818	220,030	-	220,030	232,432	-	232,432
City Clerk	217,889	224,051	-	224,051	238,880	-	238,880
Election	91,302	155,000	-	155,000	90,000	-	90,000

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2021 Prop Adjustment	2021 Prop Revised Budget
(001) GENERAL FUND-continued							
Community & Economic Development	2,188,040	2,519,919	-	2,519,919	2,666,419	-	2,666,419
Current Planning	715,817	775,895	-	775,895	806,399	-	806,399
Long Range Planning	196,147	240,978	-	240,978	254,760	-	254,760
Building	1,135,909	1,303,025	-	1,303,025	1,387,786	-	1,387,786
Economic Development	140,167	200,021	-	200,021	217,474	-	217,474
Parks, Recreation & Community Services	2,407,609	2,930,296	-	2,930,296	2,998,234	-	2,998,234
Human Services	370,123	482,039	-	482,039	493,607	-	493,607
Administration	341,371	312,761	-	312,761	323,442	-	323,442
Recreation	297,314	479,387	-	479,387	488,877	-	488,877
Senior Services	180,325	243,300	-	243,300	247,197	-	247,197
Parks Facilities	424,886	444,965	-	444,965	453,027	-	453,027
Fort Steilacoom Park	619,238	789,994	-	789,994	811,231	-	811,231
Street Landscape Maintenance	174,352	177,850	-	177,850	180,853	-	180,853
Police	22,929,739	24,460,328	28,000	24,488,328	24,985,911	28,000	25,013,911
Command	3,413,795	3,840,266	-	3,840,266	4,046,127	-	4,046,127
Jail Service	365,591	700,000	-	700,000	700,000	-	700,000
Dispatch Services/SS911	2,048,834	1,995,290	28,000	2,023,290	1,995,290	28,000	2,023,290
Investigations	3,898,138	4,326,224	-	4,326,224	4,399,550	-	4,399,550
Patrol	7,522,202	7,743,477	-	7,743,477	7,902,057	-	7,902,057
Special Units	291,102	268,573	-	268,573	270,533	-	270,533
SWAT/Special Response Team	46,209	70,730	-	70,730	70,730	-	70,730
Neighborhood Policing Unit (Formerly Crime Prevention)	1,287,326	1,328,064	-	1,328,064	1,353,865	-	1,353,865
Contracted Services (Extra Duty, offset by Revenue)	900,942	775,000	-	775,000	775,000	-	775,000
Community Safety Resource Team (CSRT)	370,379	478,116	-	478,116	484,226	-	484,226
Training	749,949	812,777	-	812,777	833,722	-	833,722
Traffic Policing	883,041	955,384	-	955,384	967,924	-	967,924
Property Room	229,129	314,888	-	314,888	321,692	-	321,692
Reimbursements	276,459	64,650	-	64,650	64,650	-	64,650
Emergency Management	47,987	39,640	-	39,640	39,640	-	39,640
Animal Control	319,129	357,249	-	357,249	370,905	-	370,905
Road & Street/Camera Enforcement	279,528	390,000	-	390,000	390,000	-	390,000
Non-Departmental	107,234	136,925	9,463	146,388	136,925	9,463	146,388
Citywide	107,234	136,925	-	136,925	136,925	-	136,925
IT 6-Year Strategic Plan & Fleet/Equip Reserves	-	-	9,463	9,463	-	9,463	9,463
Interfund Transfers	1,462,408	1,866,823	896	1,867,719	1,873,112	896	1,874,008
Transfer to Fund 101 Street O&M	981,149	1,381,902	896	1,382,798	1,390,574	896	1,391,470
Transfer to Fund 105/190 Abatement Program	35,000	35,000	-	35,000	35,000	-	35,000
Transfer to Fund 201 GO Bond Debt Service	446,260	449,921	-	449,921	447,538	-	447,538
Subtotal Operating Expenditures	\$34,990,543	38,298,313	38,359	38,336,672	\$39,211,802	\$38,359	\$39,250,161
OPERATING INCOME (LOSS)	5,332,205	1,928		(108,986)	914,149		875,790
As a % of Operating Expenditures	15.2%	0.01%		-0.28%	2.33%		2.23%

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2021 Prop Adjustment	2021 Prop Revised Budget
(001) GENERAL FUND-continued							
<i>OTHER FINANCING SOURCES:</i>							
Grants, Donations/Contrib, 1-Time	3,423,957	100,250	356,217	456,467	100,250	-	100,250
Contributions/Donations/Other	58,319	100,250	55,446	155,696	100,250	-	100,250
Grants	3,365,638	-	300,771	300,771	-	-	-
Transfers In	-	-	-	-	-	-	-
Transfer In - Fund 501 Fleet & Equipment	-	-	-	-	-	-	-
Subtotal Other Financing Sources	\$3,423,957	\$100,250	\$356,217	\$456,467	\$100,250	\$0	\$100,250
<i>OTHER FINANCING USES:</i>							
Capital & Other 1-Time	4,042,415	185,656	1,137,566	1,323,222	137,028	250,000	387,028
Municipal Court	76,136	8,800	53,875	62,675	8,279	-	8,279
City Council	-	-	-	-	-	-	-
City Manager	1,666	17,131	-	17,131	2,005	-	2,005
Administrative Services	5,447	8,190	-	8,190	7,705	-	7,705
City-Wide COVID-19 Grants	2,877,860	-	-	-	-	-	-
IT 6-Year Strategic Plan & I/S Charges to be Allocated	-	-	352,363	352,363	-	-	-
Legal/Clerk	34,302	8,825	30,313	39,138	8,302	-	8,302
Community & Economic Development	293,575	14,663	249,055	263,718	13,796	-	13,796
Parks, Recreation & Community Services	156,355	40,262	121,628	161,890	14,358	-	14,358
Police	597,075	87,785	330,332	418,117	82,583	250,000	332,583
Interfund Transfers	1,014,676	880,000	663,497	1,543,497	745,465	75,000	820,465
Transfer Out - Fund 101 Street	-	-	75,460	75,460	-	-	-
Transfer Out - Fund 105 Property Abatement/RHSP	50,000	50,000	24,287	74,287	50,000	-	50,000
Transfer Out - Fund 106 Public Art	-	-	-	-	-	-	-
Transfer Out - Fund 192 SSMCP	50,000	50,000	-	50,000	50,000	-	50,000
Transfer Out - Fund 301 Parks CIP	494,129	80,000	563,750	643,750	212,500	75,000	287,500
Transfer Out - Fund 302 Transportation CIP	393,547	700,000	-	700,000	432,965	-	432,965
Transfer Out - Fund 311 Sewer	27,000	-	-	-	-	-	-
Subtotal Other Financing Uses	\$5,057,091	\$1,065,657	\$1,801,063	\$2,866,719	\$882,491	\$325,000	\$1,207,493
Total Revenues and Other Sources	\$43,746,705	\$38,400,491	\$283,662	\$38,684,153	\$40,226,201	\$0	\$40,226,201
Total Expenditures and other Uses	\$40,047,634	39,363,970	\$1,839,422	\$41,203,391	\$40,094,292	\$363,359	\$40,457,651
Beginning Fund Balance:	\$9,874,049	\$5,767,631	\$7,805,489	\$13,573,121	\$4,804,154	\$6,249,732	\$11,053,886
Ending Fund Balance:	\$13,573,121	\$4,804,152	\$6,249,729	\$11,053,886	\$4,936,063	\$5,886,373	\$10,822,435
Ending Fund Balance as a % of Gen/Street Operating Rev	33.0%	12.2%		28.2%	12.0%		26.3%
Reserve - Total Target 12% of Gen/Street Operating Rev	\$4,941,561	\$4,711,458		\$4,702,752	\$4,936,063		\$4,936,063
2% Contingency Reserves	\$823,594	\$785,243		\$783,792	\$822,677		\$822,677
5% General Fund Reserves	\$2,058,984	\$1,963,108		\$1,959,480	\$2,056,693		\$2,056,693
5% Strategic Reserves	\$2,058,984	\$1,963,108		\$1,959,480	\$2,056,693		\$2,056,693
Unreserved / (12% Adopted Reserves Shortfall):	\$8,631,559	\$92,695		\$6,351,135	\$0		\$5,886,373

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 101 STREET OPERATIONS & MAINTENANCE							
<i>REVENUES:</i>							
Permits	147,196	106,500	-	106,500	152,500	-	152,500
Engineering Review Fees	40	-	-	-	-	-	-
Motor Vehicle Fuel Tax	709,693	855,410	-	855,410	855,410	-	855,410
Subtotal Operating Revenues	\$ 856,929	\$ 961,910	\$ -	\$ 961,910	\$ 1,007,910	\$ -	\$ 1,007,910
<i>EXPENDITURES:</i>							
Street Lighting	353,182	337,210	-	337,210	337,210	-	337,210
Traffic Control Devices	235,746	430,523	-	430,523	431,508	-	431,508
Snow & Ice Response	28,747	45,500	-	45,500	45,500	-	45,500
Road & Street Preservation	1,190,004	1,514,151	-	1,514,151	1,568,811	-	1,568,811
I/S Charges to be Allocated			896	896	-	896	896
Subtotal Operating Expenditures	\$1,807,679	\$2,327,384	\$896	\$2,328,280	2,383,029	896	2,383,925
OPERATING INCOME (LOSS)	(\$950,750)	(\$1,365,474)	(\$896)	(\$1,366,370)	(\$1,375,119)	(\$896)	(\$1,376,015)
<i>OTHER FINANCING SOURCES:</i>							
Grants	12,000	-	-	-	-	-	-
Donations/Contributions	-	-	-	-	-	-	-
Judgments, Settlements/Miscellaneous	319	-	-	-	-	-	-
Permits Deposits for Professional Services	-	-	-	-	-	-	-
Transfer In From General Fund	981,149	1,381,902	76,356	1,458,258	\$1,390,574	\$896	\$1,391,470
Subtotal Other Financing Sources	\$993,467	\$1,381,902	76,356	1,458,258	\$1,390,574	\$896	\$1,391,470
<i>OTHER FINANCING USES:</i>							
Grants/Other	-	-	20,139	20,139	-	-	-
Building, Vehicles, Equipment	47,507	16,428	-	16,428	15,454	-	15,454
I/S Charges to be Allocated	-		55,321	55,321		-	-
Construction - Traffic Control	-	-	-	-	-	-	-
Subtotal Other Financing Uses	\$47,507	\$16,428	\$75,460	\$91,888	\$15,454	\$0	\$15,454
Total Revenues and Other Sources	\$1,850,396	\$2,343,812	\$76,356	\$2,420,168	\$2,398,484	\$896	\$2,399,380
Total Expenditures and other Uses	\$1,855,185	\$2,343,812	\$76,356	\$2,420,168	\$2,398,483	\$896	\$2,399,379
Beginning Fund Balance:	\$4,789	\$0	\$0	\$0	\$0	\$0	\$0
Ending Fund Balance:	\$0	\$0	\$0	\$0	\$1	\$0	\$1

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 102 REAL ESTATE EXCISE TAX							
<i>REVENUES:</i>							
Real Estate Excise Tax	3,643,117	1,800,000	-	1,800,000	1,800,000	-	1,800,000
Interest Earnings	2,413	-	-	-	-	-	-
Transfer In - Fund 301 Parks CIP	100,000	-	-	-	-	-	-
Total Revenue	\$3,745,530	\$1,800,000	\$0	\$1,800,000	\$1,800,000	\$0	\$1,800,000
<i>EXPENDITURES:</i>							
Transfer Out - Fund 201 GO Bond Debt Service	535,296	1,240,900	-	1,240,900	1,242,400	-	1,242,400
Transfer Out - Fund 301 Parks CIP	519,589	158,000	-	158,000	624,500	-	624,500
Transfer Out - Fund 302 Transportation CIP	1,219,403	679,295	139,000	818,295	-	624,000	624,000
Total Expenditures	\$2,274,288	\$2,078,195	\$139,000	\$2,217,195	\$1,866,900	\$624,000	\$2,490,900
Beginning Fund Balance:	\$800,269	\$540,095	\$1,731,415	\$2,271,510	\$261,900	\$1,592,415	\$1,854,315
Ending Fund Balance:	\$2,271,510	\$261,900	\$1,592,415	\$1,854,315	\$195,000	\$968,415	\$1,163,415

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 103 LAKEWOOD TRANSPORTATION BENEFIT DISTRICT							
<i>REVENUES:</i>							
\$20 Vehicle License Fee (Net of State Admin Fee)	881,849	835,000	-	835,000	835,000	-	835,000
Interest Earnings	904	-	-	-	-	-	-
Total Revenue	\$882,753	\$835,000	-	835,000	\$835,000	\$0	\$835,000
<i>EXPENDITURES:</i>							
WCIA Risk Assessment	-	-	-	-	-	-	-
Audit	-	-	-	-	-	-	-
Transfer to Fund 302 Transportation Capital	247,457	-	-	-	640,000	-	640,000
Total Expenditures	\$247,457	\$0	\$0	\$0	\$640,000	\$0	\$640,000
Beginning Fund Balance:	\$52,457	\$640,000	\$47,753	\$687,753	\$1,475,000	\$47,753	\$1,522,753
Ending Fund Balance:	\$687,753	\$1,475,000	\$47,753	\$1,522,753	\$1,670,000	\$47,753	\$1,717,753

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 104 HOTEL/MOTEL LODGING TAX							
<i>REVENUES:</i>							
Special Hotel/Motel Lodging Tax (5%)	\$619,508	\$571,429	-	571,429	\$714,286	\$0	\$714,286
Transient Rental income Tax (2%)	247,803	228,571	-	228,571	285,714	-	285,714
Interest Earnings	7,693	-	-	-	-	-	-
Total Revenues	\$875,005	\$800,000	\$0	\$800,000	\$1,000,000	\$0	\$1,000,000
<i>EXPENDITURES:</i>							
Administration	-	-	-	-	-	-	-
Lodging Tax Programs	456,515	672,250	-	672,250	800,000	-	800,000
Lodging Tax Programs-Transfer Out to Fund 301 Parks CIP	320,093	-	140,941	140,941	-	-	-
Transfer Out - Fund 190 Grant			-	-		-	-
Total Expenditures	\$776,609	\$672,250	\$140,941	\$813,191	\$800,000	\$0	\$800,000
Beginning Fund Balance:	\$1,560,637	\$1,247,353	\$411,680	\$1,659,033	\$1,375,103	\$270,739	\$1,645,842
Ending Fund Balance (earmarked for next year's grant awards)	\$1,659,033	\$1,375,103	\$270,739	\$1,645,842	\$1,575,103	\$270,739	\$1,845,842

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 105 PROPERTY ABATEMENT & RENTAL HOUSING SAFETY PROGRAM							
REVENUES:							
Abatement Program:	132,535	147,000	-	147,000	86,500	-	86,500
Abatement Charges	59,134	77,000	-	77,000	45,000	-	45,000
Interest Earnings	13,401	35,000	-	35,000	6,500	-	6,500
Transfer In - Fund 001 General	60,000	35,000	-	35,000	35,000	-	35,000
Rental Housing Safety Program:	204,398	225,000	24,288	249,288	225,000	-	225,000
Transfer In - Fund 001 General	25,000	50,000	24,288	74,288	50,000	-	50,000
Rental Housing Safety Program Fees	179,398	175,000	-	175,000	175,000	-	175,000
1406 Affordable Housing Program:	72,316	98,000	-	98,000	98,000	-	98,000
Sales Tax	72,316	98,000	-	98,000	98,000	-	98,000
Total Revenues	\$409,250	\$470,000	\$24,288	\$494,288	\$409,500	\$0	\$409,500
EXPENDITURES:							
Abatement	92,934	147,000	610,386	757,386	86,500	-	86,500
Rental Housing Safety Program	263,719	225,000	-	225,000	225,000	-	225,000
1406 Affordable Housing Program	-	98,000	72,316	170,316	98,000	-	98,000
Total Expenditures	\$356,653	\$470,000	\$682,702	\$1,152,702	\$409,500	\$0	\$409,500
Beginning Fund Balance:	\$605,817	\$0	\$658,414	\$658,414	\$0	\$0	\$0
Ending Fund Balance:	\$658,414	\$0	\$0	\$0	\$0	\$0	\$0
Abatement Program	\$610,386	\$0		\$0	\$0		\$0
Rental Housing Safety Program	(\$24,288)	\$0		\$0	\$0		\$0
1406 Affordable Housing Program	\$72,316	\$0		\$0	\$0		\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 106 PUBLIC ART							
REVENUES:							
Interest Earnings	722	-	-	-	-	-	-
Facility Rentals	(3,000)	7,500	-	7,500	15,000	-	15,000
Total Revenues	(\$2,278)	\$7,500	\$0	\$7,500	\$15,000	\$0	\$15,000
EXPENDITURES:							
Arts Commission Programs	-	2,000	8,000	10,000	2,000	-	2,000
Public Art	5,000	5,500	127,500	133,000	13,000	-	13,000
Total Expenditures	\$5,000	\$7,500	\$135,500	\$143,000	\$15,000	\$0	\$15,000
Beginning Fund Balance:	\$142,778	\$0	\$135,500	\$135,500	\$0	\$0	\$0
Ending Fund Balance:	\$135,500	\$0	\$0	\$0	\$0	\$0	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 180 NARCOTICS SEIZURE							
<i>REVENUES:</i>							
Forfeitures	71,670	-	-	-	-	-	-
Law Enforcement Contracts	33,485	-	-	-	-	-	-
Interest Earnings	1,197	-	-	-	-	-	-
Total Revenues	\$106,352	\$0	\$0	\$0	\$0	\$0	\$0
<i>EXPENDITURES:</i>							
Investigations /Predictive Policing	162,477	-	106,196	106,196	-	-	-
Capital Purchases	-	120,000	-	120,000	-	-	-
Total Expenditures	\$162,477	\$120,000	\$106,196	\$226,196	\$0	\$0	\$0
Beginning Fund Balance:	\$282,321	\$120,000	\$106,196	\$226,196	\$0	\$0	\$0
Ending Fund Balance:	\$226,196	\$0	\$0	\$0	\$0	\$0	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 181 FELONY SEIZURE							
<i>REVENUES:</i>							
Forfeitures/Misc/Interest	42,660	-	-	-	-	-	-
Total Revenues	\$42,660	\$0	\$0	\$0	\$0	\$0	\$0
<i>EXPENDITURES:</i>							
Investigations/Predictive Policing	2,966	-	47,837	47,837	-	-	-
Total Expenditures	\$2,966	\$0	\$47,837	\$47,837	\$0	\$0	\$0
Beginning Fund Balance:	\$8,143	\$0	\$47,837	\$47,837	\$0	\$0	\$0
Ending Fund Balance:	\$47,837	\$0	\$0	\$0	\$0	\$0	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 182 FEDERAL SEIZURE							
<i>REVENUES:</i>							
Forfeitures	63,492	-	-	-	-	-	-
Interest Earnings	1,132	-	-	-	-	-	-
Total Revenues	\$64,624	\$0	\$0	\$0	\$0	\$0	\$0
<i>EXPENDITURES:</i>							
Crime Prevention	399	-	-	-	-	-	-
Capital	163,147	120,000	40,907	160,907	-	-	-
Total Expenditures	\$163,546	\$120,000	\$40,907	\$160,907	\$0	\$0	\$0
Beginning Fund Balance:	\$259,829	\$120,000	\$40,907	\$160,907	\$0	\$0	\$0
Ending Fund Balance:	\$160,907	\$0	\$0	\$0	\$0	\$0	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 190 CDBG							
<i>REVENUES:</i>							
Grants	385,342	595,000	3,733,614	4,328,614	595,000	-	595,000
Interest Earnings	16	-	-	-	-	-	-
Miscellaneous/Contributions	3,411	-	-	-	-	-	-
Total Revenues	\$388,769	\$595,000	\$3,733,614	\$4,328,614	\$595,000	\$0	\$595,000
<i>EXPENDITURES:</i>							
Grants	402,213	595,000	5,115,338	5,710,338	595,000	-	595,000
Transfer Out - Fund 302 Transportation	8,056	-	-	-	-	-	-
Total Expenditures	\$410,269	\$595,000	\$5,115,338	\$5,710,338	\$595,000	\$0	\$595,000
Beginning Fund Balance:	\$1,403,224	\$0	\$1,381,724	\$1,381,724	\$0	\$0	\$0
Ending Fund Balance:	\$1,381,724	\$0	\$0	\$0	\$0	\$0	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 191 NEIGHBORHOOD STABILIZATION PROGRAM							
<i>REVENUES:</i>							
Grant-NSP 1	43,741	-	-	-	-	-	-
Grant-NSP 3	57,505	-	-	-	-	-	-
Abatement Charges	-	63,000	(36,000)	27,000	37,000	-	37,000
Abatement Interest	12,257	15,000	-	15,000	5,000	-	5,000
Total Revenues	\$113,503	\$78,000	(\$36,000)	\$42,000	\$42,000	\$0	\$42,000
<i>EXPENDITURES:</i>							
Grant-NSP 1	822	78,000	219,114	297,114	42,000	-	42,000
Grant-NSP 3	-	-	-	-	-	-	-
Total Expenditures	\$822	\$78,000	\$219,114	\$297,114	\$42,000	\$0	\$42,000
Beginning Fund Balance:	\$142,433	\$0	\$255,114	\$255,114	\$0	\$0	\$0
Ending Fund Balance:	\$255,115	\$0	\$0	\$0	\$0	\$0	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 192 SSMCP (SOUTH SOUND MILITARY COMMUNITIES PARTNERSHIP)							
REVENUES:							
Grants	461,015	-	7,509,228	7,509,228	-	-	-
Partner Participation	186,000	177,500	8,500	186,000	177,500	-	177,500
Misc/Other	500	-	-	-	-	-	-
Transfer In From Fund 001 General	50,000	50,000	-	50,000	50,000	-	50,000
Total Revenues	\$697,515	\$227,500	\$7,517,728	\$7,745,228	\$227,500	\$0	\$227,500
EXPENDITURES:							
OEA/SSMCP	732,928	227,500	7,535,573	7,763,073	227,500	-	227,500
Total Expenditures	\$732,928	\$227,500	\$7,535,573	\$7,763,073	\$227,500	\$0	\$227,500
Beginning Fund Balance:	\$53,431	\$0	\$18,017	\$18,017	\$0	\$0	\$0
Ending Fund Balance:	\$18,018	\$0	\$172	\$172	\$0	\$0	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 195 PUBLIC SAFETY GRANTS							
REVENUES:							
Grants	535,656	132,328	345,748	478,076	132,328	-	132,328
Total Revenues	\$535,656	\$132,328	\$345,748	\$478,076	\$132,328	\$0	\$132,328
EXPENDITURES:							
Grants	535,656	132,328	345,748	478,076	132,328	-	132,328
Total Expenditures	\$535,656	\$132,328	\$345,748	\$478,076	\$132,328	\$0	\$132,328
Beginning Fund Balance:	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Ending Fund Balance:	\$0	\$0	\$0	\$0	\$0	\$0	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 201 GENERAL OBLIGATION BOND DEBT SERVICE							
REVENUES:							
Local Sales & Use Tax	-	-	-	-	-	-	-
Transfer-In From Fund 001 General	446,260	449,921	-	449,921	447,538	-	447,538
Transfer-In From Fund 102 REET	535,296	1,240,900	-	1,240,900	1,242,400	-	1,242,400
Total Revenues	\$981,556	\$1,690,821	\$0	\$1,690,821	\$1,689,938	\$0	\$1,689,938
EXPENDITURES:							
Principal & Interest - 59th Avenue	77,000	77,000	-	77,000	77,000	-	77,000
Principal & Interest - Police Station - 2009/2016	209,006	212,594	-	212,594	210,707	-	210,707
Principal & Interest - LOCAL LED Streetlight	154,650	160,327	-	160,327	158,707	-	158,707
Principle & Interest - Transportation Bond - 2019 LTGO	540,900	1,240,900	-	1,240,900	1,243,524	-	1,243,524
Total Expenditures	\$981,556	\$1,690,821	\$0	\$1,690,821	\$1,689,938	\$0	\$1,689,938
Beginning Fund Balance:	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Ending Fund Balance:	\$0	\$0	\$0	\$0	\$0	\$0	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 202 LOCAL IMPROVEMENT DISTRICT (LID) DEBT SERVICE							
REVENUES:							
Interest	1,407	-	-	-	-	-	-
Assessments	270,724	268,920	-	268,920	247,774	-	247,774
LID 1109 Bond Proceeds for Admin Fees (Fund 302)	17,730	-	-	-	-	-	-
Total Revenues	\$289,861	\$268,920	\$0	\$268,920	\$247,774	\$0	\$247,774
EXPENDITURES:							
Combined LID 1101/1103	10,800	109,833	241,221	351,054	950	(950)	-
LID 1108	65,521	62,749	-	62,749	59,977	-	59,977
LID 1109	600	96,338	6,817	103,155	94,686	93,111	187,797
Total Expenditures	76,921	268,920	248,038	516,958	155,613	92,161	247,774
Beginning Fund Balance:	\$35,097	\$0	\$248,038	\$248,038	\$0	\$0	\$0
Ending Fund Balance:	\$248,038	\$0	\$0	\$0	\$92,161	(\$92,161)	\$0

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 204 SEWER PROJECT DEBT SERVICE							
<i>REVENUES:</i>							
Sewer Charges (4.75% Sewer Surcharge)	791,007	785,000	-	785,000	805,000	-	805,000
Interest Earnings/Other	4,127	6,600	-	6,600	9,200	-	9,200
Sanitary Side Sewer Connection Home Loan Repayment	30,413	8,881	-	8,881	20,058	-	20,058
Total Revenues	\$825,547	\$800,481	\$0	\$800,481	\$834,258	\$0	\$834,258
<i>EXPENDITURES:</i>							
Principal & Interest	482,554	480,086	-	480,086	477,618	-	477,618
Transfer To Fund 311 Sewer Capital	55,000	190,000	-	190,000	50,000	-	50,000
Total Expenditures	\$537,554	\$670,086	\$0	\$670,086	\$527,618	\$0	\$527,618
Beginning Fund Balance:	\$319,321	\$578,288	\$29,025	\$607,313	\$708,683	\$29,025	\$737,708
Ending Fund Balance:	\$607,313	\$708,683	\$29,025	\$737,708	\$1,015,323	\$29,025	\$1,044,348

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 251 LOCAL IMPROVEMENT DISTRICT (LID) GUARANTY DEBT SERVICE							
<i>REVENUES:</i>							
Interest Earnings	674	-	-	-	-	-	-
Total Revenues	\$674	\$0	\$0	\$0	\$0	\$0	\$0
<i>EXPENDITURES:</i>							
Transfer Out - Fund 001 General	-	-	-	-	-	-	-
Total Expenditures	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Beginning Fund Balance:	\$132,294	\$132,294	\$674	\$132,968	\$132,294	\$132,294	\$132,294
Ending Fund Balance:	\$132,968	\$132,294	\$674	\$132,968	\$132,294	\$132,294	\$132,294

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 301 PARKS CAPITAL							
<i>REVENUES:</i>							
Grants	2,112,047	332,000	2,277,549	2,609,549	2,518,000	-	2,518,000
Motor Vehicle Excise Tax for Paths & Trails	4,198	-	-	-	-	-	-
Funds Anticipated	-	-	-	-	-	-	-
Interest Earnings	12,768	-	-	-	-	-	-
Contributions/Donations/Utility & Developers	5,023	-	10,000	10,000	-	-	-
Transfer In From Fund 001 General	494,129	80,000	563,750	643,750	212,500	75,000	287,500
Transfer In From Fund 102 REET	519,589	158,000	-	158,000	624,500	-	624,500
Transfer In From Fund 104 Hotel/Motel Lodging Tax	320,093	-	140,941	140,941	-	-	-
Transfer In From Fund 302 Transportation CIP			-	-		-	-
Transfer In From Fund 401 Surface Water Mgmt	-	-	206,277	206,277	-	-	-
Transfer In - Fund 502 Property Management		-	-	-		-	-
Total Revenues	\$3,467,848	\$570,000	\$3,198,517	\$3,768,517	\$3,355,000	\$75,000	\$3,430,000
<i>EXPENDITURES:</i>							
Capital	3,478,905	570,000	5,779,899	6,349,899	3,355,000	75,000	3,430,000
Transfer to Fund 102 REET	100,000	-	-	-	-	-	-
Total Expenditures	\$3,578,905	\$570,000	\$5,779,899	\$6,349,899	\$3,355,000	\$75,000	\$3,430,000
Beginning Fund Balance:	\$2,716,557	\$0	\$2,605,500	\$2,605,500	\$0	\$24,118	\$24,118
Ending Fund Balance:	\$2,605,500	\$0	\$24,118	\$24,118	\$0	\$24,118	\$24,118

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 302 TRANSPORTATION CAPITAL PROJECT							
<i>REVENUES:</i>							
Motor Vehicle Excise Tax	285,676	344,330	-	344,330	330,560	-	330,560
State Transportation Package - Multi-Modal Distribution	81,341	80,440	-	80,440	79,240	-	79,240
State Transportation Package - Increased Gas Tax (MVET)	71,174	70,235	-	70,235	69,635	-	69,635
Traffic Mitigation Fees	-	-	-	-	-	-	-
Pavement Degradation Fees	28,135	-	-	-	-	-	-
Grants	2,192,897	4,786,400	3,689,028	8,475,428	4,859,598	-	4,859,598
Contributions from Utilities/Developers/Partners	150,126	150,302	-	150,302	-	-	-
LID Financing	922,757	-	-	-	-	-	-
Proceeds from Sale of Asset/Street Vacation	65,203	-	-	-	-	-	-
Interest/Other	57,861	-	-	-	-	-	-
Interfund Loan From Fleet & Equipment Reserves	-	-	-	-	-	-	-
GO Bond Proceeds	3,029,885	6,600,000	-	6,600,000	-	-	-
Transfer In - Fund 001 General	512,000	700,000	-	700,000	432,965	-	432,965
Transfer In - Fund 102 REET	1,100,950	679,295	-	679,295	-	624,200	624,200
Transfer In - Fund 103 TBD	247,457	-	139,000	139,000	640,000	-	640,000
Transfer In - Fund 190 CDBG	8,056	-	300,000	300,000	-	-	-
Transfer In - Fund 401 SWM	492,901	1,575,000	2,741,155	4,316,155	3,000,000	-	3,000,000
Total Revenues	\$9,246,420	\$14,986,002	\$6,869,183	\$21,855,185	\$9,411,998	\$624,200	\$10,036,198
<i>EXPENDITURES:</i>							
Capital Projects	14,571,364	15,732,002	10,521,219	26,253,221	9,411,998	300,000	9,711,998
Debt Issue Cost	19,500	-	-	-	-	-	-
Transfer Out - Fund 201 GO Bond Debt Service	17,730	-	-	-	-	-	-
Transfer Out - Fund 401 SWM	241,840	-	-	-	-	-	-
Intefund Loan Repayment	880,204	-	-	-	-	-	-
Total Expenditures	\$15,730,638	\$15,732,002	\$10,521,219	\$26,253,221	\$9,411,998	\$300,000	\$9,711,998
Beginning Fund Balance:	\$11,354,136	\$927,246	\$3,942,672	\$4,869,918	\$181,246	\$290,636	\$471,882
Ending Fund Balance:	\$4,869,918	\$181,246	\$290,636	\$471,882	\$181,246	\$614,836	\$796,082

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 311 SEWER CAPITAL PROJECT							
<i>REVENUES:</i>							
Grants	-	-	75,000	75,000	-	-	-
Sewer Availability Charge	297,919	165,000	-	165,000	200,000	-	200,000
Interest Earnings	5,381	-	-	-	-	-	-
Proceeds from Lien	1,543	-	-	-	-	-	-
Transfer In Fund 001 General	27,000	-	-	-	-	-	-
Transfer In From Fund 401 Surface Water Mgmt	8,000	-	-	-	-	-	-
Transfer In Fund 204 Sewer Debt (Sewer Surcharge 4.75%)	55,000	190,000	-	190,000	50,000	-	50,000
Transfer In Fund 312 Sanitary Sewer Connection Capital	-	-	-	-	-	-	-
Total Revenues	\$394,844	\$355,000	\$75,000	\$430,000	\$250,000	\$0	\$250,000
<i>EXPENDITURES:</i>							
Capital/Administration	108,745	795,000	948,461	1,743,461	130,000	-	130,000
Total Expenditures	\$108,745	\$795,000	\$948,461	1,743,461	130,000	-	130,000
Beginning Fund Balance:	\$1,245,820	\$465,586	\$1,066,333	\$1,531,919	\$25,586	\$192,872	\$218,458
Ending Fund Balance:	\$1,531,919	\$25,586	\$192,872	\$218,458	\$145,586	\$192,872	\$338,458

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 401 SURFACE WATER MANAGEMENT							
REVENUES:							
Storm Drainage Fees & Charges	4,480,680	4,351,500	-	4,351,500	4,466,400	-	4,466,400
Site Development Permit Fee	74,816	50,000	-	50,000	50,000	-	50,000
Interest Earnings & Misc	22,701	15,600	-	15,600	15,600	-	15,600
Subtotal Operating Revenues	\$4,578,197	\$4,417,100	\$0	\$4,417,100	\$4,532,000	\$0	\$4,532,000
% Revenue Change over Prior Year	4.44%						
EXPENDITURES:							
Engineering Services	1,430,305	1,860,808	-	1,860,808	1,912,690	-	1,912,690
Operations & Maintenance	623,702	961,416	407	961,823	965,501	407	965,908
Revenue Bonds - Debt Service (15-Year Life, 4%)		126,000	-	126,000	501,000	-	501,000
Transfer to Fund 001 General Admin Support	284,700	284,700	-	284,700	284,700	-	284,700
Subtotal Operating Expenditures	\$2,338,707	\$3,232,924	\$407	\$3,233,331	\$3,663,891	\$407	\$3,664,298
OPERATING INCOME (LOSS)	\$2,239,490	\$1,184,176		\$1,183,769	\$868,109		\$867,702
As a % of Operating Expenditures	95.8%	36.6%		36.6%	23.7%		23.7%
OTHER FINANCING SOURCES:							
Grants/Contributions	120,168	-	3,435	3,435	-	-	-
American Lake Management District	32,337	33,285	-	33,285	33,476	-	33,476
Flood Control Opportunity Fund	300,202	-	-	-	-	-	-
Revenue Bonds - Bond Proceeds	-	1,000,000	-	1,000,000	3,000,000	-	3,000,000
Transfer In From Fund 302 Transportation Capital	241,840	-	-	-	-	-	-
Subtotal Other Financing Sources	\$694,546	\$1,033,285	\$3,435	\$1,036,720	\$3,033,476	\$0	\$3,033,476
OTHER FINANCING USES:							
Capital/1-Time	903,821	231,665	457,883	689,548	906,270	-	906,270
American Lake Management District	14,584	29,886	31,235	61,121	30,464	-	30,464
Transfer to Fund 301 Parks CIP	-	-	206,277	206,277	-	-	-
Transfer to Fund 302 Transportation Capital	492,901	1,575,000	2,741,155	4,316,155	3,000,000	-	3,000,000
Transfer To Fund 311 Sewer Capital	8,000	-	-	-	-	-	-
Subtotal Other Financing Uses	\$1,419,305	\$1,836,551	\$3,436,550	\$5,273,101	\$3,936,734	\$0	\$3,936,734
Total Revenues and Other Sources	\$5,272,743	\$5,450,385	\$3,435	\$5,453,820	\$7,565,476	\$0	\$7,565,476
Total Expenditures and other Uses	\$3,758,013	\$5,069,475	\$3,436,957	\$8,506,432	\$7,600,625	\$407	\$7,601,032
Beginning Fund Balance:	\$4,306,289	\$1,500,405	\$4,320,614	\$5,821,019	\$1,881,315	\$887,091	\$2,768,407
Ending Fund Balance:	\$5,821,019	\$1,881,315	\$887,092	\$2,768,407	\$1,846,166	\$886,684	\$2,732,851
Ending Fund Balance as a % of Operating Rev/Exp	127.1%	42.6%		62.7%	40.7%		60.3%
17% Operating Reserves (of operating revenues)	\$0	\$0		\$0	\$0		\$0
33% Operating Reserves (of operating expenditures)	\$771,773	\$1,066,865		\$1,066,999	\$1,209,084		\$1,209,218
1% Capital Reserves	\$453,795	\$490,130		\$490,130	\$529,130		\$529,130
American Lake Management District	\$31,237	\$33,606		(\$0)	\$33,604		(\$0)
Unreserved / (Shortfall):	\$4,564,214	\$290,714		\$1,211,278	\$74,347		\$994,503

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 501 FLEET & EQUIPMENT							
<i>OPERATING REVENUES:</i>							
M&O Revenue	617,408	740,720	-	740,720	740,720	-	740,720
Interest Earnings	19,472	15,000	-	15,000	15,000	-	15,000
Total Revenues	\$636,880	\$755,720	\$0	\$755,720	\$755,720	\$0	\$755,720
<i>OPERATING EXPENDITURES:</i>							
Fuel/Gasoline	255,476	424,150	-	424,150	424,150	-	424,150
Other Supplies	11,720	3,990	-	3,990	3,990	-	3,990
Repairs & Maintenance	380,884	327,580	-	327,580	327,580	-	327,580
Other Services & Charges	516	-	-	-	-	-	-
Intergovernmental	-	-	-	-	-	-	-
Total Expenditures	\$648,597	\$755,720	\$0	\$755,720	\$755,720	\$0	\$755,720
Operating Revenue Over/(Under) Expenditures	(\$11,716)	\$0	\$0	\$0	\$0	\$0	\$0
<i>OTHER FINANCING SOURCES:</i>							
Interfund Loan (Fund 302 LID Interim Financing)	880,204	-	-	-	-	-	-
Replacement Reserves Collections	-	-	-	-	-	-	-
Capital Contribution	25,807	-	24,900	24,900	-	-	-
Proceeds From Sale of Assets	11,716	-	-	-	-	-	-
Transfer In From Fund 504 Risk Management	64,851	-	-	-	-	-	-
Total Other Financing Sources	\$982,578	\$0	\$24,900	\$24,900	\$0	\$0	\$0
<i>OTHER FINANCING USES:</i>							
Fleet & Equipment New & Replacement	490,005	484,000	203,552	687,552	252,000	-	252,000
Interfund Loan To Fund 302 LID Interim Financing	-	-	-	-	-	-	-
Transfer to Fund 001 General	-	-	-	-	-	-	-
Total Other Financing Uses	\$490,005	\$484,000	\$203,552	\$687,552	\$252,000	\$0	\$252,000
Total Revenues	\$1,619,459	\$755,720	\$24,900	\$780,620	\$755,720	\$0	\$755,720
Total Expenditures	\$1,138,602	\$1,239,720	\$203,552	\$1,443,272	\$1,007,720	\$0	\$1,007,720
Beginning Fund Balance:	\$3,780,451	\$3,940,779	\$320,529	\$4,261,308	\$3,456,779	\$141,877	\$3,598,656
Ending Fund Balance:	\$4,261,308	\$3,456,779	\$141,877	\$3,598,656	\$3,204,779	\$141,877	\$3,346,656

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 502 PROPERTY MANAGEMENT							
<i>OPERATING REVENUES:</i>							
M&O Revenue	702,611	695,603	-	695,603	698,917	-	698,917
Interest Earnings	2,278	-	-	-	-	-	-
Total Operating Revenues	\$ 704,888	\$ 695,603	\$ -	\$ 695,603	\$ 698,917	\$ -	\$ 698,917
<i>OPERATING EXPENDITURES:</i>							
City Hall Facility	374,899	381,034	-	381,034	383,354	-	383,354
Police Station	275,469	245,052	-	245,052	245,881	-	245,881
Parking Facilities/Light Rail	54,521	69,517	-	69,517	69,682	-	69,682
Total Operating Expenditures	\$ 704,888	\$ 695,603	\$ -	\$ 695,603	\$ 698,917	\$ -	\$ 698,917
Operating Revenue Over/(Under) Expenditures	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
<i>OTHER FINANCING SOURCES:</i>							
Replacement Reserve Collections / Other 1-Time	156,178	-	40,000	40,000	100,000	-	100,000
Total Other Financing Sources	\$ 156,178	\$ -	\$ 40,000	\$ 40,000	\$ 100,000	\$ -	\$ 100,000
<i>OTHER FINANCING USES:</i>							
Capital/1-Time/6-Year Property Management Plan	7,934	230,000	125,000	355,000	5,000	-	5,000
Total Other Financing Uses	\$ 7,934	\$ 230,000	\$ 125,000	\$ 355,000	\$ 5,000	\$ -	\$ 5,000
Total Revenues	\$ 861,066	\$ 695,603	\$ 40,000	\$ 735,603	\$ 798,917	\$ -	\$ 798,917
Total Expenditures	\$ 712,823	\$ 925,603	\$ 125,000	\$ 1,050,603	\$ 703,917	\$ -	\$ 703,917
Beginning Fund Balance:	\$436,057	\$389,124	\$195,176	\$584,300	\$159,124	\$110,176	\$269,300
Ending Fund Balance:	\$584,300	\$159,124	\$110,176	\$269,300	\$254,124	\$110,176	\$364,300

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 503 INFORMATION TECHNOLOGY							
<i>REVENUES:</i>							
M&O Revenue	1,337,482	1,891,129	-	1,891,129	1,952,033	-	1,952,033
Misc/Interest/Other	2,826	-	-	-	-	-	-
Total Operating Revenues	\$ 1,340,308	\$ 1,891,129	\$ -	\$ 1,891,129	\$ 1,952,033	\$ -	\$ 1,952,033
<i>EXPENDITURES:</i>							
Personnel	524,535	588,699	-	588,699	601,193	-	601,193
Supplies	60,796	179,520	-	179,520	179,520	-	179,520
Services & Charges	754,976	1,122,910	-	1,122,910	1,171,320	-	1,171,320
6-Year IT Strategic Plan	-	-	-	-	-	-	-
Total Operating Expenditures	\$1,340,308	\$1,891,129	\$0	\$1,891,129	\$1,952,033	\$0	\$1,952,033
Operating Revenue Over/(Under) Expenditures	\$0	\$0	\$0	\$0	\$0	\$0	\$0
<i>OTHER FINANCING SOURCES:</i>							
Replacement Reserve Collection	66,845	-	-	-	66,756	-	66,756
Capital Contrib & Other 1-Time /6-Year Strategic Plan	305,350	168,750	356,905	525,655	158,750	-	158,750
Total Other Financing Sources	\$372,195	\$168,750	\$356,905	\$525,655	\$225,506	\$0	\$225,506
<i>OTHER FINANCING USES:</i>							
One-Time/Capital	305,350	168,750	356,905	525,655	158,750	-	158,750
Total Other Financing Uses	\$305,350	\$168,750	\$356,905	\$525,655	\$158,750	\$0	\$158,750
Total Revenues	\$1,712,503	\$2,059,879	\$356,905	\$2,416,784	\$2,177,539	\$0	\$2,177,539
Total Expenditures	\$1,645,658	\$2,059,879	\$356,905	\$2,416,784	\$2,110,783	\$0	\$2,110,783
Beginning Fund Balance:	\$138,678	\$205,522	\$0	\$205,522	\$205,522	\$0	\$205,522
Ending Fund Balance:	\$205,523	\$205,522	\$0	\$205,522	\$272,278	\$0	\$272,278

	2020 Annual Actual	2021 Original	2021 Prop Adjustment	2021 Prop Revised Budget	2022 Original	2022 Prop Adjustment	2022 Prop Revised Budget
FUND 504 RISK MANAGEMENT							
<i>REVENUES:</i>							
M&O Revenue	1,289,027	1,396,480	9,870	1,406,350	1,396,480	9,870	1,406,350
AWC Retro Refund	128,938	-	-	-	-	-	-
Insurance Proceeds/3rd Party Recoveries	371,383	200,000	-	200,000	200,000	-	200,000
Total Revenues	\$1,789,348	\$1,596,480	\$9,870	\$1,606,350	\$1,596,480	\$9,870	\$1,606,350
<i>EXPENDITURES:</i>							
Safety Program	2,474	3,980	-	3,980	3,980	-	3,980
AWC Retro Program	37,356	37,500	-	37,500	37,500	-	37,500
WCIA Assessment	1,438,931	1,355,000	9,870	1,364,870	1,355,000	9,870	1,364,870
Claims/Judgments & Settlements	245,735	200,000	-	200,000	200,000	-	200,000
Total Expenditures	\$1,724,497	\$1,596,480	\$9,870	\$1,606,350	\$1,596,480	\$9,870	\$1,606,350
<i>OTHER FINANCING SOURCES:</i>							
Capital Contribution/1-Time M&O	-	-	-	-	-	-	-
Total Other Financing Sources	\$0	\$0	\$0	\$0	\$0	\$0	\$0
<i>OTHER FINANCING USES:</i>							
Transfer To Fund 501 Fleet & Equipment	64,851	-	-	-	-	-	-
Total Other Financing Uses	\$64,851	\$0	\$0	\$0	\$0	\$0	\$0
Total Revenues	\$1,789,348	\$1,596,480	\$9,870	\$1,606,350	\$1,596,480	\$9,870	\$1,606,350
Total Expenditures	\$1,789,348	\$1,596,480	\$9,870	\$1,606,350	\$1,596,480	\$9,870	\$1,606,350
Beginning Fund Balance:	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Ending Fund Balance:	\$0	\$0	\$0	\$0	\$0	\$0	\$0

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: May 3, 2021	TITLE: An Ordinance adopting Lakewood Station District Subarea (LSDS) Plan, Development Code (new LMC Title 18C), and amendments to LMC Title 18A.	TYPE OF ACTION: <input checked="" type="checkbox"/> ORDINANCE NO. 751 <input type="checkbox"/> RESOLUTION NO. <input type="checkbox"/> MOTION NO. <input type="checkbox"/> OTHER
REVIEW: March 8 Study Session; April 5 Public Hearing; April 19 Meeting; April 26 Study Session	ATTACHMENTS: Attachment A (Ordinance 751 with Exhibit A (LSDS Plan), Exhibit B (Development Code), and Exhibit C (Amendments to LMC Title 18A))	

SUBMITTED BY: David Bugher, Assistant City Manager for Development Services
Tiffany Speir, Long Range & Strategic Planning Manager

RECOMMENDATION: It is recommended that the City Council amend the proposed LSDS Development Code to reflect the originally included parking standards in LMC 18C.600.610. It is also recommended that the City Council approve the attached Ordinance 751: adopting the Lakewood Station District Subarea (LSDS) Plan; adopting the LSDS Development Code (new LMC Title 18C) (with or without the City Council's amendments); and adopting amendments to LMC Title 18A.

DISCUSSION: Lakewood has recognized the need for, and memorialized initial design for, a subarea plan in the Lakeview Neighborhood/Lakewood Station District since adopting its first Comprehensive Plan in 2000. In November 2019, the State Department of Commerce awarded Lakewood a grant under E2SHB 1923 to prepare a Lakewood Station District Subarea (LSDS) Plan, an implementing Hybrid Form-Based Code and a SEPA-based Planned Action. In-person and virtual stakeholder outreach was conducted between December 2019 and spring 2021 to help develop the proposed package.

Discussion, including proposed amendments to Ordinance 751, is continued on the following pages.

ALTERNATIVE(S): The Council could amend the proposed Ordinance and attachments; the Council could also not adopt the Ordinance and attachments.

FISCAL IMPACT: N/A

Prepared by
Tiffany Speir, Long Range & Strategic Planning Manager

Department Director
Dave Bugher, Assistant City Manager for Development Services



City Manager Review

BACKGROUND

On March 8, 2021, the City Council reviewed the draft Lakewood Station District Subarea (LSDS) Plan and the implementing Hybrid Form-Based Development Code as recommended by the Planning Commission; the Council also reviewed the Planned Action Ordinance. The City Council held a public hearing on April 5 on the LSDS Package (Plan, Code, and the Planned Action Ordinance (the Planned Action Ordinance itself was inadvertently left out of the April 5 meeting materials; the Planned Action SEPA Checklist was included in the meeting packet.)) The Council discussed the package on April 19 and decided to continue the package for an additional study session held April 26. Action on the Package is scheduled for May 3.

Included in this Memorandum below are the compiled amendments to Ordinances 751 and 752 for City Council consideration. Attached to this Memorandum at Attachments A-E are the items listed above.

RECOMMENDATION

It is recommended that the City Council adopt the five City-recommended amendments to Ordinances 751 by separate motions, consider and take action by motion on the amendment offered by Councilmember Brandstetter, and then adopt the amended Ordinance 751.

PROPOSED AMENDMENTS TO ORDINANCE 751:

I. Amend Ordinance 751 to edit the Plan's description of the MF3 zone as follows:

MF3 - Multi Family Three. The existing residential area of attached and detached single-family homes and low rise multifamily is zoned MF3. MF3 zoning is in areas where there is both an arterial and a nearby commercial or mixed-use district. This is intended to be a high density multifamily environment with multi-story housing with densities up to 54 units per acre. The LSDS overlay will provide form-based regulations that ~~effectively limit~~promote densities to 30-40 units per acres and incentivize row house development.

II. Amend Ordinance 751 to insert a footnote after Exhibit 23 of the LSDS Plan to clarify list of residential types.

Exhibit 23. Existing Residential Structures in the LSDS

Type of Residential Structures	Sum of Units
1 1/2 Story (finished space in attic or basement is the 1/2 story)	14
1 Story	68
2 Story	4
Apartment <= 3 Stories	20
Duplex 1 1/2 Story	2
Duplex One Story	20
Duplex Two Story	10
Home for the Elderly	5
Mixed Retail w/ Res Units	15
Multiple - Residential	662
Triplex One Story	3
Triplex Two Story	3
RV Park: 1 Apartment, 11 Mobile Homes & 26 RV Spaces	38
Grand Total	864

Source: Pierce County Assessor, BERK 2020.

Footnote: Data is based Pierce County Assessor Built-As information that the County relates to Marshall and Swift cost and depreciation tables. Thus, the unit type distinctions are more detailed and some categories may overlap traditional planning level unit type definitions (e.g. Apartment < 3 Stories and Multiple-Residential.)

III. Amend Ordinance 751 to edit the Plan’s description of the City’s adoption process:

Future Planned Public Engagement

The Draft LSDS plan ~~will go~~ has gone through a public engagement process to share and invite feedback on the draft vision, goals, policies, and environmental review information. This ~~will included~~ workshops with the Lakewood Planning Commission, an online open house, and a stakeholders meeting. The City of Lakewood ~~will hold a public hearing and a 30-day~~ held a community meeting with the Planning Commission and two SEPA comment periods to take comment on the draft Plan and regarding the Planned Action. ~~Comments received will be used to revise the Plan, which the Following a public hearing on the Subarea Plan and Form-Based Code, the~~ Planning Commission ~~will review prior to voting on~~ made a recommendation to the City Council. The City Council ~~will also hold~~ also held a public hearing on the Subarea Plan, Form-Based Code, and Planned Action prior to deciding on adoption. Comments received were used to revise the Plan and regulations.

Note: once the engagement is completed this section will be updated prior to finalization of the plan.

IV. Amend Ordinance 751 at LMC 18C.600.620(B) to reflect the originally proposed language (included below in red) regulating parking for senior citizen apartments and multifamily housing.

Land Use	LSDS Code Parking Requirements
Residential	<ul style="list-style-type: none">- Single-family: 2 per dwelling unit- Accessory dwelling: 1 per dwelling unit, provided that no additional parking is required when located within one-quarter mile of the Sounder Station. (RCW 36.70A.698)- Senior citizen apartments <u>1 per 3 dwelling units*</u>- Multifamily housing: <u>1.25 spaces per dwelling unit*</u> <p><i>*See process in Part B to prepare parking study to reduce further near station.</i></p>

4. Reduction for Housing in Proximity to Sounder Station (RCW 36.70A.620). When located within one-quarter mile of the Sounder Station, an applicant may apply for an exception allowing minimum parking requirements to be reduced at least to one parking space per bedroom or 0.75 space per unit, as justified through a parking study prepared to the satisfaction of the Community Development Director or their designee:

- housing units that are affordable to very low-income or extremely low- income individuals.
- housing units that are specifically for seniors or people with disabilities.
- market rate multifamily housing.

V. Amend Ordinance 751 to edit LMC 18C.700.720(A) as follows:

18C.700.720. Optional Master Planned Development

A. To establish specified uses in LMC 18C.200.220(B), ~~d~~Development within the Station Area District may apply for a master plan for the development ~~of (2) acres or greater.~~

VI. Councilmember Brandstetter Amendments to Ordinance 751

MOTION: Amend the Strategies subsection of the Lakewood Station District Subarea (LSDS) Plan Housing Chapter as follows:

Strategies

- Allow the highest density housing in mixed-use areas and in the Transit-Oriented Commercial zone where the availability of amenities within walking distance will encourage non-motorized transportation alternatives.
- Encourage and support missing middle housing types such as duplexes, triplexes, and small-scale multifamily as infill development in the residential area to improve housing choice and preserve affordability.
- Support the development of ground-related rowhouses as an affordable home ownership option for LSDS residents.
- Support the development of housing that meets the needs of military households.
- Partner with affordable housing organizations to support the expansion of subsidized housing in the LSDS by building new projects or by purchasing and rehabilitating existing housing.
- Work with the Washington Homeownership Center to find opportunities for investment in the LSDS.
- ~~Consider expansions of~~Review the existing and new affordable housing incentive programs ~~and new affordability incentives~~ in the LSDS, including the potential extension of the applicability area for the multi-family property tax exemption (MFTE))~~-~~ program.
- Consider programs to support residents in the LSDS as the area redevelops and reduce involuntary displacement such as a housing navigator program,~~a notice of intent to sell ordinance, just cause eviction protections, and right of first refusal incentives.~~
- Develop community benefit agreements with large investors in the LSDS.
- Provide information and support to help investors navigate affordable housing financing programs.
- Identify partners to support funding and a program that will rehabilitate existing homes such as a home repair program.

ATTACHMENT A

ORDINANCE NO. 751

AN ORDINANCE of the City Council of the City of Lakewood, Washington, adopting the Lakewood Station District Subarea Plan as an element of the Comprehensive Plan, amending the Lakewood Municipal Code by establishing a new Title 18C (Lakewood Station District Development Code), and amending Lakewood Municipal Code Chapters 18A.10 (Basic Provisions) and 18A.20 (Administration.)

I. RECITALS

WHEREAS, the City of Lakewood is a non-charter optional municipal code city as provided in Title 35A RCW, incorporated under the laws of the State of Washington, and planning pursuant to the Growth Management Act (GMA), Chapter 36.70A RCW; and

WHEREAS, the City has adopted a Comprehensive Plan under the GMA and, as provided in RCW 36.70A.080(2), is authorized to adopt a subarea plan as an optional planning element; and

WHEREAS, RCW 36.70A.130(2)(a)(i) exempts the initial adoption of a subarea plan from the GMA's limitation on Comprehensive Plan amendments to once per year; and

WHEREAS, the City's Comprehensive Plan includes text and policies authorizing and anticipating the adoption of a subarea plan for the Lakewood Station District (e.g., Section 1.3.1, 1.6.7, 2.3.7, 2.3.16, 2.4.1, Policy LU-17.3, Goal LU-21, Section 3.3.5, Goal LU-25, Policy LU-25.5, Goal LU-27 and Policies LU-27.1 – 27.3, Section 4.4, Section 4.5.2, Goal UD-9, Policy UD-9.5, Goal ED-5, and Land-Use Implementation Strategy 11.3.1); and

WHEREAS, In November, 2019, the State Department of Commerce awarded the City of Lakewood, WA a grant under ESB 1923 to: prepare a subarea plan for the Lakewood Station District per RCW 43.21C.420; prepare a planned action using a threshold determination process per RCW 43.21C.440 (1)(b)(ii); and prepare a hybrid form-based development code to implement the Lakewood Station District Subarea (LSDS) Plan; and

WHEREAS, the City prepared the LSDS Plan, Code and Planned Action after an extensive

public participation and review process for the subarea plan including an online survey, open houses, community meetings, stakeholder meetings, and study sessions and public meetings before the Planning Commission and the City Council in 2020 and 2021; and

WHEREAS, the City held the LSDS Planned Action Ordinance Community Meeting on November 4, 2020 as required by RCW 43.21C.440 (3)(b); and

WHEREAS, implementation of the LSDS Plan and amendments to the Lakewood Land Use and Development Code to adopt a new Chapter 18C (LSDS Development Code), would improve conditions for Lakewood residents and businesses. Design regulations would include standards related to: integration of the natural environment including new parks, building design, pedestrian experience and streetscapes, mixed-use building features, site planning, parking, lighting, screening, and signage; and

WHEREAS, the LSDS would increase housing capacity and commercial space and further support business investment with more flexible zoning and civic and infrastructure investments. Tax exemptions for low and moderate-income housing units in “Residential Target Areas” (RTAs) are allowed per LMC Chapter 3.64; most of the LSDS is within a RTA. Respondents to an online survey about the LSDS Plan showed a strong interest in: well designed and attractive development; sidewalks and pathways; new retail, restaurant and entertainment; new jobs in proximity to the Sounder Station; better transit, cycle and pedestrian connections to the Downtown; new mixed development to include homes (“missing middle” and affordable units), jobs, retail and services; and new open spaces and parks; and

WHEREAS, the LSDS job mix would change to more services jobs and less retail. Office and professional services jobs offer higher wages than typical retail jobs and support the increase of a jobs-housing balance. The Plan envisions and the Code encourages a dense mix of commercial and medical office, regional and local retail, services and hospitality, and high-density residential uses offering ownership and rental housing opportunities; and

WHEREAS, the Planning Commission, held a duly noticed public hearing on November

18, 2020 regarding the “LSDS Packet” (including the LSDS Plan, amendments to the Lakewood Municipal Code to adopt a new Chapter 18C (LSDS Development Code), associated amendments to the City's Comprehensive Plan text and Land Use Map, and associated amendments to the Lakewood Municipal Code at Chapters 18A.10 (Basic Provisions), and 18A.20 (Administration), reviewed the public record and made a recommendation to the City Council on February 3, 2021; and

WHEREAS, the City Council held a duly noticed public hearing on April 5, 2021 regarding the “LSDS Packet” (including the Plan, Code and Planned action) and reviewed the public record; and

WHEREAS, pursuant to RCW 36.70A.370, the City has utilized the process established by the Washington State Attorney General so as to assure the protection of private property rights; and

WHEREAS, pursuant to RCW 36.70A.106, on January 14, 2021, the City provided the Washington State Department of Commerce with a 60-day notice of its intent to adopt the amendments to City's Comprehensive Plan, and it was assigned number 2021-S-2258; and

WHEREAS, pursuant to RCW 43.21C and WAC 197-11-508, on November 12, 2020 the City submitted information to the Department of Ecology related to the Determination of Non-Significance for the LSDS SEPA Environmental Impact Checklist via the SEPA register and the materials were assigned #202005805; and

WHEREAS, on March 30, 2021, the City submitted a Revised LSDS Determination of Non-Significance (DNS) and SEPA Environmental Checklist to the Department of Ecology via the SEPA register and the materials were assigned #202101613.

II. FINDINGS

The procedural and substantive requirements of the State Environmental Policy Act (RCW 43.21C) have been complied with.

The procedural requirements of the Growth Management Act (RCW 36.70A) have been complied with.

The proposed amendments are consistent with the requirements of Revised Code of Washington (RCW), and the Washington Administrative Code (WAC.)

The proposed amendments are consistent with the City of Lakewood Comprehensive Plan.

The proposed amendments have been reviewed and processed in accordance with the requirements of Lakewood Municipal Code (LMC) Title 14 Environmental Protection, Title 14A Critical Areas, and Title 18A Land Use and Development of the City of Lakewood Municipal Code.

All of the facts set forth in the Recitals are true and correct, and are incorporated herein by reference.

All necessary public meetings and opportunities for public testimony and comment have been conducted in compliance with State law and the City's municipal code.

The Lakewood City Council finds and determines that the regulation of development and land use within the Downtown is within the City's regulatory authority.

The Lakewood City Council finds and determines that approval of such amendments to the Comprehensive Plan and Land Use and Development Code is in the best interests of the residents of Lakewood, and will promote the general health, safety and welfare.

The Lakewood City Council finds and determines that regulation of land use and development is subject to the authority and general police power of the City, and the City reserves its powers and authority to appropriately amend, modify and revise such land use controls in accordance with applicable law;

The documents and other materials that constitute the record of the proceedings upon which the City Council's action is based, which include, but are not limited to, the staff reports for the Project and all of the materials that support the staff reports for the Project, are located in the City of Lakewood, Community and Economic Development Department at 6000 Main Street SW, Lakewood,

Washington, 98499-5027. The custodian of these documents is the Assistant City Manager for

Development Services of the City of Lakewood;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON, DO ORDAIN as Follows:

Section 1. Adoption of the Lakewood Station District Subarea Plan. The Lakewood Station District Subarea Plan, filed with the Community and Economic Development Department and attached hereto as **Exhibit A**, is adopted as an optional element of the Comprehensive Plan.

Section 2. Adoption of Lakewood Station District Subarea Development Code. The City of Lakewood's Municipal Code is amended to include a new Title 18C, Lakewood Station District Subarea Development Code, filed with the Community and Economic Development Department and attached hereto as **Exhibit B**.

Section 3. Amendment of the Comprehensive Plan and Land Use Map. The City of Lakewood's Comprehensive Plan text and Land Use Map will be amended to reflect the adoption of the Lakewood Station District Subarea Plan as part of the 2021 Lakewood Comprehensive Plan amendment cycle.

Section 4. Amendment of Lakewood Municipal Code Title 18A. The City of Lakewood's Municipal Code Title 18A is amended at Chapter 18A.10 (Basic Provisions) and 18A.20 (Administration), filed with the Community and Economic Development Department and attached hereto as **Exhibit C**.

Section 5. Severability. If any portion of this Ordinance or its application to any person or circumstances is held invalid, the remainder of the Ordinance or the application of the provision to other persons or circumstances shall not be affected.

Section 6. Effective Date. That this Ordinance shall be in full force and effect thirty (30) days after publication of the Ordinance Summary.

ADOPTED by the City Council this 3rd day of May, 2021.

CITY OF LAKEWOOD

Attest:

Don Anderson, Mayor

Briana Schumacher, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney

EXHIBIT A

Lakewood Station District Subarea Plan

EXHIBIT B

Lakewood Station District Subarea Development Code (new LMC Title 18C)

EXHIBIT C

LMC Title 18A Amendments

EXHIBIT A

STATION DISTRICT SUBAREA PLAN



City of Lakewood – DRAFT March 29, 2021

Prepared for the City of Lakewood

Prepared by: BERK, Fehr & Peers, Framework, and Skipstone



Acknowledgements

Lakewood's elected and appointed officials and City staff wish to acknowledge those in the community that participated and contributed their valuable assistance in the preparation and production of this document.

Lakewood City Council (2020)

Don Anderson, Mayor
 Jason Whalen, Deputy Mayor
 Paul Bocchi
 Michael Brandstetter
 Linda Farmer
 Mary Moss
 John Simpson

Lakewood City Council (2021)

Don Anderson, Mayor
 Jason Whalen, Deputy Mayor
 Patti Belle
 Paul Bocchi
 Michael Brandstetter
 Linda Farmer
 Mary Moss

Lakewood Planning Commission (2020)

Don Daniels, Chair
 Connie Coleman-Lacadie, Vice-Chair
 James Guerrero
 Nancy Hudson-Echols
 Ryan Pearson
 John Paul Wagemann

City of Lakewood

John Caulfield, City Manager
 M. David Bugher, Community & Economic Development Director
 Tiffany Speir, Long Range and Strategic Planning Manager
 Becky Newton, Economic Development Administrator
 Paul Bucich, Public Works Director
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 Josh Steiner, Fehr & Peers

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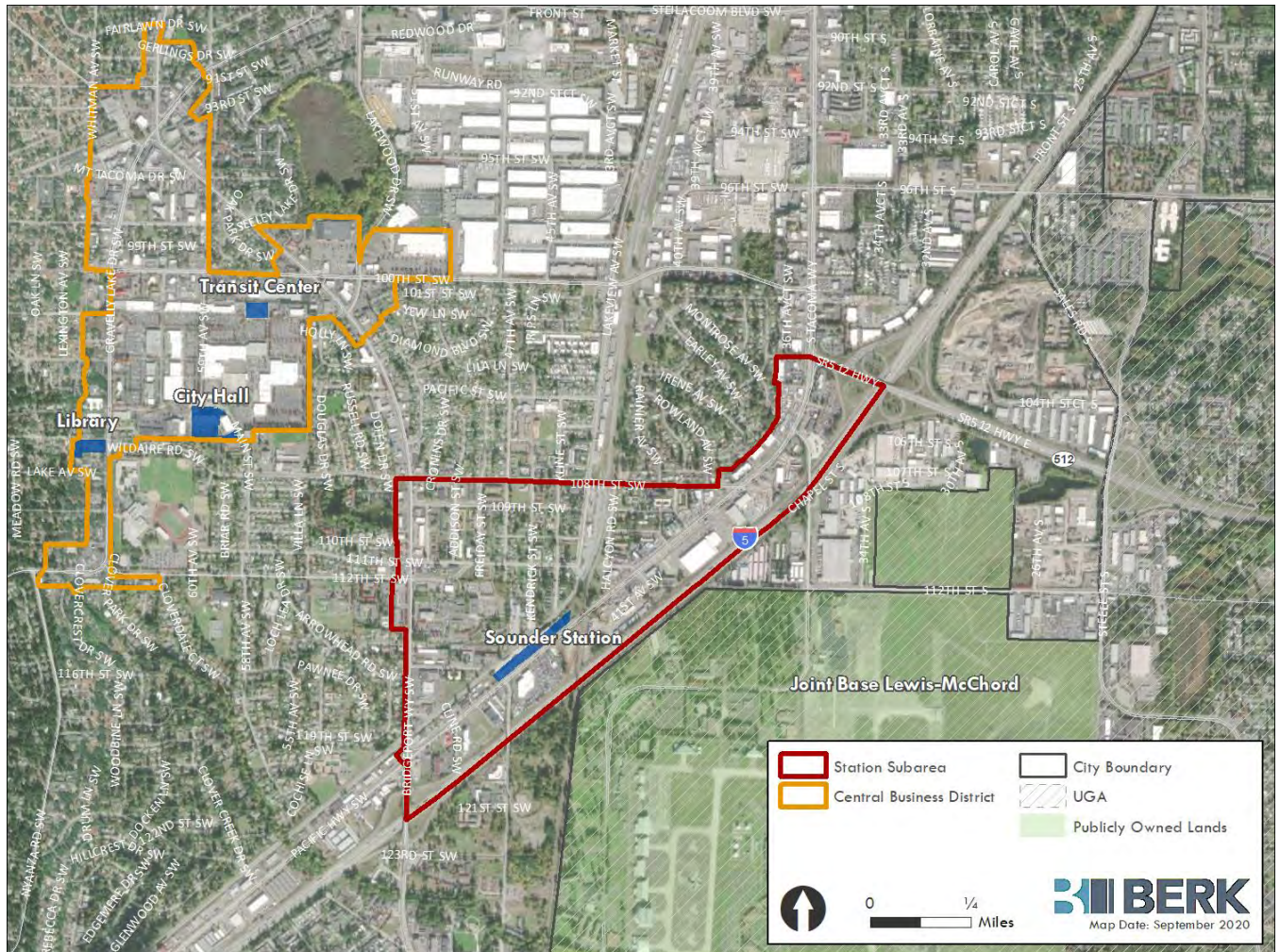
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The Lakewood Station District

The Lakewood Station District Subarea (LSDS) is an area of opportunity for future growth and development within Lakewood's Urban Center. It is located adjacent to I-5 and home to the terminus of the Sounder, which provides passenger rail service to Tacoma, Seattle, and Everett and. It already has a strong employment base that provides jobs in medical services, hospitality, retail, and restaurants. Proximity to high capacity transit and employment also make this a feasible area for compact residential development that can take advantage of such amenities. These factors make this a desirable area for a portion of Lakewood's future development.

Generally, the LSDS boundary incorporates the area within a half mile of the Sounder station but does not include areas southeast of I-5, since the freeway provides a significant barrier. Exhibit 1 shows the boundaries of the planning area for the LSDS. To capture the full transportation corridor to the north and south of the station, the subarea spans the area from the interchange with Bridgeport Way to the interchange with SR-512. Just outside the LSDS is Joint Base Lewis-McChord (JBLM), which relies on retail and services within the study area boundary. Lakewood's Town Center District is also about one mile away.

Exhibit 1. Lakewood Station District Subarea and Vicinity

Source: BERK, 2020; Pierce County Assessor, 2020.

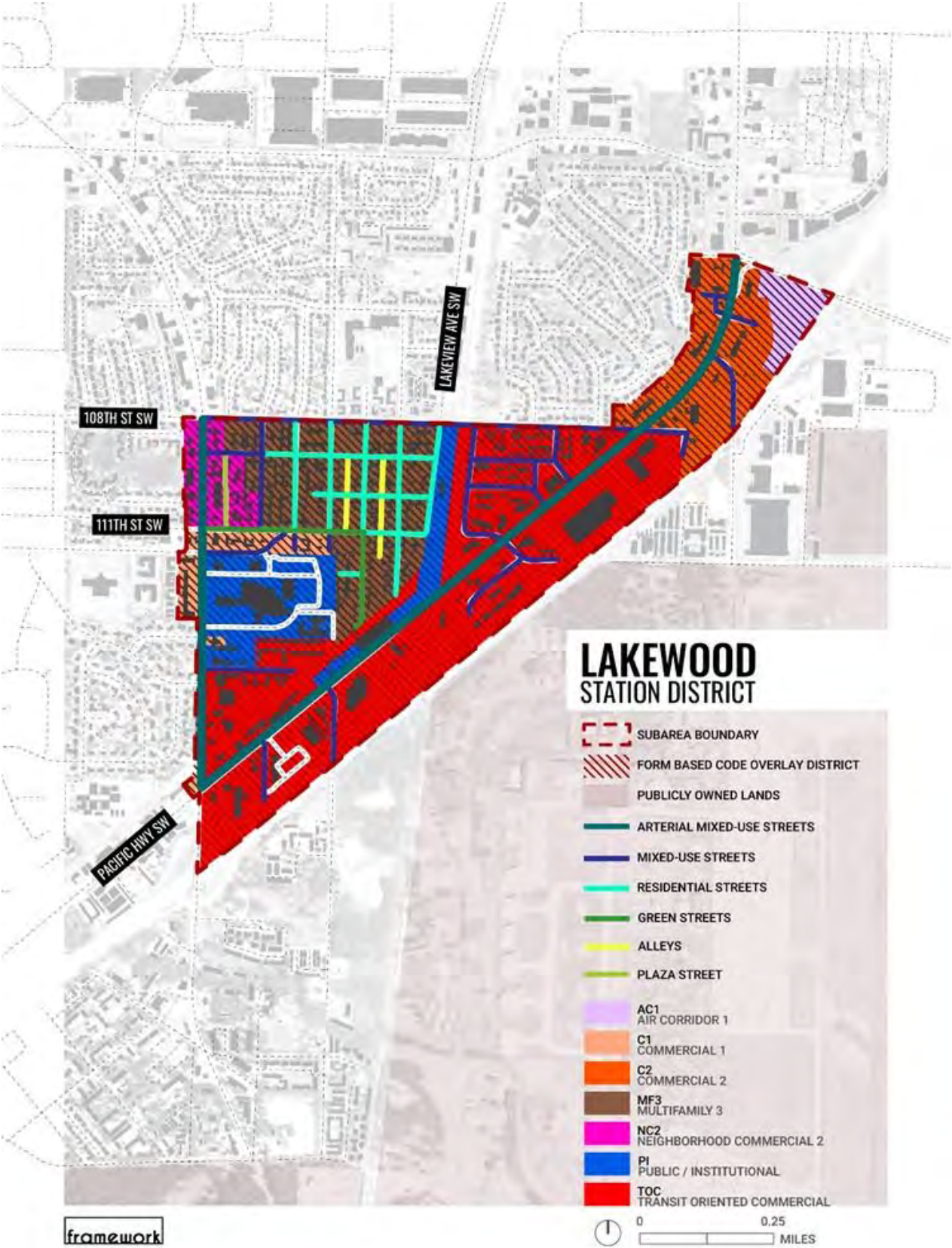
Vision and Concept

The Lakewood Station District is a multi-modal commuter hub of Lakewood and the southern terminus of Sound Transit's commuter rail service. The Lakewood Station District provides an amenity-rich, transit-oriented development node surrounding the Lakewood Station. This District offers a mixture of intensive land uses and activities supportive of direct regional transportation access via the Lakewood commuter rail station and I-5. The District implements development standards to foster a high quality, pedestrian-oriented urban environment including incentives to encourage a dense mix of commercial and medical office, regional and local retail, services and hospitality, and high-density residential uses offering ownership and rental housing opportunities, all supported by direct regional transportation access.

Exhibit 2 shows the land use concept for the LSDS. Key features of the concept for the LSDS include:

- **Transit-Oriented Development.** Residential and employment growth can be supported here because of the proximity to local and regional transit. Residential densities would be up to 40 units per acre in the residential zone (MF3) and up to 54 units per acre in the mixed-use zone (TOC). Commercial development will add employment opportunities, retail, and restaurants to serve residents and workers. As amenities and more compact urban forms are added to the LSDS, those who live and work in this area are less likely to be auto-dependent.
- **Hybrid Form-Based Code.** Land use regulations will include a hybrid form-based code. Under such a system, zoning directs the appropriate areas for residential, commercial, and mixed-use. Building and site design is based on a series of identified streetscape typologies. This allows for the continuation of a variety of uses but with a uniformed development standard that creates an identifiable look and feel for the district.
- **Affordable Housing and Homeownership.** Current residents rely on this area for low and moderate income housing, and displacement is a concern. Residential development and preservation will target housing serving households at 65%-110% of the area median income. Rowhouse residential development allows for compact residential development at an affordable price point. Ground-related units provide private and semi-private outdoor space and the opportunity for zero-lot line platted development. This provides homeownership opportunities and the chance to build wealth and equity for moderate income households in the subarea.
- **Walkable District.** New streetscapes and sidewalk improvements will make getting around the LSDS safer and more comfortable for people on foot. As housing and other amenities are added to this area, increased walkability makes it much more feasible to live and work in the subarea and reduces automobile dependency.

Exhibit 2. Lakewood Station District Subarea



Source: BERK, 2020; Pierce County Assessor, 2020.

Lakewood Landing is a 30-acre site for master planned development. The site is likely to include a mix of residential, retail, entertainment, and employment uses. Conceptually, an initial mixed-use plan with 760 units and 87,000 square feet of retail are part of Phase 1 in the “Central Core” on 11.5 acres of the 30-acre site (see Exhibit 3). Water features, carefully placed structures and sound-absorbing walls will help to mitigate noise. Future phases are under consideration. Development of the site is supported by the subarea plan but more detailed master planning will proceed on a separate track.

Exhibit 3. Lakewood Landing Location and Concept



Exhibit 4a. Lakewood Station District Location and Access



Source: Sound Transit, 2021.

Subarea Growth

Net residential growth within the LSDS is estimated to be 1,772 dwelling units, which assumes 760 units in Lakewood Landing and 962 new units from residential growth in the rest of the subarea over a 20-year period. Employment capacity would include 1,105 new jobs, plus 171 jobs for Lakewood Landing.¹ This complements the growth anticipated for the city center in the Lakewood Downtown Subarea Plan, which plans for about 2,257 dwelling units and 7,369 jobs over roughly the same time period.

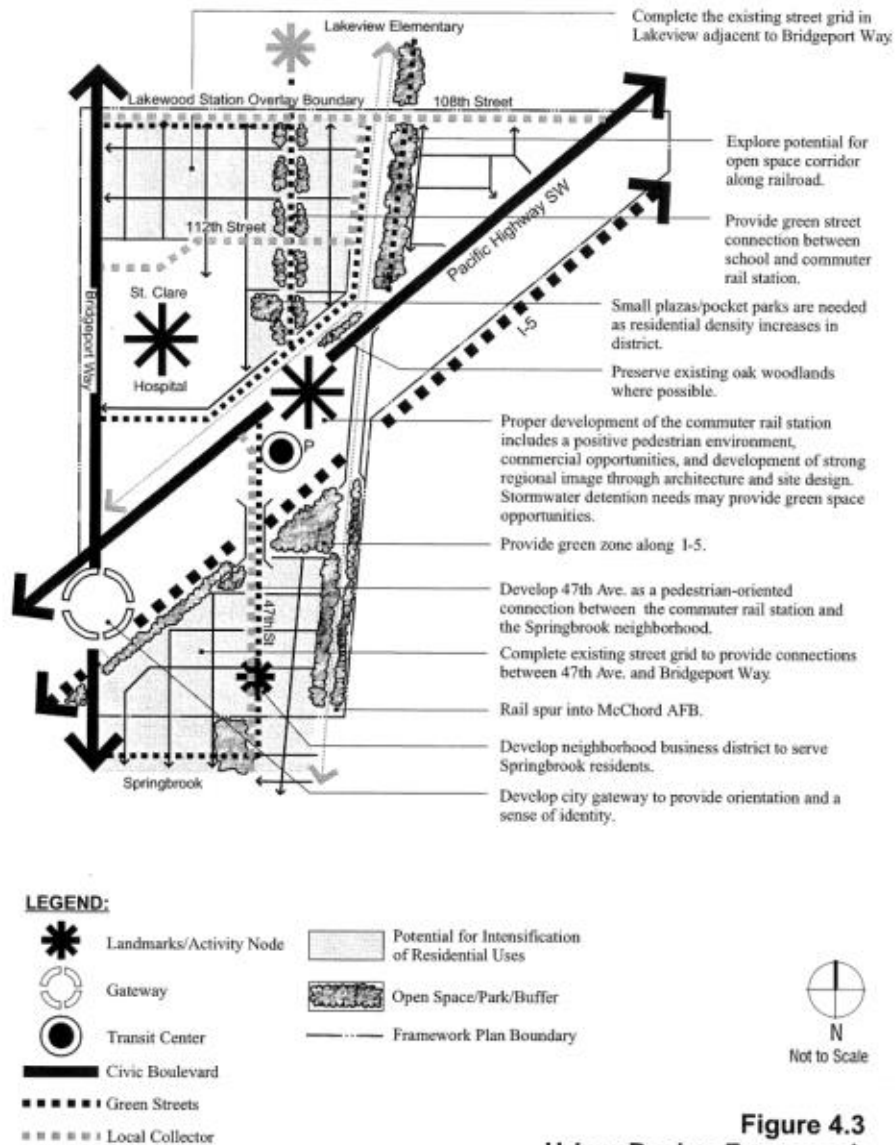
Lakewood Comprehensive Plan

Section 3.3.5 of Lakewood's Comprehensive Plan focuses on the LSDS. Goal LU-25 identifies LSDS as Lakewood's multi-modal commuter hub with supporting policies that call for a transit-oriented development district, development of a subarea plan, coordination with other agencies, and the use of bonus densities and incentives to achieve this goal. A rich mix of land uses around the station is the goal of LU-26 including regional offices, major institutions, high density residential, neighborhood businesses, and open space. The Comprehensive Plan also supports the citywide economic goal to promote a dynamic local economy with diverse housing stock and transportation options.² Transportation linkages between the LSDS and Downtown Subarea will connect people to the amenities of both places and to the region.³ An urban design framework to guide livable and attractive development is the focus of Goal LU-27, which include design guidelines, open spaces, and pedestrian connections. Both the station itself and the I-5 interchanges are major gateways into the city that should have a distinctive look and feel that continue along Pacific Highway, Bridgeport Way, and throughout the rest of the subarea.

¹ Growth numbers include anticipated development of Phase I of Lakewood Landing. Planning for future phases will occur when such phases are fully defined.

² *Lakewood Comprehensive Plan* Economic Development objectives 1.3 and 1.5.

³ *Lakewood Comprehensive Plan* section 2.4.

Exhibit 5. LSDS Planning Concept

SOURCE: EDAW, Inc. 1999

Source: Lakewood Comprehensive Plan, 2014.

Development of a special district around the Lakewood Station has been a consideration since the community's first Comprehensive Plan. It remains a part of the policy framework of Lakewood's Comprehensive Plan and is identified as a future community landmark.⁴ The 1999 high-level concept for the LSDS was included in the 2014 Comprehensive Plan as shown in Exhibit 5.

⁴ Lakewood Comprehensive Plan section 4.4

Community Input

Since the vision and concept for the Lakewood Station District had been a longstanding part of Lakewood's Comprehensive Plan, as a starting point for developing the LSDS the City tested this direction with the community to see if updates were needed. An in-person meeting conducted in late February, before public health orders were issued, with over 24 attendees invited from 17 agency and organizational stakeholders. This meeting collected information about existing conditions and planning efforts occurring in the study area. In addition, the launch of this project coincided with the early stages of the global COVID-19 pandemic. Knowing that the residents and workers in the subarea were vulnerable to the economic effects of the pandemic⁵ the City hoped to better understand local impacts to identify potential short term and long term responses.

In compliance with public health orders to the limit the spread of COVID-19, the City used remote outreach engagement options to solicit and collect community input. The City conducted an online survey during the summer of 2020. Outreach for the survey included a variety of techniques over an engagement period of about six weeks. Subarea residents received postcards and posters were hung in essential businesses and community locations to advertise the survey. Partner organizations, such as Sound Transit, shared the survey with their local mailing lists. Social media messages encouraged survey participation as well as a self-guided walkshop exercise that asked participants to walk the subarea and record their

Kid's Walkshop Response

Lakewood Station District Subarea Plan

WHAT DO YOU THINK?

First Name: Jo Age: 12

After your adventure, write or draw answers to these questions:

What did you LIKE BEST?
how there's places with room for more people

What did you LIKE LEAST?
1. how some places look nice but they don't take care of that landscape and public park

What do you WISH FOR?
A park for kids to play at. A public were it's not a danger to get to. so probably in a neighborhood.

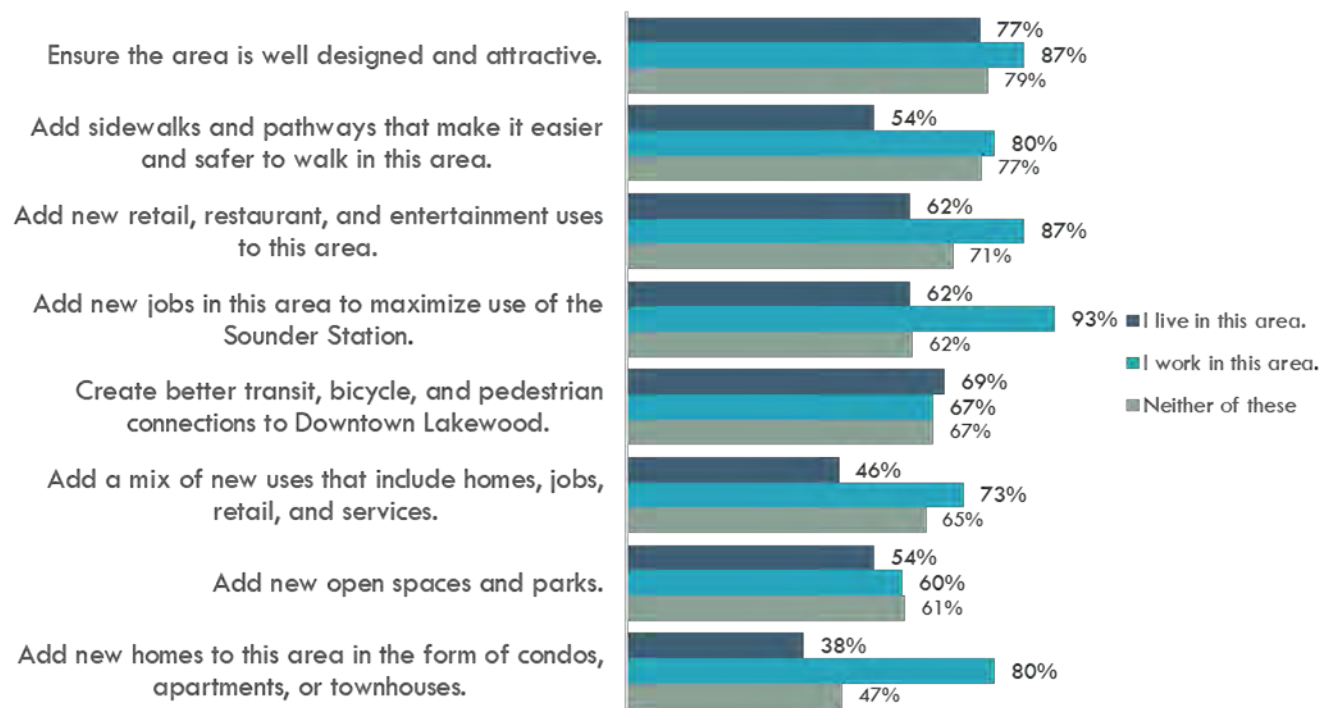
⁵ Based on socio-economic data collected in the *Situation Assessment*

impressions. The walkshop materials included an opportunity for kids to submit their ideas as well, as shown in the Kid's Walkshop Response sidebar.

The survey was offered in both English and Spanish, but the City received no responses in Spanish. Responses were analyzed as a whole and also disaggregated to look for differences in opinion based on whether the respondent lived in the subarea, worked in the subarea, or identified as neither. About 31% of the 91 respondents live or work in the subarea. Of those who identified as working in the area, 20% are business owners. More than half (58%) of the respondents live in Lakewood but outside of the study area.

Exhibit 6. Survey Responses - Future LSDS Priorities

Percentage of Group Rating Priority as Important or Very Important



Source: BERK, 2020.

Survey responses summarized in Exhibit 6 confirmed the vision and concept for the LSDS from the Comprehensive Plan. The following bullets highlight key results:

- Those who work in the subarea more strongly favored adding new homes to the area (80%) than those who live in the subarea (38%).
- Respondents identified the presence of the Sounder Station as the most important asset of the area, contributing to an

easy commute for residents and employees. The station's location was an asset for those working in the subarea because employees or customers live close by or it is easy to get there. An important asset to residents was the ability to have a home with some outdoor space.

- Changes that respondents would most like to see in the subarea included: more green space or parks, better walkability, more and new businesses, and additional housing options. More affordable housing was a desire of those who worked in the area and more community or neighborhood events were important to current residents.

Future Planned Engagement

The Draft LSDS plan will go through a public engagement process to share and invite feedback on the draft vision, goals, policies, and environmental review information. This will include workshops with the Lakewood Planning Commission, an online open house, and a stakeholders meeting. The City of Lakewood will hold a public hearing and a 30-day comment period to take comment on the draft Plan and Planned Action. Comments received will be used to revise the Plan, which the Planning Commission will review prior to voting on a recommendation to City Council. City Council will also hold a public hearing prior to deciding on adoption.

Note: once the engagement is completed this section will be updated prior to finalization of the plan.

Summary of Existing Conditions

Lakewood completed a *Situation Assessment* to identify existing conditions in the LSDS at the initiation of plan development. Information from the assessment is integrated into the analysis supporting the Planned Action and Environmental Checklist for this project. Exhibit 7 summarizes the results from the *Situation Assessment*.

Exhibit 7. Existing Conditions Summary Table

Topic	Summary
Demographics and Housing	<p>The subarea is a racially and ethnically concentrated area of poverty. There are low rates of homeownership and housing in the area is characterized by low values. These conditions place existing residents at risk of economic displacement.</p> <p>The majority of subarea residents work in service and retail industries and the majority of current employment is in these sectors. However, only about 10% of those who work in the subarea live in Lakewood. Additional moderate income housing in the subarea could provide the opportunity for some workers to live closer to work.</p> <p>In theory, the median household income in the LSDS should be able to pay the typical housing cost in the area. However, households in the subarea have high levels of housing cost burden, meaning they are paying more than 30% of their income for housing. Affordable housing and subsidized housing units could help alleviate the cost burden of existing residents.</p> <p>New multifamily housing is needed in Lakewood due to low vacancy rates. There is also demand for housing to serve military personnel and their families stationed at Joint Base Lewis McChord.</p>
Land Use	<p>Land use policies are in place to support the development of the subarea plan. Current land use patterns are auto-oriented, so transitional patterns of development will be necessary to achieve the desired land use.</p> <p>Residential and commercial land uses are in close proximity to each other. Although they are not currently integrated, this presents opportunities for mixed-use development. There are also a number of vacant and underutilized properties zoned for commercial and residential uses that provide opportunities for redevelopment.</p> <p>The subarea has few natural features and sensitive areas and does not include natural hazards that may limit future development. However, there are some environmental health hazards that will need to be addressed with future redevelopment.</p>
Zoning, Regulation, and Incentives	<p>Zoning is consistent with future land use but zoning and regulatory changes will be needed to support the transition of land use from its current condition to the desired future condition. Temporary activation of spaces in the subarea may be helpful during the transition period.</p> <p>Housing incentives may help the LSDS remain an area for households with low and moderate incomes while increasing the quality of housing through redevelopment.</p> <p>Attached residential development that can be subdivided for ownership can increase density and offer homeownership options, but parking and other development standards should be addressed.</p> <p>Proximity to Joint Base Lewis-McChord brings additional regulations to maintain safe airspace, but the City's current integrated review process minimizes complexity for applicants.</p>
Transportation	<p>Intersections currently operate at an acceptable Level of Service.</p> <p>The area is well served by two transit hubs: the Lakewood Station and the SR 512 Park and Ride.</p> <p>The City of Lakewood, Sound Transit, and Pierce Transit have plans to enhance transit connectivity and multimodal mobility in the subarea in the next decade. <i>Lost revenues due to COVID may affect these plans, but details are currently not available.</i></p>
Utilities and Public Services	<p>Infrastructure upgrades will be needed for some redevelopment plans. This includes the upgrade of water mains for capacity and fire flow, and capacity upgrades for sewer. Coordination with the Lakewood Water District and Pierce County Wastewater Utility during the update of their system plans is needed.</p> <p>Sponsoring the upfront costs of infrastructure investment may be a strategy to support new development in the LSDS.</p>



Land Use and Urban Design

The Lakewood Comprehensive Plan established land use designations based on the vision and concept for the Lakewood Station District. Implementing zoning was adopted consistent with the land use designations. These basic land use policy and regulatory structures are carried on through the LSDS Plan. In addition, the Plan applies an LSDS regulatory overlay. This overlay includes the adoption of a hybrid form-based code to direct new development according to street and building typologies. The use of these typologies will guide urban design to give the LSDS a unique feel and identity and create a thriving environment for those who live and work in the district.

Context

Existing Land Use and Design

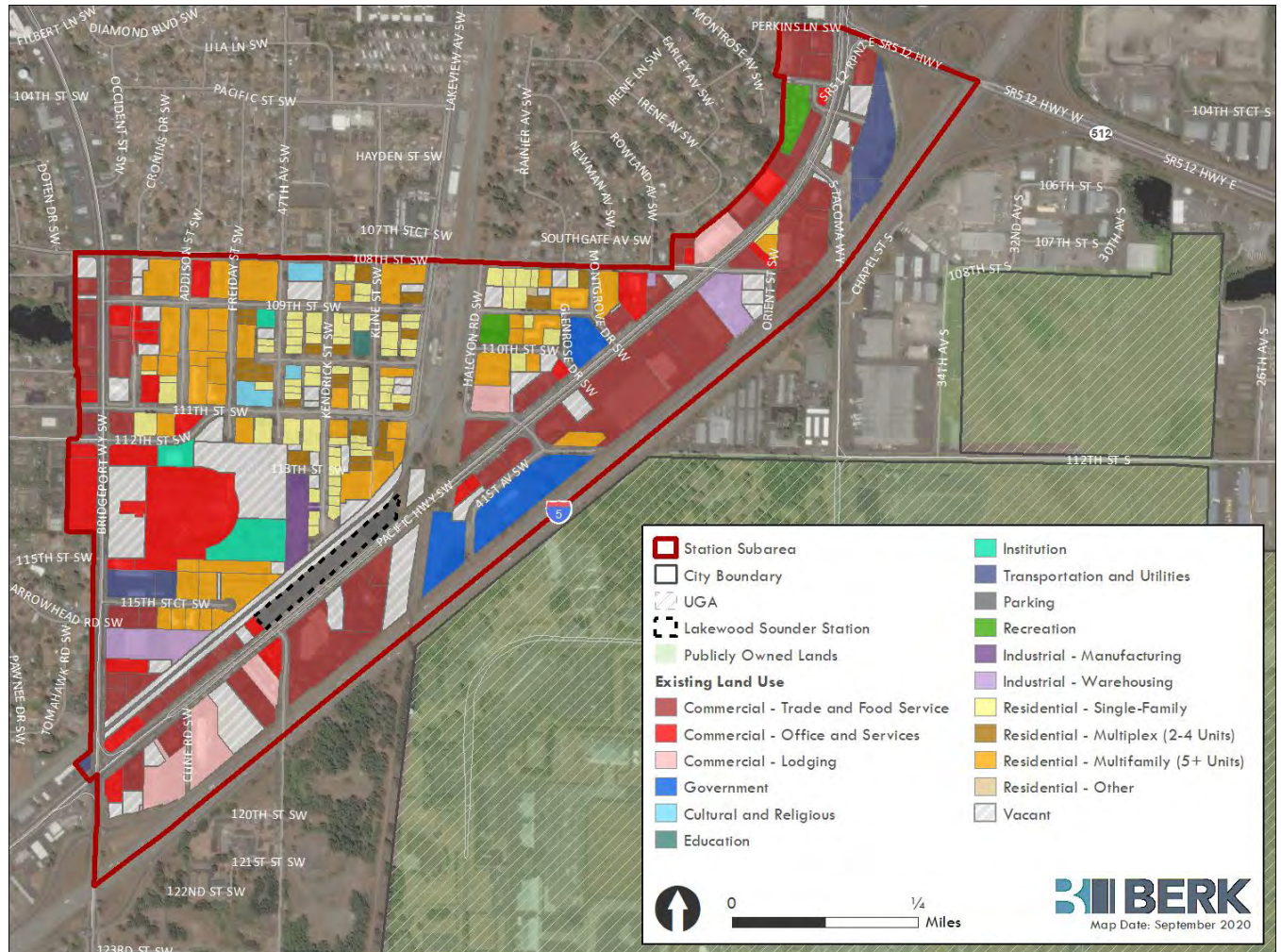
The LSDS vision and development concepts are not reflected in the existing land use or urban design. Development intensity is lower than what is planned for the subarea, as shown in Exhibit 8. Wide streets prioritize the movement of automobiles. This can make major arterials like Bridgeport Way or Pacific Highway act as barriers for pedestrian and bicyclists. Residential streets also sport rights of way over 50 ft wide with few traffic calming measures to buffer automobile traffic.



Current Urban Form in the LSDS

Sources: City of Lakewood, 2020; Google Earth, 2020.

The variety of commercial developments in the LSDS is one of its assets. Companies include a range of small unique businesses, local chains, and national companies in a range of building types that include low rise office buildings, strip commercial developments, corporate architecture, and commercial pads. Most of these building sites and adjacent streetscapes are built for auto-access with entries set far back from the sidewalk and limited pedestrian connectivity except through the parking lot. This type of streetscape and site planning is not designed at a human-scale that feels safe for walking. During public engagement walkability improvements were a top desire of residents and employees currently in the subarea.

Exhibit 8. Existing Land Use

Note: Recreation includes RV park and recreation activities.

Sources: BERK, 2020; Pierce County Assessor, 2020.

Most of the subarea is in commercial use, but there is a significant amount of residential use and some vacant lands as well. Residential uses include single-family homes (60%), small duplex or multiplex buildings (26%), and low-rise multifamily units (14%). Parcel size in the subarea is varied, as shown in Exhibit 9. Half the parcels are a half-acre or less in size, but these only account for about 40 acres in the 339-acre subarea. The 13 largest parcels account for 38% of the land in the subarea. This variety of parcels provides different types of opportunities for small and large scale redevelopment.

Exhibit 9. Parcel Size in the LSDS

Parcel Size	Parcel Count	Sum of Acreage
0.5 acre or less	169	40.25
0.5 - 1 acre	71	51.37
>1 - 5 acres	61	119.55
Greater than 5 acres	13	128.09
Total	314	339.26

Sources: Pierce County Assessor, 2020; BERK Consulting, 2020.

Although residential development is allowed at up to 54 units per acre in the LSDS currently, the average density of existing development is 11.6 units per acre. Policies and strategies in this plan aim to better utilize land for housing in the area around Lakewood Station.

Environmental Conditions

The Lakewood Station District Subarea is a highly developed urban environment that contains few natural features. It is characterized by parcelized land, pavement, and existing vegetation mostly planted for ornamental purposes. A summary of environmental conditions is shown in Exhibit 10. Additional detail can be found in the *Situation Assessment* or *SEPA Environmental Checklist*.

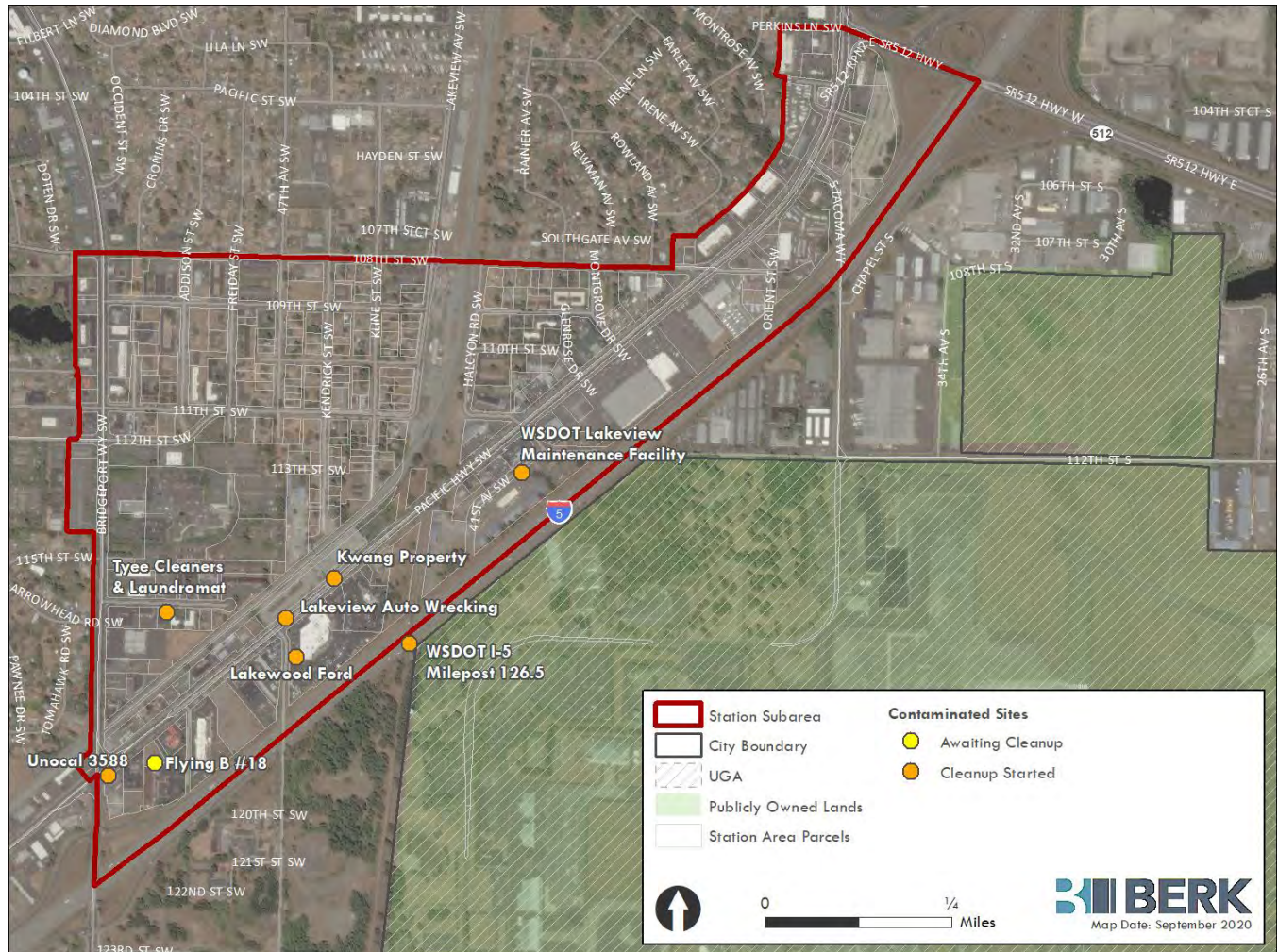
Exhibit 10. Summary of Environmental Conditions in the LSDS

Environmental Feature	LSDS Conditions	Effects on the LSDS
Waterbodies and Shorelines	There are no significant features within the LSDS.	Not applicable.
Wetlands	There are three small, isolated areas of mapped wetlands adjacent to I-5. There is a delineated wetland outside the LSDS west of Bridgeport Way.	The isolated, mapped wetlands will need to be investigated at the time of development and follow critical areas regulations. A limited number of LSDS parcels may be subject to buffer requirements identified for the wetland outside the LSDS west of Bridgeport Way.
Floodplains	A portion of the study area is a moderate (or 500-year) flood area. This is mapped along Bridgeport Way and between Pacific Highway and I-5 west of Lakeview Avenue SW.	Areas of moderate flooding are not regulated by the City's flood hazard regulations.
Aquifer Recharge Area	All of Lakewood is within the Central Pierce County Aquifer Area. This is a sole source aquifer.	New land uses that could result in groundwater contamination are limited, including things such as storage tanks, dry cleaning, and auto repair.
Geological Hazards	There are no significant features within the LSDS.	Not applicable.
Environmental Clean-up Sites	There are eight sites identified for environmental clean-up in the LSDS, as shown in Exhibit 11.	Contamination from petroleum products, metals, solvents, and lead is the result of current or past activities on these sites. ⁶ Clean-up is started on all the sites except the Flying B #18 site. Sites are in various stages of the clean-up process and subject to Washington State's Model Toxics Control Act. ⁷

Sources: Pierce County Assessor, 2020; BERK Consulting, 2020.

⁶ Specific information on each site and the clean-up efforts that are underway can be found at: <https://ecology.wa.gov/Spills-Cleanup/Contamination-cleanup/Cleanup-sites/Locate-contaminated-sites>.

⁷ RCW 70.105D.

Exhibit 11. Environmental Clean-up Sites in the LSDS

Sources: BERK, 2020; Pierce County Assessor, 2020.

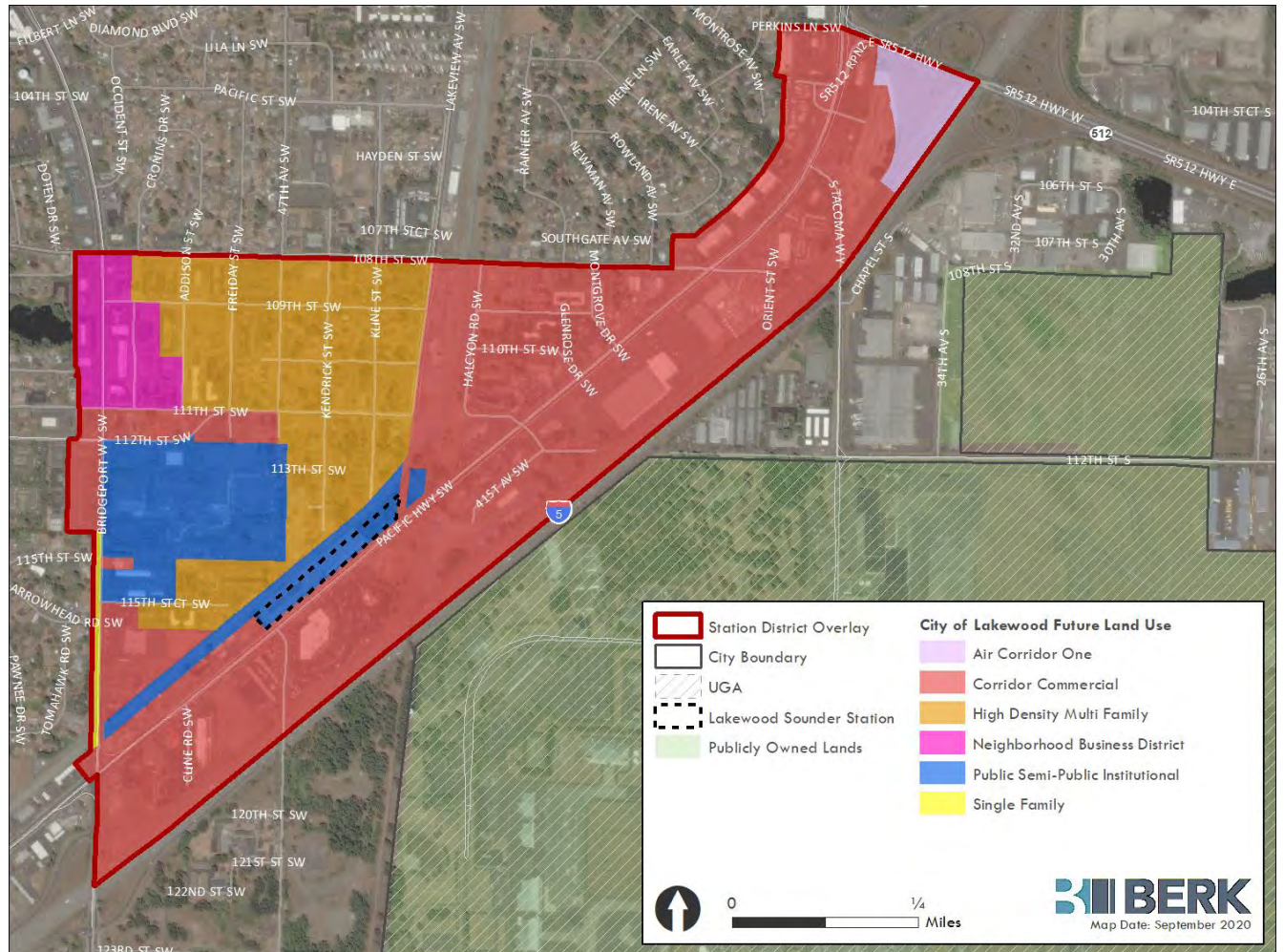
Future Land Use

The Comprehensive Plan identifies the following land use designations in the subarea, as shown in Exhibit 12:

- Corridor Commercial.** This land use is designated along Pacific Highway SW (Pacific Highway) and just north of the Hospital. It recognizes Lakewood's pattern of existing strip commercial development and is implemented by the Transit-Oriented Commercial, Commercial 1, and Commercial 2 zones. Within the LSDS, this district will remain and include opportunities for mixed-use development.

- **Neighborhood Business District.** This designation is implemented through the Neighborhood Commercial 1 zoning along Bridgeport Way SW (Bridgeport Way) that allows for smaller or strip commercial business areas to transform into compact urban development over time. It allows commercial development that serves surrounding neighborhoods and beyond, and allows for mixed-use residential development.
- **Public & Semi-Public Institutions.** St. Clare Hospital and the Lakewood Station are in this land use designation that recognizes essential moderate and large scale facilities that serve all of Lakewood.
- **High Density Multi Family.** The existing residential areas zoned Multi Family 3 are in the High Density Multi Family future land use designation. The purpose of this designation is to integrate a variety of high density housing types into adopted subareas and business districts. The LSDS Plan envisions rowhouse and other residential developments that are integrated with surrounding commercial and mixed-use development through pedestrian connections and urban design.

This Subarea Plan adds the Station District Overlay to the Comprehensive Plan Future Land Use Map. The purpose of the overlay is to recognize the area in which the LSDS vision will be applied.

Exhibit 12. Future Land Use in the LSDS

Sources: BERK, 2020; Pierce County Assessor, 2020.

Land Use Regulation

Land use regulation in the LSDS is driven by a hybrid form-based code. Assigned zoning identifies the types of uses allowed in each zoning and is consistent citywide. The form-based code creates a development regulation overlay that is unique to the LSDS. It regulates building forms and land use in a way that is unique to the subarea. Regulations to maintain land use compatibility with airfield operations at Joint Base Lewis-McChord remain in effect.⁸

⁸ Within the LSDS these regulations primarily implement lighting standards to prevent interference with aircraft operations. Additional detail is available in the *Situation Assessment* and the Lakewood Municipal Code 18.10.135.

Zoning

Zoning in the LSDS is shown in Exhibit 13. These zones are consistent with the Future Land Use map and include:

- **TOC – Transit-Oriented Commercial.** TOC zoning is shown along most of Pacific Highway in the subarea around Lakewood Station and the proposed Lakewood Landing site. This zone is unique to the LSDS. The purpose of TOC is *“an interactive mixture of uses which focus on regional transportation networks while providing for urban design, people orientation, and connectivity between uses and transportation routes.”*⁹ The mix of uses allowed in the TOC is very similar to those allowed in the Central Business District. They focus on retail and services, prohibiting space-intensive uses like auto sales, furniture and appliance stores, or industrial uses that may cause compatibility issues in a compact urban environment such as manufacturing or recycling stations. Mixed-use and multifamily residential uses are allowed at densities up to 54 units per acre.
- **C1 - Commercial One and C2 - Commercial Two.** C1 and C2 are commercial corridor districts that incorporate employment, shopping, services, offices, and light industrial uses near major arterials. A small strip of C1 is located north of the hospital, just off Bridgeport Way. Along Pacific Highway near the intersection of SR 512 is an area of C2. Both districts allow a range of businesses as permitted uses. Hotels and motels are allowed in both districts, permitted in C2, but a conditional use in C1. Commercial recreation, heavy manufacturing, shopping centers, and recycling and transfer stations characterize the type of uses that are prohibited. Residential uses are not



Lakewood Station is at the heart of the TOC zone

Source: HewittSeattle.com

⁹ Lakewood Municipal Code 18A.10.120D.5

allowed, except for allowing a caretaker's unit.

- **NC2 - Neighborhood Commercial Two.** The commercial area on Bridgeport is zoned NC2 with the intent to create a sense of urban community that serves surrounding neighborhoods that may also attract people from other areas. This zone allows a mix of residential, retail, office, and services. Residential may be multifamily or mixed-use development up to 35 units per acre. Permitted commercial uses tend to be small or mid-sized. Most light industrial and larger commercial uses are prohibited. The few that may be considered, such as auto sales or breweries, are conditional uses to help mitigate impacts and ensure district and neighborhood compatibility.
- **MF3 - Multi Family Three.** The existing residential area of attached and detached single-family homes and low rise multifamily is zoned MF3. MF3 zoning is in areas where there is both an arterial and a nearby commercial or mixed-use district. This is intended to be a high density multifamily environment with multi-story housing with densities up to 54 units per acre. The LSDS overlay will provide form-based regulations that effectively limit densities to 30-40 units per acres and incentivize rowhouse development. Where multifamily development occurs within the LSDS, ground floor commercial use is allowed.¹⁰ Many of the existing uses in the LSDS are non-conforming in this zone. Non-conforming structures may be maintained but not altered or enlarged.¹¹
- **PI - Public/ Institutional.** This zone recognizes the sites of St. Clare hospital and Lakewood Station, which are major facilities serving all of Lakewood and beyond.



St. Clare provides regional medical service

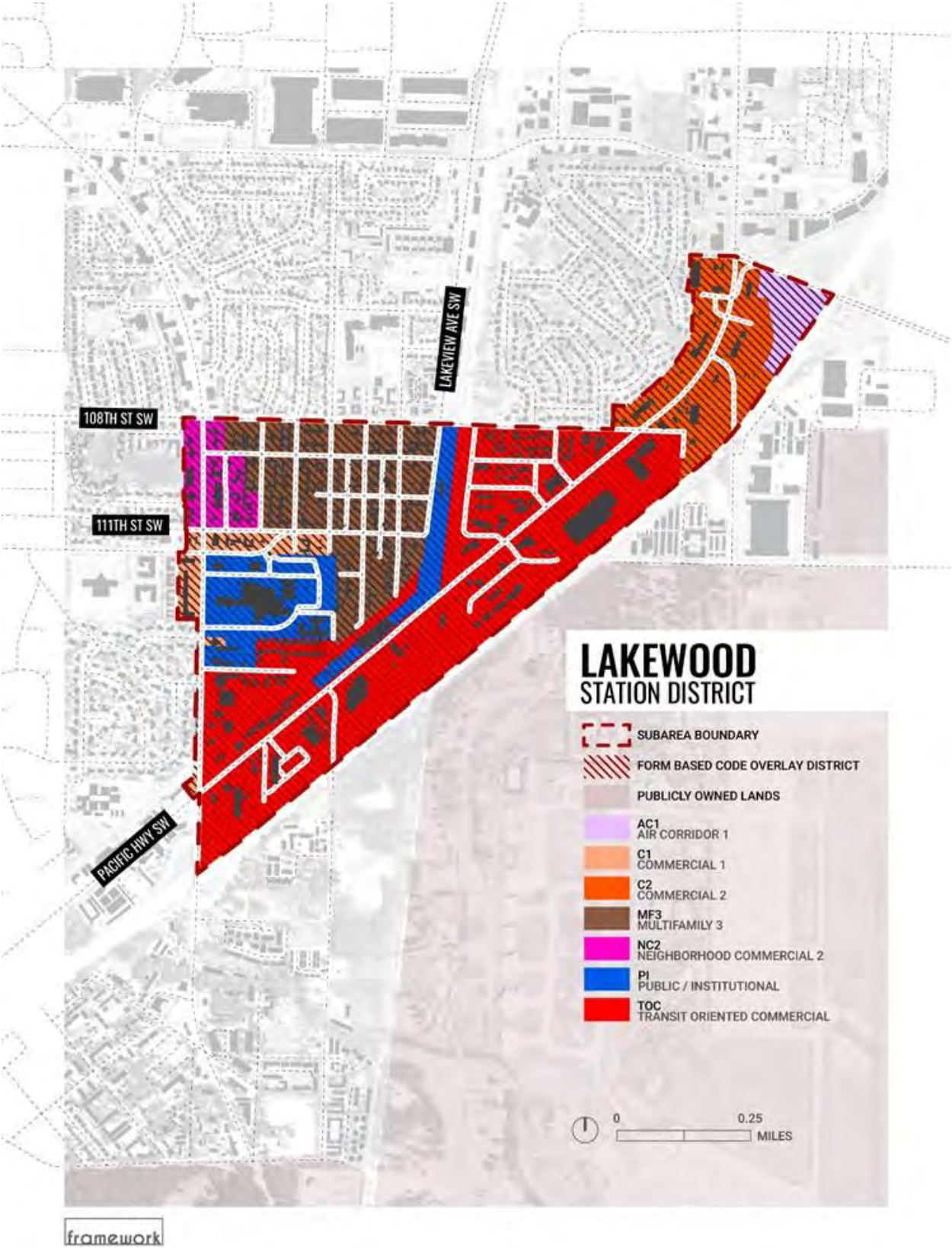
Source: CHIFrancisican.org

The zoning map adds an overlay for the Lakewood Station District (see Exhibit 13). This area links the vision to specific regulating provisions in the Lakewood Municipal Code. It allows for the implementation of a hybrid form-based code, incentive programs, and other measures within this area.

¹⁰ Lakewood Municipal Code 18A.40.040B.1

¹¹ Lakewood Municipal Code 18A.20.200

Exhibit 13. Zoning in the LSDS



Sources: BERK, 2020; Pierce County Assessor, 2021.

Hybrid Form-Based Code

Form-based codes address the relationship between buildings and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. This code type works best when applied to targeted areas that have a defined vision, such as the LSDS. Lakewood adopted a hybrid form-based code for its Downtown. The form-based code for the LSDS draws on the work done for Downtown, but is customized for the vision, goals, and conditions of the Station District.

While uses are still regulated by zone, the hybrid form-based code shapes building design to reinforce a human, neighborhood scale that feels safe and welcoming. The use of a hybrid form-based code will standardize streetscapes to provide a consistent look and feel as well as facilitate the walkability desired by the community. Off-street parking, which is necessary to support retail as the area transitions to a more compact form, will be moved behind or under buildings that are located closer to the street. New development will include 8-14' wide sidewalks and pedestrian connections between the sidewalk and the building to enhance walkability.

Transitioning from the current land use and urban form to a more compact and pedestrian-friendly environment will occur over time. The hybrid form-based code focuses retail along designated streetscapes with high traffic. This helps preserve existing retail until the area is ready for higher intensity mixed-use redevelopment with ground floor commercial.

Exhibit 14. LSDS Hybrid Form-Based Code Overlay



Sources: Lakewood Municipal Code, 2020; Framework, 2021.

The hybrid form-based code is administered through an overlay that covers the entire LSDS as shown in Exhibit 14. It overrides zoning and development regulations in the Lakewood Municipal Code if there are areas of conflict. Uses are defined by the underlying zoning designation, with some additional prohibitions intended to limit uses at a scale or impact that is incompatible with the LSDS vision. Maximum densities and height limits are also set by the underlying zoning code.

This code uses street typologies to guide the development of building types and development regulations within the subarea as shown below. The hybrid form-based code can be found in Lakewood Municipal Code Title 18C.

Street improvements in this area will work within the existing right-of-way (ROW) to ensure the safe and efficient movement of vehicles, bicycles, and pedestrians, as well as to facilitate travel by transit. As a result, street sections may vary and are subject to the approval of the Public Works Department.

Arterial Mixed-use Streets

Arterial mixed-use streets maintain the existing vehicle capacity and allow a range of land use and building frontage types and include surface parking lots. This streetscape applies to Bridgeport Way and Pacific Highway SW.

Mixed-Use Streets

Mixed-use streets support a variety of activities and functions both in the public right-of-way and development along the street edges. Street level retail is permitted, but not required, and a wider range of building frontage types are permitted, including street-level residential and office uses. Mixed-use streets also require pedestrian-oriented design and requirements may vary based on the location within Station District.

Green Streets

Green streets include vehicle travel lanes, sidewalks, bike lanes, and open space for stormwater management, landscaping, and gathering space. Green streets contribute both functional and amenity features to the streetscape to make an attractive and welcoming district as shown in Exhibit 15 and Exhibit 16. The Boulevard street in Exhibit 17 is a variety of green street that

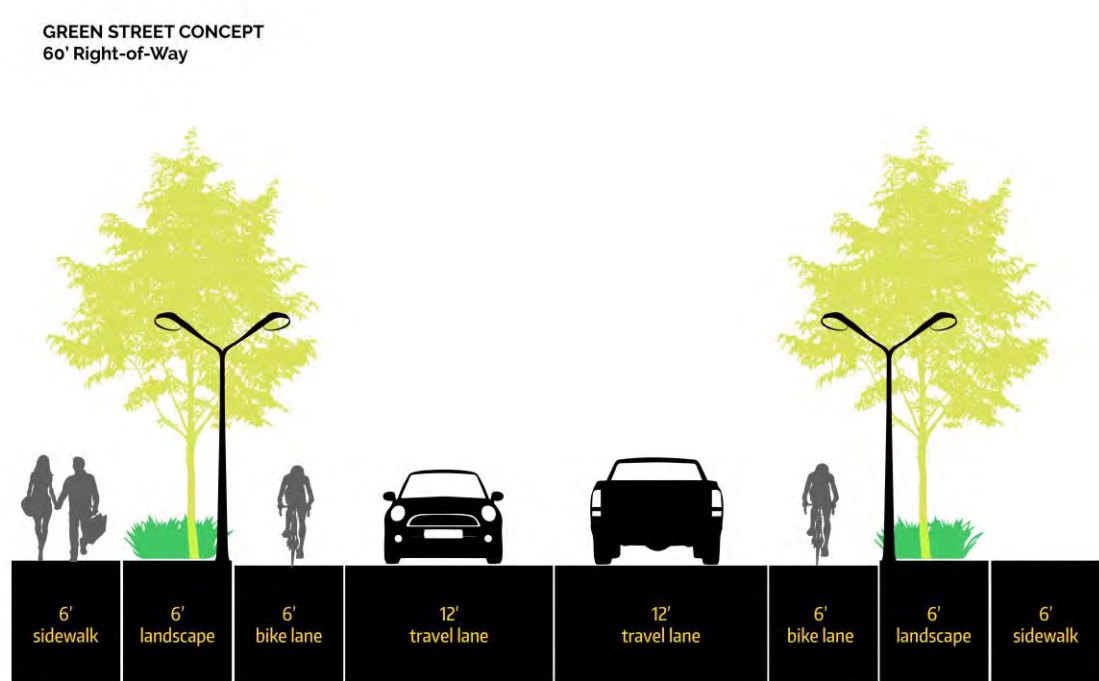
may be carried beyond the district, highlighting connections to Downtown or other nearby areas.

Exhibit 15. Green Street Concept #1 – 80' Right-of-Way



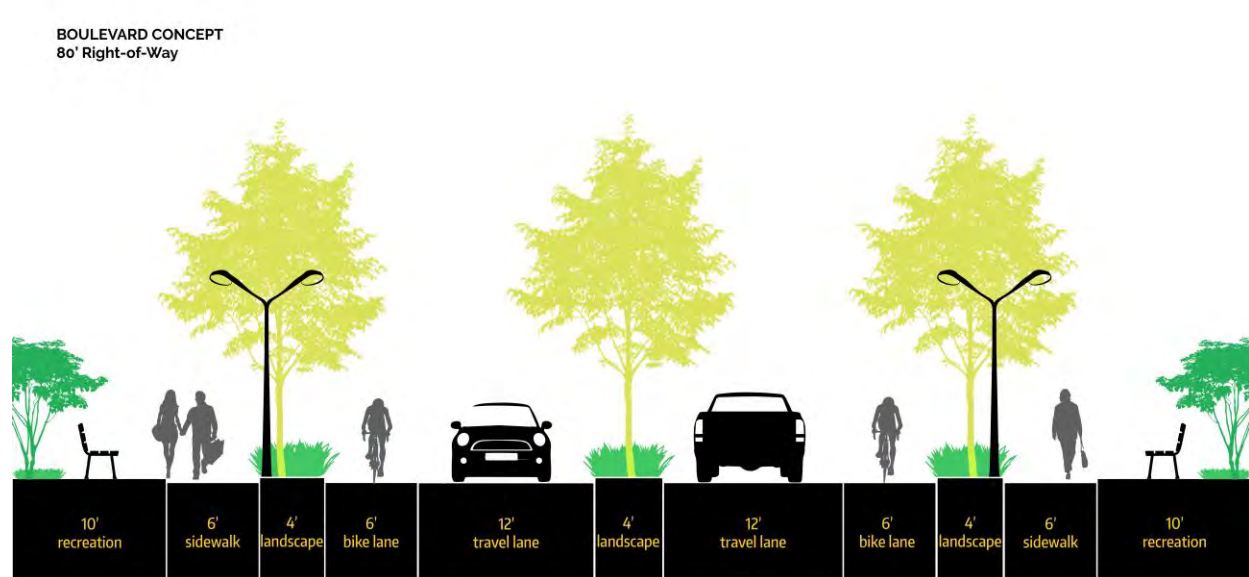
Source: Framework, 2020.

Exhibit 16. Green Street Concept #2 – 60' Right-of-Way



Source: Framework, 2020.

Exhibit 17. Boulevard Street Concept – 80' Right-of-Way



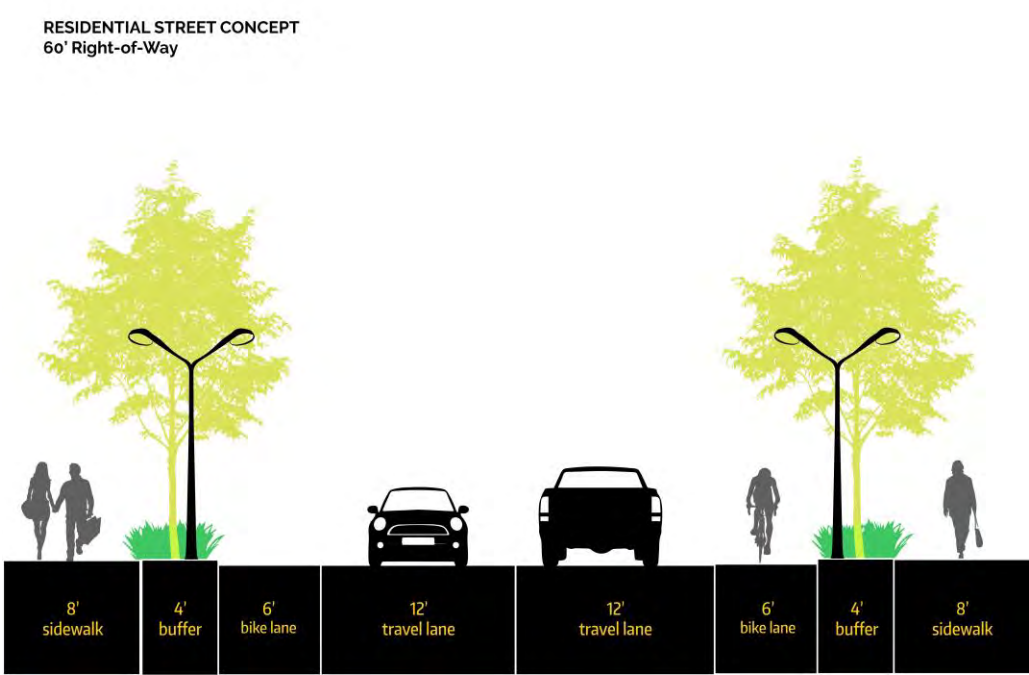
Source: Framework, 2020.

Residential Streets

Residential Streets serve residential neighborhoods and include vehicle travel with sidewalks and bike lanes. Many of the current streets identified as Residential Streets do not have curbs, gutter, or sidewalks, and there is no definition between the public right-of-way and private property. Parking occurs along the street edge, often in gravel shoulders and without a consistent pattern.

The residential street ROW in the LSDS area varies between 60' and 80' based on historical platting and construction. Exhibit 18 and Exhibit 19 show a standard residential street sections with either a 60' right-of-way or 80' right-of-way that include travel lanes, sidewalks, and bike lanes, and landscaping. In the future, bike lanes could be converted to on-street parking.

Exhibit 18. Residential Street Typology – 60' Right-of-Way



Source: Framework, 2020.

Exhibit 19. Residential Street Typology - 80' Right-of-Way



Source: Framework, 2020.

Alleys and Plazas

Alleys are encouraged to provide secondary access to properties, de-emphasize parking lots, and to promote continuous building frontages. Alleys would meet Public Works engineering standards. Along Occidental, a special Plaza designation identifies acceptable development frontages similar to Mixed Use Streets. The Plaza street concept is shared below.

Exhibit 20. Plaza Street Concept – 80’ Right-of-Way



Framework, 2020

Exhibit 21. Plaza Street Concept – 60’ Right-of-Way



Framework, 2020

Public Spaces

Green spaces and public open spaces should be located throughout the LSDS. This will result in an attractive district, provides gathering or recreation space, and is a desire of the community. The *Legacy Plan 2020*, Lakewood's plan for parks, recreation, and open space, aims to provide a public park or open space facility within a 10-minute walk of each resident.¹² Most of the LSDS is outside of a 10-minute walkshed from a City facility.¹³

Land consolidation would be needed to create a public park or open space in this area. While that opportunity is being explored, there are options to create linear open spaces and green corridors using available ROW. Such opportunities include:

- A linear park along Kendrick Street south of 111th Avenue SW that would bring green space to the heart of the district's residential area. This would better define the pedestrian connection between Lakewood Station and the Lakeview Hope Academy and facilitate the use of the playground there as a neighborhood park as envisioned in the *Legacy Plan 2020*.
- Green corridors with enhanced landscaping in areas with planned pedestrian and bicycle improvements such as 111th /112th Avenue SW.
- A pocket of open space in the alley near Occidental Street would be a plaza street as shown in Exhibit 22.
- A linear park connecting Lakewood Station to the Springbrook Area through improvements along 47th Avenue as envisioned in the *Legacy Plan 2020*.

¹² *Legacy Plan 2020*, Goal 2, Action Strategy 2.

¹³ *Legacy Plan 2020*, Figure 3-14.

Development standards will require street frontage improvements and landscaping that will increase greenery in the public realm. Mixed-use development standards will include provisions for public spaces and open spaces. Lakewood Landing will include a landscaped public plaza in its first phase to support community gathering and provide urban open space.



Conceptual Landscaped Public Plaza for Lakewood Landing

Source: Lee & Associates.2019.

Exhibit 22. Green Space Opportunities in the LSDS



Source: Framework, 2021.

Policies and Strategies

Policies

- Support transit-oriented development in the LSDS to capitalize on access to regional transit and proximity to Lakewood Station.
- Support and incentivize mixed-use development near Lakewood Station and in the commercial areas along Pacific Highway and Bridgeport Way where there is capacity for increased intensity of development.
- Integrate public open spaces into new development to create opportunities for public gathering and green spaces in the LSDS.
- Maintain land use and zoning designations that support high density residential development as part of mixed-use developments and residential redevelopment.
- Apply a hybrid-form based code to create an identifiable look and feel for the LSDS and to support safe pedestrian spaces.
- Adopt standards, uses and incentives that allow the subarea to transition from its present condition to its desired vision.
- Consider the designation of new Green Streets as opportunities arise, in coordination with public input.

Strategies

- Support the master planning of the Lakewood Landing site for mixed-use development so it will provide opportunities for high density housing, employment, business, and public space.
- Pursue opportunities to enhance landscaping or add linear parks within the public right-of-way to integrate green spaces and open spaces into the LSDS.
- Work through the *Legacy Plan 2020* to identify opportunities for public parks facilities within a 10-minute walk of subarea residents.
- Identify standards and guidelines that support transitional

parking arrangements so space used for parking can be transitioned to other uses as the area becomes less auto-dependent.

- Monitor the impact of the Lakewood Station District Overlay and amend the Plan as needed to improve outcomes.



Housing

Context

There are approximately 900 housing units in the LSDS. The majority (69%) of the units are renter-occupied.¹⁴ Vacancy rates are 13%. However, a 2018 study of the citywide Lakewood rental market showed lower vacancies for multifamily units of 3% or less.¹⁵ Market rate multifamily units had a 97% occupancy rate, while affordable units had a 99% occupancy rate citywide. Low multifamily vacancy had the effect of increasing the average costs of rent.

Market rate rental prices ranged from about \$900 to \$1,650 a month in fall 2018, depending on the unit size. Rates for affordable housing in Lakewood ranged from \$600 to \$1,800 a month. Although approximately 10% of renters within about a half mile of Lakewood Station receive housing subsidies, more than 50% experience cost burden, meaning they pay more than 30% of their household income on housing costs and earn less than 80% of the area median income.¹⁶

Median home values are about \$165,000, which is significantly lower than Lakewood's median value of \$292,000 or Pierce County's \$333,000. It is estimated that more than 30% of local homeowners experience cost-burden.

¹⁴ Estimated by ESRI Business Analyst (2019). This source makes estimates based on proportions of census blocks.

¹⁵ McCament & Rogers, 2018 *Lakewood Rental Market Survey*, October 2018.

¹⁶ City of Lakewood *Draft Consolidated Plan*, March 2020, p.13.

About 94% of subarea residents were employed with about half working in service industries, 18% in retail, and 10% in construction.¹⁷ Median income in the LSDS is \$34,673, which is lower than the \$52,002 median for Lakewood or the \$70,635 median for Pierce County.¹⁸ A household making the LSDS median income could afford to pay about \$900 in housing costs based on the housing burden definition.

Joint Base Lewis-McChord has a 99% housing occupancy rate for more than 5,000 housing units. The 70% of military households that live off-base receive a subsidy to offset rental or purchase costs. Typically, these households spend about \$1,500 to \$2,000 a month on housing.¹⁹ Despite its proximity, less than 10% of the base's workforce lived in Lakewood. This is attributed to a lack of suitable housing in good condition that is affordable to the base workforce.

Low rates of vacancy, rising rental costs, and a steady demand for off-base housing all indicate a need for low and moderate income housing in Lakewood. The LSDS is well suited to provide such housing because there is ample space for redevelopment.²⁰ Proximity to local and regional transit resources and areas of employment in the LSDS and Downtown would also allow households to manage transportation costs.

Existing housing stock in the LSDS already includes a variety of housing types, as shown in Exhibit 23.

Rental Housing Licensing Program

Lakewood has a rental housing licensing program to ensure that existing rental stock is safe and maintained and to prevent deterioration and blight. It is codified in Chapter 5.60 of the Lakewood Municipal Code (LMC). This program requires rental units more than ten years old to pass a compliance inspection every five years as a condition of receiving a business license. If deficiencies are found, the property owner must reimburse the City for tenant costs related to relocation or transitional housing. This provision prevents displacement and reduces the financial burden tenants may experience.

¹⁷ This information came from the ESRI Community Profile in February 2020, which uses 2019 data. Unless deemed essential, these top three employment sectors for subarea residents would be sensitive to the significant slowdown in economic activity due to the COVID-19 pandemic.

¹⁸ ESRI Community Profile, 2020.

¹⁹ 42% spend \$1,501 to \$2,000 on housing.

²⁰ In 2019 the density of existing development in the LSDS is only about 11.6 units per acre, far below the maximum density of 54 units per acre allowed under the zone.

Exhibit 23. Existing Residential Structures in the LSDS

Type of Residential Structures	Sum of Units
1 1/2 Story (finished space in attic or basement is the 1/2 story)	14
1 Story	68
2 Story	4
Apartment <= 3 Stories	20
Duplex 1 1/2 Story	2
Duplex One Story	20
Duplex Two Story	10
Home for the Elderly	5
Mixed Retail w/ Res Units	15
Multiple - Residential	662
Triplex One Story	3
Triplex Two Story	3
RV Park: 1 Apartment, 11 Mobile Homes & 26 RV Spaces	38
Grand Total	864

Sources: Pierce County Assessor, BERK 2020.

Housing Concepts

Lifecycle Housing

A variety of housing types is needed to meet the needs of people throughout their lifecycle. Housing needs also can vary based on factors such as household size, income, cultural traditions, interests, and preferences. All people need to have access to housing choices that are affordable to them. A family with children may have different housing needs and desires than an elderly couple or a military service person. Even within these categories, there are differences. Some families may desire a larger home to accommodate grandparents or other extended family members; other families may desire an efficient space close to civic amenities. An older couple may prefer a home with minimal maintenance to spend time on other pursuits; another couple may enjoy access to and outdoor space for gardening.

A variety of housing types allows people to find homes that meet their needs and desires at a level that they can afford. It contributes to quality of life and the flexibility to change their housing as needed. Lifecycle housing not only includes the single-family housing, apartment buildings present in the LSDS area, but also “missing middle” housing such as duplexes, triplexes, rowhouses, and smaller apartment buildings.

Exhibit 23 shows that the LSDS already includes a mix of housing types. During engagement, residents acknowledged the importance of having access to ground-related outdoor space, such as a private yard or semi-private shared courtyard. Many housing types can include ground-related outdoor space associated with individual units or incorporated as common space. They also can be built efficiently and affordably. By providing missing middle housing types, the LSDS will continue to provide important options for lifecycle housing, including small units, housing diversity, outdoor space, and affordability to those with moderate incomes.

Rowhouses

The LSDS encourages the development of rowhouses for residential areas. This type of development can successfully blend into the existing neighborhood as infill and redevelopment occurs because it is ground-related, has modest building heights, and is made of wood. It is also a compact development type that achieves densities of 30-40 units per acre. Units can be platted to offer homeownership opportunities that help local residents build wealth and equity as the neighborhood transforms and offers additional amenities.



Example Rowhouse Style Development

Sources: CGAArchitects.com, 2020 (left); Dwell.com, 2020 (middle); TheTownhouseGuy.ca, 2020 (right).

Rowhouses have minimal off-street parking requirements, which makes them functional and affordable for families. Rather than losing the ground floor to parking, they get additional ground floor living space. They are particularly well-suited for families who want the convenience of a yard, but the advantages of a more compact, walkable neighborhood, and access to transit. Where lots can be consolidated, rowhouses can scale into perimeter block housing with access to an interior common space. Parking for rowhouses is provided in collective lots on the development site and can be reconfigured or redeveloped as the area becomes less auto-dependent.

Apartments and Mixed-Use Residential Development

Apartments, whether stand-alone stacked-flats or part of a mixed-use development, are allowed in the LSDS commercial zones located along Pacific Highway and Bridgeport Way. Placing high intensity residential development in commercial areas gives residents easy access to goods, services, and entertainment. The highest density residential development is identified for the Transit-Oriented Commercial zone due to its proximity to Lakewood Station.

Larger lot sizes near the station also support larger scale development options that improve the financial feasibility of mixed-use or apartment development. Lakewood Landing is a good example of a master planned development utilizing a large available lot to build a residential development with a mix of retail and entertainment uses. Mixed-use developments like this provide housing, make an efficient use of land, and bring in amenities that reduce auto-dependence.

Mobile Home Park

The study area also contains the Oaknoll Trailer Park, located near the SR 512 interchange, but west of Pacific Highway/South Tacoma Way. Oaknoll rents about two dozen spaces that accommodate mobile homes and RVs. Some of these units are offered for rent and others are individually owned and lease a space only. Currently a non-conforming use in the C2 zone, state law protects the park from removal based on its non-conforming status.²¹



Oaknoll Trailer Park

Source: Google Earth, 2020.

The City could consider adding incentives to improve the trailer park while retaining it as a source of affordable housing. This may include incentives for landscaping and private streetscape or allowing for tiny homes on wheels to replace standard RV or obsolete manufactured homes (e.g. units built before 1976). State law allows tiny house villages in mobile home parks.²²

Affordability

There is sufficient land capacity to replace existing housing units as well as absorb additional units as redevelopment occurs in the LSDS. Supplementary policies and programs to support existing residents as the area transitions could reduce involuntary displacement. Other communities have used notice of intent to sell ordinances, right of first refusal incentives²³, and housing navigator programs²⁴ to support neighborhoods in transition. A citywide just cause eviction protection ordinance

²¹ State law indicates that cities and counties may not order the removal or phased elimination of an existing manufactured housing community because of it being a nonconforming use. Cities also cannot prohibit the entry or require the removal of a manufactured/mobile home, park model, or recreational vehicle authorized in a manufactured housing community because the community is a nonconforming use. (See RCW 35.63.160-161, RCW 35A.63.145-146, and RCW 36.70.493.)

²² State Law (SB 5383) also expanded the subdivision statute (RCW 58.17.040(5)) to allow the creation of tiny house villages and stops cities from prohibiting tiny houses in manufactured/mobile home parks.

²³ Notice of intent to sell or right of first refusal programs provide opportunities for existing tenants to reserve units when the property they live in is redeveloped.

²⁴ Housing navigator programs provide customized assistance to households in redevelopment areas.

would target reductions in arbitrary evictions.²⁵ A policy review of these types of measures, in consideration of existing policies and programs, is a recommended strategy for the LSDS.

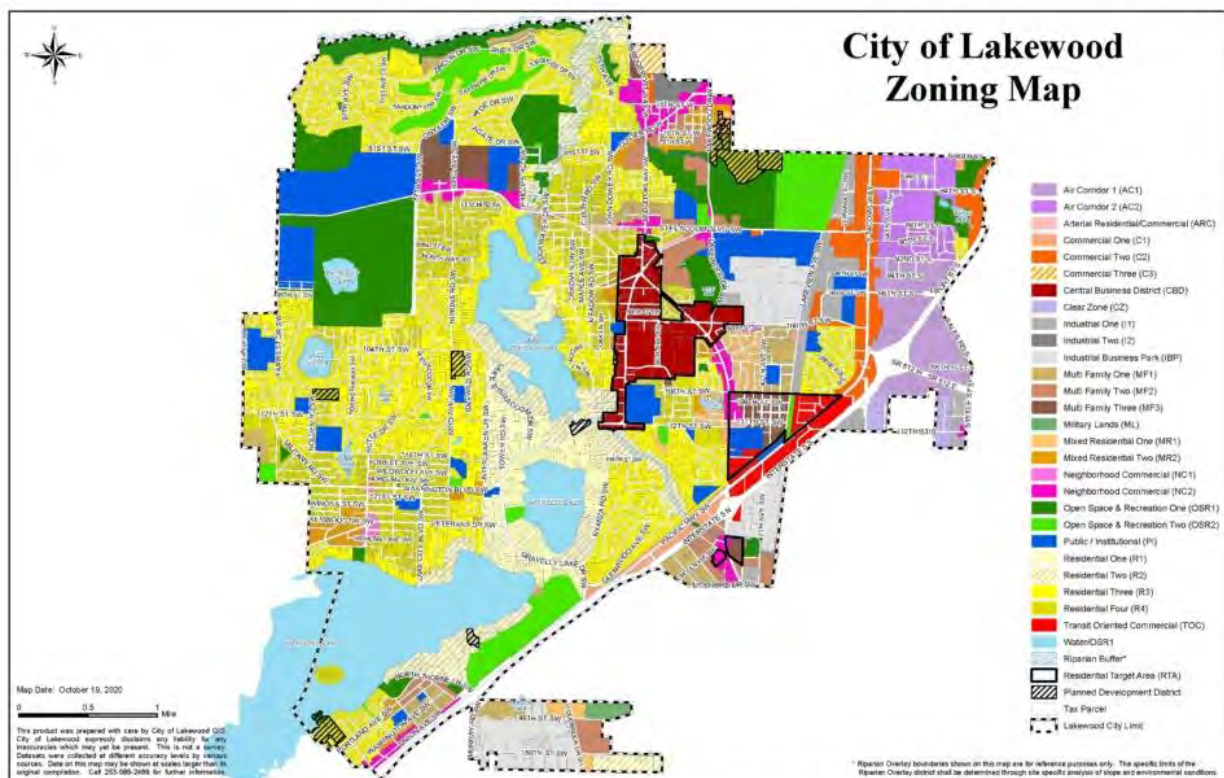
The City's Housing Incentives Program encourages the development of housing for people regardless of economic means.²⁶ Inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing are described in LMC Chapter 18A.90. Incentives are available to support the development of rental housing in all zones that allow it.²⁷ Those who create units affordable to households with very low incomes receive a bonus market rate unit or one and a half bonus market rates units for each unit affordable to households with extremely low incomes. Density bonuses are capped as a percentage of the base zoning district. This includes a 20% base density increase in MF3, a 15% increase in NC2, and a 25% increase in the TOC zone. Modifications in zone development standards such as coverage, parking, and height are allowed for projects participating in the housing incentives program. There is also a reduction in permitting fees.

Lakewood also has a multifamily property tax exemption, which exempts some types of new housing from paying ad valorem property taxes. Currently, most of the LSDS is within one of the residential target areas where the exemption may be applied. The current boundary is outlined in black as a Residential Target Area in Exhibit 24. This offers an incentive to support the types of housing envisioned for the subarea.

²⁵ Adoption would require consideration of citywide applicability and impacts.

²⁶ Lakewood Municipal Code 18A.090

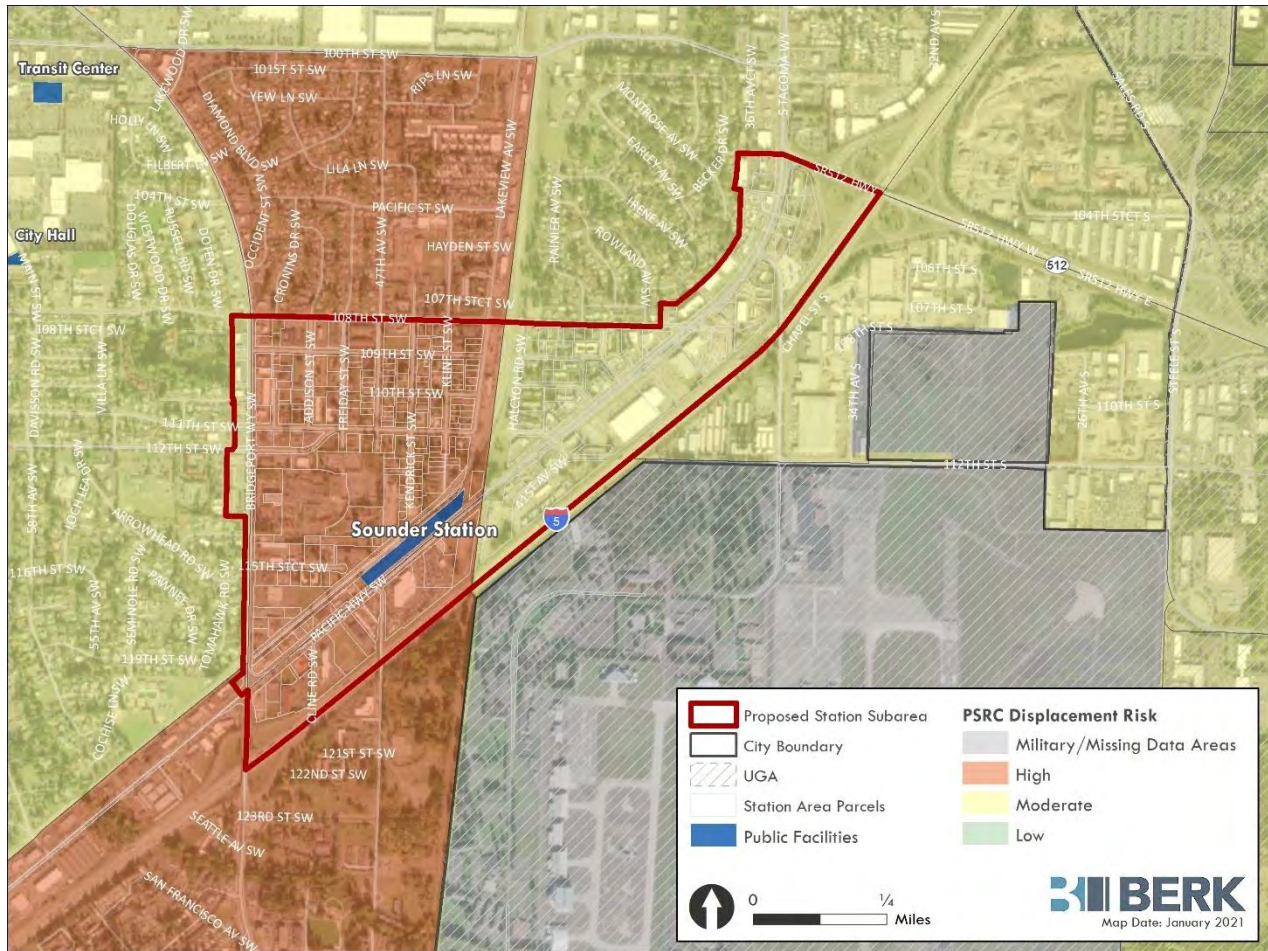
²⁷ With the exception of the construction of one single-family dwelling on one lot. 18A.090.030.

Exhibit 24. Residential Target Areas

Source: City of Lakewood, 2020.

Preventing Displacement

Current residents in the LSDS may be vulnerable to displacement with redevelopment in this area due to high rates of cost burden, low household incomes, and other factors. Exhibit 25 shows high and moderate risks of displacement in the subarea based on an analysis done by the Puget Sound Regional Council in 2017. Existing low density housing is anticipated to be replaced through small or moderate-scale redevelopment projects over the next 20 years that will increase the number of available units in the area. The focus of this plan is support additional options for low and moderate income housing, as well as a variety of housing types, is one way to prevent economic displacement.

Exhibit 25. Estimated Displacement Risk

Source: Puget Sound Regional Council, 2017; BERK, 2021.

The City of Lakewood leads or participates in plans and programs to support, create, and maintain affordable housing options for its residents. In addition, the policies and strategies in the Housing section of this plan aim to incentivize new affordable housing creation, support increased rates of homeownership, and assist residents at risk of displacement.

The following **programs support the creation of new affordable housing** in the City and study area:

- Housing Incentive Program (LMC 18.90) provides inclusionary density bonuses, development standard flexibility, and fee reductions for affordable housing development.
- Multi-family Property Tax Exemption (MFTE) gives a tax break for affordable housing development in most of the Lakewood Station District.

- Community Development Block Group (CDBG)/HOME funds from the federal government are used to support affordable housing development.
- Affordable Housing Initiative (2060) and Homeless Housing Act (2163) Programs that are joint with Pierce County and distribute funds to support affordable housing and homelessness.
- SHB 1406 Program applies a portion of the sales and use tax to use for supportive and affordable housing.

The following **programs support residents** in obtaining and maintaining high quality housing they can afford:

- Down payment assistance program by City of Lakewood and Pierce County.
- Pierce County property tax exemption for seniors and disabled.
- On-going partnership with Habitat for Humanity.
- Rental Housing Safety Program: ensures all housing units meet a consistent, minimum level of safety for tenants.

Policies and Strategies

Policies

- Encourage a variety of housing types to ensure choice for current and future residents, workers, military families, and to strengthen commercial areas.
- Support housing affordable to current LSDS residents though preservation where possible, as well as redevelopment activities.
- Incentivize the development of market rate and affordable housing options for households with incomes at 60-120% of the area median income to preserve affordability in the LSDS and avoid displacement of existing residents.
- Encourage homeownership options that allow local residents to invest in the community to gain equity and wealth.
- Attract a mix of large and small builders that can provide new housing options at a variety of scales and levels of

affordability.

Strategies

- Allow the highest density housing in mixed-use areas and in the Transit-Oriented Commercial zone where the availability of amenities within walking distance will encourage non-motorized transportation alternatives.
- Encourage and support missing middle housing types such as duplexes, triplexes, and small-scale multifamily as infill development in the residential area to improve housing choice and preserve affordability.
- Support the development of ground-related rowhouses as an affordable home ownership option for LSDS residents.
- Support the development of housing that meets the needs of military households.
- Partner with affordable housing organizations to support the expansion of subsidized housing in the LSDS by building new projects or by purchasing and rehabilitating existing housing.
- Work with the Washington Homeownership Center to find opportunities for investment in the LSDS.
- Consider expansions of existing affordable housing incentive programs and new affordability incentives in the LSDS, including the extension of the applicability area for the multi-family property tax exemption (MFTE).
- Consider programs to support residents in the LSDS as the area redevelops and reduce involuntary displacement such as a housing navigator program, a notice of intent to sell ordinance, just cause eviction protections, and right of first refusal incentives.
- Develop community benefit agreements with large investors in the LSDS.
- Provide information and support to help investors navigate affordable housing financing programs.
- Identify partners to support funding and a program that will rehabilitate existing homes such as a home repair program.



Economic Development, Business, & Employment

The LSDS is an area ripe for new investment due to its location near I-5 and the availability of local and regional transit options. Proximity to Joint Base Lewis-McChord (JBLM) provides a unique and regional market opportunity. A 2018 JBLM Regional Economic Impact Analysis shows that the base has a \$9.2 billion impact on the regional economy.²⁸ The LSDS also has the opportunity to become a health care hub with proximity to Madigan Hospital on JBLM as well as to St. Clare Hospital and Western State Hospital. Business development in the subarea could attract a greater portion of regional investment to Lakewood.

Context

The LSDS supports over 170 businesses and nearly 2,300 employees.²⁹ These numbers grow to over 640 businesses and 6,550 employees within the half mile extended study area.

²⁸ South Sound Military Partnership and University of Washington Tacoma, 2018.

²⁹ Data in this paragraph comes from ESRI Business Summary, 2019.

Retail and services are the primary businesses sectors. Services, including lodging, health care, automotive, legal, and other service industries comprise nearly half of the businesses in the subarea (46%) and employ 65% of the workers. Retail businesses, including restaurants and the sales of merchandise, gas stations, food, and other goods, comprise 29% of the businesses in the subarea and employ 22% of the workers.

Local businesses and small businesses serve the LSDS by providing opportunities for shopping, employment, and entrepreneurship. Stakeholders and public participants acknowledged the importance of small and local businesses in the LSDS visioning survey. Some provide service in Korean or Spanish to support the local community and create spaces of belonging. As the area transitions and grows, measures that prevent residential displacement and support small business will help avoid business displacement by maintaining local customer base and providing resources for continued business development.

Jobs in this area provide moderate incomes: about 44% provide an annual income above \$40,000 a year, approximately half of the median household income for Pierce County. Almost one-fifth (18%) of the workers in the subarea earned under \$15,000 a year.

The LSDS is part of the federally designated Lakeview/Kendrick Street Opportunity Zone. The opportunity zone includes two census tracts. These tracks overlap with portions of the LSDS including the Bridgeport Way interchange with I-5, St. Clare Hospital, the Sounder Station, the proposed Lakewood Landing site, and a portion of the residential area. Opportunity zones were created by the 2017 Tax Cuts and Jobs Act with the intention of supporting economic development and employment in distressed communities.³⁰ The program allows investors to defer capital gains tax for up to nine years by investing their gains in a Qualified Opportunity Zone. The federal program is funded through 2026 and supports redevelopment in the LSDS.



Local businesses serve residents and provide entrepreneurship opportunities

Source: El Mercado Latino

³⁰ Department of Commerce, 2020.

<https://www.commerce.wa.gov/growing-the-economy/opportunity-zones/>

Policies and Strategies

Policies

- Support medical office and commercial office opportunities.
- Encourage mixed-use development to expand opportunities for business and employment in the LSDS.
- Support small business development to build on the diverse offering of goods and services to residents and workers in the LSDS.

Strategies

- Promote St. Clare Hospital and satellite medical office and supportive commercial uses. Connect the residential neighborhood to the hospital with improved pedestrian and linear park opportunities.
- Support the master planning of the Lakewood Landing site for mixed-use development to expand opportunities for employment and retail development. See related Land Use policy.
- Partner with developers to identify projects eligible for participation in the Lakeview/Kendrick Street Opportunity Zone.
- Provide resources for entrepreneurs and small businesses, such as grant/loan programs, to support and recruit small, local businesses in the LSDS.
- Encourage micro-retail, or businesses that cater to specific demographic segments of the community, with business development resources and support available in other languages and flexibility in the design of cultural spaces.
- Invest in infrastructure and civic amenities consistent with this plan to attract business owners and investors.



Transportation

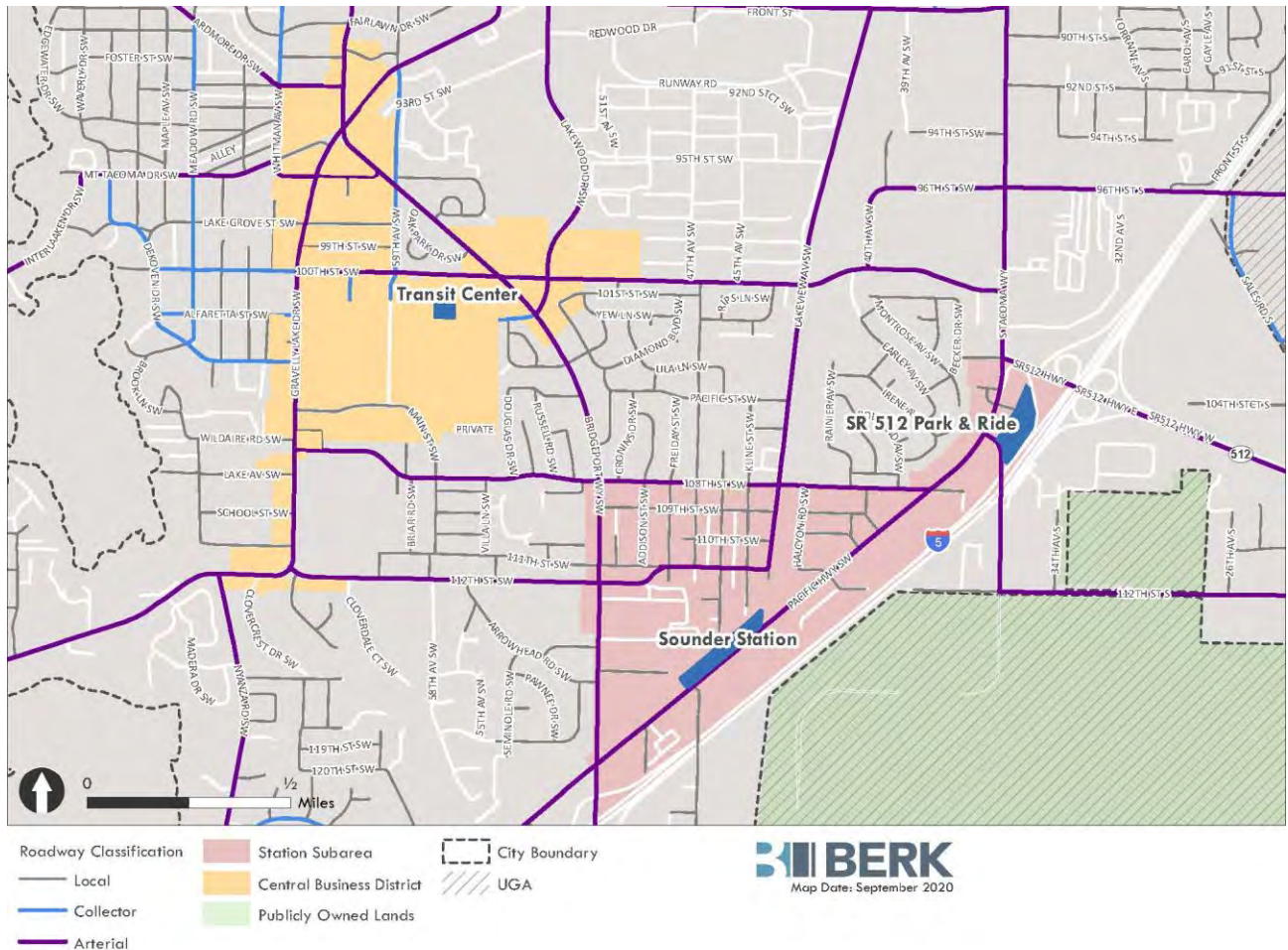
Context

The LSDS is a predominantly auto-oriented environment framed by access to I-5 to the north at the SR 512 interchange and at the Bridgeport Way interchange to the south. It is also served by major arterials such as Pacific Highway SW, Bridgeport Way, and 108th Avenue SW and a network of local access streets (see Exhibit 26).

There are two key regional transit facilities: Lakewood Station and SR 512 Park & Ride. Both Lakewood Station and SR 512 Park & Ride provide vehicle parking for transit users. The Downtown Transit Center is only about a mile away from the study area along Bridgeport Way. Enhanced streetscapes could provide better bike and pedestrian accessibility to transit resources within and nearby the LSDS.

The subarea is bisected by an existing rail line on which the Sounder and freight trains operate. While the rail line is an important regional transit and freight corridor, it has also created an environment with few roadway crossing opportunities which can affect vehicle operations and bicycle and pedestrian mobility.

Marked bicycle lanes are located in the LSDS on Pacific Highway and 108th Street SW. While sidewalks are generally located on all major streets in the project area, the residential area lacks sidewalks on most roadways. In addition to adding sidewalks and bike lanes, there is an opportunity to enhance existing street facilities. Adding landscaping that buffers non-motorized travelers from automobile traffic flows could improve safety and comfort.

Exhibit 26. Transportation Features in the LSDS and Vicinity

Sources: Fehr & Peers, 2020; Pierce County, 2020.

Level of Service

The transportation element of the Comprehensive Plan provides policy direction to address local and regional mobility. The transportation element acknowledges the increase of traffic congestion within Lakewood and seeks to mitigate it by developing a balanced multimodal system that effectively moves people, goods, and services without compromising community character. The Plan specifically calls for the incorporation of non-motorized facilities, enhanced illumination, and other pedestrian amenities into new development designs.

The City uses PM peak hour average delay to evaluate traffic operations level of service (LOS) at its intersections. The transportation element designates LOS guidelines for the city's

arterial streets and intersections. Within the study area, the City sets a LOS standard of LOS D³¹ during the weekday PM peak hour at all arterial street intersections. However, according to Policy T-20.5, the City may allow minor street stop-controlled intersections to operate below that LOS standard if those instances are thoroughly analyzed from an operational and safety perspective.

Redevelopment in the LSDS is expected to increase roadway volumes that will impact the movement of automobiles, freight, and transit. The areas where LOS may be affected, without mitigation interventions (mitigations are discussed on the following pages), include the following intersections:

- Pacific Highway / Sharondale Street SW – Increase in delay, LOS F to LOS F
- Pacific Highway / Bridgeport Way – from LOS D to LOS E
- Bridgeport Way / SB I-5 Ramp – from LOS D to LOS E

Improvements

Planned Improvements

The Six-Year Comprehensive Transportation Improvement Program (TIP) for 2021-2026 was approved by the Lakewood City Council in June 2020. This document outlines short and long term road projects, including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays, throughout the city. The following projects within the LSDS are identified in the TIP as long term improvements:

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study.
- Kendrick Street SW from 111th Street SW to 108th



111th and 112th Street Corridors

Source: City of Lakewood.

³¹ LOS D is defined in the Lakewood Comprehensive Plan as a level of traffic that is approaching unstable flow. Travel speed and freedom to maneuver are somewhat restricted, with average delays of 25 to 40 seconds per vehicle at signalized intersections. Small increases in traffic flow can cause operational difficulties at this level.

Street SW – sidewalks, street lighting, bicycle facilities.

- 112th/111th – Bridgeport to Kendrick – sidewalks, bicycle facilities, street lighting.
- 108th – Bridgeport Way to Pacific Highway – roadway patching and overlay.

Sound Transit and Pierce Transit have also identified improvements for the LSDS in their plans. The Sound Transit System Access Fund awarded funds in 2019 to support non-motorized access to Lakewood Station. These include a variety of improvements to support pedestrian and bicycle access on 11th Street SW and 112th Street SW. At this time there is no specific plan for a pedestrian crossing of Pacific Highway to connect Lakewood Station to the future Lakewood Landing site, but options should be considered. Pierce Transit's *Destination 2040* plan identifies two new Bus Rapid Transit (BRT) routes that will bring additional transit service to the subarea. A new line in 2026 would connect to Downtown Tacoma. In 2030 a BRT line would replace an existing transit line connecting Lakewood to Puyallup's South Hill.

General Mitigation Measures

Managing demand for auto travel is an important part of mitigating the traffic congestion impacts.

Transportation demand management (TDM) strategies could include subsidies or discounts for non-auto travel, education and assistance to help travelers identify non-auto commute options, rideshare and ridematch promotion, and local incentive and reward programs.



Transit near employment helps reduce auto demand

Source: City of Lakewood.

Washington state Commute Trip

Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip reduction plans and work toward meeting their mode share targets through internal programs and monitoring.

The City of Lakewood included policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not

affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote transportation demand management (TDM) strategies. With a robust TDM program in place, it is expected that actual trip generation in the Study Area could be lowered beyond the levels analyzed as part of the Subarea Plan.

Specific Mitigation Measures

The City could make signal timing improvements to increase the efficiency of impacted intersections and roadways in the subarea. Exhibit 27 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right-of-way acquisition or widening, rather only changes to signal timing. The exception is intersection of Pacific Highway and Sharondale Street, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better.

Exhibit 27. Proposed Mitigation Measures

INTERSECTION	IMPROVEMENT
Pacific Highway / Sharondale Street	Install signal using existing channelization and signal length consistent with adjacent intersections
Pacific Highway / Bridgeport Way	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.
Bridgeport Way / SB I-5 Ramp	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.

Source: Fehr & Peers, 2020.

With implementation of the potential signal timing mitigation measures outlined in the previous section and future improvements at the Bridgeport Way/I-5 ramps, the magnitude of the impacts could be mitigated such that auto, freight, and transit are not significantly impacted.

Street Standards

As noted in Land Use & Urban Design, new street standards are an important tool in the transformation of the LSDS according to its vision. New development brings the opportunity to modify frontages to improve safety and comfort for pedestrians and bicyclists. It also provides opportunities to increase green space and open space. By adopting new street standards, the LSDS will improve travel conditions across all modes.

The City of Lakewood hopes to offset the costs of complying with street standards. It will identify programs and funding sources aimed at sharing or minimizing the cost of street improvements in the LSDS.

Policies and Strategies

Policies

- Provide multi-modal transportation options to improve the flow of people and goods in the LSDS and to reduce auto-dependence.
- Support pedestrians in the LSDS with sidewalk and infrastructure improvements that enhance safety and provide a more comfortable travel experience.
- Enhance non-motorized connections to Lakewood Station and Pierce Transit facilities to encourage the use of regional and local transit.
- Connect the LSDS to Downtown with enhanced pedestrian and bicycle facilities.
- Publicly invest in the development and construction of streetscapes to support redevelopment in the LSDS.
- Consider options for crossing Pacific Highway to provide a safe and efficient linkage between Lakewood Station and the Lakewood Landing development site.

Strategies

- Apply a hybrid form-based code to support improved streetscapes that accommodate pedestrian travel.
- Add signalization improvements at Pacific Highway and Sharondale Street to prevent traffic delays and at Bridgeport Way and the I-5 northbound/southbound off ramps to prevent queuing that extends onto the freeway.
- Work with Sound Transit and Pierce Transit on the timing and location of non-motorized improvements to avoid gaps and overlaps in the system.
- Require street frontage improvements that enhance walkability and support economically viable and quality residential development.
- Identify funds, funding mechanisms, grants, and programs that allow the City of Lakewood to develop and build streetscapes in the LSDS.



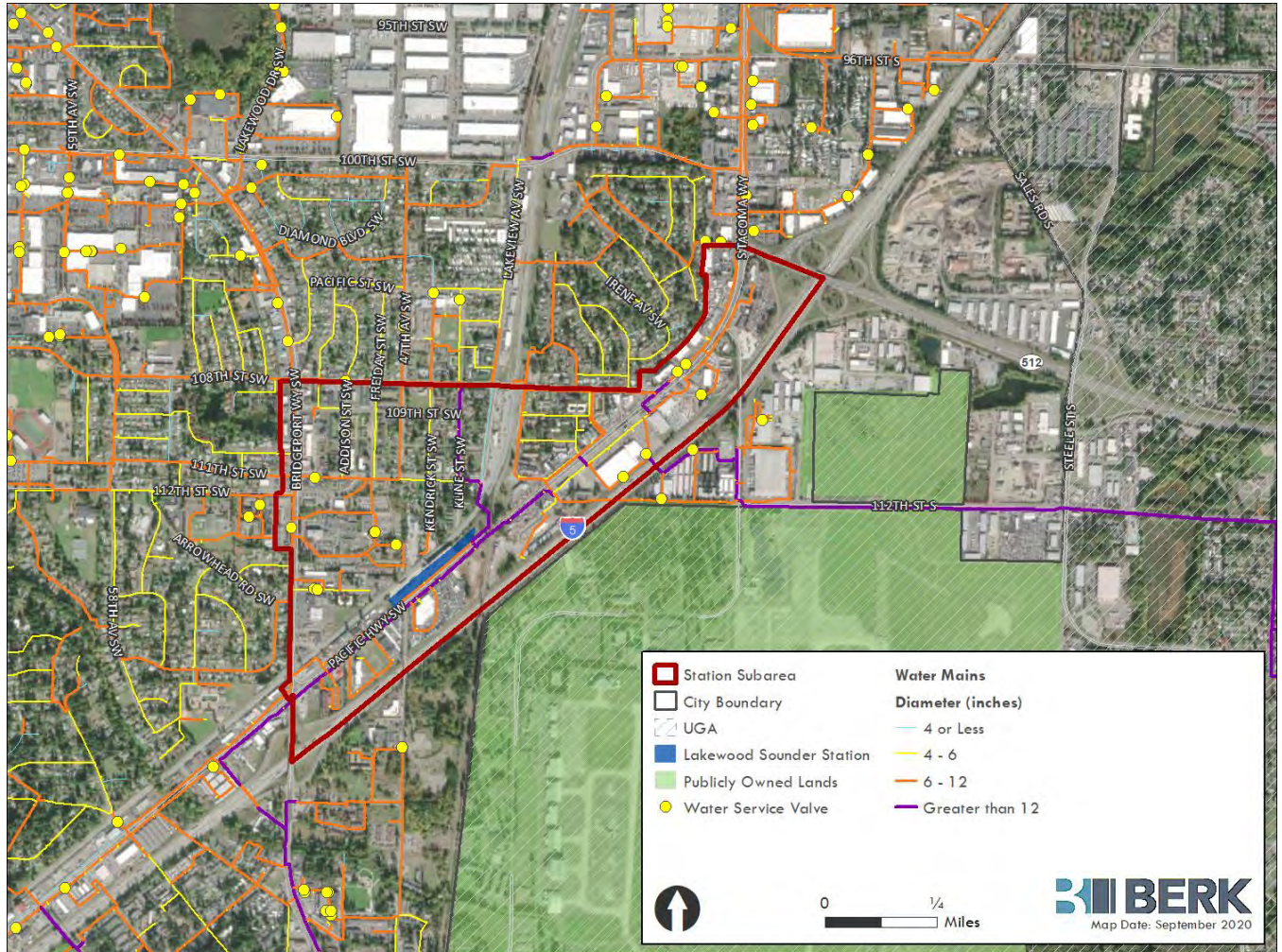
Utilities & Public Services

A variety of providers serve the LSDS with utility infrastructure and public services. Infrastructure updates and capacity changes related to the subarea plan will need to be coordinated with these providers. This should be done through participation in the updates of long-range system plans as well as shorter term capital facility improvement plans and project specific planning and permitting.

Context

Water

Water is provided by the Lakewood Water District and the subarea is fully served. As shown in Exhibit 28, the largest water mains primarily run along Pacific Highway, but a main larger than 12 inches runs roughly under Kline Street SW into the residential area. There is also a main larger than 12 inches that runs into the subarea from the southeast side of I-5. Mains 6 to 12 inches in size are well distributed throughout the subarea, running under several streets, including Bridgeport Way. These midsized mains serve St. Clare Hospital and the existing commercial development in the subarea.

Exhibit 28. Water Utility Infrastructure in LDS and Surrounding Area

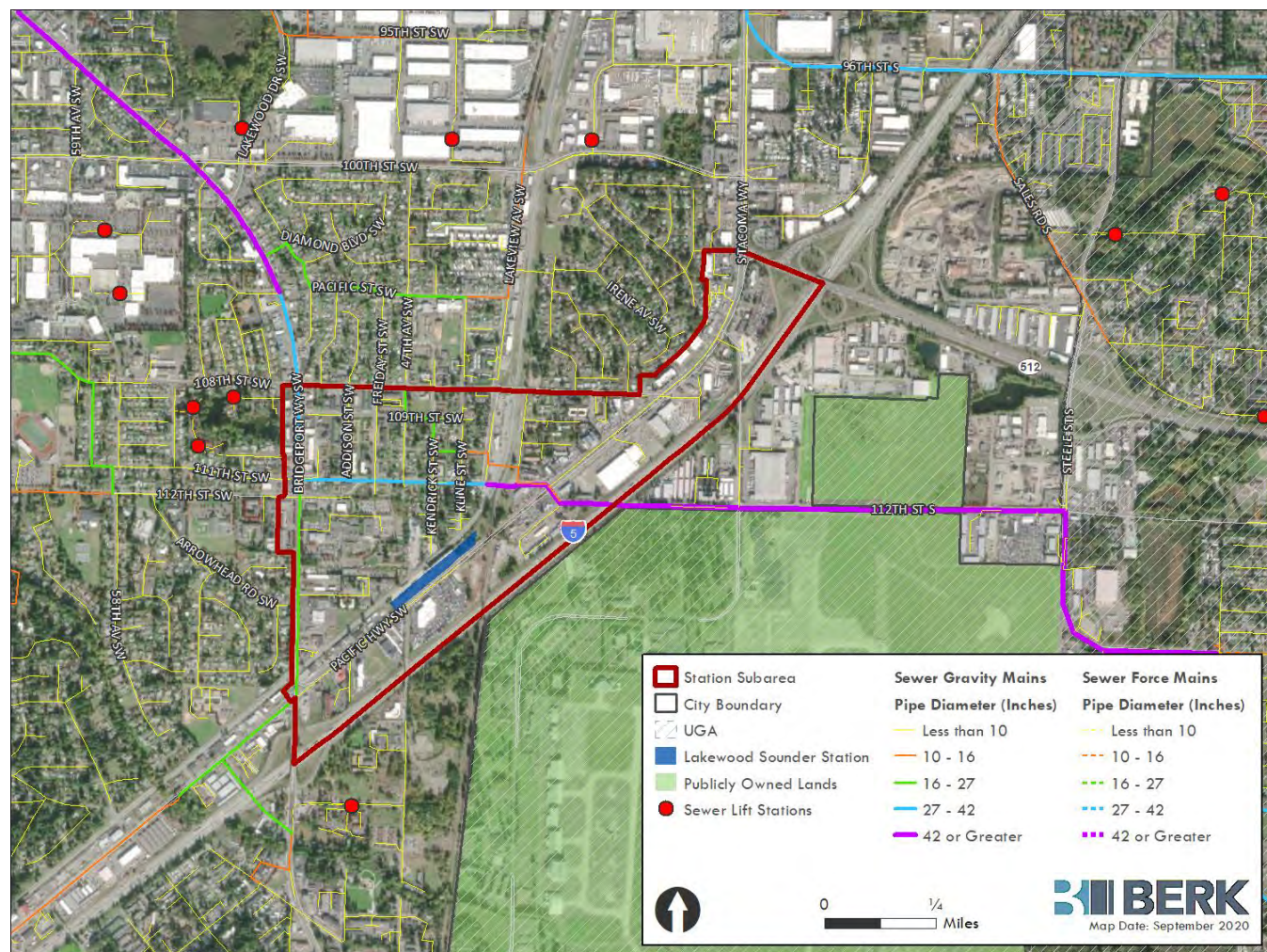
Sources: BERK, 2020; Pierce County Assessor, 2020.

The Lakewood Water District initiated a 50-year repair and replacement plan in 2014 to replace 181 miles of aging water mains. Some replacements have already occurred in the subarea, including replacements along Bridgeport Way. The repair and replacement plan focuses on the replacement of facilities that are nearing the end of their useful life and does not account for upgrades or extensions to support new development. District policy requires the developer to pay for system improvements related to new development. Depending on the location and intensity of new development in the subarea, this may include water main upgrades or line extensions to provide additional capacity or fire flow. Coordination of upgrades with the 50-year repair and replacement program is recommended and could provide cost and timing efficiency for redevelopment projects in the subarea.

Wastewater

Wastewater service in Lakewood is provided by Pierce County Planning and Public Works. Most of the sewer pipes shown in Exhibit 29 were installed in the 1980s (some later) and are primarily 8-inch diameter PVC pipes, which have a 100-year lifetime. Larger interceptors are typically made of concrete, which has a service life of 50 to 100 years. Generally, the sewer infrastructure is considered in good condition with plenty of remaining service life and no current need for large scale line replacements or upgrades.

Exhibit 29. Wastewater Infrastructure in the LSDS and Surrounding Area



Sources: BERK, 2020; Pierce County Public Works, 2020; Pierce County Assessor, 2020.

The most recent system plan is the 2010 Unified Sewer Plan, adopted in 2012. Pierce County was scheduled to begin an update in late 2020. The Plan accounted for zoning densities up to 54 units per acre, which should be adequate for most development within the LSDS. Master planned development at

Lakewood Landing may occur at a higher intensity and require wastewater system capacity upgrades. The update of the Unified Sewer Plan provides an opportunity to plan for future development at Lakewood Landing.

Pierce County coordinates quarterly with the City of Lakewood to discuss upcoming and future projects. A Sewer Improvement Plan (SIP) was adopted in September 2019, addressing capital facility planning from 2020-2040 and identifying funding for the next six years of capital facility improvements. Reviewing the SIP in light of the LSDS and Lakewood Landing development is recommended.

Energy

Lakeview Light and Power provides electrical service to the subarea and Puget Sound Energy provides natural gas service. Maps showing the locations of the lines providing natural gas service are not available and most facilities are located underground. Puget Sound Energy is working on an updated Integrated Resource Plan to ensure continued delivery of service to its existing service areas in the region and to address opportunities for enhanced conservation and reduced environmental impacts.

Lakeview Light and Power is working on increased system capacity and flexibility. There is a five-year capital budget for repowering the substation at Pacific Highway and Bridgeport Way. Improvements to the substation will allow Lakeview Light and Power to rebalance loads as needed to accommodate fluctuations in usage. Increased demand for electrical service should be possible to accommodate, but specific plans may be needed to address any heavy commercial users and electric vehicle charging capacity. Line infrastructure is located mostly above ground within the subarea. As redevelopment occurs, taller buildings will likely require utility undergrounding for safety, but Lakewood does not have a policy or regulation requiring undergrounding with new development.

Emergency Response

Emergency response services in Lakewood are provided by West Pierce Fire and Rescue and the Lakewood Police Department.

The Lakewood Police Department is one of the largest departments in the state. It operates six patrol districts as well as specialized units in criminal investigation, K9, traffic, and marine response. The subarea is part of the Pacific Patrol District, which is authorized for up to 28 sworn personnel.³² All patrol districts, including the Pacific District, are operated out of the police station in the civic campus in Downtown, less than a mile from the subarea.

Emergency management functions for Lakewood are part of the Police Department. The mission of emergency management is to assist with mitigation, preparation, response, and recovery from natural disasters and other community emergencies. This includes a variety of trainings and informational materials to help residents and businesses prepare for major emergency events.

West Pierce Fire and Rescue provides fire and emergency medical response to Lakewood, University Place, and Steilacoom. Station 20 is located within the subarea along Pacific Highway. Services offered by West Pierce Fire and Rescue include emergency medical response, fire suppression, fire prevention, and community education. They also sponsor Community Emergency Response Team (CERT) trainings that prepare residents to be the first line of response in their neighborhoods following a disaster prior to the arrival of emergency responders.



Emergency response is readily available

Source: West Pierce Fire and Rescue.

³² Lakewood Police Department 2018 Annual Report, updated in 2019.

Policies and Strategies

Policies

- Ensure there is adequate utilities infrastructure to support redevelopment of the LSDS according to the vision.
- Consider future agreements to support infrastructure development in the LSDS such as development agreements, latecomers agreements, fee assessments, improvement districts, and other tools.

Strategies

- Coordinate with public service and utility providers during the update and development of long-range plans and system improvement plans to ensure there is adequate support for anticipated growth and development of the LSDS subarea.
- Work with public service and utility providers to ensure that capital improvement plans include the projects necessary to support development in the LSDS subarea.



Implementation Plan

The following table identifies responsibilities for implementing the vision for the LSDS. It outlines the direction, responsible entity, and timeline for plan actions. The timelines include ongoing, short term (up to three years), midterm (three to five years), or long term (more than five years).

	Direction	Timeline	Department
Land Use & Design	Support the master planning of the Lakewood Landing site for mixed-use development so it will provide opportunities for high density housing, employment, business, and public space.	Ongoing	Community Development and Economic Development
	Pursue opportunities for enhancing landscaping or adding linear parks within the public right-of-way to integrate green spaces and open spaces into the LSDS.	Long term	Community Development and Parks and Recreation
	Work through the <i>Legacy Plan 2020</i> to identify opportunities for public parks facilities within a 10-minute walk of subarea residents.	Midterm	Parks and Recreation
	Identify standards and guidelines that support transitional parking arrangements so space used for parking can be transitioned to other uses as the area becomes less auto-dependent.	Short term	Community Development
	Monitor the impact of the Lakewood Station District Overlay and amend the Plan as needed to improve outcomes.	Ongoing	Community Development

	Direction	Timeline	Department
Housing	Allow the highest density housing in mixed-use areas and in the Transit-Oriented Commercial zone where the availability of amenities within walking distance will encourage non-motorized transportation alternatives.	Ongoing	Community Development
	Encourage and support missing middle housing types such as duplexes, triplexes, and small-scale multifamily as infill development in the residential area to improve housing choice and preserve affordability.	Ongoing	Community Development
	Support the development of ground-related rowhouses as an affordable home ownership option for LSDS residents.	Ongoing	Community Development
	Support the development of housing that meets the needs of military households.	Ongoing	Community Development
	Partner with affordable housing organizations to support the expansion of subsidized housing in the LSDS by building new projects or by purchasing and rehabilitating existing housing.	Ongoing	Community Development
	Work with the Washington Homeownership Center to find opportunities for investment in the LSDS.	Ongoing	Community Development
	Consider expansions of existing affordable housing incentive programs and new affordability incentives in the LSDS, including the extension of the applicability area for the multi-family property tax exemption.	Short term	Community Development
	Consider programs to support residents in the LSDS as the area redevelops and reduce involuntary displacement such as a housing navigator program, a notice of intent to sell ordinance, just cause eviction protections, and right of first refusal incentives.	Short term	Community Development
	Develop community benefit agreements with large investors in the LSDS.	Ongoing	Community Development
	Provide information and support to help investors navigate affordable housing financing programs.	Short term	Community Development and Economic Development

	Direction	Timeline	Department
Housing, continued	Identify partners to support funding and a program that will rehabilitate existing homes such as a home repair program.	Short term	Community Development
Economic Development, Business, & Employment	Promote St. Clare Hospital and satellite medical office and supportive commercial uses. Connect the residential neighborhood to the hospital with improved pedestrian and linear park opportunities.	Short term	Community Development and Economic Development
	Support the master planning of the Lakewood Landing site for mixed-use development to expand opportunities for employment and retail development. See <i>related Land Use policy</i> .	Ongoing	Community Development and Economic Development
	Partner with developers to identify projects eligible for participation in the Lakeview/Kendrick Street Opportunity Zone.	Midterm	Community Development and Economic Development
	Provide resources for entrepreneurs and small businesses, such as grant/loan programs, to support and recruit small, local businesses in the LSDS.	Ongoing	Economic Development
	Encourage micro-retail, or businesses that cater to specific demographic segments of the community, with business development resources and support available in other languages and flexibility in the design of cultural spaces	Ongoing	Economic Development
	Invest in infrastructure and civic amenities consistent with this Plan to attract business owners and investors.	Long term	Public Works
Transportation	Apply a hybrid form-based code to support improved streetscapes that accommodate pedestrian travel.	Ongoing	Community Development
	Add signalization improvements at Pacific Highway and Sharondale Street to prevent traffic delays, and at Bridgeport Way and the I-5 northbound/southbound off ramps to prevent queuing that extends onto the freeway.	Long term	Public Works
	Work with Sound Transit and Pierce Transit on the timing and location of non-motorized improvements to avoid gaps and overlaps in the system.	Ongoing	Public Works

	Direction	Timeline	Department
	Require street frontage improvements that enhance walkability and support economically viable and quality residential development.	Ongoing	Community Development and Public Works
	Publicly invest in the development and construction of streetscapes to support redevelopment in the LSDS.	Ongoing	Community Development and Public Works
Utilities	Coordinate with public service and utility providers during the update and development of long-range plans and system improvement plans to ensure there is adequate support for anticipated growth and development of the LSDS subarea.	Midterm	Public Works
	Work with public service and utility providers to ensure that capital improvement plans include the projects necessary to support development in the LSDS subarea.	Midterm	Public Works

LAKEWOOD STATION DISTRICT

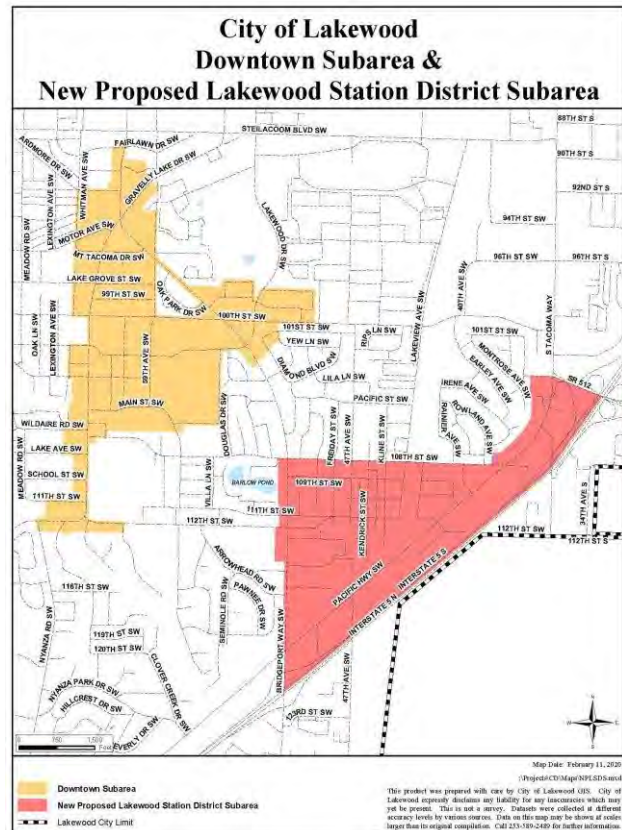
FORM-BASED CODE

Overview

The City of Lakewood is adopting a subarea plan for the Station District. The Station District is located southwest of Downtown and is centered around the Sounder Station. The City adopted a new subarea plan and hybrid form-based code for Downtown in the fall of 2018; the City intends to adopt a similar hybrid form-based code for the Station District that addresses the specific context of the Station District, including land use, streets and transportation, open space, and desired urban form. This framework will address how best to apply a hybrid form-based code to the Station District.

The draft code includes the following sections:

1. Station District
 - Purpose.
 - Vision and Objectives.
 - The Regulation Plan Map.
 - Definitions.
 - Relationship to Other Regulations.
2. Land Use and Zoning
 - Application within Station District.
 - Land Use.
 - District-Wide Development Standards.
3. Streets and Blocks
4. Site Design, Buildings, and Frontage
5. Landscaping, Open Space, and Green Infrastructure
 - Basic Standards.
 - Special Landscape Treatments.
 - Common and Open Space Standards.
 - Green Infrastructure.
6. Parking



Downtown and Station District Subareas

City of Lakewood, 2019

Title 18C

STATION DISTRICT DEVELOPMENT CODE

Chapters:

18C.100	Station District
18C.200	Land Use and Zoning
18C.300	Streets and Blocks
18C.400	Site Design, Buildings, and Frontage
18C.500	Landscaping, Open Space, and Green Infrastructure
18C.600	Parking

Chapter 18C.100

STATION DISTRICT

Sections:

18C.100.100	Background.
18C.100.105	Purpose.
18C.100.110	Vision and Objectives.
18C.100.120	The Regulating Plan Map.
18C.100.130	Definitions.
18C.100.140	Relationship to Other Regulations.

18C.100.100 Background. The Station Area District is defined by its location along major transportation corridors including Pacific Highway, I-5, and the railroad (See Figure 18C.100-1). These transportation corridors limit street, bike, and pedestrian connections. However, a pedestrian bridge connects the Sounder Station to the neighborhood north of the railroad tracks. Land use in the district varies from large auto-oriented uses to multi-family and single-family housing. Major arterial streets in the district include Pacific Highway and Bridgeport Way. Within the confines of the Regulating Plan Maps, there are several dead-end streets. Some of these streets were the result of the construction of I-5 which began in the late 1950's and culminated in November 1968. Other dead-end streets are associated with the current railroad right-of-way which existed well before the establishment of many of the residential subdivisions within the area, and, finally, prior to Lakewood's incorporation, inadequate county subdivision code regulation that did not take into account urban development.

18C.100.105 Purpose.

The purpose of the Station District is to provide for coordinated and high-quality development including new housing, commercial development, parks and open space, and multi-modal transportation infrastructure within the Subarea. The Station District Hybrid Form-Based Code Overlay is intended to guide private development and changes in the public right-of-way to support the goals and implementation of the Station District Subarea Plan.

18C.100-1. Station District



Framework, 2020

18C.100.110 Vision and Objective.

The Lakewood Station District is a multi-modal commuter hub of Lakewood and is the southern terminus of Sound Transit's commuter rail service. The Lakewood Station District provides an amenity-rich, transit-oriented development node surrounding the Lakewood Station. This District offers a mixture of intensive land uses and activities supportive of direct regional transportation access via the commuter rail station and I-5. The District implements development standards to foster a high quality, pedestrian-oriented urban environment, including incentives to encourage a dense mix of commercial and medical office, regional and local retail, services and hospitality, and high-density residential uses offering ownership and rental housing opportunities, all supported by direct regional transportation access.

18C.100.120 The Regulating Plan Map.

The regulating plan translates the community vision into a map. The regulating plan designates the locations, subdistricts, and streets that are intended to embody specific physical characteristics. It specifies the location and applicability of specific design treatments and identifies where they are required. The regulating plan works in tandem with the development standards, tables, and figures in this chapter, the rest of Title 18A and the LMC to define the shape, size, and location of streets through connections, infill blocks, buildings, and landscaping.

18C.120-1 Regulating Plan Map with Zoning



Framework, 2020

18C.120-2 Regulating Plan Map – Street Typologies



Framework, 2020

18C.100.130 Definitions.

Definitions in this chapter shall be consistent with LMC 18A.10.180, Definitions. Interpretations of certain terms and phrases shall be consistent with LMC 18A.10.040, Rules of Construction.

18C.100.140 Relationship to Other Regulations.

In the case of a conflict between the regulations in Chapter 18C.100 LMC, Station District, and the rest of the Lakewood Municipal Code, the regulations in Chapter 18C.100 LMC, Station District Form-Based Code Overlay, shall control, with the exception of LMC Title 15, which will govern issues of public health and safety.

Chapter 18C.200

LAND USE AND ZONING

Sections:

- 18C.200.210 Application within Lakewood Station District.**
- 18C.200.220 Land Uses.**
- 18C.200.230 District-Wide Development Standards.**

18C.200.210 Application within Station District.

The Lakewood Station District Subarea (LSDS) has several base zones that are applied as shown in the map below. The zones are described as follows:

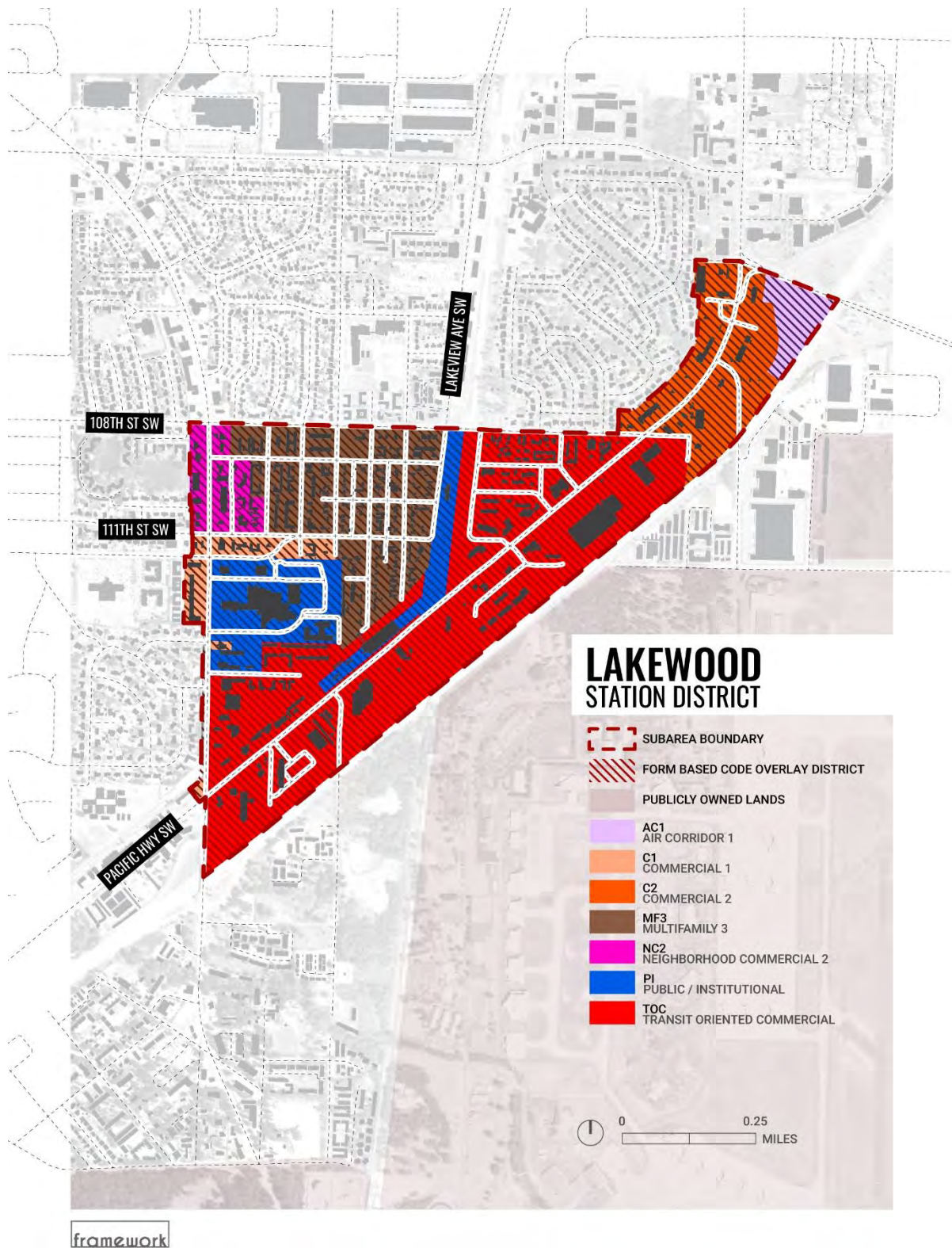
- A. The Multifamily 3 (MF3) zoning district is intended to integrate urban, high-density, multi-story housing in close proximity to a principal or minor arterial, with commercial/residential districts. The MF 3 zoning districts are predominantly located adjacent to land zoned NC2, CBD, or SD.
- B. The Neighborhood Commercial 2 (NC2) zoning district is intended to foster a sense of urban community in Lakewood. The district provides for a concentrated mix of activities, including residential, retail, office, and local services, which may serve the surrounding neighborhood or may serve more than one (1) neighborhood and attract people from other areas.
- C. The Commercial 1 (C1) and Commercial 2 (C2) zoning districts promote employment, services, retail, and business uses serving and linking neighborhoods to Lakewood's major transportation networks. The geographic relationship of the corridors to major road networks and their limited integration with adjacent neighborhoods promote employment, services, retail, and business/light industrial uses linked to access the major transportation networks. The C3 zoning district is distinguished by its arterial location and focus on "big-box" type uses which form an anchor for a large-scale commercial development.
- D. The Transit-Oriented Commercial (TOC) zoning district is an interactive mixture of uses which focus on regional transportation networks while providing for urban design, people orientation, and connectivity between uses and transportation routes.
- E. The Public/Institutional (PI) zoning district provides for moderate-scale and large-scale activities relating to the purpose of state and local governmental entities, except for military uses which are separately designated and zoned; special districts; and semi-public institutions providing

necessary public services. The designation allows for the specialized needs of providing public services to all areas of Lakewood.

F. The purpose of the Clear Zone (CZ), Air Corridor 1 (AC1), and Air Corridor 2 (AC2) zoning districts is to promote land use and development that is compatible with the aircraft noise and accident potential associated with the proximity to McChord Field, which is a part of the Joint Base Lewis-McChord (JBLM). McChord Field hosts three C-17 airlift squadrons. The potential risk to life and property from hazards associated with military aircraft operations necessitates control of the intensity, type, and design of land uses within the air corridor.

G. The Open Space and Recreation 1 (OSR1) and Open Space and Recreation 2 (OSR2) zoning districts provide for open space and public or semi-public recreational activities throughout the City.

18C.200-1. Station District Zoning Map



Framework, 2020

18C.200.220 Land Use.

A. Allowed and Prohibited Uses. All residential uses, civic uses, and commercial uses and their respective uses and their related accessory uses as defined in Chapter 18A.40 LMC are permitted within the Station District except for those prohibited uses listed below. Permitted uses are subject to the approval of all required development permits. Uses identified as prohibited below that legally existed prior to the adoption of this code are considered non-conforming.

1. *Prohibited uses in the AC1 zoning district:*
 - a. None.
2. *Prohibited uses in the MF3 zoning district:*
 - a. Building & landscape materials;
 - b. Convenience store;
 - c. General retail.
3. *Prohibited uses in the NC2 zoning district:*
 - a. Auto and vehicle sales/rental;
 - b. Building & landscape materials;
 - c. Solid waste transfer station;
 - d. Vehicle services, major repair/body work.
4. *Prohibited uses in the TOC zoning district:*
 - a. Accessory caretaker's unit.
5. *Prohibited uses in the C2 zoning district:*
 - a. Auto and vehicle sales/rental;
 - b. Manufacturing, assembling, & packaging, medium;
 - c. Manufacturing, assembling, & packaging, heavy;
 - d. Metal products fabrication, machine and welding;
 - e. Mobile home, RV, and boat sales;
 - f. Recycling facility – processing facility;
 - g. Repair service - equipment, large appliances.
 - h. Vehicle services, major repair/body work;
6. *Prohibited uses in the C1 zoning district:*
 - a. Auto and vehicle sales/rental;
 - b. Building & landscape materials;
 - c. Construction/heavy equipment sales and rental;

- d. Enhanced services facility;
- e. Equipment rental;
- f. Fuel dealer;
- g. Furniture/fixtures manufacturing, cabinet shop;
- h. Laundry, dry cleaning plant;
- i. Manufacturing, assembling, & packaging, medium;
- j. Manufacturing, assembling, & packaging, heavy;
- k. Mobile home, RV, and boat sales;
- l. Outdoor storage;
- m. Pawnbrokers and secondhand dealers;
- n. Recycling facility – processing facility;
- o. Repair service - equipment, large appliances;
- p. Secondhand stores;
- q. Storage, personal storage facility;
- r. Type 4 group home;
- s. Type 5 group home;
- t. Vehicle services, major repair/body work;
- u. Vehicle storage;
- v. Warehouse;
- w. Warehouse retail;
- x. Wholesaling and distribution.

B. Allowed Use Conditions. The following uses are allowed, provided the conditions listed below are met:

1. Drive-through facilities; *provided:*

- a. Drive-through facilities are limited to one drive-through lane per establishment;
- b. Drive-through facilities must have a primary customer entrance and cannot provide customer service exclusively from a drive-through or walk-up window;
- c. Drive-through facilities shall be designed so that vehicles, while waiting in line to be served, will not block vehicle or pedestrian traffic in the right-of-way;

d. Drive-through facilities shall be appropriately and attractively screened from the public right-of-way:

ii. Drive-through lanes shall only be placed parallel to a road if separated by a distance of 30 feet, or if fully screened by a 15-foot landscape setback with a designed landscape berm (six feet high at center of berm in 15-foot landscape setback) or three-and-one-half-foot decorative masonry wall;

iii. Drive-through lanes oriented perpendicular to a public right-of-way shall include landscape screening to shield headlights from shining directly into an abutting or adjacent street right-of-way.

C. Conditionally Permitted Uses.

1. Pipelines.

2. Manufacturing, assembling and packaging, light is allowed in the Station District, but such uses shall be as part of mixed-use development with commercial, retail, or residential uses.

D. Permitted Uses in the MF3 Zone within the LSDS.

1. Rowhouses.

2. Townhouses.

3. Three-family residential, attached.

E. Permitted Uses in the MF3 Zone within the LSDS with a master plan approval.

1. Multifamily use, four (4) or more units,

2. Mixed uses, when consistent with the following:

a. The commercial use is only permitted on the ground floor of the development.

b. A minimum ratio of four (4) square feet of multifamily use to one (1) square foot of commercial use.

18C.200.230 District-Wide Development Standards.

Unless otherwise specifically modified by an adopted development agreement or master plan, in addition to the regulations and requirements contained in other sections of this title and LMC Title 15, the following property development standards apply to all land and buildings in the Station District:

A. *Density*. Maximum density is based on the standards in the base zoning districts.

B. *Lot Size*. There is no minimum established lot size for the Station District. Proposed uses and the applicable design standards in this chapter shall be used to establish the minimum lot size for a project.

C. *Lot Coverage.* There is no minimum established lot size for the Station District. Proposed uses and the applicable design standards in this chapter shall be used to establish the minimum lot size for a project.

- D. *Setbacks*. The minimum distance setbacks for the Station District shall be determined by frontage type in Chapter 18C.400 LMC, except where increased setbacks due to building/fire code requirements apply.
- E. *Building Height*. The maximum building height, not including any applicable height bonus, for the Station District shall be determined by the underlying zoning districts.
- F. *Large-scale Commercial Facilities*. Large-scale commercial facilities shall meet the additional requirements specified in LMC [18A.70.050\(J\)](#).
- G. *Design*. Design features shall be required as set forth in Chapter 18C.300 LMC, Streets and Blocks, and Chapter 18C.400 LMC, Site Design, Buildings, and Frontage, and the community design standards in LMC [18A.70.040](#).
- H. *Tree Preservation*. Significant tree identification and preservation and/or replacement shall be required as set forth in Chapter [18A.70](#) LMC, Article III, Tree Preservation.
- I. *Landscaping*. Landscaping shall be provided as set forth in Chapter 18C.500 LMC and Chapter [18A.70](#) LMC, Article II, Landscaping.
- J. *Parking*. Parking shall conform to the requirements of Chapter 18C.600 LMC and Chapter [18A.80](#) LMC, Parking.
- K. *Signs*. Signage shall conform to the requirements of Chapter [18A.100](#) LMC, Sign Regulations.

Chapter 18C.300

STREETS AND BLOCKS

Sections:

18C.300.310 Streets and Blocks.

18C.300 Streets and Blocks.

A. *Street Grid and Blocks.*

1. *Street Grid.* A circulation plan must be submitted for review by the City as part of any development permit in the Station District unless waived by the City upon finding the project will not impact circulation or the enhancement of the public street grid. New development and redevelopment must demonstrate the plan supports and accommodates the expansion of the public street grid to improve circulation for vehicles, pedestrians, and bicyclists.
2. *Block Size.* Block Size. The maximum block size is 400 feet and the maximum distance permitted between public streets. New public street alignments shall be consistent with the regulating plan map. The City may approve modifications to the street alignments and waive the 400-foot maximum block size to take advantage of existing travel corridors, the location of utilities, and required improvements.
3. *Private Streets.* Private streets shall only be permitted when the City has determined there is no public benefit for circulation in the Station District. All private streets must be constructed to public standards.
4. *Mid-Block Connections/Alleys.* A minimum 20-foot-wide mid-block connection (alley) shall be provided at the midpoint along each block face or every 200 feet. The mid-block connections shall be designed to accommodate service needs and for pedestrian use and be free from permanent obstructions.
5. *Street Sections.* The typical street sections provided below are the minimum requirements for the design of public streets. The City may approve modifications to the typical street section based on localized conditions and adjacent land uses. Modifications may include adding or removing on-street parking, wider sidewalks, loading zones, bicycle facilities, and transit accommodations.

6. *Block Development.* The minimum number of buildings per 400 feet of block frontage is four, or one building per 100 feet, to create variety in the streetscape experience and support human-scale design. A single building may meet this requirement through building design and architecture that visually appears as multiple buildings. The City may approve modifications to this requirement based on site-specific conditions, including parcel ownership and configuration.

B. Street Types.

1. *Mixed-Use Streets.* Mixed-use streets support a variety of activities and functions both in the public right-of-way and development along the street edges. Street level retail is permitted, but not required, and a wider range of building frontage types are permitted, including street-level residential and office uses. Mixed-use streets also require pedestrian-oriented design and requirements may vary based on the location within Station District.

2. *Arterial Mixed-Use Street.* Arterial mixed-use streets maintain the existing vehicle capacity and allow a range of land use and building frontage types and include surface parking lots.

3. *Residential Street.* Residential streets serve residential neighborhoods and include vehicle travel with sidewalks and bike lanes.

4. *Green Street.* Green streets include vehicle travel lanes, sidewalks, bike lanes, and open space for stormwater management, landscaping, and gathering space.

C. *Street Standards and Frontage Types.* The following street standards are the minimum required and modifications to the standards may be approved by the City upon finding that the modification is consistent with the vision and objectives in LMC 18C.100.110 , supports pedestrian-oriented design, and balances the need for traffic flow to minimize congestion.

18C.300-1. Street Standard and Frontage Types

Street type	Sidewalk width	Linear	Forecourt	Plaza	Landscape	Porch /Stoop /Terrace	Parking
Mixed-Use Street	10' minimum	P	P	P/R1	P	P	X
Arterial Mixed-Use Street	As determined by Public Works	P	P	P	P	P	P
Residential Street	8' minimum	X	P	X	P	P	X
Green Street	8' minimum	X	P	X	P	P	X
Plaza Street	12' minimum or shared street	P	P	P	P	X	X

P=permitted, X=prohibited, R=required

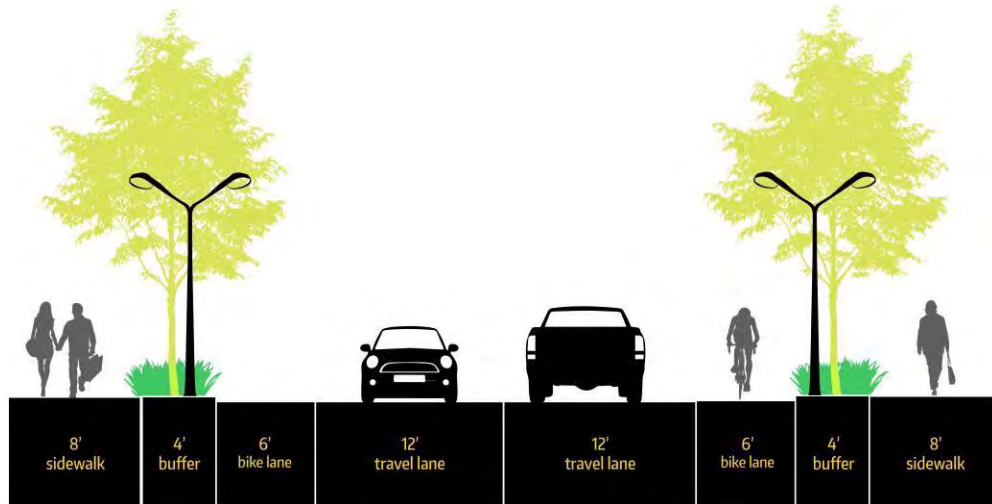
1 Required when on a corner lot.

D. *Street Sections.* The following street sections show the basic elements of the streetscape for each of the streets highlighted in Station District. The City may approve modifications to the typical cross sections based on site-specific conditions including adjacent land uses, traffic management, parking needs, and right-of-way constraints.

1. *Residential Street Typology.* Many of the streets identified as Residential Streets do not have curbs, gutter, or sidewalks and there is a lack of definition between the public right-of-way and private property. Parking occurs along the street edge often in gravel shoulders and without a consistent pattern. Established residential street ROW varies in the Station District. Figure 18C.300-2 below shows a standard residential street section with a 60' right-of-way that includes travel lanes, sidewalks, and bike lanes. In the future, bike lanes could be converted to on-street parking. Figure 18C.300-3 shows an 80' right-of-way with native plantings on either outer edge of the right-of-way.

18C.300-2. Residential Street Typology – 60' ROW

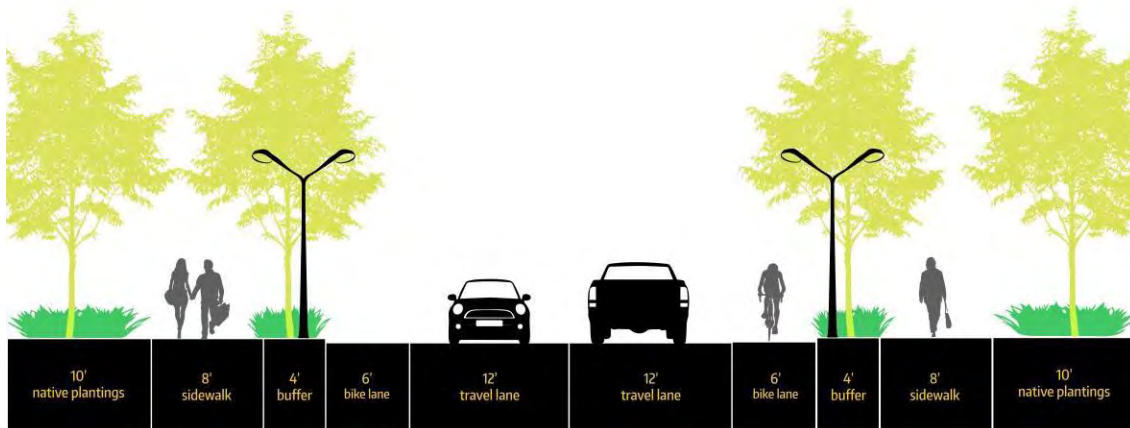
RESIDENTIAL STREET CONCEPT
60' Right-of-Way



Framework, 2020

18C.300-3. Residential Street Typology – 80' ROW

RESIDENTIAL STREET CONCEPT
80' Right-of-Way



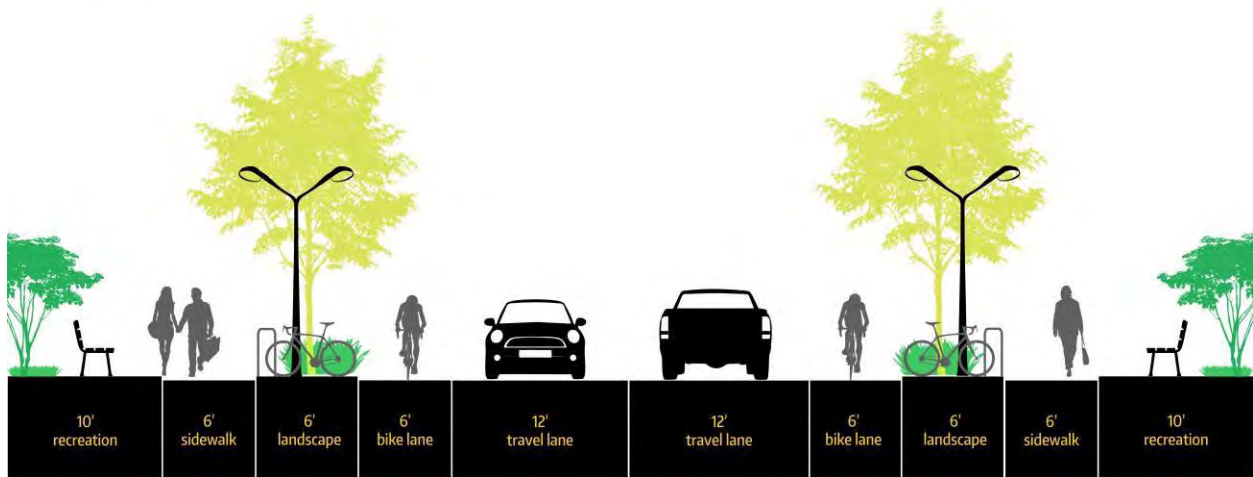
Framework, 2020

2. *Green Street Typology.* Kendrick Street SW connects to the Sounder Station to the South and to other neighborhoods to the north. The existing right-of-way width varies from between 60' and 80' and is an opportunity to integrate green street elements. The southern block of Kendrick

St SW appears to have been recently improved, so the following concepts focus on the blocks north of 111th St SW as shown in Figures 18C.300-4 through 6 below.

18C.300-4. Green Street Concept #1 – 80' Right-of-Way

GREEN STREET CONCEPT
80' Right-of-Way



Framework, 2020

18C.300-5. Boulevard Concept – 80' Right-of-Way

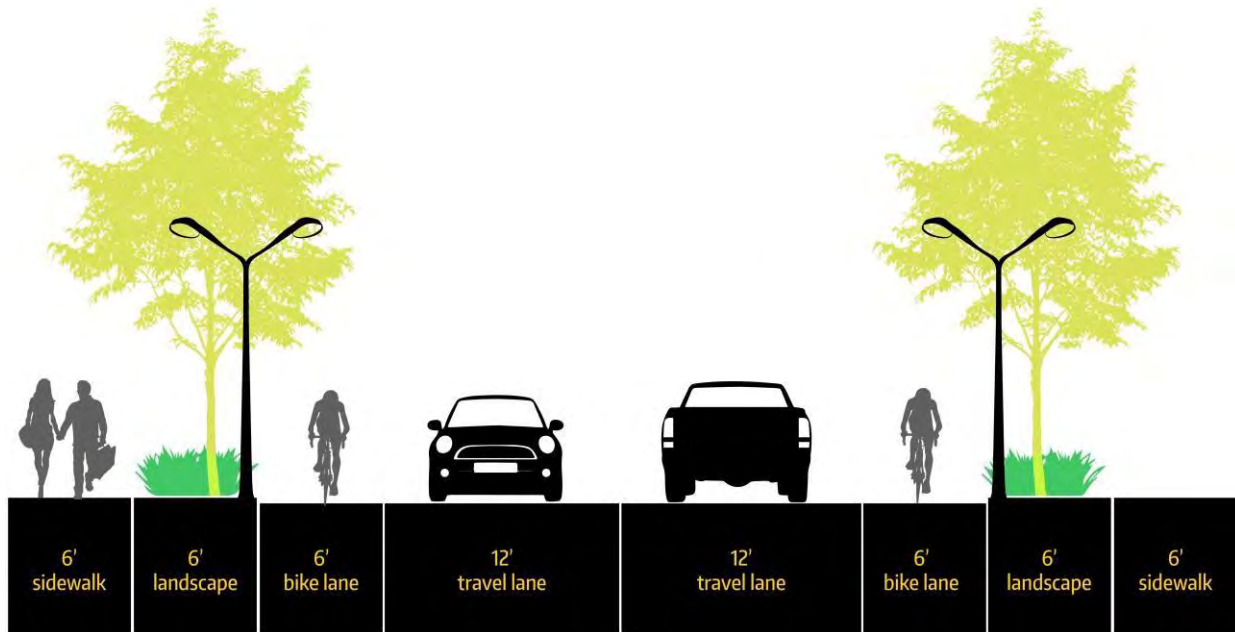
BOULEVARD CONCEPT
80' Right-of-Way



Framework, 2020

18C-300-6. Green Street Concept #2 – 60' Right-of-Way

GREEN STREET CONCEPT 60' Right-of-Way



Framework, 2020

E. *Plaza Street.* Plaza streets are located along commercial and mixed-use zoning with active uses and may vary in width. Plaza streets are designed primarily for people and activities while allowing for vehicle access to adjacent properties. Plaza streets may have dedicated sidewalks or use a shared street design with appropriate traffic calming design features. Plaza streets should include high-quality materials, integrated landscaping and trees with open space, street furniture such as places for people to sit, bike parking, lighting, public art opportunities, and a flexible design that can support a variety of events and activities such as times where the street may be temporary closed to vehicle traffic.

18C-300-7. Plaza Street Concept – 80' Right-of-Way

80' PLAZA STREET
CONCEPT



Framework, 2020

18C-300-8. Plaza Street Concept – 60' Right-of-Way

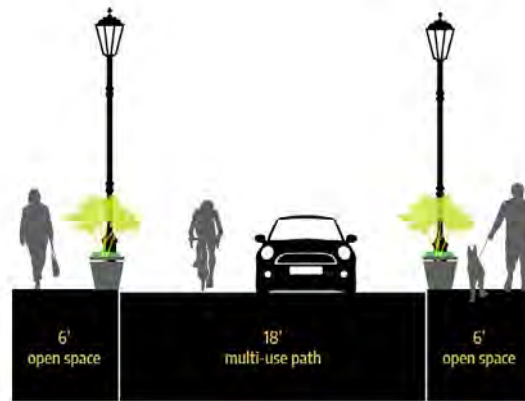
60' PLAZA STREET
CONCEPT



Framework, 2020

18C-300-8. Plaza Street Concept – 30' Right-of-Way

30' PLAZA STREET
CONCEPT



Framework, 2020

F. *Alleys.* Alleys are encouraged to provide secondary access to properties, de-emphasize parking lots, and to promote continuous building frontages. Alleys shall meet Public Works engineering standards.

G. *Weather Protection – Easements.* Weather protection or building overhangs that extend over public rights-of-way may be permitted by the Community Development Director subject to execution of an easement and requirements for maintenance by the property owner or developer.

1. Access to existing and future utilities within and under the street and boulevard must be maintained.
2. Freedom of movement of existing and future vehicular and pedestrian activity must not be restricted.
3. Weather protection depth and percentage coverage shall be consistent with LMC 18C.400.410(A) for frontage types.

4. All overhead weather protection shall be placed at a height that relates to architectural features of the building and adjacent storefront weather protection, while offering effective protection from weather. Weather protection shall have a minimum vertical clearance of eight feet, measured from the sidewalk, and should not be greater than 12 feet above the pedestrian sidewalk level.

5. The slope of the weather protection feature shall allow for proper drainage and self-cleaning action of rain and wind. Materials used should be durable and require minimum maintenance.

H. When Required. Street Frontages shall be implemented consistent with LMC 12.09.031.

Chapter 18C.400

SITE DESIGN, BUILDINGS, AND FRONTAGE

Sections:

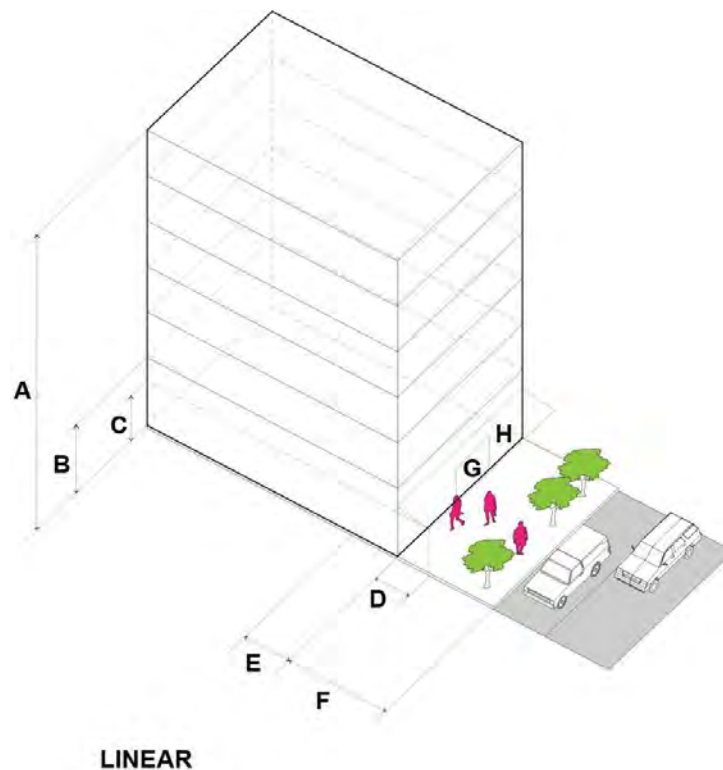
18C.400.410 Site Design, Buildings, and Frontage.

18C.400.410 Site Design, Buildings, and Frontage.

A. *Frontage Types.* The building frontage types below address the required standards for the relationship of buildings to the edge of the street and other site plan and design requirements. The permitted frontage types vary by street type as shown in LMC 18C.300.310(C). The building setback may be modified as approved by the City when necessary to expand the width of the right-of-way to accommodate the desired street design and cross section.

1. *Linear.* The linear building frontage has zero setback from the street edge and is the primary frontage type on retail streets and is also appropriate for land uses such as townhouses and row houses.

18C.400-1. Linear Frontage Type



Framework, 2020

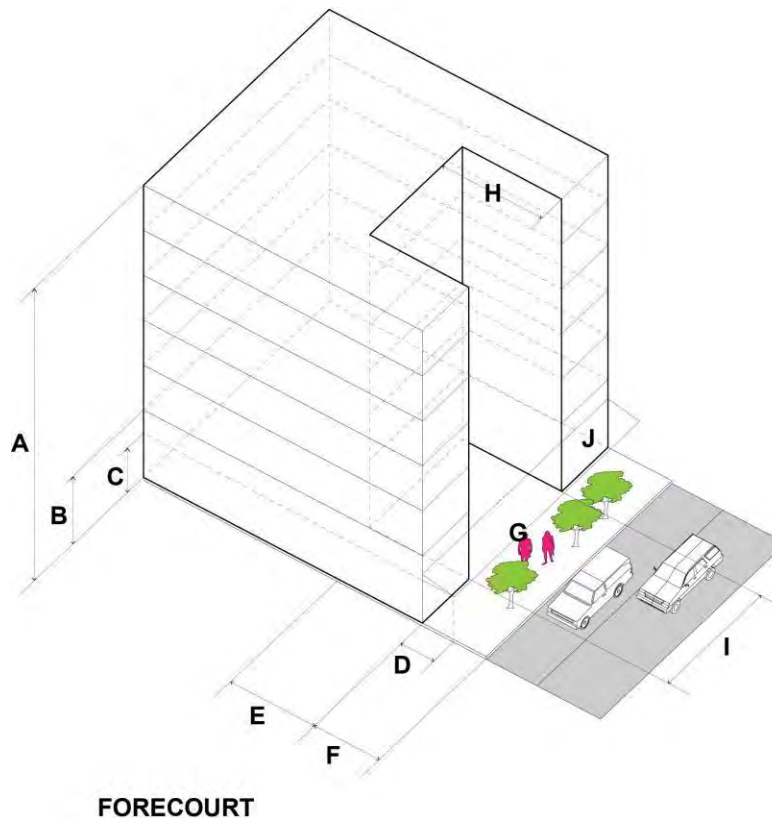
18C.400-2. Linear Frontage Standards

Standard		Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A	Building Height	Base Zone	Base Zone	n/a
B	First Floor Minimum Height	16'	16'	n/a
C	Weather Protection Height	10'	10'	n/a
D	Weather Protection Minimum Depth and Linear Frontage	10' Depth Weather protection required for minimum of 60% of frontage	10' Depth Weather protection required for minimum of 60% of frontage	n/a
E	Building Maximum Setback from Right-of-Way	10'	10'	n/a
F	Building Setback Minimum	0'	0'	n/a
G	Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	n/a
H	Window Transparency	A minimum 60% transparency required at street level between 30" and 8' for nonresidential uses	none	n/a

1 The maximum setback shall be up to 10 feet where the Public Works Engineering Director has determined property may be needed to accommodate the City's planned right-of-way at full build-out. Otherwise, the maximum setback is zero feet.

2. *Forecourt.* The forecourt building frontage type has an open area at the entrance along the street edge. This building type is applicable to a wide range of land use types and mixed-use development.

18C.400-3. Forecourt Frontage Type



Framework, 2020

18C.400-4. Forecourt Frontage Standards

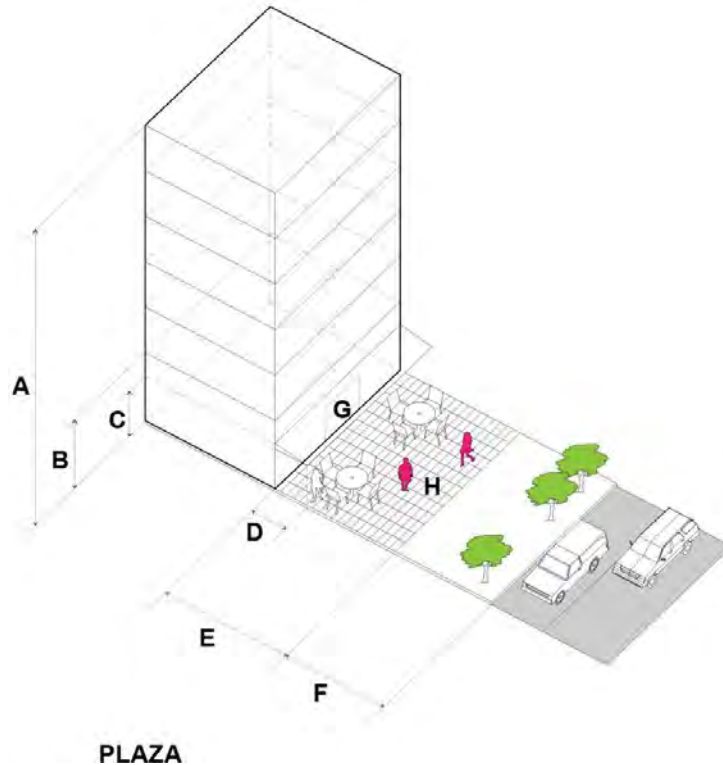
Standard	Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	Base Zone	Base Zone	Base Zone
B First Floor Minimum Height	16'	16'	n/a
C Weather Protection Height	10'	10'	n/a
D Weather Protection Minimum Depth and Forecourt Frontage	15' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	15' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	n/a
E Building Maximum Setback from Right-of-Way	10'	10'	20'
F Building Setback Minimum	0'	0'	10'
G Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required
H Forecourt Depth	10' minimum;	10' minimum;	10' minimum

Standard		Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
I	Forecourt Width	30' maximum	30' maximum	30' maximum
		10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum
J	Window Transparency	A minimum 60% transparency required at street level between 30" and 8' for nonresidential uses	none	none

1 The maximum setback shall be up to 10 feet where the Public Works Engineering Director has determined property may be needed to accommodate the City's planned right-of-way at full build-out. Otherwise, the maximum setback is zero feet.

3. *Plaza*. The plaza frontage type includes a pedestrian-oriented plaza between the building and the street edge. The plaza frontage type is applicable to retail and dining uses and can support activities such as outdoor dining, public art displays, seating, entertainment, and events. The plaza must be designed to support human activity and support a safe and inviting streetscape environment.

18C.400-5. Plaza Frontage Type



Framework, 2020

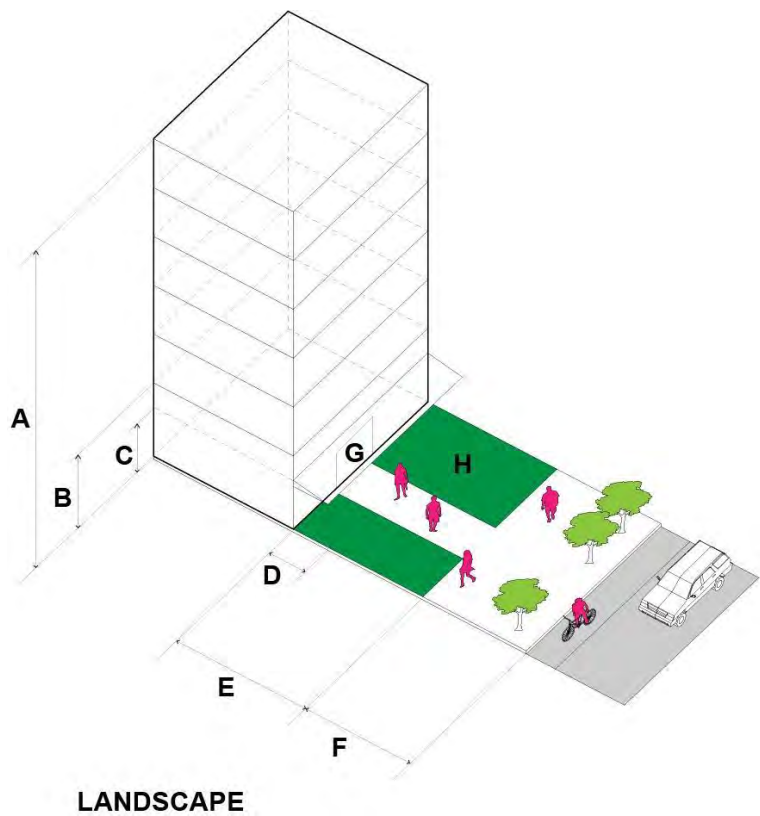
18C.400-6. Plaza Frontage Standards

Standard		Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A	Building Height	90'	90'	n/a
B	First Floor Minimum Height for depth of 30 feet from street: Commercial Uses	16'	16'	n/a
C	Weather Protection Height	10'	10'	n/a
D	Weather Protection Minimum Depth and Linear Frontage	5' min depth; 60% of frontage along sidewalk must have weather protection	5' min depth; 60% of frontage along sidewalk must have weather protection	n/a
E	Building Maximum Setback from Right-of-Way	5'-25'	5'-25'	n/a
F	Building Setback Minimum	5'	5'	n/a
G	Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	n/a
H	Plaza Design	Plaza area must be designed for human activity and include seating and other amenities	Plaza area must be designed for human activity and include seating and other amenities	n/a
I	Window Transparency	A minimum 60% transparency required at street level between 30" and 8' for nonresidential uses	none	n/a

1 The maximum setback shall be up to 25 feet where the Public Works Engineering Director has determined property may be needed to accommodate the City's planned right-of-way at full build-out. Otherwise, the maximum setback is five feet.

4. *Landscape.* The landscape frontage has landscaping between the building and street edge. The landscape frontage is not permitted on retail streets and is appropriate for office and residential uses particularly when on the ground floor.

18C.400-7. Landscape Frontage Type



Framework, 2020

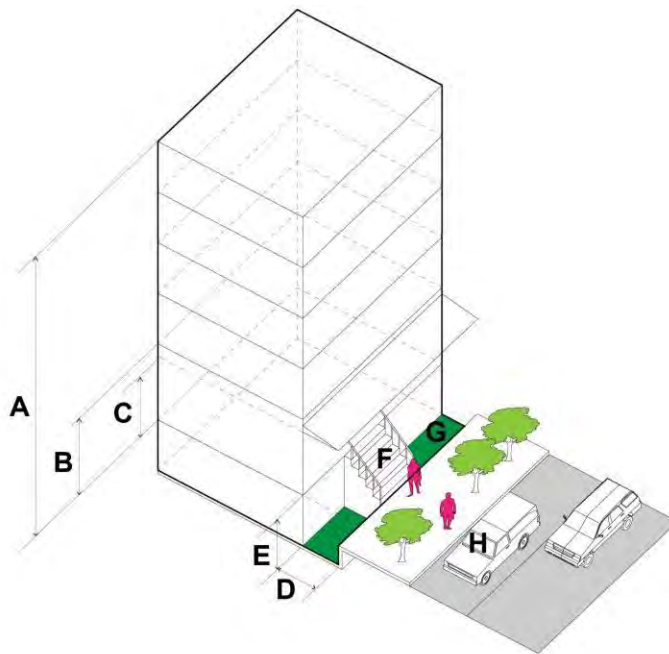
18C.400-8. Landscape Frontage Standards

Standard		Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A	Building Height	Base Zone	Base Zone	Base Zone
B	First Floor Minimum Height for depth of 30 feet from street: Commercial Uses	16'	16'	n/a
C	Weather Protection Height	10'	10'	10'
D	Weather Protection Minimum Depth and Linear Frontage	5' minimum depth; required over primary entrance	5' minimum depth; required over primary entrance	5' minimum depth; required over primary entrance
E	Building Maximum Setback from Right-of-Way	20'	20'	20'
F	Building Setback Minimum	10'	10'	10'

	Standard	Mixed-Use Street	Mixed-Use Arterial	Residential Street/Green Street
G	Front Entrance	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage
H	Landscape Requirements	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.

5. *Porch/Stoop/Terrace*. This frontage type has the building set back from the street edge to accommodate a porch, stoop, or terrace to serve as the primary access to the building. This frontage type is applicable primarily to residential and nonretail commercial uses and is not permitted on retail streets.

18C.400-9. Porch/Stoop/Terrace Frontage Type



PORCH/STOOP/TERRACE

Framework, 2020

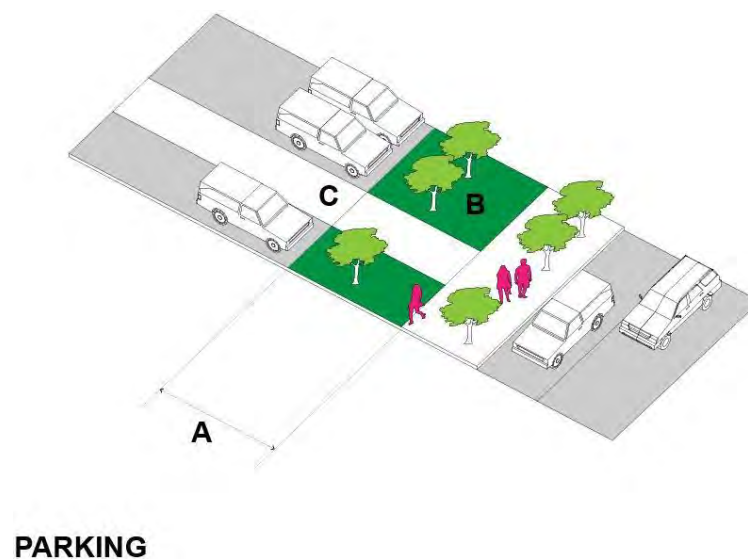
18C.400-10. Porch/Stoop/Terrace Frontage Standards

Standard	Mixed-Use Street/Plaza Street ¹	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	Base Zone	Base Zone	Base Zone
B First Floor Minimum Height for depth of 30 feet from street: Commercial Uses	16'	16'	n/a
C Weather Protection Height	10'	10'	10'
D Threshold Depth	4' minimum	4' minimum	4' minimum
E Threshold Height	Below-grade maximum 4'; Above-grade maximum 5'	Below-grade maximum 4'; Above-grade maximum 5'	Below-grade maximum 4'; Above-grade maximum 5'
F Front Entrance	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage
G Landscape Requirement	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.

¹ Prohibited on plaza street

6. *Parking.* The parking frontage type is only permitted on arterial mixed-use streets and is the only frontage type where parking is allowed between the building and the street edge. Landscape and trees are required in the landscaped area between the street edge and the parking area.

18C.400-11. Parking Frontage Type



Framework, 2020

18C.400-12. Parking Frontage Standards

Standard	Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A Landscape Buffer Width	n/a	Minimum 10'; Maximum 20'	n/a
B Landscape Requirements	n/a	The landscape area shall comply with the City's landscaping requirements.	n/a
C Pedestrian Walkway	n/a	Minimum width – <u>see LMC 18C.500.520</u>	n/a

B. *Civic Uses.* Sites and buildings where the primary use is civic should be designed to reflect a public use and therefore may deviate from the frontage standards. Civic buildings and sites should be recognizable and accessible as public buildings in Station District and to reflect civic pride in the community. The following standards address the design of civic uses:

1. Civic uses are permitted on the ground level of all street types except the low-impact mixed-use streets and may be designed in accordance with any of the frontage types except parking.
2. Civic uses are not subject to the maximum setback requirements.
3. Civic uses may provide the main entry on the side of the building when necessary to the function of the site, including safety and security. A pedestrian connection shall be provided from the sidewalk to the entry.
4. The design sites and buildings for civic uses should incorporate public open space when feasible and practical and provide for a variety of activities such as public art, seating, play equipment, games, and events.

C. *Townhouse/Rowhouse Development.* The following standards apply to townhouse and rowhouse development:

1. Lot widths for townhouses and rowhouses may be less than the 50' minimum required for new parcels.
2. Site access to garages and parking area shall be consolidated with the use of alleys or a common driveway to shared parking areas.
3. Individual driveways are prohibited from any street with a street type designation and all on-site parking shall be located in the rear or in garages.

Chapter 18C.500

LANDSCAPE, OPEN SPACE, AND GREEN INFRASTRUCTURE

Sections:

- 18C.500.510** **Basic Standards.**
- 18C.500.520** **Special Landscape Treatments.**
- 18C.500.530** **Common and Open Space Standards.**
- 18C.500.540** **Green Infrastructure.**

18C.500.510 Basic Standards.

Landscaping shall be provided consistent with Chapter 18C.70 LMC, Article II, Landscaping, except as supplemented or superseded by this title.

18C.500.520 Special Landscape Treatments.

The following special landscape treatments are required or permitted along street types within the Station District:

18C.520-1. Special Landscape Treatments

Street Type	Foundations	Plazas/Courtyards	Pedestrian Walkways
Mixed-Used Street	R	P	R
Arterial Mixed-Used Street	P	P	R
Residential Street/Green Street	R	P	P

P = permitted, R= required

1 Plaza landscaping is required when the plaza frontage is selected on a street.

A. *Foundations.* Buildings shall be accented by foundation plantings. Foundation plantings may frame doors, anchor corners, or screen undesirable areas. Foundation plantings shall allow for five feet of unobstructed movement on the sidewalk. Examples of foundation plantings include raised planter boxes, containers, and similar.

B. *Plazas and Courtyards.* Where provided, consistent with frontage standards, plazas or courtyards shall meet the following standards:

1. Plazas and courtyards shall be visible and accessible to the public.
2. Minimum dimensions of plazas and courtyards are 20 feet. Courtyards may qualify as common open space per LMC 18C.500.530, Common and Open Space Standards.

3. Plazas or courtyards shall include one or more element from each category below.

- a. *Special Paving.* Scored concrete, stained/colored concrete, concrete pavers, paving inlays, mosaics, or other special paving material.
- b. *Landscaping.* Trees, shrubs, trellises, flowers, or container plants. Landscaping shall be consistent with this chapter and Chapter 18A.70 LMC, Article II, Landscaping.
- c. *Seating Area.* Benches or low seating walls. Walls or benches shall be a minimum of four feet long. Seating shall be provided at a ratio of at least one seat per 60 square feet of plaza area or open space.

C. *Pedestrian Walkways.* Pedestrian walkways are required as follows:

- 1. *Street to Building.* Required between a public right-of-way and building entrances where the building is set back from the street, sidewalk, or parking area.
- 2. *Parking.* Required between parking lots and building entrances.
- 3. *Mid-Block Connections.* One pedestrian walkway shall be provided between the street and the rear property line, spaced an average of every 200 feet or less of street frontage.

D. *Dimensional Standards.*

1. *Pedestrian Walkways.*

- a. *Width.* Six feet minimum, exclusive of landscaping or site furnishings, except that mid-block crossings shall have minimum walkway widths of 12 feet.
- b. *Lines of Sight.* Walkways shall be located and designed with clear sight lines for safety.
- c. *Definition.* Pedestrian walkways shall be defined through paving, landscaping, furnishings, and lighting.

2. *Design.* Pedestrian walkways shall be defined using a combination of one or more of the following techniques:

- a. *Special Paving.* Scored concrete, stained/colored concrete, concrete pavers, paving inlays, mosaics, or other special paving material. Not all portions of the pedestrian walkway are

required to be paved; however, a minimum of four feet in width of the pedestrian walkway shall provide an all-weather walking surface.

b. *Architectural Features.* Trellises, railing, low seat walls, weather protection, bollards, or other architectural features. Chain link fences are not allowed.

c. *Landscaped Edges.* A continuous, landscaped area consistent with Type II, Streetscapes, where the pedestrian walkway is from the street to building or mid-block connection, otherwise Type III, Open Space, is required, per Chapter 18A.70 LMC, Article II, Landscaping. If the walkway abuts a driveway or driving aisle on both sides, the landscaped edge shall apply to both sides.

d. *Lighting.* Pedestrian walkways shall include lighting such as pedestrian lights, bollards, and accent lighting to assist pedestrian navigation and promote a safe and comfortable walking space.

18C.500.530 Common and Open Space Standards.

The purpose of this section is to provide residents with access to usable, privately maintained and owned open space to create opportunities for active living and respite for on-site residents.

A. Common Open Space – Private.

1. Each mixed-use or residential development shall provide a common open space per dwelling unit of 100 square feet per dwelling unit.

2. For the purposes of this section, common open space means an open-air area intended for use by all residents, guests, employees or patrons of a site and may include lawns, gardens, squares, plazas, courtyards, terraces, barbecue and picnic areas, games court or multi-use recreational areas, and other types of built space.

3. Design Standards.

a. Required setback areas shall not count toward the open space requirement unless they are part of an open space that meets the other requirements of common open space.

b. Space shall have a minimum dimension of 20 feet in any direction to provide functional leisure or recreational activity. This dimension can be adjusted by the Community

Development Director based on site conditions such as topography or irregular lot geometry.

c. Space shall feature paths or walkable areas, landscaping, seating, lighting, play structures, sports courts, or other pedestrian amenities to make the area more functional and enjoyable for a range of users.

4. Common space shall be separated from ground level windows, streets, service areas and parking lots with landscaping, low-level decorative fencing (no chain link), or other treatments as approved by the Community Development Director that enhance safety and privacy for both the common open space and dwelling units.

5. The space shall be oriented to receive sunlight and = face south if possible. Open space may also face east or west, but not north, unless the Community Development Director determines that site conditions such as topography or irregular lot geometry warrant waiving this requirement. The common open space shall be designed to provide landscaping that defines the open space but permits surveillance from units and roads.

6. The space must be accessible from the dwelling units. The space must be oriented to encourage activity from local residents.

7. No more than 30 percent of the area may be covered by a structure.

8. The common open space may include multi-use storm water detention facilities if the Community Development Director determines that the facilities are designed to function as common open space by providing an enhanced natural or visually aesthetic design.

9. The common space shall be designed to ensure that the open space network addresses Crime Prevention Through Environmental Design (CPTED) principles such as security and surveillance from residential units. Common recreational spaces shall be located and arranged to allow windows to overlook them.

10. The common space shall provide adequate lighting in the open space network, but place and shield lighting so that it does not glare into housing units.

B. Private Open Space.

1. Each dwelling unit shall have a private open space, at a minimum of 48 square feet with a minimum width or depth of six feet.
2. For the purposes of this section, private open space includes individual decks, balconies, or patios.

C. *Fee in Lieu for Portion of Open Space.*

1. Within the station district planned action area, a development may pay a fee in lieu of providing common open space or a portion of the private open space to support park land purchase and improvements within the station district planned action area for urban nodal or urban linear parks identified in the station district subarea plan or the legacy plan for parks, recreation & open space master plan.
2. The fee in lieu is allowed for each 100 square feet of common open space not provided. No less than 50 square feet of common open space per unit shall be provided on site.
3. The fee in lieu is allowed for balconies or patios not provided on street-front facade. No less than 50 percent of the units shall include private open spaces.
4. The fee shall be equal to the average fair market value of the land otherwise required to be provided in on-site common or private open space.
5. Fee Payment and Use.
 - a. The payment shall be held in a reserve account by the City and may only be expended to fund a capital improvement for parks and recreation facility identified in the station district subarea plan or the legacy plan for parks, recreation & open space master plan;
 - b. The payment shall be expended in all cases within five years of collection; and
 - c. Any payment not so expended shall be refunded with interest to be calculated from the original date the deposit was received by the City and at the same rate applied to tax refunds pursuant to RCW 84.69.100; however, if the payment is not expended within five years due to delay attributable to the developer, the payment shall be refunded without interest.

18C.500.540 Green Infrastructure.

- A. Development shall implement storm water standards that incorporate low impact development (LID) principles and standards consistent with City standards in Chapter 12.11 LMC.
- B. Where on-site filtration is feasible, it shall be provided.

- C. Permeable surfaces shall be incorporated into plazas, courtyards, and pedestrian walkways, unless demonstrated infeasible to the satisfaction of the City Engineer, and shall meet City standards per Chapter 12.11 LMC.
- D. Open ponds shall be designed with both natural landscape and urban access and edge treatments consistent with Chapter 18A.70 LMC, Article II.
- E. Native and/or drought tolerant landscaping shall be incorporated into required landscape plans.

Chapter 18C.600

PARKING

Sections:

18C.600.610 **Parking.**

18C.600.610 Parking.

A. *Off-Street Parking Requirements.* The following off-street parking requirements supersede the requirements in Chapter 18A.80 LMC. Uses not listed below must comply with the requirements in Chapter 18A.80 LMC.

B.

18C-600-1. Off-Street Parking Requirements

Land Use	Vehicular Parking Requirement	Bicycle Parking Requirement
Residential	Single-family: 2 per dwelling unit Accessory dwelling: 1 per dwelling unit, provided that no additional parking is required when located within one-quarter mile of the Sounder Station. (RCW 36.70A.698) Senior citizen apartments: No minimum Multifamily housing: No minimum	Meet rates and standards of: Chapter 18A.80
Retail. Services, Restaurants	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Office	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Street level retail 3,000sq.ft. or less per business	None where there is available public parking within 500' or abutting on-street parking designed to serve street level retail	Meet rates and standards of: Chapter 18A.80

B. *Parking Reductions or Increases.* The amount of required parking may be reduced or eliminated, or increased above the maximum, based on a site-specific parking study that demonstrates one or more of the following:

1. *Reduction Due to Shared Parking at Mixed-Use Sites and Buildings.* A shared use parking analysis for mixed-use buildings and sites that demonstrates that the anticipated peak parking demand will be less than the sum of the off-street parking requirements for specific land uses.

2. *Reduction Due to Public Parking Availability.* The availability of public parking to accommodate the parking demand generated by the site or building. The City may approve a reduction in the amount of required parking by up to 50 percent for any parking stalls that will be open and available to the public. On-street parking may be considered for the reduction; any new on-street parking provided will be counted toward the required parking availability.

3. *Reduction Due to Lower Parking Demand or Increase Based on Greater Parking Demand.* Demonstrating that anticipated parking demand will be less than the minimum parking required, or greater than the maximum allowed, shall be based on collecting local parking data for similar land uses on a typical day for a minimum of eight hours.

4. *Reduction for Housing in Proximity to Sounder Station (RCW 36.70A.620).* When located within one-quarter mile of the Sounder Station, an applicant may apply for an exception allowing minimum parking requirements to be reduced at least to one parking space per bedroom or 0.75 space per unit, as justified through a parking study prepared to the satisfaction of the Community Development Director or their designee:

- a. housing units that are affordable to very low-income or extremely low-income individuals.
- b. housing units that are specifically for people with disabilities.

In determining whether to grant a parking reduction, the Community Development Director may also consider if the project is proposed in an area with a lack of access to street parking capacity, physical space impediments, or other reasons supported by evidence that would make on-street parking infeasible for the unit.

C. *Parking Location and Design.* Parking shall be located behind the [building](#) or in a [structure](#) except in locations where the parking frontage type is [permitted](#).

D. *Shared Parking.* Shared parking is encouraged to support a walkable and pedestrian-oriented Station District where people can park once and visit multiple destinations. Off-site shared parking may be authorized per the standards in Chapter [18A.80](#) LMC.

E. *Public Parking.* Public parking is permitted as a principal or accessory use in the Station District subject to the frontage and design standards.

F. *Dimensional Standards.* Parking stall and circulation design shall meet the standards of Chapter [18A.80](#) LMC.

Chapter 18C.700 ADMINISTRATION

18C.700.710. Form-Based Code Review

- A. All development in the Station Area District requiring a land use or building permit shall be subject to form-based code review consistent with the procedures in LMC 18.B.700.710 provided that all references to Downtown District shall be interpreted as the Station Area District.

18C.700.720. Optional Master Planned Development

- A. To establish specified uses in LMC 18.C.200.220 Development within the Station Area District may apply for a master plan for the development of (2) acres or greater.
- B. The master plan proposal shall be considered a Process II application for the purposes of Title 18A.20 and consolidated with any other relevant permit applications.
- C. Applications for a master planned development shall be filed with the Community and Economic Development Department on forms provided by the Department along with established fees. An applicant for a master planned development shall submit the following items to the City, unless the Director finds in writing that one or more submittals are not required due to unique circumstances related to a specific development proposal:
 - 1. A detailed narrative identifying how the master plan meets each criteria in subsection D.
 - 2. A site plan with the heading "Master Planned Development Site Plan" that includes information including street frontage types, building footprints, proposed landscaping, open space and parks and/or recreational areas including trails, public spaces, pedestrian walkways, parking locations, and proposed setbacks;
 - 3. Conceptual elevation drawings illustrating facade and building design elements, including height, overall bulk/mass and density and proposed residential design features that will provide for a superior development;
 - 4. A conceptual landscape plan/map showing the proposed location and types of vegetation and landscaping. The landscape plan may also be incorporated into the master planned development site plan and narrative;
 - 5. A phasing plan, if the development will occur in distinct phases, with a written schedule detailing the timing of improvements;
 - 6. A draft development agreement, if proposed by the applicant, or as required by the City; and

7. All of the submittal requirements in Chapter 18A.20 LMC, Article I.
- D. A master planned development shall only be granted after the Director has reviewed the proposed use and has made written findings that all of the standards and criteria set forth below have been met or can be met subject to conditions of approval:
 1. The master planned development is consistent with the comprehensive plan; and
 2. The master planned development is consistent with the vision and objectives of the station area plan in LMC 18C.100.110; and
 3. The master planned development proposes a mixed income development that includes:
 - a. Housing only includes units for permanent residential occupancy, as defined in LMC 3.64.010.
 - b. At least 10 percent of the units are provided as one or more of the following unit types:
 - i. Townhouses or Rowhouses ; and/or
 - ii. Live/work units; and/or
 - iii. Family-sized housing, where units contain two or more bedrooms and the design includes access to outdoor recreation space suitable for children where adults can appropriately supervise and easily view children.
 - c. The master plan includes one or more of the following:
 - i. Development demonstrates variety in unit types, tenure, or affordability that fits a need demonstrated in the Comprehensive Plan Housing Element or Consolidated Plan; and/or
 - ii. On-site units dedicated to serving persons displaced due to redevelopment within one-half mile of the Sounder Station as documented in a relocation assistance plan and affidavits; and/or
 - iii. A park open to the public due to an easement, dedication, or contract that meets system needs identified in the legacy plan for parks, recreation & open space master plan and meets relevant city design standards.
 4. The design is compatible with and responds to the existing or intended character, appearance, quality of development and physical characteristics of the subject property and immediate vicinity; and

5. Roads and streets, whether public or private, within and contiguous to the site comply with guidelines for construction of streets and the street frontage standards; and
6. Each phase of the proposed development, as it is planned to be completed, contains the parking spaces, open space, recreation space, landscaping and utility area necessary for creating and sustaining a desirable and stable environment; and
7. The master planned development is consistent with the planned action ordinance (Ordinance XXX).

18C.700.730. Update of Nonconforming Lots

All nonconforming lots in the Station Area District are subject to full compliance and proportional compliance regulations pursuant to LMC 18B.700.730 Update of Nonconforming Lots provided that all references to Downtown District shall be interpreted as the Station Area District.

EXHIBIT C

Amendments to LMC Title 18A

18A.10.140 Establishment of subareas.

Per RCW 36.70A.080(2), in order to plan for and regulate the use of land and structures in a manner which recognizes that residential neighborhoods and business areas within Lakewood vary one from another in desired character, subareas may be established as optional elements of the comprehensive plan and implementing zoning regulations may be adopted as a title of the Lakewood Municipal Code (LMC).

Subarea plans are implemented in part through the adoption of use, development, performance, or procedural regulations specific to the subarea or to a portion or portions of the subarea. Regulations which are specific to a subarea or portions of a subarea are located in the title of the LMC concerning the subarea.

The following subareas and subarea plans are established:

Name	Symbol	Code Title
Downtown Subarea Plan	DSAP	18B
This section is reserved Lakewood Station District Subarea Plan	Reserved LSDSP	Reserved 18C

18A.10.180 Definitions

“Rowhouse” means any of two or more identical or nearly identical attached dwelling units that share a common wall on one or both sides of the unit.

“Townhouse” means a type of attached multifamily dwelling in a row of at least two (2) such units in which each unit has its own front and rear access to the outside, no unit is located over another unit, and each unit is separated from any other unit by one (1) or more vertical common fire-resistant walls. Townhouses may be situated or grouped in different layouts within a development.

18A.20.208 Applicability – Nonconformities.

A. This article shall apply to legally existing nonconformities, except the following items, which shall be governed by the standards set forth in the chapters identified below:

1. Nonconforming uses and structures within the Downtown Subarea Plan, as governed in LMC Title 18B.
2. Nonconforming uses and structures within the Lakewood Station District Subarea Plan, governed in LMC Title 18C.
23. Nonconforming signs as defined in Chapter 18A.100 LMC, Signs.
34. Nonconforming wireless service facilities as defined in Chapter 18A.95 LMC, Wireless Service Facilities.
45. Nonconforming mobile home parks as defined in LMC 18A.40.110(C), Manufactured Home Parks. Manufactured and mobile home parks which were legally approved prior to the effective date of this title may continue to exist; provided, that the density of the park does not increase over the number of dwelling units legally existing on the effective date of this

title. Manufactured home sites within legally nonconforming manufactured home parks may continue to be used; provided, that the placement of newer manufactured homes does not result in encroachment of the dwelling beyond the lot space boundaries or into the right-of-way and fire code requirements for structure spacing are met.

~~56~~. Nonconforming sexually oriented businesses as defined in the Chapter 18A.50 LMC, Article III, Sexually Oriented Businesses Overlay.

~~67~~. Permit applications at the time of this title's passage that constitute vested development.

i. Future plans to further develop property shall not constitute a basis for nonconformity status, whether or not documented in the public record, except when they constitute a vesting.

ii. Nothing in this article shall be construed to require a change in plans, construction, or intended use related to vested development, though it may thereafter be regulated as a nonconformity. [Ord. 726 § 2 (Exh. B), 2019.]

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: May 3, 2021	TITLE: Ordinance adopting a SEPA Planned Action related to the Lakewood Station District Subarea (LSDS) Plan.	TYPE OF ACTION: <u> X </u> ORDINANCE NO. 752 ____ RESOLUTION NO. ____ MOTION NO. ____ OTHER
REVIEW: March 8, 2021 Study Session; April 5 Public Hearing; April 19 Meeting; April 26 Study Session	ATTACHMENTS: Attachment A (Ordinance 752 (LSDS Planned Action Ordinance and Exhibits)); Attachment B (SEPA Environmental Checklist)	

SUBMITTED BY: David Bugher, Assistant City Manager for Development Services
Tiffany Speir, Long Range & Strategic Planning Manager

RECOMMENDATION: It is recommended that the City Council approve the attached Ordinance 752 adopting a SEPA Planned Action related to the Lakewood Station District Subarea (LSDS) Plan.

DISCUSSION: Lakewood has recognized the need for, and memorialized initial design for, a subarea plan in the Lakeview Neighborhood/Lakewood Station District since adopting its first Comprehensive Plan in 2000. In November 2019, the State Department of Commerce awarded Lakewood a grant under E2SHB 1923 to prepare a Lakewood Station District Subarea (LSDS) Plan, an implementing Hybrid Form-Based Code and a SEPA-based Planned Action. In-person and virtual stakeholder outreach was conducted between December 2019 and Spring 2021 to help develop the proposed package.


Discussion is continued on the following page.

ALTERNATIVE(S): The Council could amend the proposed Ordinance; the Council could also not adopt the Ordinance.

FISCAL IMPACT: N/A

Prepared by
Tiffany Speir, Long Range & Strategic Planning Manager

Department Director
Dave Bugher, Assistant City Manager for Development Services



City Manager Review

BACKGROUND

On March 8, 2021, the City Council reviewed the draft Lakewood Station District Subarea (LSDS) Plan and the implementing Hybrid Form-Based Development Code as recommended by the Planning Commission; the Council also reviewed the Planned Action Ordinance. The City Council held a public hearing on April 5 on the LSDS Package (Plan, Code, and the Planned Action Ordinance (the Planned Action Ordinance itself was inadvertently left out of the April 5 meeting materials; the Planned Action SEPA Checklist was included in the meeting packet.)) The Council discussed the package on April 19 and decided to continue the package for an additional study session held April 26. Public comment was possible at the April 12, 19, and 26 Council meetings. Action on the Package is scheduled for May 3.

RECOMMENDATION

It is recommended that the City Council adopt Ordinance 752.

ATTACHMENT A

ORDINANCE NO. 752

AN ORDINANCE of the City Council of the City of Lakewood, Washington, adopting a SEPA Planned Action related to the Lakewood Station District Subarea.

I. RECITALS

WHEREAS, the State Environmental Policy Act (SEPA) and its implementing regulations provide for the integration of environmental review with land use planning and project review through the designation of planned actions by jurisdictions planning under the Growth Management Act (GMA), such as the City of Lakewood (“City”); and

WHEREAS, Section 43.21C.440 of the Revised Code of Washington (RCW), Sections 197-11-164 through 172 of the Washington Administrative Code (WAC) allow for and govern the adoption and application of a planned action designation under SEPA, and Section 14.02.030 of the Lakewood Municipal Code (LMC) adopts Chapter 197-11 WAC by reference as amended; and

WHEREAS, the designation of a planned action expedites the permitting process for projects in proximity to major transit facilities when the impacts have been previously addressed in a Threshold Determination; and

WHEREAS, a subarea of the City commonly referred to as the “Station District”, as depicted on the map attached hereto as **Exhibit A** and incorporated herein by this reference, has been identified as a planned action area for future redevelopment to a mixed-use center (“Planned Action Area”) within a half-mile of a major transit stop under RCW 43.21C.440(1)(b)(ii) consisting of the existing Sounder Station and associated properties and the planned bus rapid transit route (BRT) 2 in the Pierce Transit Destination 2040 anticipated to be operational in 2026; and

WHEREAS, the City has developed and adopted a subarea plan complying with the GMA (RCW 36.70A), dated May 3, 2021, to guide the redevelopment of the Planned Action Area (“Station District Plan”); and

WHEREAS, after extensive public participation and coordination with all affected parties, the City, as lead SEPA agency, issued the Station District Planned Action Threshold Determination of Non-Significance (“Threshold Determination”) dated November 12, 2020 and revised on May 3, 2021 as appropriate, which identifies the impacts and mitigation measures associated with planned development in the Planned Action Area as identified in the Station District Plan; and

WHEREAS, the City desires to designate a Planned Action under SEPA for the Station District (“Planned Action”); and

WHEREAS, adopting a Planned Action for the Station District with appropriate standards and procedures will help achieve efficient permit processing and promote environmental quality protection; and

WHEREAS, the City is amending the Lakewood Comprehensive Plan to incorporate maps, text, and policies specific to the Station District; and

WHEREAS, the City is adopting zoning and development regulations concurrent with the Station District Plan to implement said Plan, including this Ordinance; and

WHEREAS, the City Council finds that adopting this Ordinance and its Exhibits is in the public interest and will advance the public health, safety, and welfare.

II. FINDINGS

The procedural and substantive requirements of the State Environmental Policy Act (RCW 43.21C) have been complied with.

The procedural requirements of the Growth Management Act (RCW 36.70A) have been complied with.

The proposed action is consistent with the requirements of Revised Code of Washington, and the Washington Administrative Code.

The proposed action is consistent with the City of Lakewood Comprehensive Plan.

The proposed amendments have been reviewed and processed in accordance with the requirements of Title 14 Environmental Protection, Title 14A Critical Areas, and Title 18A Land Use and Development of the City of Lakewood Municipal Code.

All of the facts set forth in the Recitals are true and correct, and are incorporated herein by reference.

All necessary public meetings and opportunities for public testimony and comment have been conducted in compliance with State law and the City's municipal code.

The Lakewood City Council finds and determines that the regulation of development and land use within the Station District is within the City's regulatory authority.

The Lakewood City Council finds and determines that approval of such amendments to the Comprehensive Plan and Land Use and Development Code is in the best interests of the residents of Lakewood, and will promote the general health, safety, and welfare.

The Lakewood City Council finds and determines that regulation of land use and development is subject to the authority and general police power of the City, and the City reserves its powers and authority to appropriately amend, modify, and revise such land use controls in accordance with applicable law.

The Planned Action Threshold Determination adequately identifies and addresses the probable significant environmental impacts associated with the type and amount of development planned to occur in the designated Planned Action Area.

The mitigation measures identified in the Planned Action Threshold Determination, attached to this Ordinance as **Exhibit B** and **Exhibit C**, and incorporated herein by reference, together with adopted City development regulations are adequate to mitigate significant adverse impacts from development within the Planned Action Area.

The Lakewood Station District Subarea Plan and Planned Action Threshold Determination identify the location, type, and amount of development that is contemplated by the Planned Action.

Future projects that are implemented consistent with the Planned Action will protect the environment, benefit the public, and enhance economic development.

The City provided several opportunities for meaningful public involvement and review in the Station District Plan and Planned Action Threshold Determination processes, including a community meeting consistent with RCW 43.21C.440; has considered all comments received; and, as appropriate, has modified the proposal or mitigation measures in response to comments.

Essential public facilities as defined in RCW 36.70A.200 are excluded from the Planned Action as designated herein and are not eligible for review or permitting as Planned Action Projects unless they are accessory to or part of a project that otherwise qualifies as a Planned Action Project.

The designated Planned Action Area is located entirely within an Urban Growth Area.

Implementation of the mitigation measures identified in the Planned Action Threshold Determination will provide for adequate public services and facilities to serve the proposed Planned Action Area.

The documents and other materials that constitute the record of the proceedings upon which the Planning Commission's recommendations are based, including, but not limited to, the staff reports for the Project and all of the materials that support the staff reports for the Project, are located in the City of Lakewood, Community and Economic Development Department at 6000 Main Street SW, Lakewood, Washington, 98499-5027. The custodian of these documents is the Assistant City Manager for Development Services of the City of Lakewood.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. Adoption of City Council Findings. The Findings of the City Council are adopted as part of this Ordinance.

Section 2. Purpose. The purpose of this Ordinance is to:

A. Combine environmental analysis, land use plans, development regulations, and City codes and ordinances together with the mitigation measures in the Planned Action Threshold Determination to

mitigate environmental impacts and process Planned Action development applications in the Planned Action Area;

B. Designate the Station District shown in **Exhibit A** as a Planned Action Area for purposes of environmental review and permitting of designated Planned Action Projects pursuant RCW 43.21C.440;

C. Determine that the Planned Action Threshold Determination meets the requirements of a planned action Threshold Determination pursuant to the State Environmental Policy Act (SEPA);

D. Establish criteria and procedures for the designation of certain projects within the Planned Action Area as “Planned Action Projects” consistent with RCW 43.21C.440;

E. Provide clear definition as to what constitutes a Planned Action Project within the Planned Action Area, the criteria for Planned Action Project approval, and how development project applications that qualify as Planned Action Projects will be processed by the City;

F. Streamline and expedite the land use permit review process by relying on the Planned Action Threshold Determination; and

G. Apply applicable regulations within the City’s development regulations and the mitigation framework contained in this Ordinance for the processing of Planned Action Project applications and to incorporate the applicable mitigation measures into the underlying project permit conditions in order to address the impacts of future development contemplated by this Ordinance.

Section 3. Procedures and Criteria for Evaluating and Determining Planned Action Projects within the Planned Action Area.

A. Planned Action Area. This “Planned Action” designation shall apply to the area shown in Exhibit A of this Ordinance.

B. Environmental Document. A Planned Action Project determination for a site-specific project application within the Planned Action Area shall be based on the environmental analysis contained in the Planned Action Threshold Determination. The mitigation measures contained in Exhibit B and Exhibit C of this Ordinance are based upon the findings of the Planned Action Threshold

Determination and shall, along with adopted City regulations, provide the framework the City will use to apply appropriate conditions on qualifying Planned Action Projects within the Planned Action Area.

C. Planned Action Project Designated. Land uses and activities described in the Planned Action Threshold Determination, subject to the thresholds described in Subsection 3.D of this Ordinance and the mitigation measures contained in Exhibit B and Exhibit C of this Ordinance, are designated “Planned Action Projects” pursuant to RCW 43.21C.440. A development application for a site-specific project located within the Planned Action Area shall be designated a Planned Action Project if it meets the criteria set forth in Subsection 3.D of this Ordinance and all other applicable laws, codes, development regulations, and standards of the City, including this Ordinance, are met.

D. Planned Action Qualifications. The following thresholds shall be used to determine if a site-specific development proposed within the Planned Action Area was contemplated as a Planned Action Project and has had its environmental impacts evaluated in the Planned Action Threshold Determination:

(1) Qualifying Land Uses.

(a) Planned Action Categories: The following general categories/types of land uses are defined

in the Station District Plan and can qualify as Planned Actions:

- i. Townhome or cottage dwelling units
- ii. Multi-family dwelling units
- iii. Commercial Office
- iv. Services,
- v. Medical
- vi. Hotel and Lodging
- vii. Retail and Eating and Drinking Establishments
- viii. Open Space, Parks, Plazas, Trails, Gathering Spaces, Recreation
- ix. Civic and Cultural Facilities
- x. Governmental and Utility Facilities

xi. Other uses allowed in the Station District Development Code, LMC Title 18C.

(b) Planned Action Project Land Uses: A primary land use can qualify as a Planned Action Project land use when:

- i. it is within the Planned Action Area as shown in Exhibit A of this Ordinance;
- ii. it is within one or more of the land use categories described in Subsection 3.D(1)(a) above; and
- iii. it is listed in development regulations applicable to the zoning classifications applied to properties within the Planned Action Area.

A Planned Action Project may be a single Planned Action land use or a combination of Planned Action land uses together in a mixed-use development. Planned Action land uses may include accessory uses.

(c) Public Services: The following public services, infrastructure, and utilities can also qualify as Planned Actions: onsite roads, utilities, parks, trails, and similar facilities developed consistent with the Planned Action Threshold Determination mitigation measures, City and special district design standards, critical area regulations, and the Lakewood Municipal Code.

(2) Development Thresholds:

(a) Land Use: The following thresholds of new land uses are contemplated by the Planned Action:

FEATURE	PLANNED ACTION
Residential Dwellings (units): Net 2020-2035	1,722
Jobs: Net 2020-2035	1,276

(b) Shifting development amounts between land uses in identified in Subsection 3.D(2)(a) may be permitted when the total build-out is less than the aggregate amount of development reviewed in the Planned Action Threshold Determination; the traffic trips for the Planned Action are not exceeded; and, the development impacts identified in the Planned Action

Threshold Determination are mitigated consistent with Exhibit B and Exhibit C of this Ordinance.

- (c) Further environmental review may be required pursuant to WAC 197-11-172, if any individual Planned Action Project or combination of Planned Action Projects exceeds the development thresholds specified in this Ordinance and/or alter the assumptions and analysis in the Planned Action Threshold Determination.

(3) Transportation Thresholds:

- (a) Trip Ranges & Thresholds. The number of new PM peak hour trips anticipated in the Planned Action Area and reviewed in the Planned Action Threshold Determination for 2035 is as follows:

The net increase between Planned Action and Existing Conditions is 1,950 vehicle trips. Total rounded vehicle trips by scenario tested include:

- i. Existing – 2,530
- ii. No Action – 3,810
- iii. Planned Action – 4,480

- (b) Concurrency. All Planned Action Projects shall meet the transportation concurrency requirements and the Level of Service (LOS) thresholds established in **LMC 18A.50.195**.

- (c) Traffic Impact Mitigation. Transportation mitigation shall be provided consistent with mitigation measures in Exhibit B and Exhibit C of this Ordinance, attached hereto and incorporated by this reference.

- (d) The responsible City official shall require documentation by Planned Action Project applicants demonstrating that the total trips identified in Subsection 3.D(3)(a) are not exceeded, that the project meets the concurrency and intersection standards of Subsection 3.D(3)(b), and that the project has mitigated impacts consistent with Subsection 3.D (3)(c).

- (e) Discretion.

i. The responsible City official shall have discretion to determine incremental and total trip generation, consistent with the Institute of Traffic Engineers (ITE) Trip Generation Manual (latest edition) or an alternative manual accepted by the City's Public Works Director at his or her sole discretion, for each project permit application proposed under this Planned Action.

ii. The responsible City official shall have discretion to condition Planned Action Project applications to meet the provisions of this Planned Action Ordinance and the Lakewood Municipal Code.

(4) Elements of the Environment and Degree of Impacts. A proposed project that would result in a significant change in the type or degree of adverse impacts to any element(s) of the environment analyzed in the Planned Action Threshold Determination would not qualify as a Planned Action Project.

(5) Changed Conditions. Should environmental conditions change significantly from those analyzed in the Planned Action Threshold Determination, the City's SEPA Responsible Official may determine that the Planned Action Project designation is no longer applicable until supplemental environmental review is conducted.

E. Planned Action Project Review Criteria.

(1) The City's SEPA Responsible Official, or authorized representative, may designate as a Planned Action Project, pursuant to RCW 43.21C.440, a project application that meets all of the following conditions:

(a) the project is located within the Planned Action Area identified in **Exhibit A** of this Ordinance;

(b) the proposed uses and activities are consistent with those described in the Planned Action Threshold Determination and Subsection 3.D of this Ordinance;

(c) the project is within the Planned Action thresholds and other criteria of Subsection 3.D of this Ordinance;

- (d) the project is consistent with the Lakewood Comprehensive Plan including the policies of the Station District Plan incorporated into the Comprehensive Plan and the regulations of the Station District Plan integrated into the Lakewood Municipal Code;
- (e) the project's significant adverse environmental impacts have been identified in the Planned Action Threshold Determination;
- (f) the project's significant impacts have been mitigated by application of the measures identified in **Exhibit B** and **Exhibit C** of this Ordinance and other applicable City regulations, together with any conditions, modifications, variances, or special permits that may be required;
- (g) the project complies with all applicable local, state and/or federal laws and regulations and the SEPA Responsible Official determines that these constitute adequate mitigation; and
- (h) the project is not an essential public facility as defined by RCW 36.70A.200, unless the essential public facility is accessory to or part of a development that is designated as a Planned Action Project under this Ordinance.

- (2) The City shall base its decision to qualify a project as a Planned Action Project on review of the SEPA Checklist form in WAC 197-11 and review of the Planned Action Project submittal and supporting documentation, provided on City required forms.

F. Effect of Planned Action Designation.

- (1) Designation as a Planned Action Project by the City's SEPA Responsible Official means that a qualifying project application has been reviewed in accordance with this Ordinance and found to be consistent with the development parameters and thresholds established herein and with the environmental analysis contained in the Planned Action Threshold Determination.
- (2) Upon determination by the City's SEPA Responsible Official that the project application meets the criteria of Subsection 3.D and qualifies as a Planned Action Project, the project shall not require a SEPA threshold determination, preparation of a Threshold Determination, or be subject to further review pursuant to SEPA. Planned Action Projects will still be subject to all other applicable City,

state, and federal regulatory requirements. The Planned Action Project designation shall not excuse a project from meeting the City's code and ordinance requirements apart from the SEPA process.

G. Planned Action Project Permit Process. Applications submitted for qualification as a Planned Action Project shall be reviewed pursuant to the following process:

- (1) Development applications shall meet all applicable requirements of the Lakewood Municipal Code and this Ordinance in place at the time of the Planned Action Project application. Planned Action Projects shall not vest to regulations required to protect public health and safety.
- (2) Applications for Planned Action Projects shall:
 - (a) be made on forms provided by the City;
 - (b) include the SEPA checklist in WAC 197-11;
 - (c) meet all applicable requirements of the Lakewood Municipal Code and this Ordinance.
- (3) The City's SEPA Responsible Official shall determine whether the application is complete and shall review the application to determine if it is consistent with and meets all of the criteria for qualification as a Planned Action Project as set forth in this Ordinance.
- (4) (a) If the City's SEPA Responsible Official determines that a proposed project qualifies as a Planned Action Project, he/she shall issue a "Determination of Consistency" and shall mail or otherwise verifiably deliver said Determination to the applicant; the owner of the property as listed on the application; and federally recognized tribal governments and agencies with jurisdiction over the Planned Action Project, pursuant to RCW 43.21C.440(3)(b).
 - (b) Upon issuance of the Determination of Consistency, the review of the underlying project permit(s) shall proceed in accordance with the applicable permit review procedures specified in Chapter 18A.02 LMC, except that no SEPA threshold determination, Threshold Determination, or additional SEPA review shall be required.
 - (c) The Determination of Consistency shall remain valid and in effect as long as the underlying project application approval is also in effect.

- (d) Public notice and review for qualified Planned Action Projects shall be tied to the underlying project permit(s). If notice is otherwise required for the underlying permit(s), the notice shall state that the project qualifies as a Planned Action Project. If notice is not otherwise required for the underlying project permit(s), no special notice is required by this Ordinance.
- (5) (a) If the City's SEPA Responsible Official determines that a proposed project does not qualify as a Planned Action Project, he/she shall issue a "Determination of Inconsistency" and shall mail or otherwise verifiably deliver said Determination to the applicant; the owner of the property as listed on the application; and federally recognized tribal governments and agencies with jurisdiction over the Planned Action Project, pursuant to RCW 43.21C.440.
- (b) The Determination of Inconsistency shall describe the elements of the Planned Action Project application that result in failure to qualify as a Planned Action Project.
- (c) Upon issuance of the Determination of Inconsistency, the City's SEPA Responsible Official shall prescribe a SEPA review procedure for the non-qualifying project that is consistent with the City's SEPA regulations and the requirements of state law.
- (d) A project that fails to qualify as a Planned Action Project may incorporate or otherwise use relevant elements of the Planned Action Threshold Determination, as well as other relevant SEPA documents, to meet the non-qualifying project's SEPA requirements. The City's SEPA Responsible Official may limit the scope of SEPA review for the non-qualifying project to those issues and environmental impacts not previously addressed in the Planned Action Threshold Determination.
- (6) To provide additional certainty about applicable requirements, the City or applicant may request consideration and execution of a development agreement for a Planned Action Project, consistent with RCW 36.70B.170 et seq.
- (7) A Determination of Consistency or Inconsistency is a Process I land use decision and may be appealed pursuant to the procedures established in Chapter 18A.02 LMC. An appeal of a

Determination of Consistency shall be consolidation with any pre-decision or appeal hearing on the underlying project application.

Section 4. Monitoring and Review.

A. The City should monitor the progress of development in the designated Planned Action area biennially to ensure that it is consistent with the assumptions of this Ordinance and the Planned Action Threshold Determination regarding the type and amount of development and associated impacts and with the mitigation measures and improvements planned for the Planned Action Area.

B. This Planned Action Ordinance shall be reviewed by the SEPA Responsible Official every two (2) years from its effective date in conjunction with the City's regular Comprehensive Plan review or docket cycle, as applicable. The review shall determine the continuing relevance of the Planned Action assumptions and findings with respect to environmental conditions in the Planned Action Area, the impacts of development, and required mitigation measures (**Exhibit B** and **Exhibit C**). Based upon this review, the City may propose amendments to this Ordinance or may supplement or revise the Planned Action Threshold Determination.

Section 5. Severability. If any portion of this Ordinance or its application to any person or circumstances is held invalid, the remainder of the Ordinance or the application of the provision to other persons or circumstances shall not be affected.

Section 6. Effective Date. That this Ordinance shall be in full force and effect thirty (30) days after publication of the Ordinance Summary.

ADOPTED by the City Council this 3rd day of May, 2021.

CITY OF LAKEWOOD

Attest:

Don Anderson, Mayor

Briana Schumacher, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney

EXHIBIT A. PLANNED ACTION AREA



Map Note: The right of way for Bridgeport Way between the East Commercial Area and Colonial District is included for consistent landscaping. Abutting land use is not included in that segment.

EXHIBIT B. MITIGATION DOCUMENT

The Planned Action Threshold Determination has identified significant beneficial and adverse impacts that are anticipated to occur with the future development of the Planned Action Area, together with a number of possible measures to mitigate those significant adverse impacts.

A Mitigation Document is provided in this Exhibit B-1 to establish specific mitigation measures based upon significant adverse impacts identified in the Planned Action Threshold Determination. The mitigation measures in this Exhibit B-1 shall apply to Planned Action Project applications that are consistent with the Preferred Alternative range reviewed in the Planned Action Threshold Determination and which are located within the Planned Action Area (see Exhibit A).

Where a mitigation measure includes the words “shall” or “will,” inclusion of that measure in Planned Action Project application plans is mandatory in order to qualify as a Planned Action Project. Where “should” or “would” appear, the mitigation measure may be considered by the project applicant as a source of additional mitigation, as feasible or necessary, to ensure that a project qualifies as a Planned Action Project. Unless stated specifically otherwise, the mitigation measures that require preparation of plans, conduct of studies, construction of improvements, conduct of maintenance activities, etc., are the responsibility of the applicant or designee to fund and/or perform to the satisfaction of the City’s SEPA Responsible Official or authorized designee.

Any and all references to decisions to be made or actions to be taken by the City’s SEPA Responsible Official may also be performed by the City’s SEPA Responsible Official’s authorized designee.

Section B-1. Mitigation Required for Development Applications

Transportation

1. Applicants for development shall provide traffic assessments to the satisfaction of the SEPA Responsible Official or their designee to determine transportation signal improvements phasing consistent with the Threshold Determination Transportation Analysis.

Environmental Health

2. Planned actions shall orient residential uses to be located a minimum of 500 feet from the centerline of I-5.
3. Applicants for development shall conduct a site assessment to determine if contamination is present from past use.

Cultural Resources

4. Applicants for development shall include Inadvertent Human Remains Discovery Language recommended by DAHP as a condition of project approval.

Section B-2. Advisory Notes to Applicants:

Applicable Regulations

Additionally, the Planned Action Threshold Determination identifies specific regulations that act as mitigation measures. These are summarized in this section by Threshold Determination topic. All applicable federal, state, and local regulations shall apply to Planned Action Projects. Planned Action Project applicants shall comply with all adopted regulations where applicable including those listed in the Planned Action Threshold Determination and those not included in the Planned Action Threshold Determination. In addition, this section identifies voluntary water and energy conservation measures that may be implemented by new development.

Earth

LMC Chapter 12.10 provides regulations addressing: storm drainage, earthwork (excavation and fill), soil reports, and erosion controls.

Air Quality

Application of the State Energy Code can improve energy efficiency and reduce greenhouse gas emissions. (LMC Chapter 15.05) Over time new standards will also be implemented: The 2019 Legislature adopted new standards that will increase the efficiency of these buildings and reduce emissions. The Legislature also increased efficiency standards for appliances. (Washington State Department of Ecology, 2019)

Water Resources

Application of Washington State Department of Ecology Stormwater Manual and its Engineering Standards Manual at LMC Chapter 12.11.

Stormwater management will be required of all new development whether under the No Action or Planned Action Alternatives. (LMC Chapter 12.11)

Plants and Animals

Planned Actions would implement LMC Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. When redevelopment would occur, perimeter landscaping and parking area landscaping would be required. Additionally significant trees would be retained where they exist.

The City maintains critical area regulations protecting fish and wildlife habitat conservation areas. Though none are mapped in the study area, if some were identified they would be protected under either the No Action or Planned Action Alternatives. (LMC Chapter 14.154)

Energy

All future development would be subject to the State Energy Code. (LMC Chapter 15.05)

Environmental Health

The State Model Toxics Control Act (MTCA) sets standards for cleanup of lower levels of contaminants that are incorporated into new development and redevelopment parcels noted to have contamination potential.

The City applies relevant standards regarding hazardous materials handling in the International Fire Code and Zoning Codes.

Land Use, Aesthetics, Recreation

Planned Actions shall comply with the Station District Subarea Plan and Title 18C Station District Development Code.

Housing

The City does provide inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing in Chapter 18A.90 Housing Incentives Program.

Light and Glare

LMC 18A.60.095 Outdoor Lighting Code provides lighting standards to reduce light and glare and to avoid interference with JBLM operations.

Historic and Cultural Preservation

Washington State has a number of laws that oversee the protection and proper excavation of archaeological sites (RCW 27.53, WAC 25-48), human remains (RCW 27.44), and historic cemeteries or graves (RCW 68.60). The Governor's Executive Order 05-05 requires state agencies to integrate DAHP, the Governor's Office of Indian Affairs, and concerned tribes into their capital project planning process. This executive order affects any capital construction projects and any land acquisitions for purposes of capital construction not undergoing Section 106 review under the National Historic Preservation Act of 1966.

Under RCW 27.53, DAHP regulates the treatment of archaeological sites on both public and private lands and has the authority to require specific treatment of archaeological resources. All precontact resources or sites are protected, regardless of their significance or eligibility for local, state, or national registers. Historic archaeological resources or sites are protected unless DAHP has made a determination of "not-eligible" for listing on the state and national registers.

The City applies Lakewood Municipal Code Chapter 2.48 Protection and Preservation of Landmarks.

Transportation, Public Services, and Utilities

All development is required to be served by primary infrastructure including sewer, water, stormwater, and transportation per LMC 18A.60.140 Concurrency.

Street frontage standards apply per the Station District Subarea Plan and Title 18C Station District Development Code and LMC 12.09.031.

Parking standards apply per Title 18C Station District Development Code and LMC 18A.80.030. commercial and multifamily uses.

Section B-3. Public Agency Strategies

The City will implement strategies in the Subarea Plan largely through the Form-Based Code, but will also determine implementation through its code docketing process.

Land Use

The City would propose to add the subarea to its multi-family tax exemption for affordable housing in LMC Chapter 3.64 Tax Incentive Urban Use Center Development.

Housing

Based on recent state laws, tiny homes and recreational vehicles must be allowed as a primary residence. (See ESSB 5183 (2019)). RCW 35.21.684, allows for tiny homes in manufactured home parks, and allows local jurisdictions more flexibility to authorize them in other zones). The City would propose amendments to address these recent state laws applicable across the city as well as in the subarea.

Transportation, Public Services, and Utilities

The City addresses public service levels of service in its Capital Facilities Plan Element. The element is updated periodically to ensure that proposed growth and change can be served.

EXHIBIT C TRANSPORTATION IMPROVEMENTS

C-1 Lakewood Transportation Improvement Program Improvements

The Six-Year Comprehensive Transportation Improvement Program (TIP) for 2021-2026 was approved by the Lakewood City Council in June 2020, and is expected to be updated annually. This document outlines short and long term road projects, including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays, throughout the city. The following projects within the planned action area are identified in the TIP as long term improvements:

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study.
- Kendrick Street SW from 111th Street SW to 108th Street SW – sidewalks, street lighting, bicycle facilities.
- 112th/111th – Bridgeport to Kendrick – sidewalks, bicycle facilities, street lighting.
- 108th – Bridgeport Way to Pacific Highway – roadway patching and overlay.

C-2 Planned Action Mitigation

Exhibit 1 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right-of-way acquisition or widening, rather only changes to signal timing to increase the efficiency of impacted intersections and roadways in the subarea. The exception is intersection of Pacific Highway and Sharondale Road, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better.

Per Planned Action requirements in Section B-1 planned action development in the study area will provide information about expected trips and will provide a traffic study where applicable per LMC 12.09.028. This analysis will affirm the mitigation approach and phasing, such as signal timing at Bridgeport Way/I-5 on and off ramps. It will also affirm the City' concurrency standards are met in LMC 18A.60.140.

Exhibit 1. Proposed Mitigation Measures

ID	INTERSECTION	NO ACTION	ALT 1	ALT 1 MITIGATED
4	Pacific Highway / Sharondale Road			
	Install signal using existing channelization and signal length consistent with adjacent intersections	F>100	F>100	A/4
8	Pacific Highway / Bridgeport Way			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.	D/54	E/78	D/49
11	Bridgeport Way / SB I-5 Ramp			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.	D/41	E/60	D/52

Source: Fehr & Peers, 2020.

ATTACHMENT B

SEPA ENVIRONMENTAL CHECKLIST

Lakewood Station District Subarea Planned Action

WAC 197-11-960

Purpose of checklist:

The State Environmental Policy Act (SEPA), chapter [43.21C](#) RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An environmental impact statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

Instructions for applicants:

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

Use of checklist for nonproject proposals:

For nonproject proposals complete this checklist and the supplemental sheet for nonproject actions (Part D). The lead agency may exclude any question for the environmental elements (Part B) which they determine do not contribute meaningfully to the analysis of the proposal.

For nonproject actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.

A. Background

1. Name of proposed project, if applicable:

Lakewood Station District Subarea Plan, Form-Based Code, and Planned Action

2. Name of applicant:

City of Lakewood

3. Address and phone number of applicant and contact person:

Tiffany Speir, Long Range & Strategic Planning Manager
Community Development Department
City of Lakewood
City Hall, 6000 Main St. SW
Lakewood, WA 98499
tspeir@cityoflakewood.us
253-983-7702

4. Date checklist prepared:

July 27, 2020; updated March 29, 2021

5. Agency requesting checklist:

City of Lakewood

6. Proposed timing or schedule (including phasing, if applicable):

Adoption Spring 2021

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

Future site specific development is anticipated under the nonproject subarea plan and development regulations.

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

Comprehensive Plan EIS, June 2000. The EIS contained a preferred alternative and two other alternatives, including a no action alternative and mixed-use alternative. The principal strategy of the preferred alternative was to: Protect established neighborhoods; Develop intensification within the city's spine, which stretched north along Bridgeport Way from the Lakewood Station, past the Town Center and the Colonial Center, through to the Custer Road neighborhood; Focused residential density in several neighborhoods, notably Springbrook, Tillicum, and Custer; and Increasing the employment base by converting parts of the Woodbrook Neighborhood into an industrial center. The preferred alternative provided 'development capacity' from an estimated 17,500 new residents and 12,275 new jobs.

Comprehensive Plan Supplemental EIS, 2003. There were 10 comprehensive plan amendments proposed in 2003 that would collectively redesignate numerous sections of the City of Lakewood from their existing land use and zoning designations to new designations. The majority of these amendments were relatively minor, parcel-specific

inconsistencies between the adopted future land use plan and existing or intended land uses. However, one amendment along Bridgeport Way, north of 75th Street SW, reduced high-density residential development in favor of commercial development (Wal-Mart).

Final Lakewood Downtown Subarea Plan and Draft Planned Action EIS, July 2018. The following proposals were evaluated for the Downtown to help support desired change and development: Subarea Plan, Comprehensive Plan land use, policy, and capital facility plan amendments, new hybrid form-based zoning standards, and upfront environmental review through a Planned Action consistent with RCW 43.21 c.440 and SEPA rules in WAC 197-11.

Lakewood Station District Subarea Situation Assessment – April 2020. Provides current conditions and trends across natural and built environment topics.¹

Lakewood Station District Subarea Transportation Technical Memo – October 2020. Provides transportation evaluation of the No Action and Planned Action Alternatives. See Appendix.

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.

A portion of the study area is under consideration for a master planned mixed use development called the Lakewood Landing.

10. List any government approvals or permits that will be needed for your proposal, if known.

The non-project action requires review and recommendations by the Lakewood Planning Commission and review and approval by the Lakewood City Council. It has undergone a 60-day review by state agencies led by the Washington Department of Commerce. It has been shared with regional agencies like Puget Sound Regional Council and Pierce County as well.

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.)

Comparison of Alternatives

Two alternatives are considered in this expanded checklist: the No Action Alternative that maintains current plans and regulations and a Planned Action. The Planned Action would expand housing and job opportunities.

The study area is an opportunity for future growth and development within Lakewood. First, there are several neighborhoods within a half mile of the Lakewood Station with a mix of multi-family and mixed residential zones that are developed at densities lower than planned or zoned. Second, with its proximity to I-5 and the Sounder regional commuter rail, the study area is a good location for employment. Third, since there are few environmental constraints, this is an ideal place to explore housing types and densities that take advantage of the proximity to high capacity transit. Fourth, the Town Center District is also only about a mile away (Exhibit 4) so there is also an opportunity to deepen connections between the two subareas.

Under the Planned Action, the City of Lakewood proposes to adopt a subarea plan for the Lakewood Station District, as well as a form-based code. As allowed under RCW 43.21 c.440, the City is developing a planned

¹ This report is available in City Council packet: <https://cityoflakewood.us/wp-content/uploads/2020/08/2020-08-10-Council-Agenda.pdf>.

action using a threshold determination process.

Following are key elements of the current plan (No Action proposal) and the Planned Action proposal.

Exhibit 1. No Action and Proposed Action Comparison

Element	No Action	Proposed Action
Land Use/Zoning	<ul style="list-style-type: none"> ▪ Existing Comprehensive Plan <ul style="list-style-type: none"> ○ Corridor Commercial ○ High Density Multi-Family ○ Neighborhood Business District ○ Open Space & Recreation ○ Public Semi-Public Institutional ▪ Existing Zoning <ul style="list-style-type: none"> ○ Commercial: C1, C2, NC2, TOC ○ MF3 ○ OSR2 ○ PI ▪ Density Range: 35 du/ac NC2 and 54 du/ac MF3 and TOC 	<ul style="list-style-type: none"> ▪ Retain Comprehensive Plan and Zoning designations, except: <ul style="list-style-type: none"> ○ Update Open Space & Recreation designation/zone. ▪ Zoning allowances: <ul style="list-style-type: none"> ○ Create new chapter in Title 18 for hybrid form-based code. Will serve functionally as an overlay zone. Address building and street typologies similar to Downtown, adapted for study area. ○ Update MF3 zone regarding incentives and standards for rowhouses/townhouses. ○ Consider Live/Work opportunities within areas where mixed use and townhomes are desired. Consider implications for home occupation types. ○ Promote Townhouses and Rowhouses at 30-40 du/ac. In form-based code, address 30 foot height and appropriate setbacks. Address minimum lot width and size allowing for townhomes and rowhouses. ▪ Land Use Assumptions: <ul style="list-style-type: none"> ○ Integrate Lakewood Landing. ○ Density Range: 35 du/ac NC2; 30-40 du/ac MF3 in study area; 54 du/ac TOC. Mixed use in proximity to station in MF3 as allowed currently.
Growth Capacity	<ul style="list-style-type: none"> ▪ The 2014 Buildable Lands Report indicated a low capacity of 258 new dwelling units and 1,186 jobs. <ul style="list-style-type: none"> ○ Application of 2014 Buildable Lands Report zone-based assumptions to underutilized and 	<ul style="list-style-type: none"> ▪ If achieving up to 40 units per acre in the MF3 zoned area and 54 units per acre in mixed use zones, capacity for residential would be about 1,019 new units. Lakewood Landing would result in another 760 planned units or a total of 1,779 dwellings.

Element	No Action	Proposed Action
	vacant lands shows a potential for up to 1,343 new dwelling units.	<ul style="list-style-type: none"> Job capacity would equal 1,186 jobs, plus Lakewood Landing jobs. Early phase includes retail jobs. Later phases include office jobs.
Growth Assumptions	<ul style="list-style-type: none"> Transportation Model 2010-2035: <ul style="list-style-type: none"> Households Distributed per Traffic Model: 962 Jobs Distributed per Traffic Model: 1,193 	<ul style="list-style-type: none"> Transportation Model 2010-2035: <ul style="list-style-type: none"> Households Distributed per Buildable Land Capacity Shares illustrating middle density distribution across MF3 zone (35 units per acre average): $962 + 760 = 1,722$ Jobs: $1,105 + 171$ Lakewood Landing retail jobs = 1,276
Housing	Zoning allows a variety of attached housing types up to 54 du/ac in TOC and MF 3 and 35 du/ac in NC 2.	<ul style="list-style-type: none"> Observed density in MF3 is nearly 12 units per acre. If redevelopable sites achieved 35 units per acre, the density would increase to about 26 units per acre zone wide or 65% of a maximum 40 units per acre. Focus on missing middle ownership: 65%-110% of area median income. Promote Townhouses and Rowhouses at 30-40 du/ac. Allow for/attract small builders as well as larger builders. Consider property assembly in area (10 acres brick & masonry plant). Opportunities for mixed use and open space.
Jobs	Service and retail are dominant sectors.	<ul style="list-style-type: none"> Encourage updated retail. Such as through façade improvement programs with existing properties and allowances for new commercial and mixed uses.
Parks and Open Space	Green streets, plazas and pocket parks, and oak tree preservation are highlights in the current Comprehensive Plan.	<ul style="list-style-type: none"> Address Open Space & Recreation designation/zone. Integrate Open Space into Larger Mixed Use Developments. Lakewood Landing: Courtyard Green streets and Alley Plaza near Occidental, within current rights of way

Element	No Action	Proposed Action
Transportation	<ul style="list-style-type: none"> ▪ Served by Pierce Transit, Intercity Transit, and Sound Transit <ul style="list-style-type: none"> ○ Sounder South Capacity Expansion ○ Sound Transit Access Improvements ○ Pierce Transit Long Range Plan ▪ Limited pedestrian and bike facilities to be improved – consistent with Comp Plan Transportation Element <ul style="list-style-type: none"> ○ Improvements Sidewalk Improvements – Kendrick Drive from 111th St to 108th St SW ○ Non-Motorized Access Improvements from 2009 NMTP and ST Access Improvement Study ○ Bridgeport Way Sidewalks ○ 111th/112th St SW Sidewalk Gap ○ 111th St SW Bike Lanes ○ Real-time parking availability signage ○ Bike Parking (short and long-term)consistent with TIP 	<p>Similar improvements as No Action plus:</p> <p>Streetscape Typologies</p> <ul style="list-style-type: none"> ▪ Allocate excess right-of-way for non-motorized uses ▪ Prioritize space for active modes over on-street parking ▪ Enhanced crossings <p>Connections</p> <ul style="list-style-type: none"> ▪ Bridge across I-5 (47th Avenue SW) <p>Other</p> <ul style="list-style-type: none"> ▪ Consider street vacation (depending on citywide policy review)

Lakewood Landing

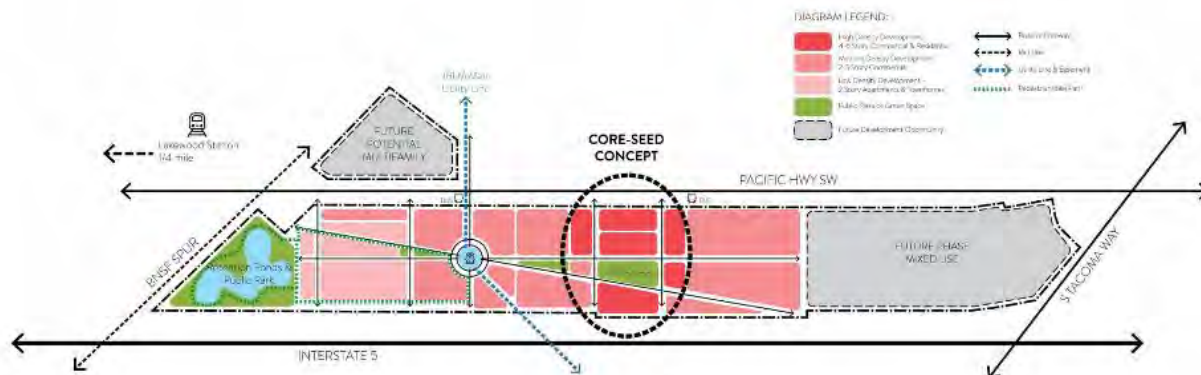
Lakewood Landing is a 30-acre site poised for mixed use and employment development. The site is adjacent to Sound Transit commuter rail station and between Pacific Highway and I-5. An initial mixed use plan with 760 units and 87,000 square feet of retail are part of Phase 1 in the Central Core. A Northwest concept, an aviation themed restaurant overlooking the McChord runway, coffee shops, a craft beer tap room, boutique shops, restaurants, and an interpretive educational park are all part of the long-term vision. Water features, carefully placed structures, and gabion walls will help to mitigate noise.

Phase I, Central Core

- 11.5 acres
- 5 over 2 construction and one level of underground parking
- 760 residential units
- 87,000 square feet of retail (about 171 employees)
- 1,474 parking spaces, including a parking field

Future phases are under consideration.

Exhibit 2. Lakewood Landing Location and Concept



Source: City of Lakewood 2020.

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist.

The proposed LSDS subarea boundary encompasses 231.1 parcel acres, and 329.5 gross acres; see Exhibit 3. Generally, the boundary incorporates the area within a half mile of the Sounder station and associated Sound

The subarea spans the area from the interchange with Bridgeport Way to the interchange with SR-512, to capture the full transportation corridor to the north and south of the station. In order to analyze transportation connections to Lakewood's Downtown, the transportation evaluation reviews an extended area north and west of the primary subarea (Exhibit 4). The extended study area looks at ways to enhance multi-modal transportation linkages. The extended study area does not address land use and housing beyond the LSDS.

Exhibit 3. Lakewood Station District Study Area



Source: BERK, 2020; Pierce County Assessor, 2020.

Exhibit 4. Lakewood Station in Proximity to Downtown



Source: BERK, 2020; Pierce County Assessor, 2020.

B. Environmental Elements

1. EARTH

a. General description of the site:

(circle one): Flat, rolling, hilly, steep slopes, mountainous, other _____

The study area is generally flat.

b. What is the steepest slope on the site (approximate percent slope)?

Slopes are 0 to 6 percent, with a representative slope of 3%.

c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils.

The study area is entirely mapped as 41A—Spanaway gravelly sandy loam. Properties include:

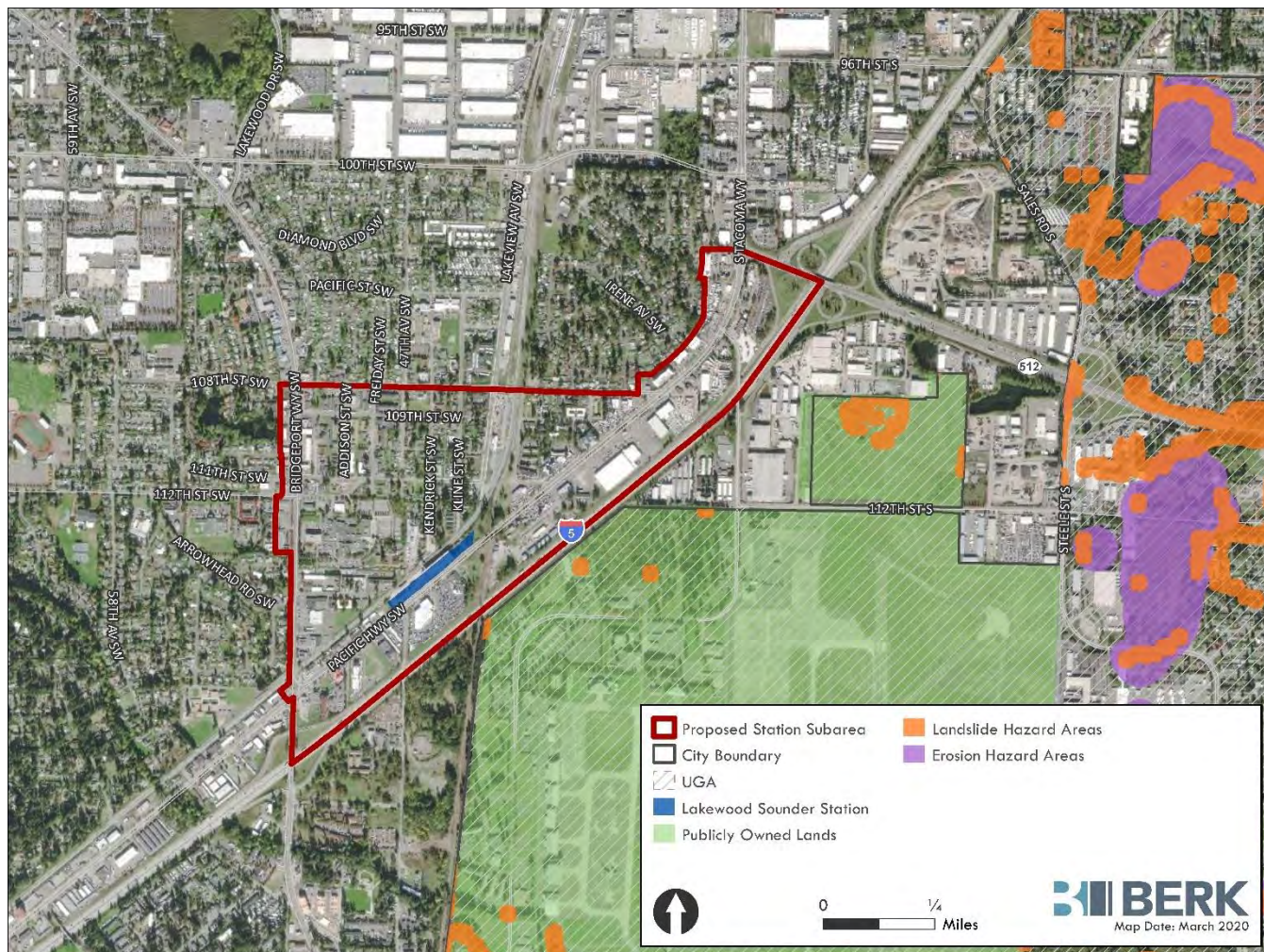
- Landform: Outwash plains
- Slope: 0 to 6%
- Depth to water table: More than 80 inches
- Hydric soil rating: No

Soil Survey Area: Pierce County Area, Washington, Survey Area Data: Version 16, Jun 4, 2020.

d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.

No. The City map of geologic hazards do not show liquefaction, erosion, or landslide hazard areas. See Exhibit 5.

Exhibit 5. Geologic Hazards



Source: City of Lakewood, BERK 2020.

e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites and require fill, excavation, or grading.

f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites and during construction expose soils to erosion.

g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

The applicable zones allow for about 70% impervious surfaces (MF3) to 100% impervious surfaces (NC2, TOC, C1 and C2). (LMC 18A.60.030 and 18A.60.040) Future site-specific development under either the No Action or Planned Action Alternatives may replace or increase impervious surfaces consistent with zoning.

h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

LMC Chapter 12.10 provides regulations addressing: storm drainage, earthwork (excavation and fill), soil reports, and erosion controls.

2. AIR

a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites. During construction there is a potential for dust. During operation there is potential for added emissions due to vehicular travel generated by the new growth. The Planned Action would increase dwellings and jobs in the study area and have a greater local potential for air emissions. Regionally, focusing growth in and near multimodal transportation opportunities could reduce vehicle miles travelled and associated air emissions compared to development on the fringe of the urban area. (Urban Land Institute, 2010)

Puget Sound Regional Council (PSRC) has identified that its 2019-2022 transportation improvement program. The Regional TIP includes continued implementation and expansion of commuter rail service between Seattle and Lakewood, and resurfacing of Pacific Hwy and South Tacoma Way between 108th ST SW and SR 512. The Air Quality Conformity Analysis indicates that the emissions from the projects and programs in the plan are below the established daily motor vehicle emissions budgets for PM2.5 and NOX. (Puget Sound Regional Council, 2018)

b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

The study area is located within a PM2.5 Maintenance Area, and in a former CO and Ozone maintenance area. I-5 forms the eastern border of the study area and results in air emissions to existing and future development.

c. Proposed measures to reduce or control emissions or other impacts to air, if any:

Providing for more mixed use, residential, and job opportunities in proximity to the in proximity to the Sounder Station and travel by transit is likely by a portion of residents and employees.

Application of the State Energy Code can improve energy efficiency and reduce greenhouse gas emissions. (LMC Chapter 15.05) Over time new standards will also be implemented: The 2019 Legislature adopted new standards that will increase the efficiency of these buildings and reduce emissions. The Legislature also increased efficiency standards for appliances. (Washington State Department of Ecology, 2019)

Dense landscaping along roadways can reduce air pollutants by up to 50% (Deshmukh, 2019) Green infrastructure is another source of potential air emission mitigation at a microscale (Tiwari, 2019). As part of the Subarea Plan update associated with the Planned Action, the City can promote landscaping and green infrastructure. It can also address orientation and location of residential uses in mixed use developments to reduce the potential for localized air quality effects and improve compatibility; for example residential development can be focused away from I-5 and business uses can buffer residential uses near the freeway.

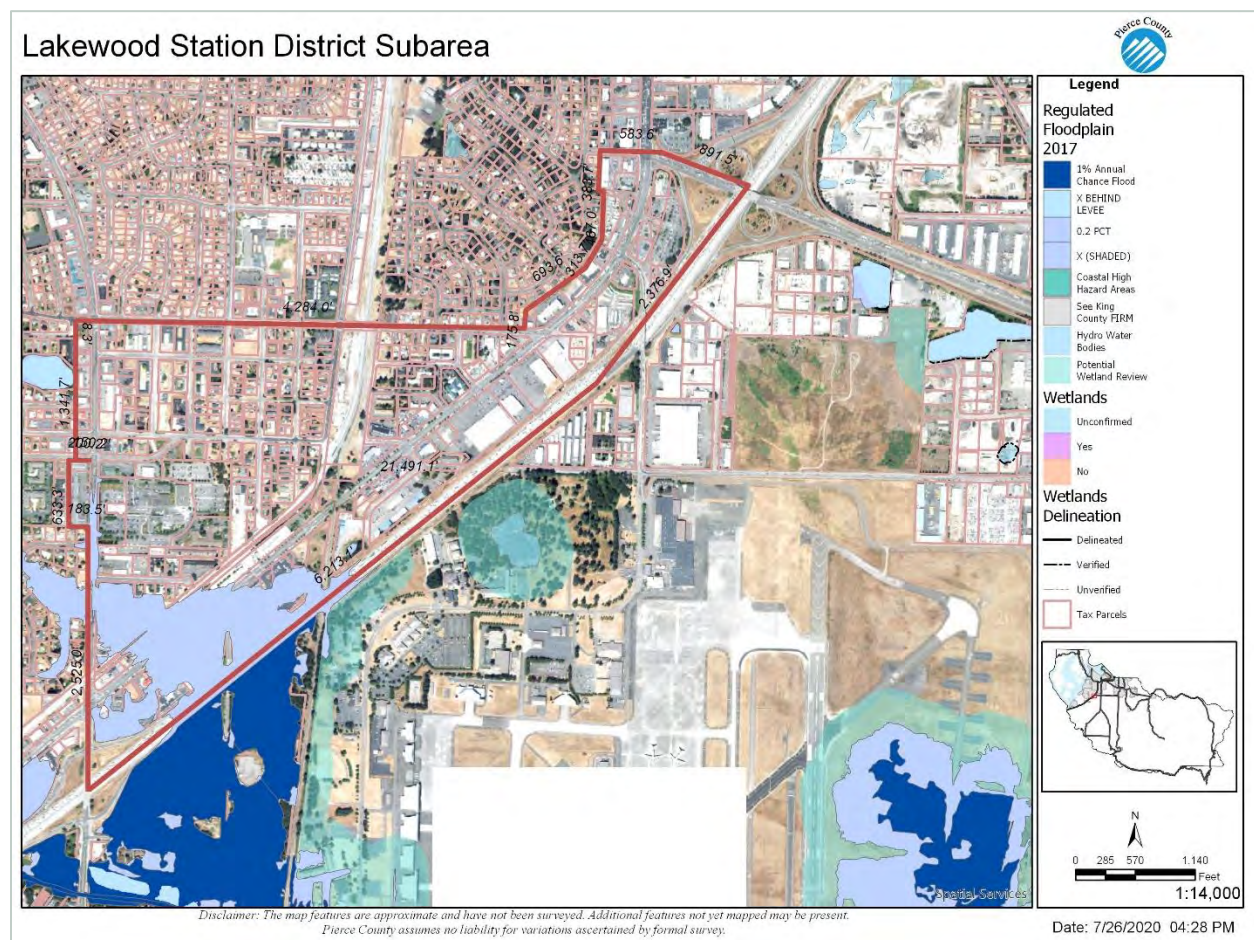
3. WATER

a. Surface Water:

1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

Exhibit 6 shows the hydrology of the area. There are no significant hydrological features within the subarea. Outside and abutting the northwest corner of the subarea is Hidden Lake, which is a delineated wetland just west of Bridgeport Way. Critical area buffer requirements may affect a limited number of parcels in the subarea west of Bridgeport. Along small lakes, regulated activities are subject to a 35-foot buffer requirement. (LMC 14.154.060) Wetland protection requirements depend on habitat quality and wetland categories, and buffers could range from 40 to 225 feet.

Exhibit 6. Hydrology in the LDS and Surrounding Area



Source: Pierce County PublicGIS, 2020.

2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

Redevelopment may occur on sites near Hidden Lake but would be subject to critical area buffers.

3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites and require fill, excavation, or grading though none of these activities are anticipated in a wetland given lack of presence. Fill of wetlands is not proposed as part of the legislative actions associated with the subarea plan and form-based code. Any activities affecting wetland buffers would be subject to Lakewood critical area regulations.

4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

No surface water withdrawals or diversions are anticipated. A public water system supplies the areas. Creeks are not located in the study area.

5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

See Exhibit 6. Zone X (shaded) is considered a moderate flood hazard area and is the area between the limits of the base flood and the 0.2-percent-annual-chance (or 500-year) flood. It is mapped along Bridgeport Way and between Pacific Highway and I-5 west of Lakeview Avenue SW. These are not regulated by the City's flood hazard regulations, which apply to special flood hazard areas, defined as the area that will be inundated by the flood event having a 1-percent chance of being equaled or exceeded in any given year.

6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

No point discharges of waste material to surface waters are anticipated. All development will be subject to the City's storm water management regulations that address nonpoint runoff. (LMC Chapter 12.11)

b. Ground Water:

1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

No wells are proposed. The area is served by the Lakewood Water District.

2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

No septic systems are proposed. The area is served by a public sewer system operated by Pierce County Planning and Public Works Sewer Utility.

c. Water runoff (including stormwater):

1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

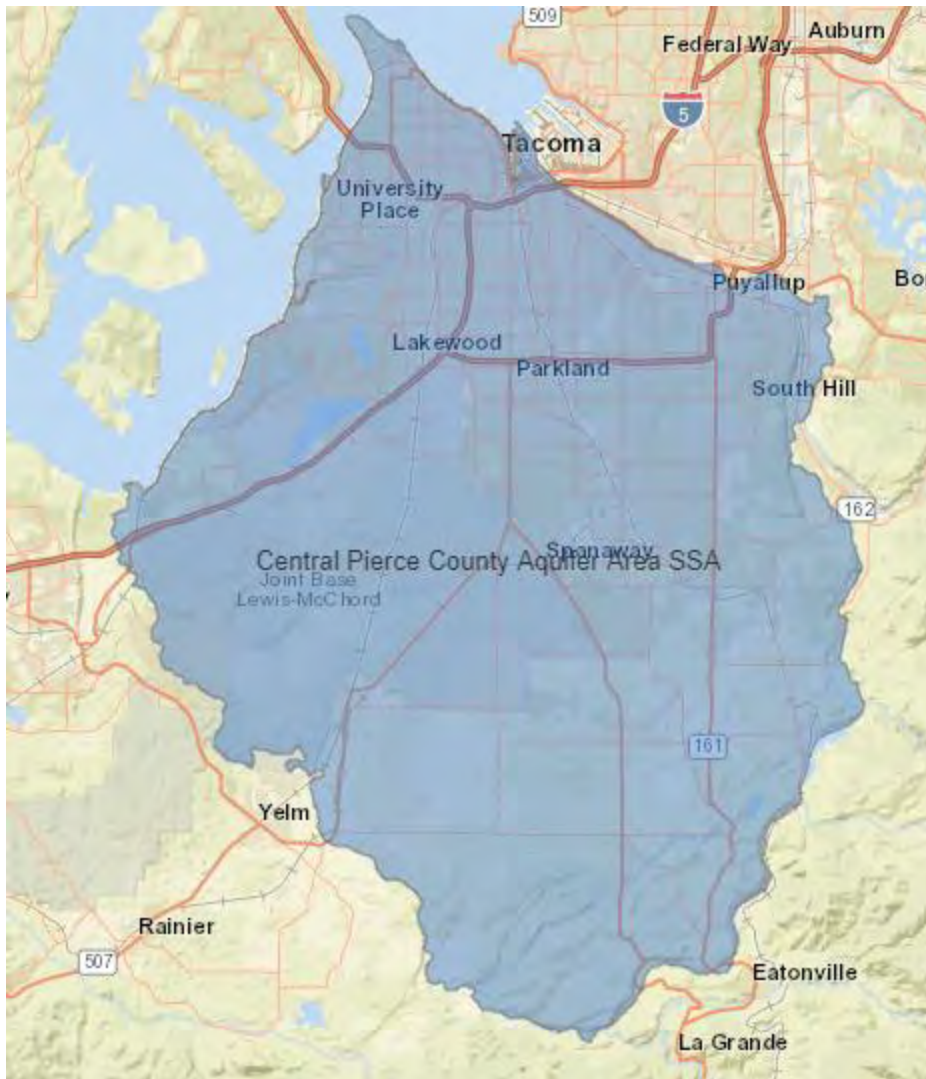
Stormwater runoff may occur though water quality and quantity is regulated, and should improve as sites developed under older regulations redevelop with newer regulations. Stormwater management will be required of all new development whether under the No Action or Planned Action Alternatives. (LMC Chapter 12.11) The City

applies the Washington State Department of Ecology Stormwater Manual and its Engineering Standards Manual.

2) Could waste materials enter ground or surface waters? If so, generally describe.

The City limits land uses that could result in groundwater contamination such as storage tanks, auto repair and auto washing facilities, dry cleaners, and others within the Clover/Chambers Creek Aquifer Basin boundary that encompasses the entire study area and beyond throughout Lakewood, Tacoma, and Central Pierce County. (LMC Chapter 14.150) See Exhibit 7. See C.1 regarding stormwater management.

Exhibit 7. Sole Source Aquifer in Central Pierce County



Source: USEPA 2020.

3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe.

The applicable zones allow for about 70% impervious surfaces (MF3) to 100% impervious surfaces (NC2, TOC, C1 and C2). (LMC 18A.60.030 and 18A.60.040) Future site-specific development under either the No Action or Planned Action Alternatives may replace or increase impervious surfaces consistent with zoning.

However, the Washington State Department of Ecology Stormwater Manual requires water quality treatment, including use of low impact development methods wherever feasible. (LMC Chapter 12.11)

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any:

Application of Washington State Department of Ecology Stormwater Manual and its Engineering Standards Manual at LMC Chapter 12.11.

Stormwater management will be required of all new development whether under the No Action or Planned Action Alternatives. (LMC Chapter 12.11)

The Subarea Plan and Form-Based Code proposed by the Planned Action includes alternative street cross sections and frontage improvements that would create an opportunity for added green infrastructure and stormwater management.

4. PLANTS

a. Check the types of vegetation found on the site:

The area is developed and has ornamental landscaping.

☒deciduous tree: alder, maple, aspen, other

☒evergreen tree: fir, cedar, pine, other

☒shrubs

☒grass

☐pasture

☐crop or grain

☐Orchards, vineyards or other permanent crops.

☐wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other

☐water plants: water lily, eelgrass, milfoil, other

☐other types of vegetation

b. What kind and amount of vegetation will be removed or altered?

Ornamental vegetation could be removed. However, landscaping will be required on sites.

c. List threatened and endangered species known to be on or near the site.

No threatened and endangered species are mapped in the study area. Hidden Lake abutting the study area to the west is a mapped aquatic habitat/wetlands under the State's Priority Habitats and Species program. (Washington State Department of Fish and Wildlife, 2020) It is protected by buffers. See Section B.3 above.

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

Future development would implement LMC Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. When redevelopment would occur, perimeter landscaping and parking area landscaping would be

required. Additionally significant trees would be retained where they exist.

The Planned Action Alternative would improve design standards applicable to the study area and provide standards for the public realm including street trees and green infrastructure.

e. List all noxious weeds and invasive species known to be on or near the site.

Noxious weeds are not mapped in detail, but along I-5 generally it is identified as an area with Spotted Knapweed. (Pierce County Weed Board, 2019) Noxious weeds are regulated by: RCW Chapter 17.10, and WAC Chapter 16-750.

5. ANIMALS

a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site.

Examples include:

✓birds: hawk, heron, eagle, songbirds, other:

✓mammals: deer, bear, elk, beaver, other: urban adapted wildlife

___fish: bass, salmon, trout, herring, shellfish, other _____

b. List any threatened and endangered species known to be on or near the site.

No threatened and endangered species are mapped near the study area. (Washington State Department of Fish and Wildlife, 2020)

c. Is the site part of a migration route? If so, explain.

All of Washington State is included in the Pacific Flyway that is traversed by migratory birds. (US Fish and Wildlife Service, 2020)

d. Proposed measures to preserve or enhance wildlife, if any:

The City maintains critical area regulations protecting fish and wildlife habitat conservation areas. Though none are mapped in the study area, if some were identified they would be protected under either the No Action or Planned Action Alternatives. (LMC Chapter 14.154)

e. List any invasive animal species known to be on or near the site.

No known invasive animal species have been identified in the study area.

6. ENERGY AND NATURAL RESOURCES

a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.

Lakeview Light and Power provides electrical service to the subarea and Puget Sound Energy provides natural gas service. Power and natural gas are available and are anticipated to serve future growth under either the No Action or Planned Action Alternatives.

b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe.

Heights range by zone from 60 to 90 feet but are lower at boundaries with lower intensity residential zones and should allow for solar energy use by adjacent uses (see LMC 18A.60.100). With the Form Based Code, heights would vary based on building type, and no building typologies would exceed current zoning and may be lower. For example, a townhouse/rowhouse style building would have a height more like three stories. Pedestrian areas in commercial zones must be designed with solar exposure in mind. (LMC 18A.70.040)

c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:

All future development would be subject to the State Energy Code. (LMC Chapter 15.05)

7. ENVIRONMENTAL HEALTH

a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe.

1) Describe any known or possible contamination at the site from present or past uses.

Exhibit 8 shows sites of known contamination in the LSDS. Apart from a dry-cleaning site located north of Pacific Highway and south of St. Clare Hospital, the sites are located between Pacific Highway and I-5. Contamination from petroleum products, metals, solvents, and lead is the result of current or past activities on these sites.² Clean-up is started on all the sites except the Flying B #18 site. Sites are in various stages of the clean-up process and subject to Washington State's Model Toxics Control Act.³

² Specific information on each site and the clean-up efforts that are underway can be found at: <https://ecology.wa.gov/Spills-Cleanup/Contamination-cleanup/Cleanup-sites/Locate-contaminated-sites>.

³ RCW 70.105D.

Exhibit 8. Environmental Clean-up Sites in the LDS and Surrounding Area



Source: BERK, 2020; Pierce County Assessor, 2020.

2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity.

The presence of I-5 is also a potential environmental health risk for those living in the subarea according to the Centers for Disease Control and Prevention.⁴ Those living within about 500 feet of a major highway have increased risk for exposure to carbon monoxide, nitrogen dioxide, ozone, and particulates. There are a variety of potential health impacts including increased incidences of asthma and lung disease, cardiovascular disease, cancer, and adverse birth outcomes. Children, older adults, households with low incomes, and those with underlying health conditions may be at higher risk for impacts.

⁴ <https://ephtracking.cdc.gov/showProximityToHighways.action>

3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project.

Local businesses such as auto sales or service operations may store toxic or hazardous chemicals but are regulated by local development regulations and state laws.

4) Describe special emergency services that might be required.

No special emergency services are anticipated. Commercial businesses would require City review of building and fire codes, and inspections by West Pierce Fire and Rescue.

5) Proposed measures to reduce or control environmental health hazards, if any:

The State Model Toxics Control Act (MTCA) sets standards for cleanup of lower levels of contaminants that are incorporated into new development and redevelopment parcels noted to have contamination potential. The City applies relevant standards regarding hazardous materials handling in the International Fire Code and Zoning Codes. Compliance with the Lakewood Municipal Code including aquifer protection regulations also addresses hazardous materials storage and handling.

It is recommended that the Planned Action Ordinance incorporate the following mitigation measure:

- The potential for airborne toxics from high volume roads like I-5 can be mitigated through site and building design and by increasing access to transit and non-motorized transportation options. Residential uses could be located 500 feet from the centerline of I-5 at a minimum.
- Applicants for development shall conduct a site assessment to determine if contamination is present from past use.

b. Noise

1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

I-5 is a primary source of traffic noise, and to a lesser extent traffic on Pacific Highway. Sounder Station operations also contribute to transportation noise sources. JBLM military operations result in aircraft noise.

2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

New residential and employment growth could contribute traffic trips and add cumulatively to noise. However, locating new housing and jobs in proximity to the Sounder Station could allow for more transit usage.

Land use could locate in proximity to noise sources, including residential uses, exposing population to noise.

3) Proposed measures to reduce or control noise impacts, if any:

LMC Chapter 8.36 Noise Control applies to nuisances under both studied alternatives.

Under the Planned Action, the following are proposed:

- Limiting residential uses within 500 feet of I-5 centerline to address air quality concerns could also be beneficial for noise reduction.

- Within Lakewood Landing, measures to reduce noise include: water features, carefully placed structures, and gabion walls.

8. LAND AND SHORELINE USE

a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe.

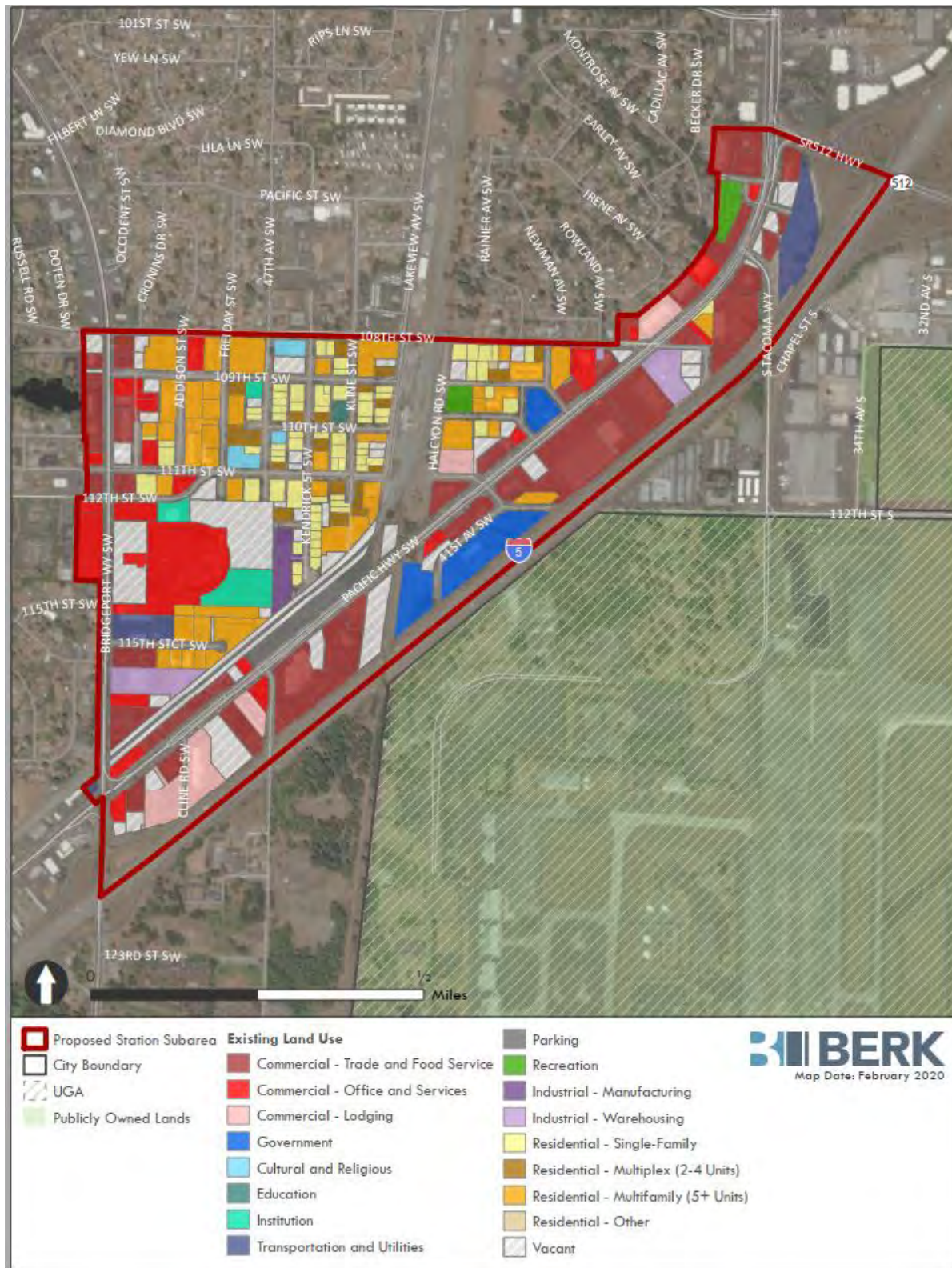
Most of the study area is in commercial use, followed by multifamily and single family residential combined, and vacant lands. See Exhibit 9 and Exhibit 10.

Exhibit 9. Current Land Use and Acres

Category	Acres	Share
Commercial - Food Service	8.7	4%
Commercial - Lodging	9.0	4%
Commercial - Office	4.5	2%
Commercial - Retail/Wholesale Trade	45.5	22%
Commercial - Service	10.5	5%
Cultural and Religious	1.9	1%
Education	0.5	0%
Government	9.0	4%
Industrial - Manufacturing	2.5	1%
Industrial - Warehousing	5.7	3%
Institution	13.9	7%
Parking	3.5	2%
Recreation*	2.7	1%
Residential - Multifamily (5+ Units)	29.4	14%
Residential - Multiplex (2-4 Units)	5.3	3%
Residential - Other	0.1	<1%
Residential - Single-Family	15.6	7%
Transportation and Utilities*	6.9	3%
Vacant	35.7	17%
Grand Total	211.0	100%

This includes a RV park at Halcyon that could be reclassified as multifamily. At 109th is Water District Well and should be Utility though it provides open space values.

Exhibit 10. Existing Land Use



Note: Recreation includes RV park and recreation activities.

Source: BERK, 2020; Pierce County Assessor, 2020.

b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use?

The site is urban and not used for agriculture. The area is not designated as a resource land of long-term significance.

1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how:

Not applicable. The study area is in the city limits and contains and is surrounded by urban lands.

c. Describe any structures on the site.

The sites contain residential and commercial structures. Commercial structures apart from the hospital and lodging are generally one or two stories in height. The hospital is 2-3 stories in height. The Candlewood Hotel is 3 stories. The Sounder Station Garage extends 4 stories. Residential structures vary in height but are generally less than three stories. See Exhibit 11.

Exhibit 11. Description of Residential Structures in Study Area

Type of Residential Structures	Sum of Units
1 1/2 Story Fin	14
1 Story	68
2 Story	4
Addon Only Res	0
Apartment <= 3 Stories	20
Detached Garage	0
Duplex 1 1/2 Story	2
Duplex One Story	20
Duplex Two Story	10
Home For the Elderly	5
Mixed Retail w/ Res Units	15
Multiple - Residential	662
Triplex One Story	3
Triplex Two Story	3
RV Park: 1 Apartment, 11 Mobile Homes & 26 RV Spaces	38
Grand Total	864

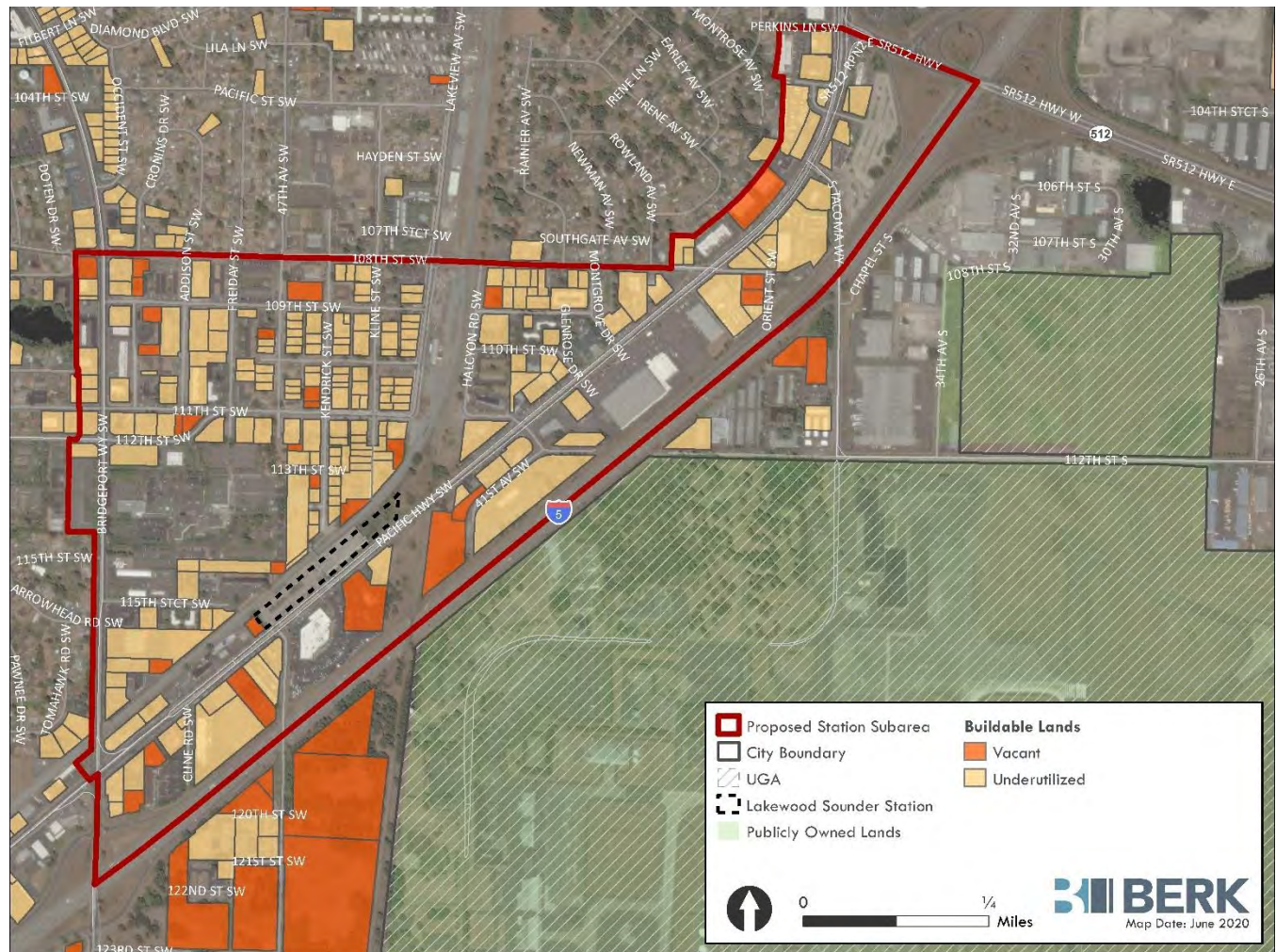
Source: Pierce County Assessor, BERK 2020.

d. Will any structures be demolished? If so, what?

Structures may be demolished and replaced by new residential and commercial development.

Sites most likely to change were identified by a 2014 Buildable Lands Report prepare by Pierce County in conjunction with cities. See Exhibit 12.

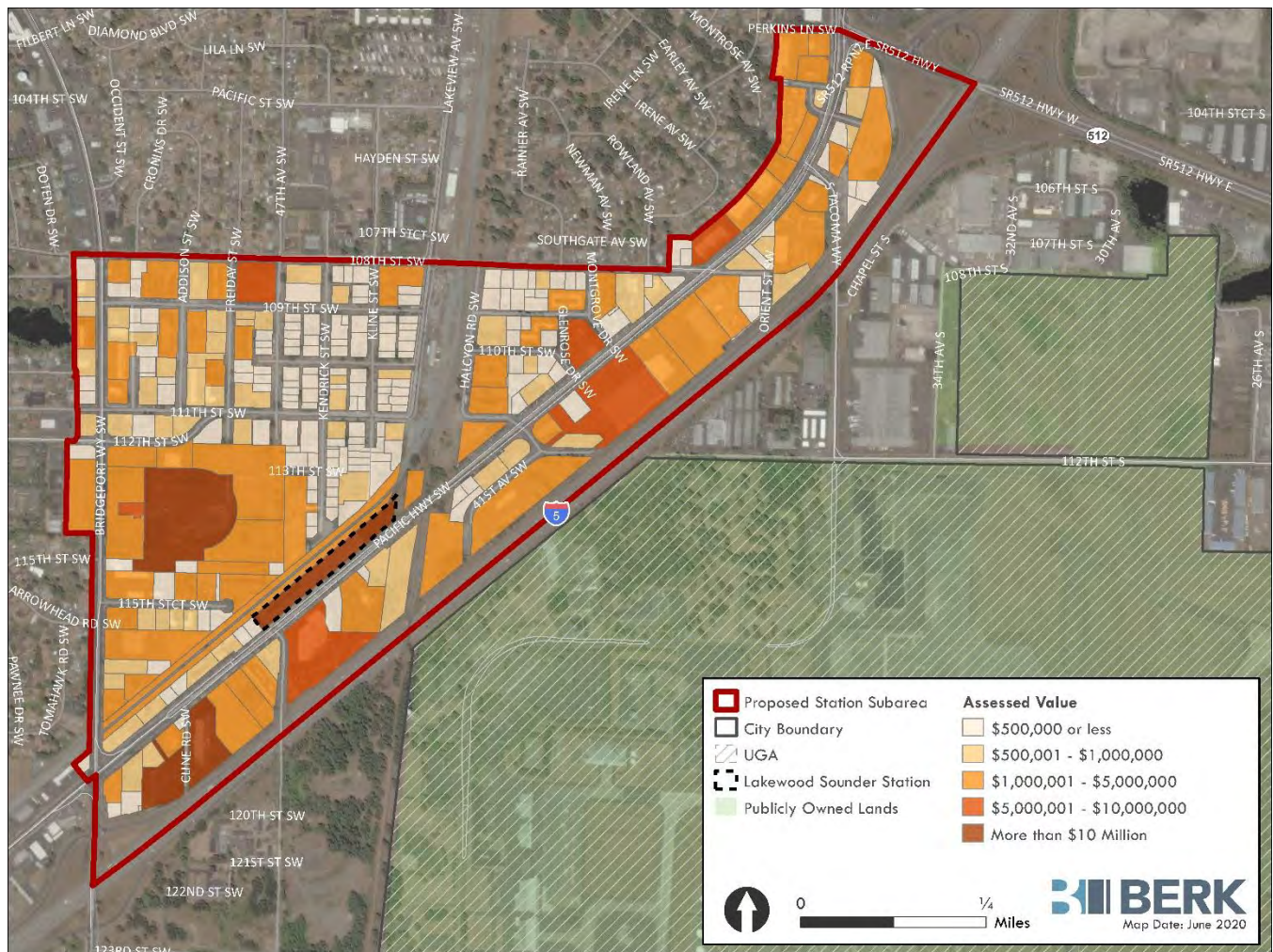
Exhibit 12. Buildable Lands: Vacant and Underutilized Properties



Source: Pierce County 2014, BERK 2020.

While the buildable lands data relies on older 2010 information published in 2014, the information is still relatively intact reviewing building to land values. Where land values are relatively equal or higher than building values, changes to land use are more likely. See Exhibit 13.

Exhibit 13. Building to Land Values



Source: Pierce County 2014, BERK 2020.

e. What is the current zoning classification of the site?

Most of the study area is zoned Transit-Oriented Commercial (TOC), Public Institutional, Multifamily 3 (MF3) and Commercial 2 (C2). See Exhibit 14. See also 12.a below.

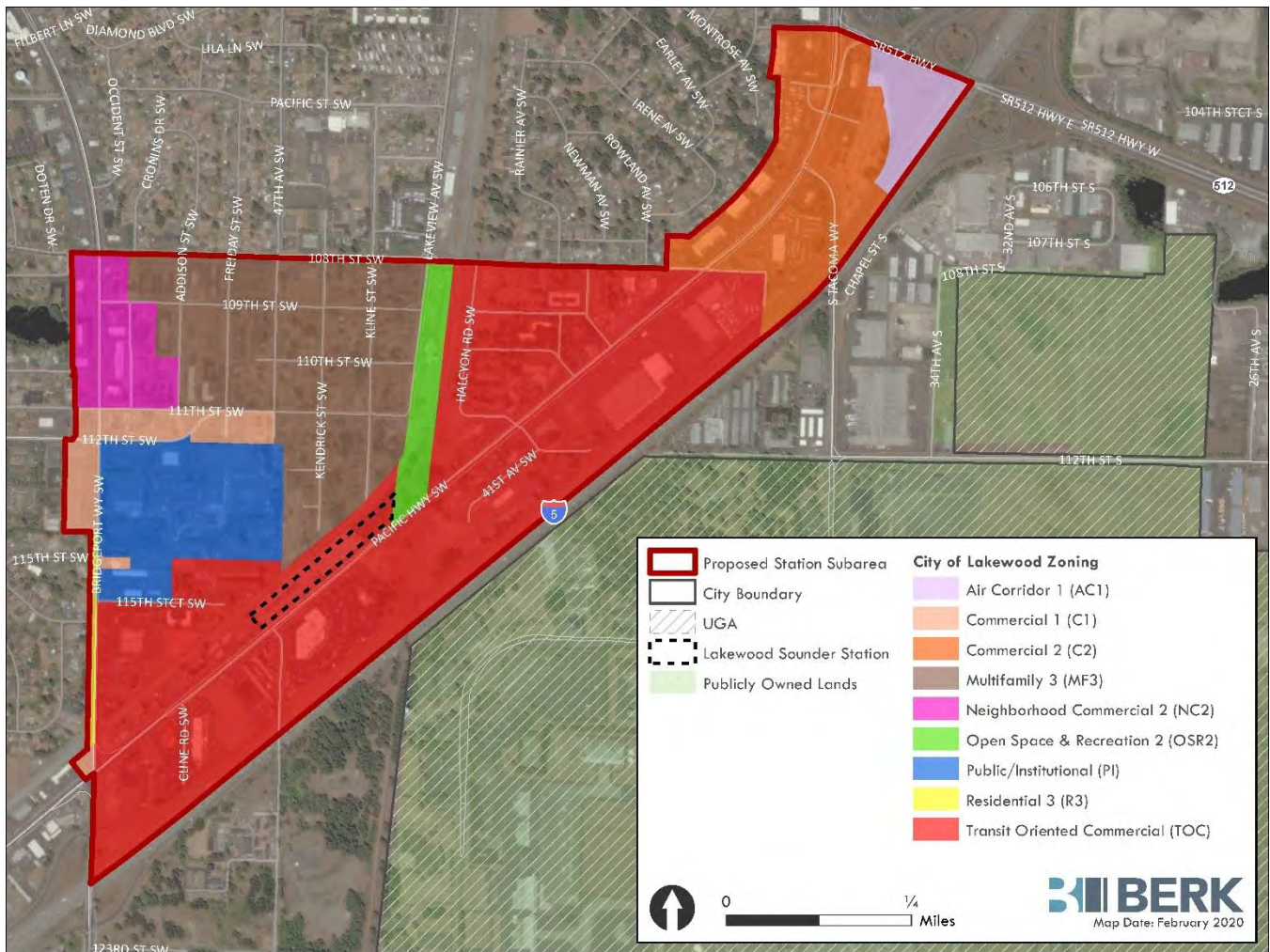
Exhibit 14. Current Zoning Acres

Zone District	Acres
Air Corridor 1	0.3
Commercial 1	7.9
Commercial 2	25.1
Multifamily 3	41.1
Neighborhood Commercial 2	10.6
Open Space & Recreation 2	0.6

Zone District	Acres
Public Institutional	46.0
Transit Oriented Commercial	99.5
Grand Total	231.1

Source: City of Lakewood, BERK 2020.

Exhibit 15. Current Zoning Map



Source: City of Lakewood, BERK 2020.

Under the Planned Action, typologies for buildings would be allowed based on a regulating plan that varies street typologies. See Exhibit 16 for the Station District Regulating Plan. With the Station District Form Based Code, building typologies would be defined; see Exhibit 17. A townhouse/rowhouse style building typology is added compared to Downtown.

Exhibit 16. Draft Planned Action Regulating Plan – Station District



Source: Framework 2021.

Exhibit 17. Form-Based Code Frontage Types – Station District

Street type	Sidewalk width	Linear	Forecourt	Plaza	Landscape	Porch /Stoop /Terrace	Parking
Mixed-Use Street	10' minimum	P	P	P/R1	P	P	X
Arterial Mixed-Use Street	As determined by Public Works	P	P	P	P	P	P
Residential Street	8' minimum	X	P	X	P	P	X
Green Street	8' minimum	X	P	X	P	P	X
Plaza Street	12' minimum or shared street	P	P	P	P	X	X

P=permitted, X=prohibited, R=required

1 Required when on a corner lot.

Description

Linear. The linear building frontage has zero setback from the street edge and is the primary frontage type on retail streets and is also appropriate for land uses such as townhouses and row houses.

Forecourt. The forecourt building frontage type has an open area at the entrance along the street edge. This building type is applicable to a wide range of land use types and mixed-use development.

Plaza. The plaza frontage type includes a pedestrian-oriented plaza between the building and the street edge. The plaza frontage type is applicable to retail and dining uses and can support activities such as outdoor dining, public art displays, seating, entertainment, and events.

Landscape. The landscape frontage has landscaping between the building and street edge. The landscape frontage is not permitted on retail streets and is appropriate for office and residential uses particularly when on the ground floor.

Porch/Stoop/Terrace. This frontage type has the building set back from the street edge to accommodate a porch, stoop, or terrace to serve as the primary access to the building. This frontage type is applicable primarily to residential and nonretail commercial uses and is not permitted on retail streets.

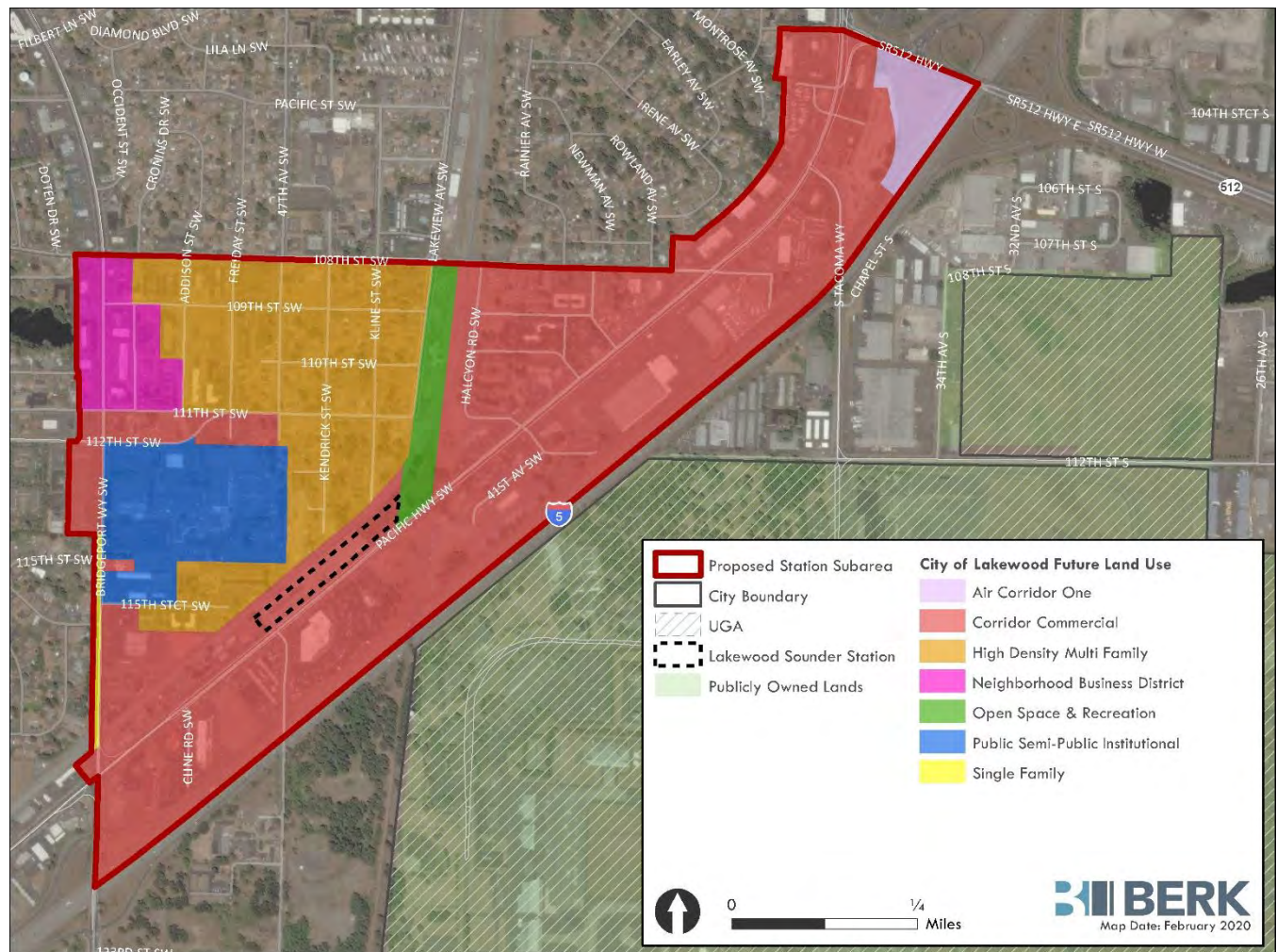
Parking. The parking frontage type is only permitted on arterial mixed-use streets and is the only frontage type where parking is allowed between the building and the street edge. Landscape and trees are required in the landscaped area between the street edge and the parking area.

The proposed code encourages the use of alleys to provide secondary access to properties, de-emphasize parking lots, and to promote continuous building frontages. Alleys shall meet Public Works engineering standards. Along Occidental, a special designation for Plaza Street identifies acceptable development frontages similar to Mixed Use Streets.

f. What is the current comprehensive plan designation of the site?

Most of the study area is designated as Corridor Commercial, or High Density Multi Family. There are smaller areas of Neighborhood Business District and Public Semi-Public Institutional. See Exhibit 18. See also 12.a below.

Exhibit 18. Current Comprehensive Plan



g. If applicable, what is the current shoreline master program designation of the site?

There are no waterbodies or designated shorelines in the study area.

h. Has any part of the site been classified as a critical area by the city or county? If so, specify.

The area is considered an aquifer recharge area and contains a 500-year floodplain. There are no mapped wetlands or streams or geologic hazards. See Sections B1 and B3. There are no mapped threatened and endangered plants or animals; see Sections B.4 and B.5.

i. Approximately how many people would reside or work in the completed project?

Growth capacity ranges from 1,345 to 1,780 dwellings. There would be at least 1,190 to 1,280 jobs rounded, with a few thousand commercial jobs likely in later phases of the Lakewood Landing site. The lower range

represents the No Action Alternative and the Planned Action represents the higher range. Growth assumptions are similar to growth capacity though slightly lower to test a little less than maximum capacity (e.g. midrange density in amended MF3 zone). See Exhibit 19.

Population associated with 1,722 households under the Planned Action would equal about 4,047 persons assume 2.35 persons per household. Under the No Action, the expected population would equal 2,261.

Exhibit 19. Study Area Growth Capacity and Assumptions

	No Action Alternative	Planned Action Alternative
Growth Capacity	<ul style="list-style-type: none"> ■ The 2014 Buildable Lands Report indicated a low capacity of 258 new dwelling units and 1,186 jobs. <ul style="list-style-type: none"> ○ Application of 2014 Buildable Lands Report zone-based assumptions to underutilized and vacant lands shows a potential for up to 1,343 new dwelling units. 	<ul style="list-style-type: none"> ■ If achieving up to 40 units per acre in the MF3 zoned area and 54 units per acre in mixed use zones, capacity for residential would be about 1,019 new units. Lakewood Landing would result in another 760 planned units or a total of 1,779 dwellings. ■ Job capacity would equal 1,186 jobs, plus Lakewood Landing jobs. Early phase includes retail jobs (approximately 171). Later phases will be determined but could include several thousand office jobs.
Growth Assumptions	<ul style="list-style-type: none"> ■ Transportation Model 2010-2035: <ul style="list-style-type: none"> ○ Households Distributed per Traffic Model: 962 ○ Jobs Distributed per Traffic Model: 1,193 	<ul style="list-style-type: none"> ■ Transportation Model 2010-2035: <ul style="list-style-type: none"> ○ Households Distributed per Buildable Land Capacity Shares illustrating middle density distribution across MF3 zone (35 units per acre average): $962 + 760 = 1,722$ ○ Jobs: 1,105 plus 171 Lakewood Landing = 1,276

j. Approximately how many people would the completed project displace?

The study area has portions considered at moderate and high displacement risk. (Puget Sound Regional Council, 2019)⁵ Residents and businesses may be priced out of the area as new development occurs.

k. Proposed measures to avoid or reduce displacement impacts, if any:

There is sufficient growth capacity to replace existing job space as sites redevelop. The City can promote development standards that allow for commercial space in mixed use areas to maintain affordable rents (e.g. adjust parking ratios for businesses less than a certain size similar to Downtown).

There is sufficient capacity to replace existing housing as the sites redevelop. The City does provide inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing in Chapter 18A.90

⁵ According to PSRC, displacement risk is a composite of indicators representing five elements of neighborhood displacement risks: socio-demographics, transportation qualities, neighborhood characteristics, housing, and civic engagement.

Housing Incentives Program.

The intent of the Planned Action is to create more incentives for missing middle ownership housing such as townhomes/rowhouses. The City could add the area to its multi-family tax exemption for affordable housing in LMC Chapter 3.64 Tax Incentive Urban Use Center Development.

l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

The No Action Alternative would implement Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. The Planned Action would implement similar landscaping and tree presentation standards and custom design standards for the buildings and public realm in a hybrid form-based code.

Under the Planned Action, rowhouses/townhouses (e.g. 35 feet focusing on building types such as duplexes, triplexes or rowhouses in the MF3 zone would be less disruptive for existing single family residents that are located in pockets of the zone.

m. Proposed measures to reduce or control impacts to agricultural and forest lands of long-term commercial significance, if any:

None required.

9. HOUSING

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.

Currently there are 864 dwellings; see Exhibit 11. Growth capacity ranges from 1,345 to 1,780 dwellings. See Exhibit 19.

b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

About 159 of the total permanent dwellings and RVs could be displaced due to redevelopment on underutilized lands. The Oakknoll Trailer Park is on 1.7 acres and would not meet all the criteria of LMC 18A.40.110 due to its acreage. It may not meet space per unit, open space, and other standards.

Exhibit 20. Units and Potential for Displacement

Unit Type	Units on Vacant and Underutilized Land
Comm Land With SFR	1
Duplex 2 Units	14
Fourplex 4 Units	3
Multi Fam Apts 5 Units Or More	26
Other Residential	1
Single Family Dwelling	71
Triplex 3 Units	5

Grand Total	121
Grand Total With RV and Mobile Homes	159

Source: Pierce County Assessor, BERK 2020.

c. Proposed measures to reduce or control housing impacts, if any:

The draft subarea plan includes strategies to reduce displacement effects. There is sufficient capacity to replace existing housing as the sites redevelop. The City does provide inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing in Chapter 18A.90 Housing Incentives Program. The City could also implement RCW 36.70A.545 which allows for an increased density bonus for affordable housing located on property owned by a religious organization. The City's program in 18A.90 could be adapted to this recent state law, which requires that 100% of the units be affordable.

The intent of the Planned Action is to create more incentives for missing middle ownership housing such as townhomes/rowhouses. The City could add the study area to its multi-family tax exemption for affordable housing in LMC Chapter 3.64 Tax Incentive Urban Use Center Development.

Based on recent state laws, tiny homes and recreational vehicles must be allowed as a primary residence. (See ESSB 5183 (2019)). RCW 35.21.684, allows for tiny homes in manufactured home parks, and allows local jurisdictions more flexibility to authorize them in other zones). The City will implement strategies in the Subarea Plan largely through the Form-Based Code, but will also determine implementation through its code docketing process.

10. AESTHETICS

a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

Heights range by zone from 60 to 90 feet but are lower at boundaries with lower intensity residential zones (see LMC 18A.60.100). With the Form Based Code, building typologies would be similar to those applied in Downtown. Heights would vary based on building type, and no building typologies would exceed current zoning standards and may be lower. For example, a townhouse/rowhouse style building would have a height more like three stories.

Exhibit 21. Current Height by Zone

	MF3	NC2	TOC	CBD	C1	C2
Building height	80 feet	60 feet	90 feet	90 feet	60 feet	60 feet

Source: Lakewood Municipal Code, Title 18, BERK 2020.

b. What views in the immediate vicinity would be altered or obstructed?

There are no protected views in the study area. Greater building heights could result in view of Mount Rainier.

c. Proposed measures to reduce or control aesthetic impacts, if any:

The No Action Alternative would implement Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. The Planned Action would implement similar landscaping and tree presentation standards and custom design standards for the buildings and public realm in a hybrid form-based code.

Under the Planned Action, promoting MF3 building heights around three stories/35 feet, and focusing on building types such as duplexes, triplexes or rowhouses is less disruptive for existing single family residents.

11. LIGHT AND GLARE

a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

Future development allowed in the study area would add interior and exterior lights to buildings and parking lots to address night time visibility.

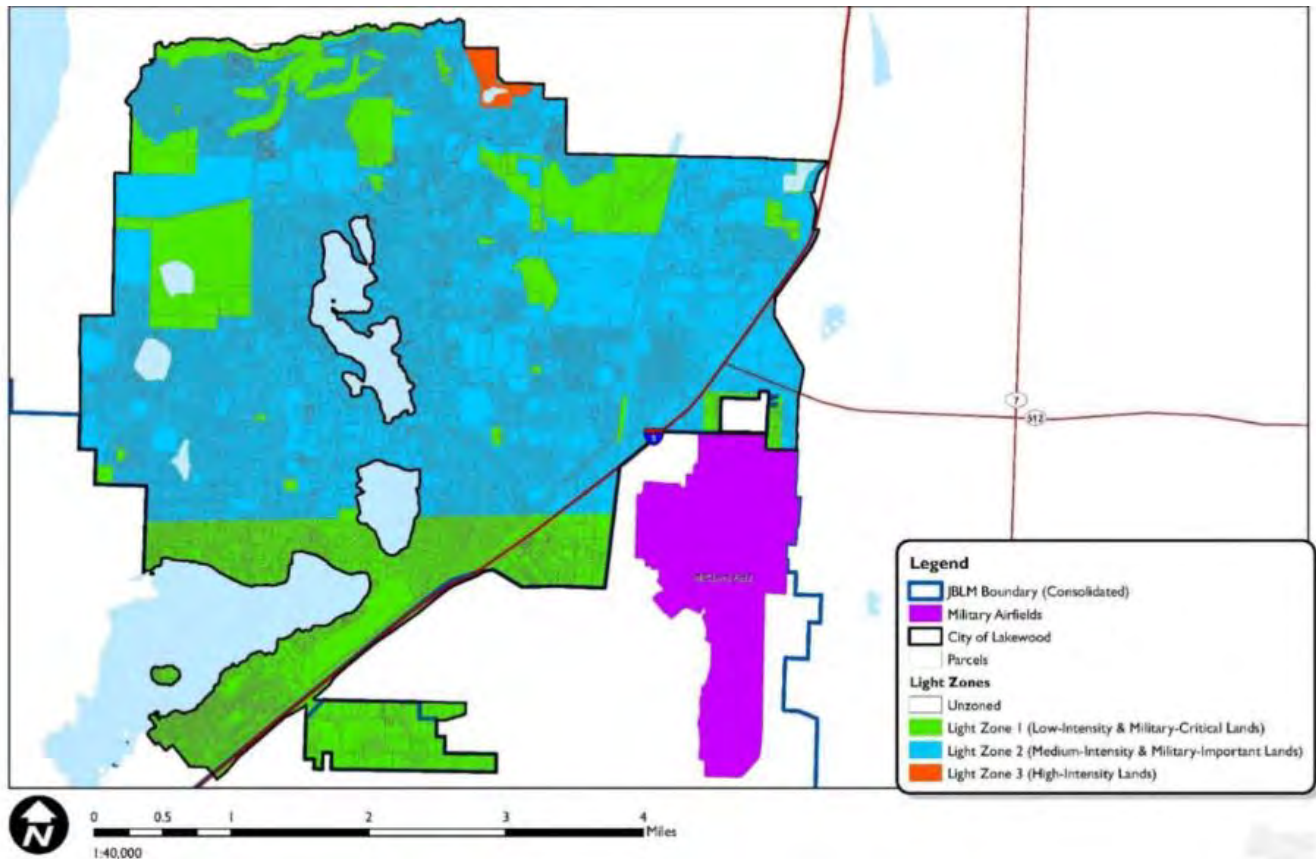
b. Could light or glare from the finished project be a safety hazard or interfere with views?

Impacts are not anticipated from typical residential and commercial uses, which are regulated by City lighting standards.

The subarea is subject to lighting standards and requirements to prevent interference with aircraft operation at JBLM.⁶ The northern portion of the subarea, near the interchange with SR-512 is part of Light Zone 2 and the rest of the subarea is in Light Zone 1 (Exhibit 22). Light Zone 1 is applicable to low-intensity land uses with low levels of exterior lighting at night. Light Zone 2 applies to medium intensity uses with model levels of exterior lighting such as residential, mixed-use, and commercial areas. Lighting standards are also intended to reduce light pollution, conserve energy, and provide safety and security. Generally, the code requires lighting to be shielded to prevent light shining above the luminary and to prevent light spill over on to adjacent properties.

⁶ 18A.60.095

Exhibit 22. Lakewood Light Zones



Source: Lakewood Municipal Code, 18A.60.095

c. What existing off-site sources of light or glare may affect your proposal?

Highways and streets abutting the study area like I-5 produce light.

d. Proposed measures to reduce or control light and glare impacts, if any:

LMC 18A.60.095 Outdoor Lighting Code provides lighting standards to reduce light and glare and to avoid interference with JBLM operations.

12. RECREATION

a. What designated and informal recreational opportunities are in the immediate vicinity?

Early conceptions of the subarea included the development of a linear park along the railway tracks on land now owned by Sound Transit. See Exhibit 23.

The Open Space designation/zone may be modified to be in a Public/Institutional designation and zone as the railway tracks where the land is owned by Sound Transit and not likely to become a dedicated off-street open space. However, there are opportunities for linear parks/trails.

Exhibit 23. Lakewood Station District Subarea Concept

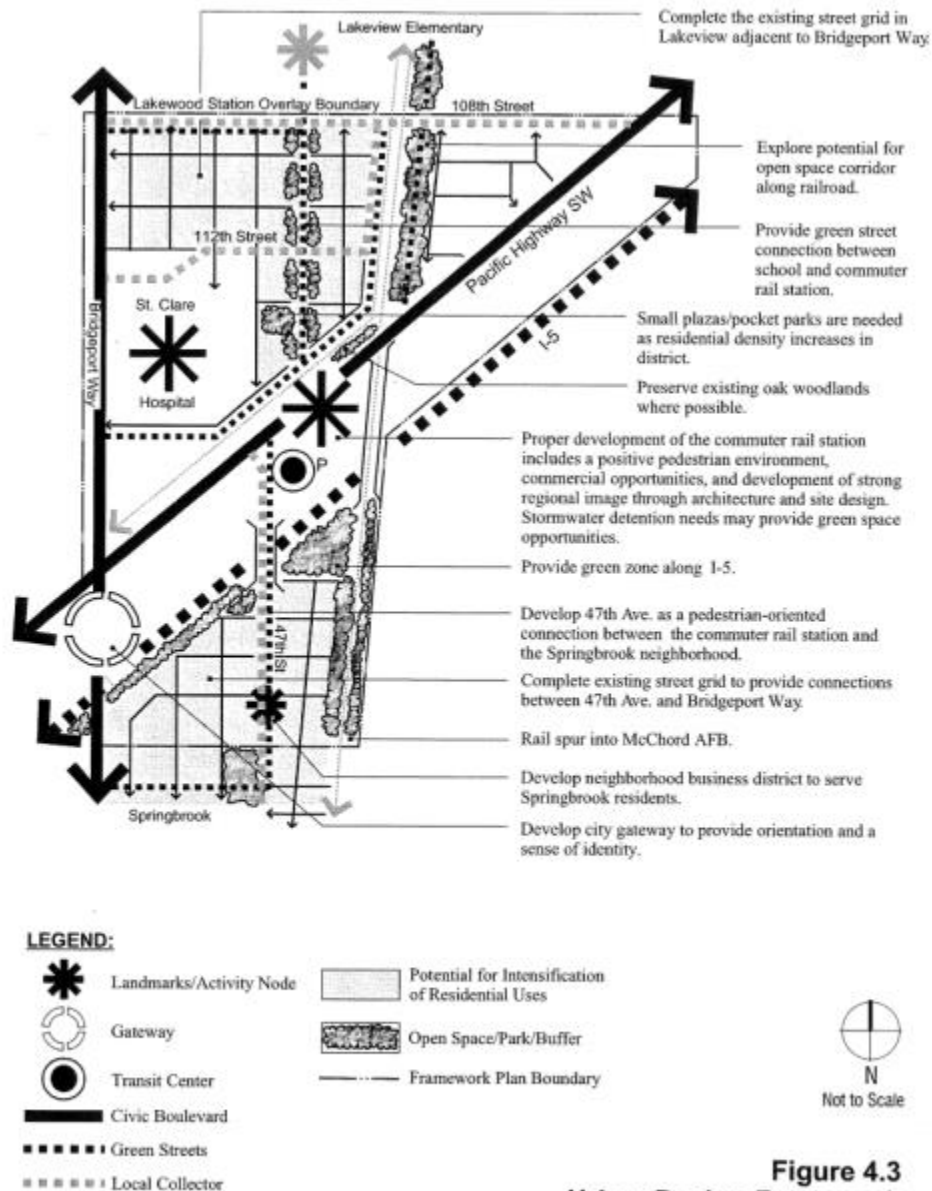


Figure 4.3
Urban Design Framework
for Lakewood Station District

SOURCE: EDAW, Inc. 1999

Source: Lakewood Comprehensive Plan, 2014.

b. Would the proposed project displace any existing recreational uses? If so, describe.

No parks exist or would be displaced. The area is beyond a 10-minute walk of parks per the City's Legacy Parks Plan 2020 (Figure 3-14).⁷ The City's plan calls for a park in or near the Lakewood Station District. With planned sidewalk improvements (see B.14) and street frontage improvements there is an opportunity to provide for linear parks.

The Planned Action proposes onsite open space standards for multifamily and commercial uses similar to the

⁷ See: <https://cityoflakewood.us/wp-content/uploads/2020/05/FINAL-Legacy-Plan-2020-w-pg-numbers-flattened-1.pdf>.

Downtown. The Subarea Plan includes the following public space concepts:

- Lakewood Landing will include a landscaped public plaza in its first phase to support community gathering and provide urban open space.
- A linear park along Kendrick Street south of 111th Avenue SW that would bring green space to the heart of the district's residential area. This would better define the pedestrian connection between Lakewood Station and the Lakeview Hope Academy and facilitate the use of the playground there as a neighborhood park as envisioned in the *Legacy Plan 2020*.
- Green corridors with enhanced landscaping in areas with planned pedestrian and bicycle improvements such as 111th /112th Avenue SW.
- A pocket of open space in the alley near Occidental Street.
- A linear park connecting Lakewood Station to the Springbrook Area through improvements along 47th Avenue as envisioned in the *Legacy Plan 2020*.

Public and semi-private open spaces will help to balance the intensity of land use in the LSDS. This could include the development of plazas, pocket parks, courtyards, or other landscaped open spaces that would provide opportunities for gathering and passive recreation.

c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

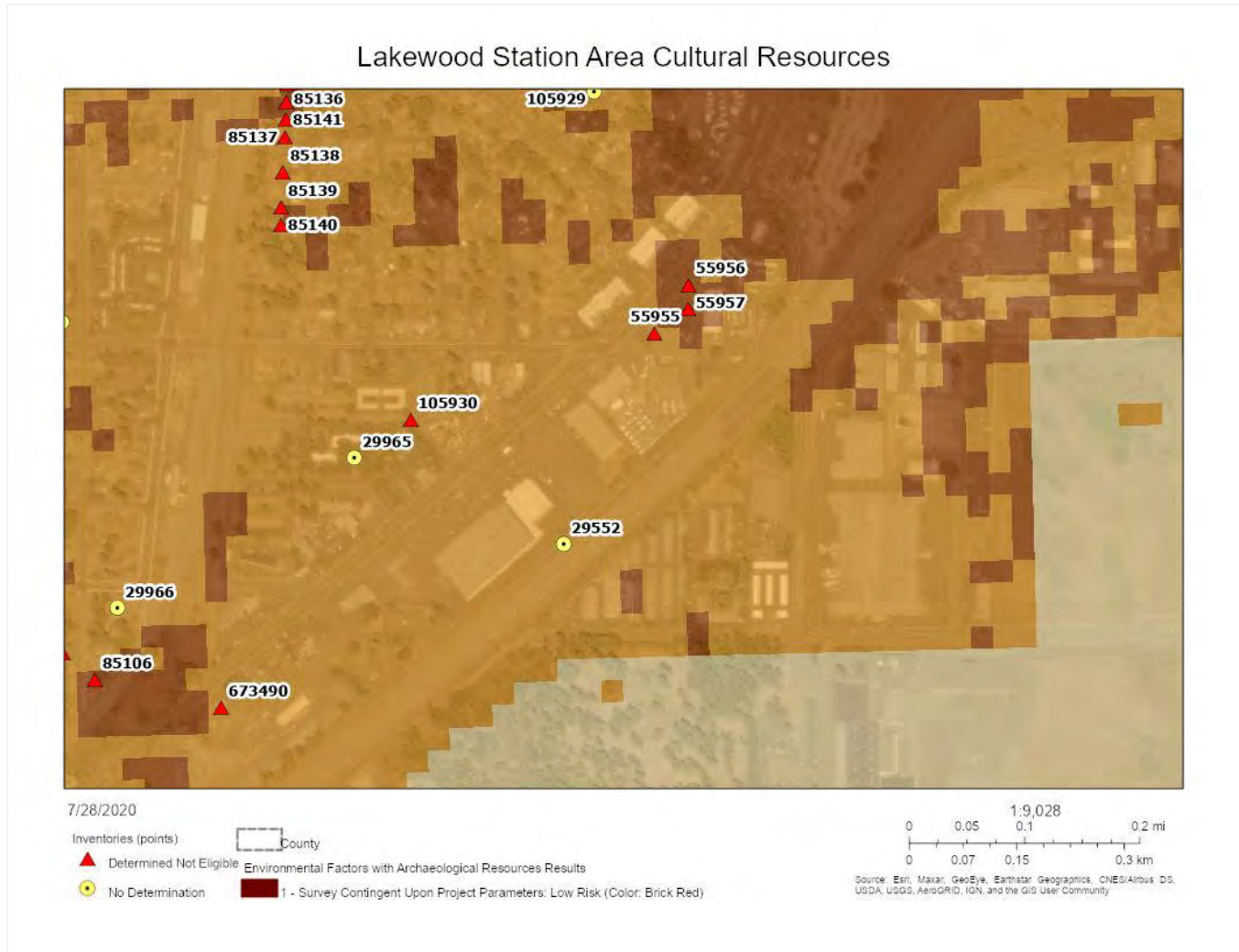
Under the Planned Action there would be application of common and private open space standards. Green Streets and other public spaces listed in B.12.b above are identified for enhanced pedestrian and landscaping experience. The form-based code and subarea plan include cross sections for streets that fit into current rights of way.

13. HISTORIC AND CULTURAL PRESERVATION

a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers? If so, specifically describe.

No structures are determined as eligible, and some are undetermined according to the Department of Archaeology and Historic Preservation (DAHP). See Exhibit 24. There are several properties that have structures that are over 45 years old. See Exhibit 25.

Exhibit 24. Department of Archaeology and Historic Preservation Historic and Cultural Resources

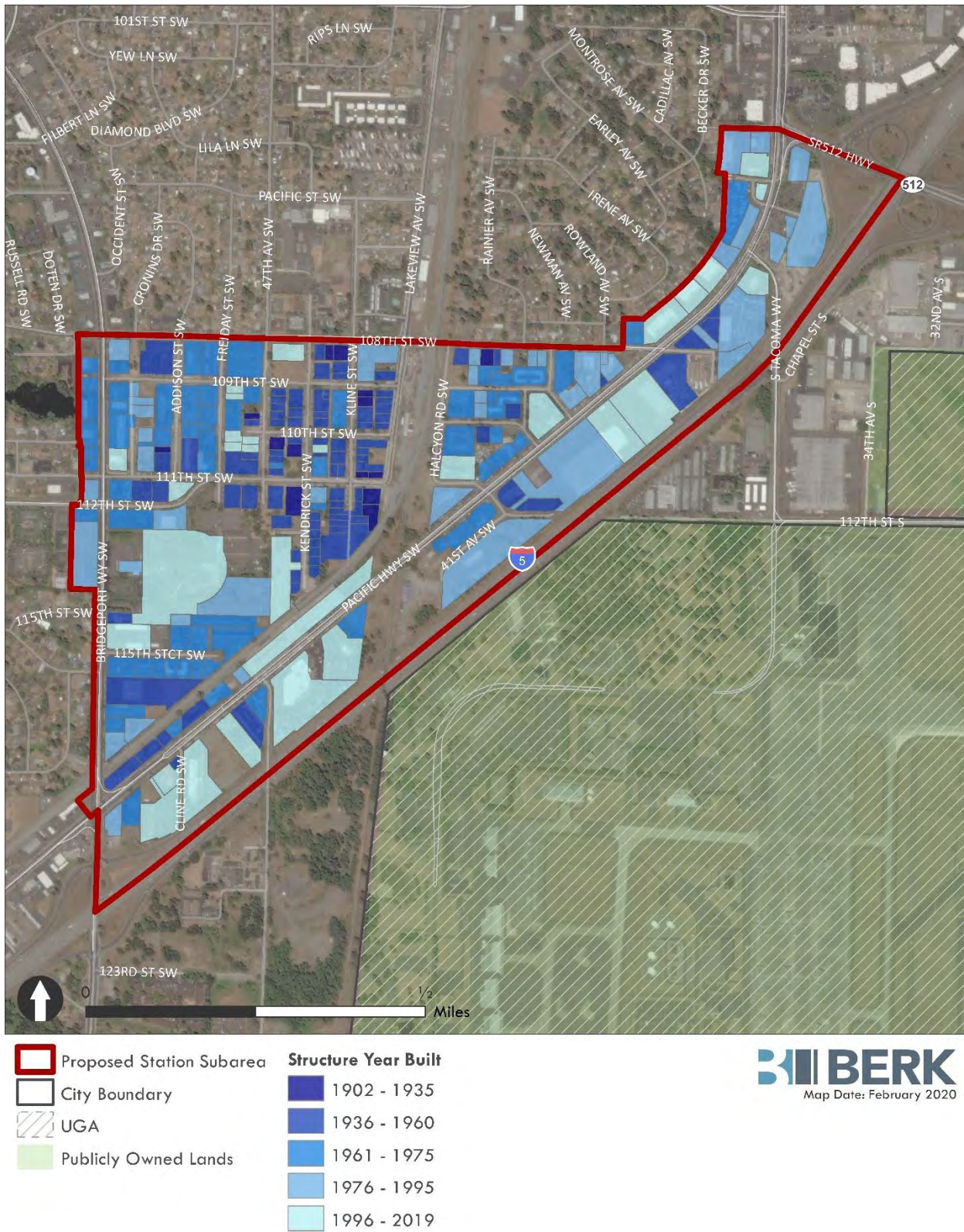


Source: Department of Archaeology and Historic Preservation, WISAARD, 2020.

Additional Legend:

- 1 - Survey Contingent Upon Project Parameters: Low Risk (Color: Brick Red)
- 2 - Survey Contingent Upon Project Parameters: Moderately Low Risk (Color: Burnt Orange)
- 3 - Survey Recommended: Moderate Risk (Color: Orange)
- 4 - Survey Highly Advised: High Risk (Color: Pale Yellow)
- 5 - Survey Highly Advised: Very High Risk (Color: Brightest Yellow/Canary Yellow)

Exhibit 25. Age of Buildings



Source: Pierce County Assessor, BERK 2020.

b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources.

There are areas of low to moderate risk of finding cultural resources per Exhibit 24.

c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc.

Methods include review of Pierce County Assessor Records, DAHP WISAARD program, and Lakewood's local register review.

d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required.

Washington State has a number of laws that oversee the protection and proper excavation of archaeological sites (RCW 27.53, WAC 25-48), human remains (RCW 27.44), and historic cemeteries or graves (RCW 68.60). The Governor's Executive Order 05-05 requires state agencies to integrate DAHP, the Governor's Office of Indian Affairs, and concerned tribes into their capital project planning process. This executive order affects any capital construction projects and any land acquisitions for purposes of capital construction not undergoing Section 106 review under the National Historic Preservation Act of 1966.

Under RCW 27.53, DAHP regulates the treatment of archaeological sites on both public and private lands and has the authority to require specific treatment of archaeological resources. All precontact resources or sites are protected, regardless of their significance or eligibility for local, state, or national registers. Historic archaeological resources or sites are protected unless DAHP has made a determination of "not-eligible" for listing on the state and national registers.

The City applies Lakewood Municipal Code Chapter 2.48 Protection and Preservation of Landmarks.

It is proposed that the following mitigation measures be made part of the Planned Action Ordinance in order to be considered Planned Action. This would include:

- Include Inadvertent Human Remains Discovery Language recommended by DAHP as a condition of project approval.

14. TRANSPORTATION

a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any.

Major roadways in the subarea include Pacific Highway, 108th Street SW, and Bridgeport Way. These are classified as either principal or minor arterials with 35 mph posted speed limits. Pacific Highway provides a north-south connection between Tacoma and Lakewood, with access to 1-5 ramps and the Lakewood Station within the subarea. See Exhibit 26.

Exhibit 26. Road Classifications in Study Area and Vicinity



Source: City of Lakewood, Fehr & Peers 2020.

b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop?

Lakewood Station is a focal point of many regional commuter trips including service to downtown Seattle via the Sounder train in addition to bus service to local and regional cities operated by Sound Transit and Intercity Transit. The SR 512 Park & Ride provides bus connections for local and regional commuters to Lakewood Town Center, Tacoma, Puyallup, SeaTac, Dupont, and Olympia and communities throughout the South Sound. Pierce Transit, Sound Transit, and Intercity Transit provide bus service to the SR 512 Park & Ride via Pacific Highway and I-5, Bridgeport Way, and 108th Street SW. Both Lakewood Station and SR 512 Park & Ride provide vehicle parking for transit users.

Exhibit 27. Transit Service in Study Area and Vicinity



Source: Pierce Transit, City of Lakewood, Fehr & Peers 2020.

c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate?

There is very little on-street parking on arterials in the study area, but on-street parking exists on several local streets. Commercial uses tend to have dedicated parking lots adjacent to their buildings, frequently buffering the building from the street. St Clare Hospital provides off-street parking at multiple lots on the hospital's campus. Parking intended for transit users is provided at the Lakewood Station garage (600 spaces) and at the SR 512 Park & Ride (493 stalls). During the daytime, the SR 512 Park & Ride is at about 90 percent capacity. Bicycle parking is provided at Lakewood Station garage via bicycle racks and lockers.

Under the No Action Alternative, future uses must comply with the requirements in Chapter 18A.80 LMC. The Planned Action would follow parking standards for residential, retail, and office uses, and otherwise follow LMC Chapter 18A.80 for others.

Exhibit 28. Station District Proposed Specific Parking Standards

LAND USE	PARKING REQUIREMENT: STAFF RECOMMENDATIONS	BICYCLE PARKING
Residential	<p>Single-family: 2 per dwelling unit</p> <p>Accessory dwelling: 1 per dwelling unit, provided that no additional parking is required when located within one-quarter mile of the Sounder Station. (RCW 36.70A.698)</p> <p>Senior citizen apartments 1 per 3 dwelling units*</p> <p>Multifamily housing: 1.25 spaces per dwelling unit*</p> <p>*A process is included to prepare parking study to reduce further near station.</p>	Meet rates and standards of: Chapter 18A.80
Retail, Services, Restaurants	<p>2 per 1,000 GSF minimum;</p> <p>3 per 1,000 GSF maximum</p>	Meet rates and standards of: Chapter 18A.80
Office	<p>2 per 1,000 GSF minimum;</p> <p>3 per 1,000 GSF maximum</p>	Meet rates and standards of: Chapter 18A.80
Street level retail 3,000sq.ft. or less per business	None where there is available public parking within 500' or abutting on-street parking designed to serve street level retail	Meet rates and standards of: Chapter 18A.80

Source: Lakewood Municipal Code, draft 18C Form Based Code.

City staff recommendations are included in the table above. The Planning Commission has recommended no minimum parking for senior citizen or multifamily housing, and proponents would consider parking demand and market forces to determine spaces.

d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private).

Planned Improvements

The Six-Year Comprehensive Transportation Improvement Program (TIP) for 2021-2026 was approved by the Lakewood City Council in June 2020. This document outlines short and long term road projects, including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays, throughout the city. The following projects within the LSDS are identified in the TIP as long term improvements:

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study.
- Kendrick Street SW from 111th Street SW to 108th Street SW – sidewalks, street lighting, bicycle facilities.
- 112th/111th – Bridgeport to Kendrick – sidewalks, bicycle facilities, street lighting.
- 108th – Bridgeport Way to Pacific Highway – roadway patching and overlay.

Sound Transit and Pierce Transit have also identified improvements for the LSDS in their plans. The Sound Transit System Access Fund awarded funds in 2019 to support non-motorized access to Lakewood Station. These include a variety of improvements to support pedestrian and bicycle access on 11th Street SW and 112th Street SW. Pierce Transit's *Destination 2040* plan identifies two new Bus Rapid Transit (BRT) routes that will bring additional transit service to the subarea. A new line in 2026 would connect to Downtown Tacoma. In 2030 a BRT line would replace an existing transit line connecting Lakewood to Puyallup's South Hill.

Specific Mitigation Measures

The City could make signal timing improvements to increase the efficiency of impacted intersections and roadways in the subarea. Exhibit 29 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right-of-way acquisition or widening, rather only changes to signal timing. The exception is intersection of Pacific Highway and Sharondale Street, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better.

Exhibit 29. Proposed Mitigation Measures

ID	Intersection	No Action	Alt 1	Alt 1 Mitigated
4	Pacific Highway / Sharondale Street			
	Install signal using existing channelization and signal length consistent with adjacent intersections	F>100	F>100	A/4
8	Pacific Highway / Bridgeport Way			

ID	Intersection	No Action	Alt 1	Alt 1 Mitigated
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.	D/54	E/78	D/49
11	Bridgeport Way / SB I-5 Ramp			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.	D/41	E/60	D/52

Source: Fehr & Peers, 2020.

With implementation of the potential signal timing mitigation measures outlined in the previous section and future improvements at the Bridgeport Way/I-5 ramps, the magnitude of the impacts could be mitigated such that auto, freight, and transit are not significantly impacted.

Development in the study area would provide information about its expected trips and will provide a traffic study where applicable per LMC 12.09.028. This analysis can affirm the mitigation approach and phasing, such as signal timing at Bridgeport Way/I-5 on and off ramps.

Encouraging bike usage would reduce transportation demand on local roads and state highways. The City could continue applying its required bicycle parking standards by use in 18A.80.030.

Street Standards

Street frontage standards are part of the proposed Title 18C Station District Development Code.

e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

All of Lakewood, including the subarea, is within the Lakewood Military Coordination & Notice Area (MCNA).⁸ Jurisdictions within the MCNA coordinate with JBLM prior to the approval of zoning and Comprehensive Plan amendments. Current zoning has already been subject to MCNA review, but the City notifies JBLM of all land use and building permits, subdivisions, and site plans to provide opportunity for comment.

Parts of Lakewood are also subject to airport compatible land use restrictions. The subarea is not within the most restrictive aircraft safety zones, but is within the Inner Horizontal Surface imaginary surface area for the safe operation of aircraft around JBLM.⁹ JBLM reviews proposed development to determine if the use is prohibited or could interfere with pilot vision, communication, radar, or other elements of safe operation. Typically, uses that produce steam, dust, glare that may impair visual operation, or those that attract birds, are prohibited.

The subarea is bisected by an existing rail line on which Sounder and freight operate. While the rail line is an important regional transit and freight corridor, it has also created an environment with few roadway crossing opportunities which can affect vehicle operations and bicycle and pedestrian mobility.

The City's 2016 Comprehensive Plan identifies designated truck routes for freight as a transportation goal. Designated major truck streets are primary routes for goods movement throughout the City. Designation as a major truck street helps Lakewood's Public Works Transportation division determine street design, traffic management plans, and pavement improvement projects that allow and facilitate the movement of larger vehicles along the designated Way. Bridgeport Way and South Tacoma Way are designated as truck routes in WSDOT's Freight and Good Transportation System (FGTS) 2019 update, as shown in Exhibit 30.¹⁰ In addition to the designated routes, 108th Street SW serves as a de facto freight route for trucks heading northbound on I-5 due to the limited height of the railroad bridge crossing I-5 at JBLM. These trucks exit the freeway at Bridgeport, take 108th to Pacific Highway, and then reenter I-5 at SR-512.

⁸ 18A.10.135.6

⁹ 18A.10.135.10B

¹⁰ <https://www.wsdot.wa.gov/sites/default/files/2006/02/13/washington-freight-and-goods-transportation-system-2019.pdf>

Exhibit 30. Freight and Good Transportation System Routes



Source: WSDOT, 2019; Fehr & Peers, 2020; City of Lakewood, 2020.

f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates?

The net increase between Planned Action and Existing Conditions is 1,950 vehicle trips. Total rounded vehicle trips by scenario tested include:

- Existing – 2,530
- No Action – 3,810
- Planned Action – 4,480

g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe.

This is not applicable. There are no areas in use for agricultural and forest products.

h. Proposed measures to reduce or control transportation impacts, if any:

See “d” above for planned improvements.

Other mitigation includes:

- Washington state Commute Trip Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip reduction plans and work toward meeting their mode share targets through internal programs and monitoring.
- The City of Lakewood included policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote transportation demand management (TDM) strategies.

15. PUBLIC SERVICES

a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe.

Emergency Services

Emergency response services in Lakewood are provided by West Pierce Fire and Rescue and the Lakewood Police Department. St. Clare hospital also supports a range of medical services including a 24 hour emergency room.

Future development would add new residents and employees that could increase the demand for emergency services.

Comprehensive Plan Policy P-5.1 establishes police response time objectives:

PS-5.1: Provide police protection with a three-minute response time for life-threatening emergencies (Priority 1), a six-minute response time for crimes in progress or just completed (Priority 2), and a routine/non-emergency response time of 20 minutes (Priority 3).

The status of response times is unknown.

The department 101 commissioned officers that service the City, and in 2016 responded to 49,569 calls for service. (City of Lakewood, 2020) With this information, an effective level of service can be calculated, resulting in about 1.72 officers per 1,000 residents.

Exhibit 31. Police Services Effective Level of Services Standards

Year	Population	Officers	Officers Per 1,000 Residents
2020	60,030	101	1.68

Source: (City of Lakewood, 2020)

The demand for the number of police officers under the Planned Action is anticipated to be 6.8, and under the No Action Alternative 3.8 officers.

Lakewood has adopted policies setting level of service standards for West Pierce Fire and Rescue (WPFR):

- PS-1.1: Maintain a Washington Surveying and Rating Bureau (or successor agency) rating of ISO Class 3 or better; and
- PS-4.2: Provide a four-minute initial time standard for EMS calls.

WPFR has met the PS-1.1 LOS standard with a class 3 WSRB through 2020.¹¹

WPFR has a turnout time for all incidents of 110 seconds (less than 2 minutes) and a response time of 6 minutes for a first arriving unit with a “first responder” or higher. In 2017, the goal of a 110 second response time was met 70% of the time. The goal of arrival of a unit with first responder or higher level capability at an emergency medical incident was met 83% of the time.¹²

A common effective level of service standard is to look at fire response personnel per 1,000 capita. This helps compare service capabilities over-time and across jurisdictions. Fire suppression personnel are often trained in emergency medical services, and there is overlap in the number of full time equivalents (FTEs) for each activity.

Based on the expected population of the No Action Alternative, there would be a demand for 3.44 firefighters, and 6.15 under the Planned Action.

Exhibit 32. Fire Services Effective Level of Services Standards

Year	District Population	Firefighters / EMT/Medics	Firefighters Per 1,000 Residents
2020	100,000	152 Firefighters/EMTs	1.52

Source: [WPFR Budget 2020](#).

There is no level of service for the St. Clare Hospital. Relevant Comprehensive Plan policies include:

¹¹ See: <https://www.westpierce.org/west-pierce-fire-rescue-maintains-class-3-rating/>.

¹² See: <https://www.westpierce.org/wp-content/uploads/Annual-Report-2017-with-52.33-report.pdf>.

PS-14.7 Encourage the location of medical clinics and services near transit facilities.

Public Schools

Future residents would increase the demand for public schools. Public school services are provided by the Clover Park School District (CPSD). The study area is primarily served by Lakeview Hope Academy Elementary School and to a small degree by Four Heroes Elementary School. Lochburn and Hudtloff Middle Schools also serve the study area. Clover Park High School serves the entire study area.¹³

To estimate student generation, it is also possible to consider the number of households in the district in relation to the number of students. The number of households in the Clover Park School district is 29,700 as of 2019 based on State of Washington Office of Financial Management (OFM) small area estimates. There are 13,789 students in the district as of 2019. Thus, the effective student per household ratio is 0.46.

Under the Planned Action, there would be 792 students in new households, and under the No Action Alternative there would be a potential for 447 new students.

b. Proposed measures to reduce or control direct impacts on public services, if any.

The growth will occur through 2035 and beyond and would contribute taxes to help support services; growth will be incrementally absorbed.

The City addresses public service levels of service in its Capital Facilities Plan Element. The element is updated periodically to ensure that proposed growth and change can be served.

16. UTILITIES

a. Circle utilities currently available at the site:

electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other ____

All power, water, and sewer services are available. Lakeview Light and Power provides electrical service to the subarea and Puget Sound Energy provides natural gas service.

Water service is provided by Lakewood Water District. District policy requires the developer to pay for system improvements related to new development. Depending on the location and intensity of new development in the subarea this may include water main upgrades or line extensions to provide additional capacity or fire flow. Coordination of water line upgrades with the District's 50-year repair and replacement program could provide cost and timing efficiency for redevelopment projects in the subarea. Fire flow requirements would be applied at the time of building and construction permits. See Appendix A.

Wastewater service in Lakewood is provided by Pierce County Public Works. See Appendix A. Most of the sewer pipes were installed in the 1980's (some later) and are primarily 30 inch diameter PVC pipes, which have a 100-year lifetime.

Pierce County coordinates quarterly with the City of Lakewood to discuss upcoming and future projects. A Sewer Improvement Plan was adopted in September 2019, addressing capital facility planning from 2020-2040 and identifying funding for the next six years of capital facility improvements. There are no listed projects within or near the subarea identified in this plan.

¹³ See: <http://apps.schoolsitedlocator.com/index.html?districtCode=49275>.

The most recent system plan is the 2010 Unified Sewer Plan, adopted in 2012. Pierce County is scheduled to begin an update to this plan in 2020. The plan accounted for zoning densities in place today, e.g. up to 54 units per acre. Development that occurs at a higher intensity than the existing conditions may require wastewater system capacity upgrades. New development or redevelopment at the levels envisioned by the future land use plan will require larger collection lines. Update of the Unified Sewer Plan provides an opportunity to plan for future development in the LSDS.

The Lakewood Landing Project would have densities of 66 units per acre if considering the Phase 1 site area of 11.5 acres, but would have densities at 25 units per acre across the entire 30 acre site. Based on the whole site area, densities would be consistent with the TOC zone that has a maximum of 54 units per acre. Coordination regarding the phasing of development and needs for sewer service improvements would assist in development implementation.


b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

Potential strategies for water and sewer include coordination with service providers by each Planned Action Applicant consistent with LMC 18A.60.140 Concurrency:

- Water mains may need to be upgraded for capacity or fire flow to support redevelopment. This may be done efficiently and at lower cost if strategic upgrades are coordinated with the Lakewood Water District's 50-year repair and replacement plan.
- Participation in the update of Pierce County's Unified Sewer Plan will help to identify future capacity challenges, needed upgrades to collection lines, and funding options for replacement.

C. Signature

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature 

Name of signee: Lisa Grueter, AICP

Position and Agency/Organization: Principal, BERK Consulting

Date Submitted: July 27, 2020; updated March 29, 2021.

D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS

(do not use this sheet for project actions)

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise?

See B.2, B.3, and B.7.

Proposed measures to avoid or reduce such increases are:

See B.2, B.3, and B.7.

2. How would the proposal be likely to affect plants, animals, fish, or marine life?

See B.4 and B.5.

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

See B.4 and B.5.

3. How would the proposal be likely to deplete energy or natural resources?

See B.7.

Proposed measures to protect or conserve energy and natural resources are:

See B.7.

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains, or prime farmlands?

See B.3, B.4, B.5, B.12, and B.13.

Proposed measures to protect such resources or to avoid or reduce impacts are:

See B.3, B.4, B.5, B.12, and B.13.

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

See B.8 regarding land use.

Proposed measures to avoid or reduce shoreline and land use impacts are:

The integration of the LSDS Subarea Plan may require minor consistency edits to the Comprehensive Plan. For example, replacement of the urban design diagram shown in Exhibit 23 and references from the Comprehensive Plan to the Subarea Plan in the Land Use Element, Transportation Element, and Capital Facilities Element as appropriate.

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

See B.14 and B.15.

Proposed measures to reduce or respond to such demand(s) are:

See B.14 and B.15.

7. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

The area has limited natural environment features, but all development will be required to address critical area and water quality regulations.

Transportation

EXISTING FEATURES

This section presents a multimodal transportation evaluation of current conditions in the Lakewood Station District Subarea (LSDS). Existing transportation conditions are documented for pedestrians, bicycles, automobiles, freight, transit, and parking.

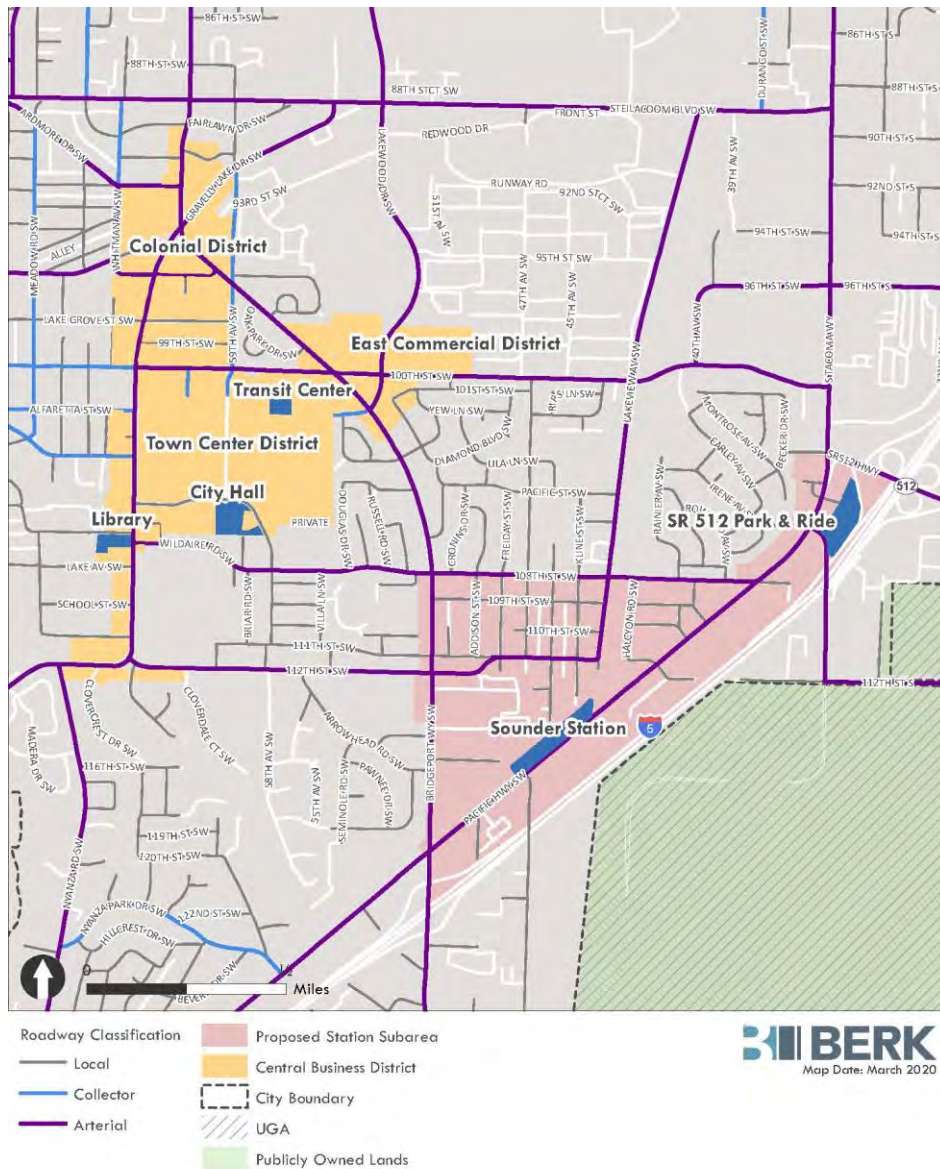
The study area is a predominantly auto-oriented environment, however the area also includes two important regional transit facilities: Lakewood Station and the SR-512 Park & Ride. Major roadways in the study area include Pacific Highway, 108th Street SW, and Bridgeport Way SW. These are classified as either principal or minor arterials with 35 mph posted speed limits. Pacific Highway provides a north-south connection between Tacoma and Lakewood, with access to I-5 ramps and Lakewood Station within the study area. **Exhibit 1** shows transportation connections in and near the LSDS.

The study area contains two key regional transportation facilities: Lakewood Station and SR 512 Park & Ride. Lakewood Station is a focal point of many regional commuter trips including service to downtown Seattle via the Sounder train in addition to bus service to local and regional cities operated by Sound Transit and Intercity Transit. The SR 512 Park & Ride provides bus connections for local and regional commuters to Lakewood Town Center, Tacoma, Puyallup, SeaTac, Dupont, and Olympia and communities throughout the South Sound. Pierce Transit, Sound Transit, and Intercity Transit provide bus service to the SR 512 Park & Ride via Pacific Highway and I-5, Bridgeport Way, and 108th Street SW. Both Lakewood Station and SR 512 Park & Ride provide vehicle parking for transit users.

The study area is bisected by an existing rail line on which Sounder and freight operate. While the rail line is an important regional transit and freight corridor, it has also created an environment with few roadway crossing opportunities which can affect vehicle operations and bicycle and pedestrian mobility.

Roadway Network

Exhibit 1. Transportation Features in the Extended Study Area



Source: Fehr & Peers, 2020; Pierce County, 2020.

Parking

There is very little on-street parking on arterials in the study area, but on-street parking exists on several local streets. Commercial uses tend to have dedicated parking lots adjacent to their buildings, frequently buffering the building from the street. St Clare Hospital provides off-street parking at multiple lots on the hospital's campus. Parking intended for transit users is provided at the Lakewood Station garage (600 spaces) and at the SR 512 Park & Ride (493 stalls). During the daytime, the SR 512 Park & Ride is at about 90 percent capacity. Bicycle parking is provided at Lakewood Station garage via bicycle racks and lockers.

Transit Network

The Lakewood Station District Subarea is served by Pierce Transit, Intercity Transit, and Sound Transit, and includes Lakewood Station and the SR 512 Park & Ride. **Exhibit 2** shows routes serving the area and their associated headways, and Exhibit 3 shows transit pathways and bus stops. Lakewood Station serves six bus routes, providing connections to Tacoma, Puyallup, Olympia, Lacey, and Seattle. The SR 512 Park & Ride serves three bus routes providing connections to Tacoma, Puyallup, and Sea-Tac Airport. Bus stops are present along many of the study area's arterials including Bridgeport Way, 108th Street SW, and Pacific Highway SW. Bus routes run at 15 to 30 minute headways (frequencies) throughout the peak periods, with routes 592 (DuPont-Seattle) and 612 (Olympia-Tacoma Dome Station) running most frequently. Off-peak headways vary substantially, ranging between 12 and 120 minutes.

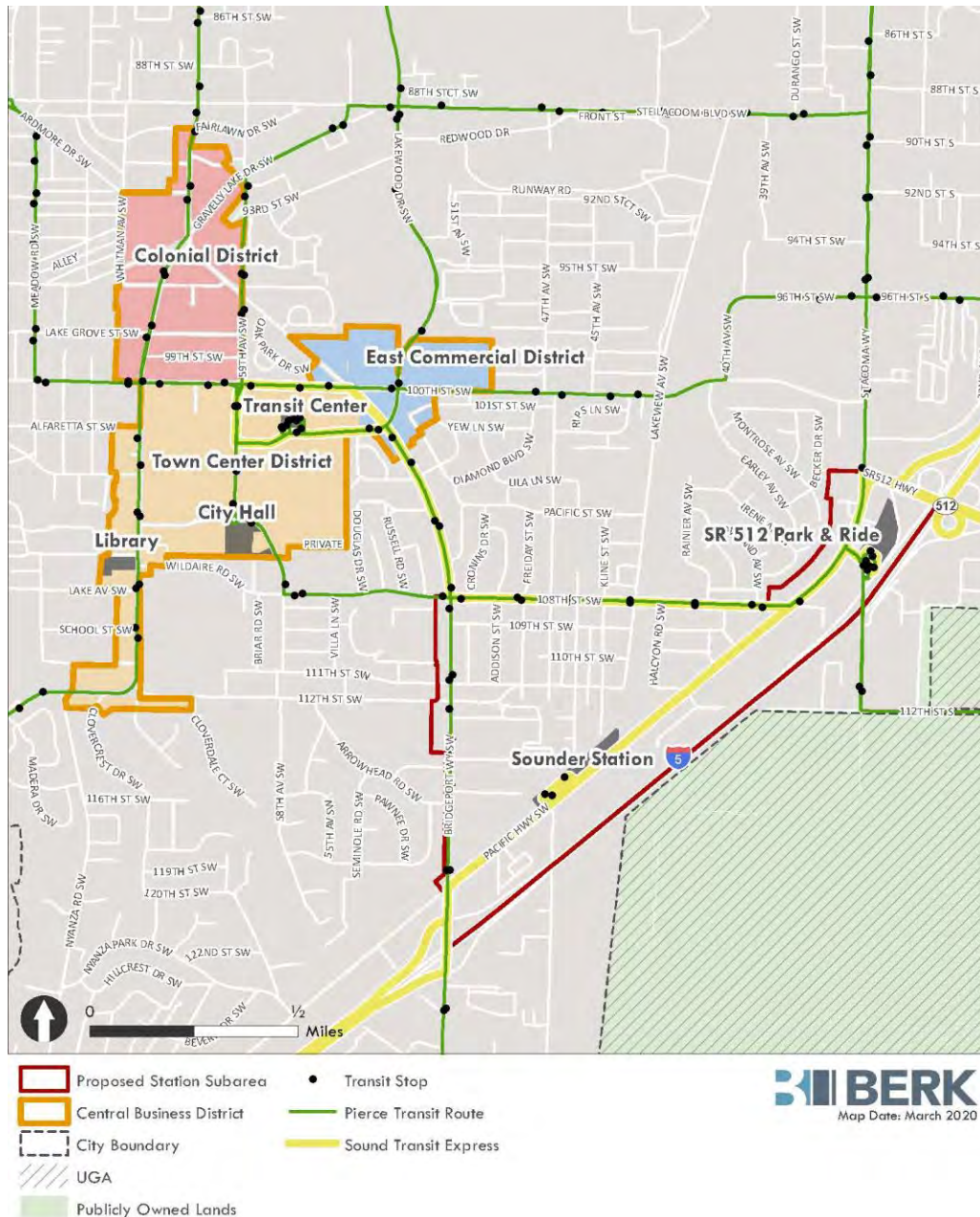
Sound Transit's commuter train, the Sounder, has a stop at Lakewood Station in the southeast portion of the study area. The Sounder provides a regional transit connection between Lakewood, Tacoma, and Seattle during AM and PM commute times.

Exhibit 2. Existing Bus Routes.

ROUTE	DESTINATIONS	PEAK HEADWAY (IN MINUTES)	OFF-PEAK HEADWAY (IN MINUTES)
Sounder	Lakewood - Seattle	20	20-45
3	Lakewood to Downtown Tacoma	30	30
4	Lakewood to South Hill Mall	30	30
574	Lakewood to Sea-Tac Airport	30	30
580	Lakewood to Puyallup Station/South Hill Park and Ride	20	20-40
592	Dupont to Seattle	15	12-15
594	Lakewood to Seattle	20	20-30
612	Olympia to Tacoma Dome Station	15	40-120
620	Olympia to Tacoma Mall	30	60-90

Source: Pierce Transit, 2020; Intercity Transit, 2020; and Sound Transit, 2020.

Exhibit 3. Transit in the Extended Study Area



Source: Fehr & Peers, 2020; Sound Transit; Pierce Transit; Intercity Transit

Non-Motorized Network

Marked bicycle lanes are located on Pacific Highway from Lakewood Station south to Gravelly Lake Drive SW and north from Sharondale Street SW to the South Tacoma Way/SR 512 interchange. Bicycle lanes are also located on 108th Street SW from Bridgeport Way to Pacific Highway. While sidewalks are generally located on all major streets in the project area, the residential neighborhood north of

Pacific Highway lacks sidewalks on most roadways. Due to the train tracks and lack of dedicated facilities, direct non-motorized connection from neighborhoods to Pacific Highway is limited. As a result, accessing Lakewood Station via bicycle or walking can also be challenging. Crossing opportunities are limited to the Lakewood Station pedestrian walkway, Bridgeport Way, and 108th Street SW. Although there is sidewalk infrastructure on Pacific Highway and Bridgeport Way, these roadways are high volume and high speed within the study area. This creates a more uncomfortable pedestrian experience that may discourage non-motorized use on these roadways.

Freight Network

The City's 2016 Comprehensive Plan identifies designated truck routes for freight as a transportation goal. Designated major truck streets are primary routes for goods movement throughout the city. Designation as a major truck street helps Lakewood's Public Works Transportation Division determine street design, traffic management plans, and pavement improvement projects that allow and facilitate the movement of larger vehicles along the designated street. Bridgeport Way and S Tacoma Way are designated as truck routes in WSDOT's Freight and Good Transportation System (FGTS) 2019 Update.¹ Exhibit 4 shows the FGTS routes that run through the study area.

¹ <https://www.wsdot.wa.gov/sites/default/files/2006/02/13/washington-freight-and-goods-transportation-system-2019.pdf>

Exhibit 4. FGTS Routes within the Study Area



Source: WSDOT, 2019, Fehr & Peers, 2020.

Traffic Operations

Study Intersections

Within the project study area, traffic operations at ten locations were analyzed, as shown in **Exhibit 5**. At Lakewood Station, the garage entrance and north and south bus driveways were also analyzed, bringing the total number of study intersections to 12. These intersections are located on key roadway connections, including Pacific Highway, Bridgeport Way SW, and 108th Street SW and are most likely to be affected by potential land use changes.

Exhibit 5. Study Area Intersections



Source: Fehr & Peers, 2020.

Intersection Level of Service

The City uses PM peak hour average delay to evaluate traffic operations level of service (LOS) at its intersections. Traffic operations were analyzed using the Synchro software package. The Synchro network reflects the study area's existing roadway network including segment and intersection geometry, signal timings, and recent traffic counts (2018-2020). For signalized, roundabout, and all-way stop controlled intersections, the LOS is based on the average delay for all approaches. For minor street stop controlled intersections, the LOS is based on the movement with the highest delay. **Exhibit 6** summarizes the LOS and delay thresholds specified in the Sixth Edition of the Highway Capacity Manual (HCM), which is a standard methodology for measuring intersection performance.

The Transportation Element designates level of service guidelines for the city's arterial streets and intersections. Within the study area, that City sets a standard of LOS D during the weekday PM peak hour at all arterial street intersections. However, according to Policy T-20.5, the City may allow minor street stop-controlled intersections to operate below that LOS standard if those instances are thoroughly analyzed from an operational and safety perspective.

Exhibit 6. LOS/Delay Thresholds for Signalized and Unsignalized Intersections.

LOS	SIGNALIZED INTERSECTIONS (DELAY IN SECONDS)	UNSIGNALIZED INTERSECTIONS (DELAY IN SECONDS)
A	≤ 10	≤ 10
B	> 10 to 20	> 10 to 15
C	> 20 to 35	> 15 to 25
D	> 35 to 55	> 25 to 35
E	> 55 to 80	> 35 to 50
F	> 80	> 50

Source: Highway Capacity Manual (Transportation Research Board, 2017).

This study considers 12 intersections, 11 of which are signalized. **Exhibit 7** summarizes the existing intersection LOS at the study intersections. The level of service analysis suggests that automobiles generally move through the study area with acceptable levels of delay during the PM peak period. All study intersections operate at the City's LOS D standard or better, although some approaches may operate with higher delay. Most intersections operate at LOS C or higher, which represents stable conditions with moderate congestion levels for an urban area. Three intersections - South Tacoma Way & SR 512, Pacific Highway & 108th Street, Pacific Highway & Bridgeport Way all operate at LOS D during the PM peak period, which indicates traffic conditions are approaching unstable flow.

As mentioned above, these are intersections that are affected by regional travel patterns, such as afternoon commute congestion stemming from I-5. According to local stakeholders, during certain congestion events on I-5 roadway users may be using Pacific Highway to bypass interstate traffic.

Exhibit 7. Existing PM Peak Hour Intersection Level of Service and Delay.

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
1	South Tacoma Way / SR 512 ¹	Signal	D/35
2	Pacific Highway / South Tacoma Way ¹	Signal	C/21
3	Pacific Highway / 108 th Street ¹	Signal	D/37
4	Pacific Highway / Sharondale Road	TWSC ²	C/23
5	Pacific Highway / Sounder Station Garage Entrance ¹	Signal	A/6
6	Pacific Highway / Sounder Station North Transit Access ¹	Signal	B/12
7	Pacific Highway / Sounder Station South Transit Access ¹	Signal	B/18
8	Pacific Highway / Bridgeport Way	Signal	D/41
9	108 th Street / Lakeview Drive	Signal	B/14
10	Bridgeport Way / 112 th Street	Signal	C/27
11	Bridgeport Way / SB I-5 Ramp ¹	Signal	B/16
12	Bridgeport Way / NB I-5 Ramp ¹	Signal	B/18

1. This intersection required the use of HCM 2000 methodology, due to non-standard traffic signal phasing.

2. TWSC = Two way stop control

Source: Fehr & Peers, 2020.

FUTURE PLANS

Transportation Improvements

This section describes existing local area plans and planned improvements to the transportation network. Some long-range plans identify strategies for the development of the subarea, and others provide specific roadway improvements such as bicycle and pedestrian facilities. Future transportation improvements will be incorporated into the No Action and Planned Action alternatives developed as part of this project.

Lakewood Comprehensive Plan (2019)

Last revised in 2019, the Comprehensive Plan articulates the City's vision for its future for the next 20 years. The City envisions the Lakewood Station District Subarea as a combination of transit oriented commercial, neighborhood commercial, public and institutional land, and a mix of multifamily land uses. LSDS is designated as a Regional Growth Center under Puget Sound Regional Council's VISION 2040.

Transportation Element (2019)

The transportation element of the Comprehensive Plan provides policy direction to address local and regional mobility. The transportation element acknowledges the increase of traffic congestion within Lakewood and seeks to mitigate it by developing a balanced multimodal system that effectively moves people, goods, and services without compromising community character. The Plan specifically strategizes for the incorporation of non-motorized facilities, enhanced illumination, and other pedestrian amenities into new development designs.

Six-Year Transportation Improvement Program: 2020-2025

The Six-Year comprehensive Transportation Improvement Program (TIP) for 2020-2025 was approved by the Lakewood City Council in June 2020. This document outlines short- and long-term road projects – including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays – throughout the city. The following projects are identified in the TIP.

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study
- Kendrick Street SW from 111th Street SW to 108th Street SW – sidewalks, street lighting, bicycle facilities
- 111th Street SW and 112th Street SW bicycle and pedestrian improvements
- 47th Avenue SW bridge improvements supporting non-motorized mobility

Additional projects included in the TIP are discussed in the No Action alternative section.

Sound Transit System Access Fund 2019

The Sound Transit Board of Directors awarded System Access Funds in September 2019 for various non-motorized improvements intended to facilitate connection to transit services. Some of these projects have also been included on the City's Transportation Improvement Program. Among these projects are proposed improvements on 111th Street SW and 112th Street SW in Lakewood. These improvements include curb, gutter, sidewalks, bicycle lanes, street lighting, pavement overlay, and associated storm drainage on both sides of these two streets between Bridgeport Way SW and Kendrick Street SW. Kendrick Street SW provides direct access to the Lakewood Sounder Station via a pedestrian bridge. Although this project will not have a direct impact on traffic operations at the study intersections, it represents a substantial improvement to the non-motorized network within the study area.

Destination 2040: Pierce Transit Long Range Plan Update

Destination 2040 is Pierce Transit's first Long Range Plan. This plan was originally adopted in 2016 and is currently being updated with the most recent available draft released in February 2020. Under this plan, two new Bus Rapid Transit (BRT) routes are being proposed that include services within the study area in 2026 and in 2030.

- **Downtown Tacoma to Lakewood BRT Line:** This BRT line would run from Downtown Tacoma, through the South Tacoma Sounder Station and along South Tacoma Way/Pacific Highway to the SR 512 Park & Ride. The line would then run along 108th Street SW toward the Lakewood Mall Transit Center. It is anticipated that this line would begin service in 2026.

- **Lakewood to South Hill BRT Line:** This BRT line would replace the existing Route 4, which currently runs along South Tacoma Way south of the SR 512 Park & Ride before running east/west along 112th Street SW towards the South Hill Mall Transit Center in Puyallup. It is anticipated that this line would begin service in 2030.

KEY FINDINGS AND PLANNING IMPLICATIONS

- All intersections operate at LOS D or better in current conditions.
- Planning for multimodal improvements connecting to Lakewood Station is programmed for implementation within the next 6 years.
- The area is well-served by transit with two existing hubs at Lakewood Station and the SR 512 Park & Ride. Both Sound Transit and Pierce Transit are enhancing transit connectivity and mobility at Lakewood Station over the next decade.

ALTERNATIVES EVALUATION

No Action Alternative

The No Action Alternative serves as the baseline for the impact analysis of the Planned Action Alternative. It represents the operation of the transportation system if no zoning or network changes were made in the Study Area. However, some growth would continue to occur under the No Action Alternative. The travel model used to forecast vehicle volumes for the No Action Alternative is consistent with the Planned Action model developed for the Lakewood Downtown Plan EIS.

This section summarizes analysis results and environmental impacts of the No Action Alternative. Specifically, the following definitions are used to identify impacts under the No Action Alternative:

- **Auto and Freight:** average vehicle delay below LOS D at a study intersection.
- **Transit:** average vehicle delay below LOS D at a study intersection through which transit routes travel.

Pedestrian, bicycle, and parking impacts are discussed qualitatively. As defined above, this EIS identifies impacts if future transportation operations under the No Action Alternative are not expected to meet the City's adopted level of service standards.

Intersection Level of Service

Exhibit 8 summarizes the average vehicle delay for each study intersection. By 2035, traffic volumes would increase due to the land use growth that would occur within the Study Area itself as well as citywide and regional growth not associated with the Study Area. Therefore, delay at individual intersections is also expected to increase although changes to the future land use are moderate. Of the 12 study intersections, six are expected to drop by at least one LOS grade compared to existing conditions, though only one intersection is expected to operate below the City's LOS D standard.

The Pacific Highway/Sharondale Road intersection is anticipated to operate at LOS F, which is below the City standard. It should be noted that the City has a policy stating that it may allow minor street stop-

controlled intersections (such as this intersection) to operate below the LOS D standard if they are thoroughly analyzed from an operational and safety perspective.

Exhibit 8. 2035 No Action PM Peak Hour Intersection Level of Service and Delay.

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
1	South Tacoma Way / SR 512 ¹	Signal	D/50
2	Pacific Highway / South Tacoma Way ¹	Signal	D/41
3	Pacific Highway / 108 th Street ¹	Signal	D/46
4	Pacific Highway / Sharondale Road	TWSC ²	F/>100
5	Pacific Highway / Sounder Station Garage Entrance ¹	Signal	A/7
6	Pacific Highway / Sounder Station North Transit Access ¹	Signal	B/14
7	Pacific Highway / Sounder Station South Transit Access ¹	Signal	B/19
8	Pacific Highway / Bridgeport Way	Signal	D/54
9	108 th Street / Lakeview Drive	Signal	C/24
10	Bridgeport Way / 112 th Street	Signal	D/35
11	Bridgeport Way / SB I-5 Ramp ¹	Signal	D/41
12	Bridgeport Way / NB I-5 Ramp ¹	Signal	C/29

1. This intersection required the use of HCM 2000 methodology, due to non-standard traffic signal phasing.

2. TWSC = Two way stop control

Source: Fehr & Peers, 2020.

Vehicle Queuing

Vehicle queuing analysis was completed at select locations to determine potential impacts to state highways. Although the overall intersection LOS meets the City standard, queuing at Bridgeport Way & the I-5 southbound off-ramp is expected to extend from the southbound off-ramp onto I-5, which could impact traffic flow, including buses, on I-5 near this interchange.

Transit

No transit impacts are expected under the No Action Alternative.

Pedestrian and Bicycle

Under the No Action Alternative, the pedestrian and bicycle network would remain similar to its current state. The City plans to implement sidewalk projects on 47th Avenue SW, 111th and 112th Streets SW, Kendrick Street SW, and 115th Street SW. The City also plans to implement bicycle facility projects on 47th Avenue SW, 108th Street SW, 111th and 112th Streets SW, and Kendrick Street SW. Many of these projects combine pedestrian and bicycle improvements as part of the same project and may also include improvements to roadway lighting. The 47th Avenue SW project enhances non-motorized connectivity over the existing I-5 bridge between Pacific Highway SW and Lakewood Station to the neighborhood on the opposite side of I-5.

Development is expected to meet the City design standards related to bicycle and pedestrian facility accommodations so no significant adverse impacts to pedestrians or bicycles are identified under the No Action Alternative. A full list of known planned pedestrian and bicycle projects is below.

Pedestrian Improvements

- 47th Avenue SW from 121st Street SW to Pacific Highway – New Sidewalks
- 111th & 112th Street SW - Bridgeport Way to Kendrick Street SW – Construct sidewalks to fill gaps in pedestrian network
- 112th Street SW from Gravelly Lake Drive SW to Bridgeport Way SW – New Sidewalks
- Kendrick Street SW from 111th Street SW to 108th Street SW – New Sidewalks
- 115th Court SW from Bridgeport Way SW to Kendrick Pedestrian Bridge – New Sidewalks and pedestrian lighting

Bike Improvements

- 47th Avenue SW from 121st Street SW to Pacific Highway – New Bike Lanes
- 108th Street SW from Bridgeport Way SW to Lakeview Avenue SW – New Bike Lanes
- 111th Street SW Bike Lanes – Bridgeport Way SW to 112th Street SW
- 112th Street from Gravelly Lake Drive SW to Bridgeport Way SW – New Bike Lanes
- Kendrick Street SW from 111th Street SW to 108th Street SW – New Bike Facilities

Parking

Off-street parking is expected to be provided for residential, retail and other uses throughout the Study Area. Residences in the study area generally have their own off-street parking, allowing the roadway right-of-way to be reallocated to other modes as needed. New developments in the Study Area would be required to meet the City's parking requirements, so no adverse impacts to parking are expected.

Safety

Traffic volumes in the Study Area are projected to increase by 2035. With higher volumes, there is potential for an increased number of collisions. However, there is no indication that collision rates at intersections or along segments would increase. Therefore, no safety impacts are identified under the No Action Alternative. In fact, some of the projects planned that improve or construct new pedestrian and bicycle facilities may result in a safety benefit.

Planned Action Alternative

One Planned Action Alternative was developed which incorporates an increase in residential and retail land use at the Lakewood Landing and within the study area as compared to the No Action Alternative. These land use changes would result in an increase in the number of trips made within the Study Area; vehicle trips are expected to increase by 700 compared to the No Action Alternative. This section summarizes analysis results and environmental impacts of the Planned Action Alternative. Specifically, the following definitions are used to identify intersection impacts:

- **Auto and Freight:** average vehicle delay below LOS D at a study intersection that operated acceptably under the No Action Alternative; or an increase in delay of at least 5 seconds at a study intersection already expected to operate below LOS D under the No Action Alternative.
- **Transit (applicable to study intersections through which transit routes travel):** average vehicle delay below LOS D at a study intersection that operated acceptably under the No Action Alternative; or an increase in delay of at least 5 seconds at a study intersection already expected to operate below LOS D under the No Action Alternative.

Pedestrian, bicycle, safety, and parking impacts are discussed qualitatively in comparison to the No Action Alternative. An impact is defined if a project would preclude or fail to implement a City-identified bicycle or pedestrian improvement.

Intersection Level of Service

Exhibit 9 summarizes level of service and average vehicle delay for each study intersection. The Planned Action Alternative's proposed land use growth would result in increased volumes compared to the No Action Alternative. There were no major changes to roadway or intersection capacity between the No Action and Planned Action scenarios. As defined above, impacts are evaluated in comparison to the No Action Alternative. The following significant auto and freight impacts are expected under the Planned Action Alternative.

- Intersection 4 – Pacific Highway / Sharondale Road SW – Increase in delay, LOS F to LOS F
- Intersection 8 – Pacific Highway / Bridgeport Way – from LOS D to LOS E
- Intersection 11 – Bridgeport Way / SB I-5 Ramp – from LOS D to LOS E

Because bus routes pass through all of the aforementioned intersections, these intersections are also considered as significant transit impacts. Potential measures to mitigate the impacts on these intersections are presented in the Mitigation Measures section.

Exhibit 9. 2035 Planned Action PM Peak Hour Intersection Level of Service and Delay.

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
1	South Tacoma Way / SR 512 ¹	Signal	D/52
2	Pacific Highway / South Tacoma Way ¹	Signal	D/42
3	Pacific Highway / 108 th Street ¹	Signal	D/48
4	Pacific Highway / Sharondale Road SW	TWSC	F/>100

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
5	Pacific Highway / Sounder Station Garage Entrance ¹	Signal	A/7
6	Pacific Highway / Sounder Station North Transit Access ¹	Signal	B/16
7	Pacific Highway / Sounder Station South Transit Access ¹	Signal	C/21
8	Pacific Highway / Bridgeport Way	Signal	E/78
9	108 th Street / Lakeview Drive	Signal	C/24
10	Bridgeport Way / 112 th Street	Signal	D/41
11	Bridgeport Way / SB I-5 Ramp ¹	Signal	E/60
12	Bridgeport Way / NB I-5 Ramp ¹	Signal	C/31

1. This intersection required the use of HCM 2000 methodology, due to non-standard traffic signal phasing.

2. TWSC = Two way stop control

Source: Fehr & Peers, 2020.

Vehicle Queuing

In the Planned Action Alternative, queuing is expected to extend onto I-5 at Bridgeport Way and the I-5 southbound off-ramp, and at Bridgeport Way and the I-5 northbound off-ramp. Queuing at the southbound off-ramp was identified as an impact under the No Action Alternative, with the 95th percentile queue length expected to increase substantially with the Planned Action Alternative. Queuing on the I-5 northbound off-ramp was not expected to be an issue under the No Action Alternative, but substantial queuing is expected under the Planned Action Alternative. Therefore, queueing impacts to I-5 are expected under the Planned Action Alternative.

Pedestrian and Bicycle

The pedestrian and bicycle environment will be similar to the No Action Alternative. However, several local roadways are expected to be redesigned to better accommodate walking and bicycling through the implementation of street typology design proposed as part of this project. These typologies will reallocate right of way on residential roadways to sidewalks, bicycle facilities, and other improvements that enhance the non-motorized experience in these areas. Because of these additional benefits of the Planned Action Alternative and because development is expected to meet the City design standards related to bicycle and pedestrian facility accommodations, no significant adverse impacts to pedestrians and bicycles are identified under the Planned Action Alternative.

Parking

Select residential streets in the study area have been identified to be redesigned to better accommodate non-motorized travel through application of street typologies. As part of this effort, on-street parking may be removed or changed to other modes and uses. Many households in this area currently have off-

street parking, and parking demand may be lower in the future given the study area's proximity to transit facilities. New developments in the study area would be required to meet the City's parking requirements. Therefore, no significant impacts to parking are identified under the Planned Action Alternative.

The City requires bicycle parking in LMC 18A.80.030 for commercial and multifamily uses.

Safety

Traffic volumes in the Study Area are projected to increase under the Planned Action Alternative compared to the No Action Alternative. With higher volumes, there is potential for an increased number of collisions. However, there is no indication that collision rates at intersections or along segments would increase meaningfully compared to the No Action Alternative. Bicycle and pedestrian improvements may improve roadway safety conditions for the Planned Action Alternative. Therefore, no significant adverse impacts to safety are identified under the Planned Action Alternative.

Summary of Impacts

Exhibit 10 summarizes the significant impacts for each alternative.

Exhibit 10. Summary of Transportation Impacts

TYPE OF IMPACT	NO ACTION	PLANNED ACTION
Auto and Freight	1 intersection and I-5 SB off-ramp queuing impact	3 intersections and I-5 NB and SB off-ramp queuing
Transit	1 intersection and I-5 SB off-ramp queuing impact	3 intersections and I-5 NB and SB off-ramp queuing
Pedestrian	None	None
Bicycle	None	None
Parking	None	None
Safety	None	None

Source: Fehr & Peers, 2020.

Mitigation Measures

This section identifies a range of potential mitigation strategies that could be implemented to help reduce the significance of the adverse impacts identified for four intersections in the Planned Action alternative.

Managing demand for auto travel is an important part of mitigating the traffic congestion impacts. Transportation demand management (TDM) strategies could include subsidies or discounts for non-auto travel, education and assistance to help travelers identify non-auto commute options, rideshare and ridematch promotion, and local incentive and reward programs.

Washington state Commute Trip Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip

reduction plans and work toward meeting their mode share targets through internal programs and monitoring.

The City of Lakewood included policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote transportation demand management (TDM) strategies.

The potential TDM strategies discussed here could be implemented regardless of which alternative is selected. With a robust TDM program in place, it is expected that actual trip generation in the Study Area could be lowered beyond the levels analyzed in the impacts section of this EIS.

Other Proposed Mitigation Measures

The City could make signal timing improvements to increase the efficiency of impacted intersections and roadways in the Study Area. Exhibit 11 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right of way acquisition or widening, rather only changes to signal timing. The exception is intersection 4, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better. The City is planning to include improvements at the intersections of Bridgeport Way with the I-5 NB/SB ramps in their upcoming Transportation Improvement Program. While queuing at these locations may still occur, the improvements are expected to mitigate the effects of the Planned Action Alternative.

Exhibit 11. Proposed Mitigation Measures

ID	INTERSECTION	NO ACTION	ALT 1	ALT 1 MITIGATED
4	Pacific Highway / Sharondale Road			
	Install signal using existing channelization and signal length consistent with adjacent intersections	F>100	F>100	A/4
8	Pacific Highway / Bridgeport Way			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way	D/54	E/78	D/49

ID	INTERSECTION	NO ACTION	ALT 1	ALT 1 MITIGATED
	from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.			
11	Bridgeport Way / SB I-5 Ramp			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.	D/41	E/60	D/52

Source: Fehr & Peers, 2020.

Therefore, with implementation of the potential signal timing mitigation measures outlined in the previous section and future improvements at the Bridgeport Way/I-5 ramps, the magnitude of the impacts could be mitigated such that auto, freight, and transit are not significantly impacted.

Development in the study area would provide information about its expected trips and will provide a traffic study where applicable per LMC 12.09.028. This analysis can affirm the mitigation approach and phasing, such as signal timing at Bridgeport Way/I-5 on and off ramps.

Encouraging bike usage would reduce transportation demand on local roads and state highways. The City could continue applying its required bicycle parking standards by use in 18A.80.030. The City could consider alternative standards or incentives in developments in the planned action or form-based code.

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED:

May 3, 2021

TITLE:

An Ordinance adding to and clarifying the list of declared public nuisances.

TYPE OF ACTION:

☒ ORDINANCE NO. 753
☐ RESOLUTION NO.
☐ MOTION NO.
☐ OTHER

REVIEW:

April 26, 2021

ATTACHMENTS:

Draft Ordinance

SUBMITTED BY: Dave Bugher, Assistant City Manager, Development Services.

RECOMMENDATION: It is recommended that the City Council approve the attached Draft Ordinance adding to and clarifying the list of declared public nuisances.

DISCUSSION: This subject came before the City Council on April 26, 2021, as a study session item. The proposal as presented to the Council would update the City's list of declared nuisances. While originally designed to update nuisances specific to contaminated properties, the work was expanded to update the entire nuisance code based on the experiences of the Community & Economic Development Department and CSRT.

The exercise also included a review of dangerous and unfit building codes through the International Construction Codes (ICCs) and the International Property Maintenance Code (IPMC). There were no changes proposed in either the ICCs or IPMC. It was reported that these codes work well together in addressing dangerous and unfit structures.

Council reviewed the Draft Ordinance. Councilmember Brandstetter recommended expanding 8.16.010 (R.) to include all residentially zoned properties. Mayor Anderson recommended that 8.16.010 (L.) be amended to include federal as well as state and local regulations. These two changes have been incorporated into the Draft Ordinance which is attached.

ALTERNATIVE(S): 1) Take no action and leave the list of declared nuisances as is; 2) Amend the Draft Ordinance, adding to, or deleting the list of declared nuisances.

FISCAL IMPACT: Neutral. If a public nuisance is declared, current regulations allow the City to recoup its costs through a special assessment or lien against the property.

D. Bugher

Prepared by

D. Bugher

Department Director



City Manager Review

ORDINANCE NO. 753

AN ORDINANCE OF THE CITY OF LAKEWOOD CITY COUNCIL
AMENDING TITLE 8 HEALTH AND SAFETY, CHAPTER 8.16, PUBLIC
NUISANCES, ADDING A NEW CHAPTER 8.18, ACTIVITIES THAT ARE
NOT DEEMED PUBLIC NUISANCES, AND AMENDING CHAPTER 8.26
PUBLIC NUISANCE DECLARED

WHEREAS, RCW 35A.11.020, 35A.21.160, 35.22.280, and 35.23.440 grant cities specific authority to declare what shall be a nuisance and to abate same; and

WHEREAS, Chapter 35.80 RCW grants cities the authority to declare dwellings, buildings, and structures unfit; and

WHEREAS, all construction activity and construction materials, including property maintenance, shall be governed by the State Building Code Council to include the 2018 Editions of the International Construction Codes (ICC); and

WHEREAS, the City of Lakewood adopted the 2018 Editions of the International Construction Codes (ICC), including the International Property Maintenance Code (IPMC), with local amendments, through Ordinance No. 747 in February 2021; and

WHEREAS, public nuisances as defined in Lakewood Municipal Code (LMC), Title 8, Health and Safety, unreasonably diminish the quality of life of the citizens of Lakewood by creating conditions detrimental to the health and safety of Lakewood residents, to the environment, and to the right of all residents to use and enjoy their own property; and

WHEREAS, at the request of the Lakewood City Council, a review of the dangerous and unfit building and public nuisance codes was performed; and

WHEREAS, City officials reviewed the dangerous and unfit building and public nuisance provisions of surrounding communities and counties, and conferred with City Departments; and

WHEREAS, it was found that the IPMC with existing local amendments is an effective mechanism to address dangerous and unfit buildings and structures and no need exists to make additional amendments at this time; and

WHEREAS, the LMC Title 8, Health and Safety has largely remained intact with few amendments since the City of Lakewood, incorporated; and

WHEREAS, LMC Title 8, Health and Safety, is in need of updating in order to improve the efficiency and effectiveness of the City's code enforcement process so that citizen complaints can be addressed in a timely manner while still providing property owners with due process;

WHEREAS, unresolved Code violations result in the continuation of public nuisances, which often involve health hazards, dilapidated or abandoned properties, and decreased property values, and need to be addressed in a timely and effective manner; and

WHEREAS, violations of the City's development regulations, when left unresolved, undermine the purposes of the regulations and lead to inconsistent application of the development rules in the City; and

WHEREAS, it is appropriate to update Title 8 is proposed to be amended to add and clarify the list of declared public nuisances;

NOW THEREFORE, BE IT ORDAINED by the Lakewood City Council:

Section 1. The above recitals are hereby adopted as findings of fact.

Section 2. Chapter 8.16 of the Lakewood Municipal Code, Section 8.16.010 “Nuisances Affecting Public Health,” is amended as set forth in Exhibit A, which is attached hereto and incorporated herein by reference.

Section 3. New Chapter 8.18 of the Lakewood Municipal Code, “Activities that are not deemed public nuisances,” is hereby adopted as set forth in Exhibit B, which is attached hereto and incorporated herein by reference.

Section 4. Chapter 8.23, Section 8.26.040. “Public Nuisance Declared,” is amended as set forth in Exhibit C, which is attached hereto and incorporated herein by reference.

Section 5. Severability: If any sections, sentence, clause or phrase of this Ordinance shall be held to be invalid or unconstitutional by a court of component jurisdiction, or its application held inapplicable to any person, property or circumstance, such invalidity or unconstitutionality or inapplicability shall not affect the validity or constitutionality of any other section, sentence, clause or phrase of this Ordinance or its application to any other person, property or circumstance.

Section 6. Effective Date: This Ordinance shall be in full force and effect thirty (30) days after publication of the Ordinance Summary.

ADOPTED by the Lakewood City Council on this 3rd day of May, 2021.

CITY OF LAKEWOOD

Don Anderson, Mayor

Attest:

Briana Schumacher, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney

EXHIBIT "A"

8.16.010 Nuisances affecting public health.

The following specific acts, omissions, places, conditions and things are declared to be nuisances: the erecting, maintaining, using, placing, depositing, causing, allowing, leaving, or permitting to be or remain in or upon any private lot, building, structure, or premises, or in or upon any street, avenue, alley, park, parkway, or other public or private place in the City, any one or more of the following places, conditions, things or acts to the prejudice, danger, or annoyance of others:

A. Excavations or naturally occurring holes, including, but not limited to, privies, vaults, cesspools, sumps, pits, wells or cisterns or any other similar conditions which are not secure and which constitute a concealed danger or other attractive nuisance like places which are not securely protected from flies or rats;

B. Any place where fighting between people or animals or birds is unlawfully conducted or allowed;

C. Filthy, littered or trash-covered premises, including all buildings and structures thereon and areas adjacent thereto;

D. ~~tin~~ Metal cans, steel or aluminum, bottles, glass, cans, ashes, small pieces of scrap iron, wire, metal, articles, bric-a-brac, broken stone or cement, broken crockery, broken glass, broken plaster and all other trash or abandoned material unless the same are kept in covered bins or metal receptacles approved by the City; provided, that any such receptacles approved by the Pierce County Health Officer or designee shall be deemed approved by the City;

E. Trash, litter, rags, debris, accumulations of empty barrels, boxes, crates, packing cases, mattresses, bedding, furniture and appliances, excelsior, packing hay, straw, or other packing material, lumber or construction material not neatly piled, scrap iron, tin and other metal not neatly piled which provides harborage for rodents, or other pests;

F. Any unsightly and dangerous building, billboard or structure;

G. All places used or maintained as junkyards or dumping grounds, or for the wrecking, disassembling, repair or rebuilding of automobiles, trucks, tractors or machinery of any kind, or for the storing or leaving of worn out wrecked or abandoned automobiles, trucks, tractors or machinery of any kind or of any of the parts thereof, or for the storing or leaving of any machinery or equipment used by contractors or builders or by other persons, which places are kept or maintained so as to provide harborage for rodents excluding properly zoned and licensed wrecking yards, junkyards or machinery being used;

H. The erection or use of any building, room, or other place in the city for exercise of any trade, employment, or manufacture which, by emitting noxious exhausts, particulate matter, offensive odors, or other related annoyances, is discomforting, offensive, or detrimental to the health of individuals or of the public;

I. Garbage disposed of in any manner other than provided in the Sanitary Code;

J. Garbage cans which are not impervious to rodent gnawing or do not have tight-fitting lids;

K. Any putrid, unsound or unwholesome bones, meat, hides, skins or the whole or any part of any dead animal, fish or fowl, butchers' trimmings and offal, or any waste vegetable or animal matter in any quantity, garbage human excreta or other offensive substance; provided, nothing in this subsection shall prevent the temporary retention of waste in a manner approved by the City Manager;

Animal parts, manure, excreta, or wastes including bones, meats, hides, skins, or any part of any dead animal, fish, or fowl, which are improperly handled, contained, or removed from a premises; placed in any watercourse, lake, or street; or allowed to become putrid, offensive, and injurious to the public health;

L. Storing or dumping of any poisonous or hazardous waste, material or substance in violation of Federal, Washington State, or Pierce County law that may cause contamination of soil, groundwater or surface water, or so as to allow uncontrolled access to it by any animal or person;

M. Real property in violation of Washington State's Hazardous Waste Cleanup - Model Toxics Control Act (MTCA), RCW Chapter 70.105D, excepting therefrom properties contaminated by the Asarco Tacoma Smelter Plume;

N. Blackberry vines or any tall grass or weeds over two feet in height which ~~are rodent infested~~ which allow for rodent or pest infestation;

O. Grass clippings, cut brush or cut weeds which may create a fly or rodent harborage;

P. Nests, colonies, hives or apiaries of bees, Africanized honey bees, yellow jackets, hornets or wasps which are not in full compliance with Chapter 15.60 RCW or Chapter 16-602 WAC;

Q. Any accumulation of combustible, explosive or flammable substances which are stored in a way that poses a threat or danger to life or property; and

R. The parking or storage of vehicles on residentially zoned lots in violation of the parking regulations in this Code;

S. Any building or structure declared unsafe by the city building official, or that has been abandoned or unused for longer than fifteen consecutive days and has deteriorated and become unsightly.

T. Any building or structure where construction was commenced and the exterior of the building or structure remains unfinished and the building permit has expired; or any building or structure that has been constructed or modified without permit;

U. Violation of any of the following City of Lakewood Municipal Codes: Title 12, Public Works; Title 14, Environmental Protections; Title 15, Buildings & Construction; Title 17, Subdivisions; Title 18A, Land Use & Development Code, including all adopted Subarea Plans pursuant to the Washington Growth Management Act;

V. Any violation of the adopted Shoreline Management Code, Ordinance 718, or as hereafter amended;

W. Painting of vehicles unless conducted inside an approved spray booth and/or performed inside a structure or similarly enclosed area designed and approved for such purposes;

X. The production and/or processing of fifteen (15) or more marijuana or cannabis plants in any one housing unit other than a cooperative establishment pursuant to RCW 69.51A and/or the storage or growing of said plants if any portion of such activity can be readily seen by normal unaided vision or readily smelled from a public place or the private property of another housing unit.

Y. Causing or allowing any shopping cart to be abandoned on either public or private property.

EXHIBIT “B”

Section 8.18 Activities that are not deemed public nuisances

The following shall not constitute public nuisances:

A. Compost piles less than four feet (4') in height and six feet (6') in diameter at ground level, and 30 feet (30') or more from any dwelling, and four feet (4') or more from adjoining properties;

B. Storm debris within thirty (30) days following a storm event;

C. For construction activity that requires a building permit, construction residue and debris during and for ten (10) calendar days following completion of work, unless the residue and/or debris is substantially or unreasonably impacting the general public or not neatly stacked and stored;

D. Fallen leaves, tree needles, tree fruit and similar vegetation, during the months of October through April, inclusive, except when located on public sidewalks;

E. The accumulation and temporary storage, in containers designated for such purposes, of recyclable materials pursuant to a program of recycling adopted by the City; provided, however, that such containers must not be publicly visible or they must be made available to the City's garbage or recycle contractor within fourteen (14) days after having been filled to fifty percent (50%) or more of their capacity;

F. Uncultivated, uncut or untended weeds, grass, bushes or other vegetation not constituting a health or fire hazard, existing in a natural state on undeveloped, agricultural, native growth easement or defined critical areas such as wetlands, streams, and steep slopes.

EXHIBIT “C”

All junk and inoperable vehicles identified as such by a law enforcement officer or code enforcement officer as defined in LMC 8.26.020, and found on private or public property, are declared to constitute a public nuisance subject to removal, impoundment and disposal. It is unlawful for any individual, firm, entity, or corporation to allow, cause to allow or place a junk or inoperable vehicle on any premises.

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: May 3, 2021	TITLE: Adopting the Fiscal Year 2021 Joint Tacoma and Lakewood Consolidated Annual Action Plan, including Amendment to FY 2017, 2019 and 2020 Consolidated Annual Action Plans	TYPE OF ACTION: — ORDINANCE ✓ RESOLUTION 2021-07 — MOTION — OTHER
REVIEW: February 1, 2021 and March 22, 2021 Public hearing, April 19, 2021	ATTACHMENTS: 1. Resolution 2. FY 2021 Joint Consolidated Annual Action Plan, including Amendment to the FY 2017, 2019 and 2020 Consolidated Annual Action Plans	

SUBMITTED BY: Jeff Gumm, Program Manager

RECOMMENDATION: It is recommended that the Mayor and City Council adopt the attached Draft Resolution approving the Fiscal Year 2021 (July 1, 2021 – June 30, 2022) Joint Tacoma and Lakewood Consolidated Annual Action Plan, including Amendment to FY 2017, 2019 and 2020 Consolidated Annual Action Plans for the Community Development Block Grant (CDBG) and HOME Investment Partnership Act (HOME) programs.

DISCUSSION: Each year, the U. S. Department of Housing and Urban Development (HUD) requires Community Development Block Grant (CDBG) and Home Investment Partnership Act (HOME) participating jurisdictions to submit a Consolidated Annual Action Plan identifying housing and community development needs, strategies for meeting those needs, and how funds will be used.

For Fiscal Year 2021 (July 1, 2021 – June 30, 2022), Lakewood will receive \$573,352 in CDBG funds and \$321,730 in HOME funds. (*Continued to page 2*)

ALTERNATIVE(S): The Council may modify the Joint Consolidated Annual Action Plan funding allocations and projects.

FISCAL IMPACT: Lakewood's CDBG entitlement award will be \$573,352 and Lakewood's HOME Consortium share will be \$321,730 for the FY 2021 program year. For details see Tables 1 and 2 at the end of this report.

Jeff Gumm
Prepared by

Dave Bugher
Department Director


City Manager Review

DISCUSSION CONTINUED:

FY 2021 CDBG funding allocations will include the reprogramming of \$28,708.76 in funds from the 2017 Annual Action Plan and \$170,425.32 in funds from the 2019 Annual Action Plan. These reprogrammed funds will be used to supplement the FY 2021 allocation, bringing the total CDBG funds available to \$772,486.08. Additionally, the 2020 Annual Action Plan will be amended to include a decrease of \$91 to address an incorrect annual funding allocation reported by HUD on October 22, 2020. A comprehensive list of proposed CDBG funding allocations for 2021 are detailed in Table 1.

Proposed FY 2021 HOME funding allocations are detailed in Table 2.

The Fiscal Year 2021 Joint Consolidated Annual Action Plan must be adopted by the Lakewood and Tacoma City Councils and submitted to HUD no later than May 15, 2021. Following HUD's review and approval, a contract will be issued to Lakewood for the CDBG entitlement award, and a contract will be issued to Tacoma for the HOME entitlement.

The draft version of the FY 2021 Joint Consolidated Annual Action Plan, including Amendment to FY 2017, 2019 and 2020 Consolidated Annual Action Plans, was made available for an initial 30-day citizen review and comment period from April 1 – 30, 2021. A public hearing on the plan was then held by the City Council on April 19, 2021. A single comment on the proposed use of funds was received from Mr. Dennis Haugen, who stated the funding needed to be focused on wealth building for those individuals entering the job market, and that in order to do so taxes and regulations needed to be held to a minimum.

The development of the FY 2021 Joint Consolidated Annual Action Plan involved a citizen participation process in accordance with the CDBG and HOME Citizens' Participation Plan (Resolution 2020-05). On February 1, 2020, Council reviewed goals identified in the current 5-YR 2020-2024 Consolidated Plan, ongoing activities funded through CDBG, CDBG-CV 1, 2, and 3 (coronavirus funding allocations) and HOME funding, and funding options available for the FY 2021 Annual Action Plan. On February 10, 2021, the Community Services Advisory Board (CSAB) held a public hearing to receive public testimony on housing, human services and community development needs. A draft version of the FY 2021 Plan was reviewed by the CSAB on March 10, 2021 and by Council on March 22, 2021.

FY 2021 Goals & Priorities

For FY 2021, Lakewood CDBG and HOME funds are proposed to be focused in support of all four goals addressed in the 5-YR 2020-2024 Consolidated Plan:

1. Housing instability among residents, including homelessness – Programs include the funding of the Major Home Repair/Sewer Loan program to assist with homeowner rehabilitation; neighborhood revitalization through the Neighborhood Stabilization Program 1 abatement revolving loan fund; and relocation assistance for low income tenants displaced through no fault of their own.
2. Limited supply of diverse rental and homeownership opportunities – Funding will focus on providing additional affordable homeownership opportunities through the HOME Affordable Housing Fund.
3. Need for accessible, culturally competent services – Services will include fair housing counseling, foreclosure counseling, and financial counseling services.
4. Need for safe, accessible homes and facilities – Funding will focus on the provision of new public infrastructure where none presently exists through the construction of sidewalks in a low income, predominantly residential census block group.

FY 2021 CDBG & HOME Recommended Projects (Tables 1 & 2)

Physical/Infrastructure Improvements

City of Lakewood – Phillips Road Sidewalk Project - \$300,000 - (CDBG) – Funding will provide for sidewalk construction along the west side of Phillips Rd. SW, from Agate Dr. SW to Onyx Dr. SW, serving low income households in census tract 072105 block group 2 and census tract 072106 block groups 3 & 4. Construction will provide for sidewalks where none currently exist. The project will serve a total population of 5,345, with 2,895 being low income individuals (52.48% LMI). Funding includes reprogrammed FY 2019 funds totaling \$170,425.32 originally allocated for the construction of street lighting in the Oakbrook neighborhood. Environmental review, construction and Davis-Bacon monitoring of this project will be conducted by CDBG staff.

Housing

Major Home Repair/Sewer Loan Program - \$264,106.92 - (CDBG) – Funding will be used to assist low and moderate income homeowners maintain their homes and make necessary repairs due to disasters and deferred maintenance. Funds will also provide for the construction of side sewers for low income owner-occupied households. Qualifying homeowners may receive a loan of up to \$70,000, with interest up to 1% and terms up to 20 years. Funding provides for general staff time administration of CDBG projects.

CDBG Administration of HOME Housing Services- \$15,000 – (CDBG) –Administrative costs will support Lakewood staff for ongoing expenses of HOME program, including general program management, coordination, monitoring, loan servicing, recording fees associated with loan payoff and refinancing, title fees, and evaluation of the HOME program.

Emergency Assistance for Displaced Residents - \$28,708.76 - (CDBG) – Program provides emergency and permanent housing assistance, up to \$2,000/household, for low income families displaced through no fault of their own during building and code enforcement closures, fires, and other incidences creating homelessness. Reprograms FY 2017 funding to current year. This program is administered by CDBG staff.

Affordable Housing - \$225,000- (HOME) – Funds will support the acquisition, construction and/or rehabilitation of affordable housing for low-income rentals and/or to facilitate new homeownership opportunities. Funding provides for general staff time administration of HOME projects.

Public Service- 15% cap

Housing/Foreclosure Counseling & Fair Housing Services - \$50,000 – (CDBG) – The program provides housing counseling, foreclosure counseling, and fair housing counseling for low and moderate income individuals facing eviction, foreclosure or other fair housing related issues. Program to be contracted to subrecipient organization with oversight by CDBG staff.

Economic Development

No projects recommended

Administration- 20% cap CDBG/ 10% cap HOME

CDBG Administration - \$114,670.40 - (CDBG) – Administrative costs will support overall program management, coordination, monitoring, accounting and evaluation of CDBG and HOME grants.

HOME Administration - \$32,173 – (HOME) – Tacoma portion (10%) of Lakewood’s HOME allocation for general program management, coordination, monitoring, loan servicing, and evaluation of HOME program as part of the Tacoma-Lakewood HOME consortium.

Tacoma/Lakewood HOME Consortium

City staff administers the City's CDBG housing programs, and jointly administers Lakewood's HOME funded programs with Tacoma serving as the "lead entity." Pursuant to the HOME Consortium agreement with Tacoma, funding for Tacoma's fiscal administration of Lakewood's HOME programs will be funded out of Lakewood's 10-percent cap for administration.

The HOME Consortium agreement also authorizes the Tacoma Community Redevelopment Authority (TCRA), in consultation with Lakewood, to review Lakewood's (HOME) housing loans and proposals for housing development projects and make funding decisions based on projects which meet the lending criteria of the TCRA. The TCRA will fund Lakewood projects out of Lakewood's portion of the grant.

Applications for affordable housing development, housing rehabilitation, and down payment assistance are made available throughout the fiscal year and funded with available program funds.

Conclusion and Action Requested

The Draft Fiscal Year 2021 Joint Consolidated Annual Action Plan, including Amendment to FY 2017, 2019 and 2020 Consolidated Annual Action Plans was presented to the CSAB on March 10, 2021 and to Council at your March 22, 2021 Study Session. A 30-day citizen review and comment period on the Plan, Amendments, and proposed use of funds was made available from April 1 – 30, 2021. A public hearing by the City Council was held on April 19, 2021 to receive public testimony.

It is recommended that Council adopt the Fiscal Year 2021 Joint Consolidated Annual Action Plan, including Amendment to FY 2017, 2019 and 2020 Consolidated Annual Action Plans to include projects amounting to the City's CDBG allocation of \$573,352 and \$321,730 for HOME. Following adoption of the Plan, it is required that the approved Plan be submitted to HUD for approval no later than May 15, 2021.

TABLE 1
CDBG FUNDING RECOMMENDATIONS – FY 2021
(Includes Reprogrammed CDBG)

Activity	CDBG	Reprogrammed FY 2017 CDBG	Reprogrammed FY 2019 CDBG	FY 2020 CDBG	Total
Housing – Major Home Repair/Sewer Loan	\$264,106.92	\$0	\$0	\$0	\$264,106.92
Housing – CDBG Administration of HOME Housing Services	\$15,000	\$0	\$0	\$0	\$15,000
Housing – Emergency Assistance for Displaced Residents (EADR)	\$0	\$28,708.76	\$0	\$0	\$28,708.76
Public Service – Housing/Foreclosure Counseling & Fair Housing	\$50,000	\$0	\$0	\$0	\$50,000
Infrastructure – COL Sidewalk Project – Phillips Rd.	\$129,574.68	\$0	\$170,425.32	\$0	\$300,000
Administration	\$114,670.40	\$0	\$0	\$0	\$114,670.40
Total Funding	\$573,352	\$28,708.76*	\$170,425.32**	\$0***	\$772,486.08

*FY 2017 - Reprogrammed \$28,708.76 in EADR funds to current fiscal year.

**FY 2019 – Reprogrammed funding \$170,425.32 (FY 2019 Low Income Street Lighting \$164,257.17; FY 2019 Administration \$3,392.96; and FY 2019 Section 108 Repayment – Interest \$2,775.19).

***FY 2020 – Annual Action Plan Amendment addresses CDBG allocation incorrectly reported to Lakewood on October 22, 2020. Corrected allocation provided on November 9, 2020. Initial allocation reported as \$596,006; corrected allocation reported as \$595,915 (\$91 less). Amendment to decrease FY 2020 Administration allocation by \$91 to \$119,110.20. No other changes to 2020 AAP.

TABLE 2
HOME FUNDING RECOMMENDATIONS – FY 2021

	HOME
Affordable Housing Fund	\$289,557
Administration (Tacoma 10%)	\$32,173
Total Funding	\$321,730

RESOLUTION NO. 2021-07

A RESOLUTION of the City Council of the City of Lakewood, Washington, adopting the Fiscal Year 2021 (July 1, 2021 – June 30, 2022) Joint Tacoma and Lakewood Consolidated Annual Action Plan, including Amendment to Fiscal Year 2017, 2019 and 2020 Consolidated Annual Action Plans.

WHEREAS, the City of Lakewood, Washington, qualifies as an eligible Community Development Block Grant (CDBG) entitlement city, and the City Council has elected to pursue funding through that entitlement process; and

WHEREAS, the City of Lakewood also entered into an agreement with the City of Tacoma for participation in their Home Investment Partnership Act (HOME) Consortium Programs; and,

WHEREAS, in connection therewith, the City Council provided for a CDBG Citizens' Advisory Board; and

WHEREAS, in order to receive CDBG and HOME federal dollars for Fiscal Year 2021, commencing on July 1, 2021, HUD requires participating jurisdictions to submit an Annual Action Plan defining the proposed use of CDBG and HOME funds for the Fiscal Year 2021, which Plan must be adopted by the city councils of the participating jurisdictions and submitted to HUD no later than May 15, 2021; and

WHEREAS, the City of Lakewood's Fiscal Year 2021 CDBG and HOME funding allocations from the Department of Housing and Urban Development (HUD) shall be \$573,352 for CDBG and \$321,730 for HOME; and

WHEREAS, upon HUD's review and approval, contracts would then need to be prepared for CDBG entitlements and Tacoma/Lakewood HOME Consortium members; and

WHEREAS, in connection with such funding allocations-application process, it is appropriate that the City Council adopt a Fiscal Year 2021 Consolidated Annual Action Plan, including Amendment to Fiscal Year 2017, 2019 and 2020 Consolidated Annual Action Plans.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON HEREBY RESOLVES, as Follows:

Section 1. That the Joint Tacoma and Lakewood Fiscal Year 2021 Consolidated Annual Action Plan, including Amendment to Fiscal Year 2017, 2019 and 2020 Consolidated Annual Action Plans, a copy of which is attached hereto, marked as Exhibit “A” and incorporated herein by this reference, be, and the same hereby is, adopted.

Section 2. That this Resolution shall be in full force and effect upon passage and signatures hereon.

PASSED by the City Council this 3rd day of May, 2021.

CITY OF LAKEWOOD

Don Anderson, Mayor

Attest:

Briana Schumacher, City Clerk

Approved as to Form:

Heidi Ann Wachter, City Attorney

City of Lakewood

FY 2021 Consolidated Annual Action Plan



Tacoma-Lakewood
HOME Consortium

March 2021

Executive Summary

AP-05 Executive Summary - 91.200(c), 91.220(b)

1. Introduction

The FY 2021 Consolidated Plans for the Tacoma-Lakewood Consortium provide a framework for addressing housing and community development needs in these cities. The Plans are developed for the U.S. Department of Housing and Urban Development to define how funding is to be allocated for the following federal programs: the Community Development Block Grant Program (CDBG), HOME Investment Partnership Act (HOME), and for the City of Tacoma, Emergency Shelter Grants (ESG).

The FY 2021 Annual Action Plan (July 1, 2021 – June 30, 2022), Lakewood's second under the 5-YR Consolidated Plan (2020-2024), is a one-year plan that addresses local community and economic development needs and the resources necessary to meet the needs of low and moderate income households. The Plan identifies unique needs and assets in Lakewood and Tacoma, as well as on a regional basis. Goals and priorities of the Annual Action Plan are to be consistent with national objectives and priorities established by HUD and shall be consistent with the goals and strategies identified in the 5-YR Consolidated Plan.

Strategies identified in this Plan were built upon a broad citizen participation process in consultation with public and private agencies. Through this planning process, FY 2021 policies and priorities were developed to be consistent with those long-term goals established as part of the 5-YR Consolidated Plan (2020-2024). Priority needs identified as part of the Plan include, housing instability among residents, including homelessness; a limited supply of diverse rental and homeownership opportunities; the need for accessible, culturally competent services; and the need for safe, accessible homes and facilities. All needs identified were prioritized as high.

2. Summarize the objectives and outcomes identified in the Plan

This could be a restatement of items or a table listed elsewhere in the plan or a reference to another location. It may also contain any essential items from the housing and homeless needs assessment, the housing market analysis or the strategic plan.

The 2021 Annual Action Plan will primarily focus on the following priority needs identified in the 5-YR Consolidated Plan:

- Housing instability among residents, including homelessness – Programs will focus on rehabilitation of owner-occupied housing units, neighborhood revitalization through the removal of blighted properties, and relocation assistance for low-income households displaced through no fault of their own.

- Limited supply of diverse rental and homeownership opportunities – Funding will focus on providing additional affordable homeownership opportunities.
- Need for accessible, culturally competent services – Services will include fair housing counseling, foreclosure counseling, and financial counseling services.
- Need for safe, accessible homes and facilities – Funding will provide for sidewalk construction along the west side of Phillips Rd. SW, from Agate Dr. SW to Onyx Dr. SW, serving low income households in census tract 072105 block group 2 and census tract 072106 block groups 3 & 4. The project will serve a total population of 5,345, with 2,895 being low income individuals (52.48% LMI).

Recommended funding allocations for FY 2021, including the reprogramming of unexpended FY 2017 & FY 2019 funding, include:

- *Housing Improvements* – Funding in the amount of \$264,106.92 is recommended in support of the Major Home Repair/Sewer Loan Program; \$15,000 in support of CDBG Administration of HOME housing activities; and \$28,708.76 (reallocation of 2017 Emergency Assistance for Displaced Residents- \$28,708.76) is recommended in support of Emergency Assistance for Displaced Residents. A total of \$150,000 in program income is recommended for continued support of Major Home Repair program (CDBG - \$85,000) and Neighborhood Stabilization Program 1 revolving loan fund activities (NSP1 - \$65,000).

In addition, a total of \$298,464 in HOME funding is recommended in support of housing activities directed at the expansion of affordable homeownership opportunities. HOME funding allocations for Lakewood may be found in Tacoma's 2021 Joint Consolidated Annual Action Plan.

- *Public Services* – \$50,000 is recommended in support of fair housing counseling, foreclosure counseling, and financial counseling services for those low-income households affected by the coronavirus pandemic. Funding in support of public and human services programs is also being provided as part of the City's 1% General Fund allocation totaling \$380,000 in 2021.

- *Physical/Infrastructure Improvements* – A total of \$300,000 (2021 CDBG allocation of \$129,574.68, plus reallocation of 2019 Low Income Street Lighting - \$164,257.17, 2019 Administration - \$3,392.96, and 2019 Section 108 Repayment- Interest \$2,775.19) is recommended for neighborhood-related sidewalk improvements along the west side of Phillips Rd. SW.

- *Administrative Activities* – Funding in support of general administrative activities for CDBG are recommended at \$114,670.40.

3. Evaluation of past performance

This is an evaluation of past performance that helped lead the grantee to choose its goals or projects.

The 5-YR Consolidated Plan (2020-2024) for Lakewood identified five primary goals: 1) Stabilize existing residents; 2) Increase rental and homeownership opportunities; 3) Prevent and reduce homelessness; 4) Need of accessible, culturally competent services; and 5) Support of public infrastructure improvements.

As the Consolidated Plan moves into its second year, the City has identified funding strategies consistent with the priority needs identified in the Plan. With the close its initial year, despite the many setbacks the coronavirus pandemic has presented, the City believes itself to remain on task and on track to meet the five-year goals identified in the Plan.

Lakewood, not unlike other communities, has experienced trying times over the recent year as the global impacts of the coronavirus pandemic have continued to linger. The effects of the pandemic seem to have exacerbated income disparities and household instability, particularly among minority and low-income households. With the growing economic impacts of the pandemic and resulting increase in homelessness and income stagnation, sustainable long-term housing strategies continue to dominate the needs of many low-income households and remain the next big hurdle for communities to overcome. Housing access and affordability remain a top priority.

While the City of Lakewood does anticipate some of its first year accomplishments to be a bit delayed due to the effects of shifting funding priorities and resources towards ameliorating the effects of the coronavirus pandemic, it does expect to meet the benchmarks established as part of the overall 5-YR Consolidated Plan. A complete evaluation of the performance measures, including program expenditures and accomplishments, will be provided as part of the Consolidated Annual Performance Evaluation Report (CAPER) for FY 2020.

4. Summary of Citizen Participation Process and consultation process

Summary from citizen participation section of plan.

The FY 2021 Annual Action Plan was developed in coordination with the City of Tacoma and local organizations, agencies, and stakeholders. The Plan was developed in accordance with requirements established by the U.S. Department of Housing and Urban Development and the Citizen Participation Plan as adopted by the City of Lakewood. Citizen activities included notification in the Tacoma News Tribune on January 25, 2021 of a February 10, 2021 public hearing held by city staff and the Lakewood Community Services Advisory Board (CSAB) for FY 2021 (July 1, 2021 – June 30, 2022) Community

Development Block Grant (CDBG) and HOME Investment Partnership Act (HOME) program funding. Citizen input was solicited to identify community and economic development, housing, and public service's needs.

Notification of the Draft FY 2019 Annual Action Plan will be published on March 31, 2021 in the Tacoma News Tribune indicating where the document were available for review during the 30-day citizen comment period (April 1, 2021 – April 30, 2021). A public hearing by the Lakewood City Council will be held on April 19, 2021 to solicit comments from citizens, local for-profit and non-profit agencies, neighborhood associations, the State of Washington, Pierce County, City of Tacoma, local public housing authorities, and other interested parties on the draft document. Copies of the Plan will be made available to those requesting copies. Records on the proposed housing and community development projects will be made accessible to the general public. Comments received at the public hearings held by the Lakewood CSAB and Lakewood City Council will be summarized later in this report.

5. Summary of public comments

This could be a brief narrative summary or reference an attached document from the Citizen Participation section of the Con Plan.

The following comments were received as part of the FY 2021 citizen participation process. They include testimony provided at the February 10, 2021 public hearing and the Lakewood City Council public hearing on April 19, 2021:

2021 Annual Action Plan Public Hearing – February 10, 2021

At the February 10, 2021 public hearing held by the City staff and Lakewood Community Services Advisory Board, the City multiple comments from five (5) public attendees. Comments received included support for the development of additional affordable housing units, both rental and homeowner; the need for nontraditional housing units with 4 and 5 bedrooms; increased need for housing counseling services as eviction moratoriums expire; support for multi-unit projects to make development more attractive as costs continue to escalate; support for what is seen as a growing need for transitional and shelter type housing; the need for additional supportive housing options; the need of mental and health care public services, including domestic violence and substance abuse counseling; financial assistance for small businesses; a strong need for rental and mortgage assistance programs; the development of small housing, including tiny houses and micro houses; graffiti abatement funding; the need to fund food assistance programs; land banking and land trust models for housing development; and finally, the need for additional community meeting spaces and facilities.

(Insert comments here)

6. Summary of comments or views not accepted and the reasons for not accepting them

There were no comments or views received that were not accepted.

7. Summary

FY 2020 Annual Action Plan Amendment – Includes a correction of Lakewood’s CDBG allocation incorrectly reported as \$596,006 on October 22, 2020. A corrected allocation of \$595,915 was provided by HUD on November 9, 2020. This amendment decreases FY 2020 Administration allocation by \$91 to \$119,110.20. No other changes proposed to the FY 2020 Annual Action Plan.

Section 108 – Principal repayment for 2021 includes \$31,000 in principal repayment by Curbside Motors and \$16,000 in principal for repayment by LASA for the Client Service Center project. The table below summarizes the City's Section 108 outstanding balances and scheduled principal repayments:

PROJECT	SECTION 108 BALANCE	ANTICIPATED INCOME - 2021	PRINCIPAL REPAYMENT - 2021
Curbside Motors	\$540,000	\$31,000	\$31,000
LASA Client Service Center	\$295,000	\$31,521.75	\$16,000

Table 1 - Section 108 Loan Program - 2021

PR-05 Lead & Responsible Agencies - 91.200(b)

1. Agency/entity responsible for preparing/administering the Consolidated Plan

The following are the agencies/entities responsible for preparing the Consolidated Plan and those responsible for administration of each grant program and funding source.

Agency Role	Name	Department/Agency
CDBG Administrator	LAKEWOOD	Community Development Department

Table 2 – Responsible Agencies

Narrative

In the City of Lakewood, CDBG funding is administered by the Community Development Department with oversight provided by the Council-appointed Community Services Advisory Board (CSAB) and Lakewood City Council. As a member of the Tacoma-Lakewood HOME Consortium, Lakewood receives HOME funding through the Tacoma Community Redevelopment Authority (TCRA). HOME funding allocations are initially approved by the Lakewood City Council before they are forwarded on to the TCRA for final review and approval.

Consolidated Plan Public Contact Information

City of Lakewood
Community Development Department
Attn: Mr. Dave Bugher, Assistant City Manager
6000 Main Street SW.
Lakewood, WA 98499
(253) 589-2489
dbugher@cityoflakewood.us

AP-10 Consultation - 91.100, 91.200(b), 91.215(l)

1. Introduction

The FY 2021 Annual Action Plan was developed to provide a general framework for addressing housing and community development needs in the City of Lakewood. In a larger sense, the plan involves a regional element to coordinate and address economic development, transportation, public services, special needs, housing, and homelessness throughout Pierce County. Coordination included consultation with the City of Tacoma, Pierce County, Pierce County Housing Authority, the Tacoma Housing Authority, and other stakeholders as provided in the Plan.

Provide a concise summary of the jurisdiction's activities to enhance coordination between public and assisted housing providers and private and governmental health, mental health and service agencies (91.215(l)).

As part of the planning process, the City of Lakewood established a committee to provide public oversight, comment on the process, and to review funding recommendations for both the CDBG and HOME programs. As part of the development of the 5-YR Consolidated Plan (2020-2024), the City solicited input through interviews, meetings, and public hearings to determine community need, priorities, and approaches to meeting those needs. Since the development of the 5-Year Plan, the City has continued to consult with many of the agencies and organizations originally polled.

The Pierce County Housing Authority and the Tacoma Housing Authority continue to work closely with the cities of Lakewood and Tacoma coordinating efforts to improve housing choice for low income households in both communities. Additionally, local housing and services providers such as Living Access Support Alliance (LASA), Greater Lakes Mental Health, Tacoma-Pierce County Habitat for Humanity, the Homeownership Center of Tacoma, Associated Ministries, the Fair Housing Center of Washington, and the Tacoma Pierce County Affordable Housing Consortium among others continue to provide input and support for mental health, services, and housing options both in Lakewood and Tacoma.

As part of the City's human services funding process, monthly coalition meetings are held at the City to bring together non-profits, service providers and governmental agencies to help determine and better understand the need for housing and human services in Lakewood and throughout Pierce County. Consultation with Joint Base Lewis McChord was also conducted as a part of the consolidated planning process to determine what level of assistance military personnel and veterans were experiencing with housing need, health and human services, and mental health care assistance.

Lakewood sought comment internally from City departments on housing and community development needs and services to low income and special needs populations. On a regional level, the consolidated planning process involved consultation with Pierce County Community Connections, the City of Tacoma,

United Way of Pierce County, the Pierce County Housing Authority, Tacoma-Pierce County Habitat for Humanity, the Homeownership Center of Tacoma, Living Access Support Alliance (LASA), Lakewood Community services Advisory Board, local school districts, police and fire departments, and the State of Washington to better understand the needs of at risk populations such as homeless families with children, victims of domestic violence, individuals with disabilities, ethnic minorities, and the elderly.

Additional efforts to enhance and coordinate efforts between housing, health, and services providers are described throughout the Plan.

Describe coordination with the Continuum of Care and efforts to address the needs of homeless persons (particularly chronically homeless individuals and families, families with children, veterans, and unaccompanied youth) and persons at risk of homelessness.

The Cities of Tacoma and Lakewood are actively engaged members of the Tacoma/Lakewood/Pierce Road Home County Continuum of Care Committee. The three jurisdictions are the most involved governmental entities in the Continuum, cooperatively working on programs to meet needs for housing and services. Both Tacoma and Lakewood support the Continuum's priorities focusing on the needs of the most vulnerable populations including chronically homeless persons, unaccompanied youth, families with children, and veterans, among others. The mission of the Road Home Continuum of Care is to promote community wide commitment to ending homelessness through policy and resource alignment by implementing activities to achieve the goals and objectives of the Plan to End Homelessness.

In recent years, the Continuum has moved from a single point of access, or centralized intake, to that of a coordinated entry system providing many points of entry. This system, when coupled with an active Diversion program, known as Housing Solutions Conversations, aims at keeping those in housing crisis from entering the system by supporting their own identification of a solution and prioritizes them for a specific housing referral based on their vulnerabilities and the severity of their barriers to secure housing. Through this partnership and implementation of the Five-Year Plan to Address Homelessness, stands a county-wide commitment to ensure all persons facing homelessness have access to shelter and support – no matter their social or economic circumstances or where they live within Pierce County.

As housing prices and market-rate rents continue to rise at alarming rates and housing options are pushed out of reach of many, this coordinated and cooperative effort to ensure affordable housing stability and access to a home endure as a realistic and attainable goal for all in our community.

Describe consultation with the Continuum(s) of Care that serves the jurisdiction's area in determining how to allocate ESG funds, develop performance standards for and evaluate outcomes of projects and activities assisted by ESG funds, and develop funding, policies and procedures for the operation and administration of HMIS

The Cities of Tacoma and Lakewood work closely with the Collaborative Applicant of the Continuum of Care (Pierce County) planning for allocation and use of Emergency Solutions Grant (ESG) funds. ESG policies and procedures were created and are updated periodically in cooperation with Pierce County and Tacoma to ensure that ESG subrecipients are operating programs consistently across eligible activities. Performance is reviewed by both entities. The Collaborative Applicant is also the HMIS lead and works closely with City of Tacoma to maximize use of HMIS resources and to draw data for reports on project performance and program outcomes. The City of Lakewood does not receive ESG funding.

2. Agencies, groups, organizations and others who participated in the process and consultations

N/A

Table 3 – Agencies, groups, organizations who participated

1	Agency/Group/Organization	City of Tacoma
	Agency/Group/Organization Type	Other government - Local
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Public Housing Needs Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Homelessness Strategy Non-Homeless Special Needs Market Analysis Economic Development Anti-poverty Strategy Lead-based Paint Strategy
	Briefly describe how the Agency/Group/Organization was consulted. What are the anticipated outcomes of the consultation or areas for improved coordination?	As the lead member of the Tacoma-Lakewood HOME Consortium, Tacoma remains a key partner in the development and implementation of the consolidated planning process. The City of Tacoma was consulted in the development of priorities and strategies designed to meet the various community and economic development needs identified in this Plan. Lakewood and Tacoma will continue to coordinate their efforts to ensure the goals and outcomes identified in the 5-YR Consolidated Plan (2020-2024) are satisfied.
2	Agency/Group/Organization	Pierce County Community Connections
	Agency/Group/Organization Type	Other government - County

	What section of the Plan was addressed by Consultation?	Housing Need Assessment Public Housing Needs Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Homelessness Strategy Non-Homeless Special Needs Market Analysis Economic Development Anti-poverty Strategy Lead-based Paint Strategy
	Briefly describe how the Agency/Group/Organization was consulted. What are the anticipated outcomes of the consultation or areas for improved coordination?	Pierce County Community Connections is a partner agency in a regional effort to end homelessness, encourage community and economic development efforts benefitting low income individuals, to expand safe, decent, affordable housing, and in the provision of public and human services to individuals in need. As a recipient of CDBG, HOME, and ESG funding, the County is a natural partner with the cities of Lakewood and Tacoma in determining a regional approach to housing and community development activities.
3	Agency/Group/Organization	Pierce County Housing Authority
	Agency/Group/Organization Type	PHA
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Public Housing Needs Homelessness Strategy Market Analysis Anti-poverty Strategy

	Briefly describe how the Agency/Group/Organization was consulted. What are the anticipated outcomes of the consultation or areas for improved coordination?	The Pierce County Housing Authority (PCHA) is a vital partner to the City of Lakewood in its efforts to increase and preserve affordable housing options for low income households. PCHA manages a number of programs such as scattered site public housing, Section 8 vouchers, and enterprise fund apartments to provide housing stability to many low income Lakewood households. As an operator of 285 apartment units in Lakewood, the housing authority was consulted to provide information on the need for public housing in Lakewood and in greater-Pierce County. Lakewood anticipates continued cooperation with PCHA in developing and maintaining affordable public housing options.
4	Agency/Group/Organization	Tacoma Pierce County Habitat for Humanity
	Agency/Group/Organization Type	Housing
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Homeless Needs - Families with children Homelessness Needs - Veterans Non-Homeless Special Needs Market Analysis Anti-poverty Strategy
	Briefly describe how the Agency/Group/Organization was consulted. What are the anticipated outcomes of the consultation or areas for improved coordination?	Tacoma-Pierce County Habitat for Humanity remains an integral part of Lakewood's approach to providing safe, decent, affordable housing to low income households in Lakewood and Tacoma. Lakewood recognizes the crucial link between wealth-building and homeownership, especially for minority and low income households. Consultation with the agency focused on housing need, market analysis, anti-poverty strategy, veteran housing opportunities, and special needs housing. Lakewood anticipates continued coordination with Habitat will increase affordable housing options for low income families and improvements to distressed communities as older blighted homes are replaced with newly constructed affordable single family homes.

5	Agency/Group/Organization	LASA
	Agency/Group/Organization Type	Housing Services - Housing Services-homeless
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Homelessness Strategy
	Briefly describe how the Agency/Group/Organization was consulted. What are the anticipated outcomes of the consultation or areas for improved coordination?	LASA was consulted as part of the planning process to better define and understand the scope and type of homeless need in Pierce County. LASA continues to be an integral provider in the fabric of homeless services and housing assistance to the homeless and those at risk of homelessness. With LASA's ongoing partnerships with the Pierce County and Tacoma Housing Authorities, and operation of housing and services facilities for the homeless, LASA is uniquely positioned to understand and assist the homeless population in Lakewood and Pierce County. Partnership and coordination with LASA allows Lakewood a better understanding of the needs of those living at or below the poverty rate, as well as what seems to be an ever-increasing demand for homeless services. Current partnership with LASA involves the expansion of homeless services, including temporary and shelter options for those experiencing homelessness.
6	Agency/Group/Organization	HOMEOWNERSHIP CENTER OF TACOMA
	Agency/Group/Organization Type	Housing
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Market Analysis Anti-poverty Strategy

	Briefly describe how the Agency/Group/Organization was consulted. What are the anticipated outcomes of the consultation or areas for improved coordination?	The City continues to partner with the Homeownership Center of Tacoma to create additional affordable housing options for low income households. Consultation with the agency included housing need, market analysis, and anti-poverty strategy. Lakewood anticipates continued coordination with the Homeownership Center will provide additional affordable housing options to low income households seeking homeownership opportunities through targeted redevelopment of blighted properties with newly constructed affordable single family homes.
7	Agency/Group/Organization	Greater Lakes Mental Health
	Agency/Group/Organization Type	Services-Health Health Agency
	What section of the Plan was addressed by Consultation?	Homeless Needs - Chronically homeless Homelessness Needs - Veterans Homelessness Strategy
	Briefly describe how the Agency/Group/Organization was consulted. What are the anticipated outcomes of the consultation or areas for improved coordination?	The City of Lakewood provides funding to the Behavior Health Contact Team which partners mental health professionals with police to assess persons in crisis & provide de-escalation, in-place treatment, and stabilization services. The team is a critical outreach tool for the City's effort to better understand and serve the homeless population in Lakewood and the needs that population may need in the way of mental health services.

Identify any Agency Types not consulted and provide rationale for not consulting

N/A

Other local/regional/state/federal planning efforts considered when preparing the Plan

Name of Plan	Lead Organization	How do the goals of your Strategic Plan overlap with the goals of each plan?
Continuum of Care	Pierce County	The mission of the Road Home Continuum of Care is to promote community wide commitment to ending homelessness through policy and resource alignment by implementing activities to achieve the goals and objectives of the Plan to End Homelessness.
City of Lakewood Comprehensive Plan	City of Lakewood	The plan encourages infill housing, cottage-style development, changes in zoning to permit higher densities, and incentivizes the construction of affordable housing.

Table 4 - Other local / regional / federal planning efforts

Narrative

AP-12 Participation - 91.401, 91.105, 91.200(c)

1. Summary of citizen participation process/Efforts made to broaden citizen participation Summarize citizen participation process and how it impacted goal-setting

Citizen input for the FY 2021 Annual Action Plan was conducted in accordance with the requirements established by the U.S. Department of Housing and Urban Development and the City of Lakewood Citizen Participation Plan. Citizen participation was encouraged throughout the process culminating in the development of this FY 2021 Annual Action Plan.

The process included:

- Input from community-based organizations, service providers, and local non-profit organizations.
- Oversight and review provided by the City of Lakewood Community Services Advisory Board and the Tacoma Community Redevelopment Authority (TCRA).
- Input and coordination with the City of Lakewood, City of Tacoma, and Pierce County.
- Meetings with Lakewood and Tacoma City Councils.
- Review of planning documents for local agencies.
- Solicitation of public input.

Effort to encourage citizen participation in the development of the Plan included notification of public hearings on housing and community development need, notice of availability of the plan, and community input concerning approval the final plan. Public hearings were advertised in the Tacoma News Tribune as follows:

- February 10, 2021 – Public hearing on housing and community development needs
- April 1 - April 30, 2021 – Public Comment Period
- April 19, 2021 – Public hearing on FY 2021 Annual Action Plan

A draft of the FY 2021 Annual Action Plan will be made available to the public for review beginning April 1, 2021. Comments received through the citizen participation process will be incorporated into the Plan. Formal approval of the final Plan will be made by the City Council on May 3, 2021.

Citizen Participation Outreach

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
1	Newspaper Ad	Non-targeted/broad community	Notification of February 10, 2021 public hearing on housing and community development needs for CDBG and HOME funding.	Comments detailing the public hearing are summarized in item #2 below.	N/A	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
2	Public Meeting	Non-targeted/broad	Annual Action Plan February 10, 2021 public meeting of City staff and Community Services Advisory Board, 2021	Comments received included support for the development of additional affordable housing units, both rental and homeowner; the need for nontraditional housing units with 4 and 5 bedrooms; increased need for housing counseling services as eviction moratoriums expire; support for multi-unit projects to make development more attractive as costs continue to escalate; support for what is seen as a growing need for transitional and shelter type housing; the need for additional supportive housing options; the need of mental and health services	19	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
3	Newspaper Ad	Non-targeted/broad community	Notification of April 19, 2021 public hearing and April 1 - April 30, 2021 public comment period.	Comments detailing the public hearing will be summarized in item #5 below.	N/A	
4	Internet Outreach	Non-targeted/broad community	Notification of hearings and provision of Annual Action Plan.	Comments detailing the public hearing will be summarized in item #5 below.	N/A	
5	Public Hearing	Non-targeted/broad community	April 19, 2021 public hearing held by the Lakewood City Council.	Comments to be inserted here.		
6	30-day Public Comment Period	Non-targeted/broad community	April 1 - April 30, 2021 30-day public comment period.	Comments to be inserted here.		

Table 5 – Citizen Participation Outreach

Expected Resources

AP-15 Expected Resources - 91.420(b), 91.220(c)(1,2)

Introduction

Funding for FY 2021 is provided by the U.S. Department of Housing and Urban Development through the Community Development Block Grant (CDBG) Program. Additional programmatic funding is being generated through program income and the reprogramming of unexpended funds from fiscal years 2017 and 2019. All funds received, and otherwise reprogrammed, will be allocated to activities in support of the goals and objectives identified in the 5-YR Consolidated Plan. Additional funding for public service activities will be supported with General Fund dollars in accordance with a City Council-adopted strategic plan for human services.

CDBG funding in FY 2021 will focus on housing instability among residents, including homelessness; the expansion of diverse homeownership opportunities; the need for accessible, culturally competent services; and the need for safe, accessible homes and facilities. Specifically, investments will include the rehabilitation of owner-occupied housing units, neighborhood revitalization through the removal of blighted properties, relocation assistance for low-income households, providing additional affordable homeownership opportunities through the construction of new single family homes, foreclosure and financial counseling, and fair housing activities, and construction of sidewalks along the west side of Phillips Rd. W between Agate Dr. SW and Phillips Dr. SW.

Homeless prevention services, specifically relocation assistance for low income individuals displaced through no fault of their own during fire or code enforcement actions, will continue to be addressed with reprogrammed FY 2017 CDBG funding. Finally, activities seeking the stabilization of existing residents through the creation of safe, accessible homes and facilities will focus on the redevelopment of slum and blight conditions in communities adversely impacted by disproportionate foreclosure and vacancy rates through the City's Neighborhood Stabilization Program 1 Dangerous and Nuisance Abatement Revolving Loan Fund.

Additional HOME-funded activities in support of the expansion of diverse homeownership opportunities will include the funding of the HOME Affordable Housing Fund, which is intended to support the development of new housing and homeownership opportunities for low income households. HOME funding activities are further detailed in Tacoma's Annual Action Plan as lead agency in the Tacoma-Lakewood HOME Consortium.

The City of Lakewood has employed a conservative approach for estimating CDBG and HOME allocations and program income for FY 2021 and the remainder of the 5-YR Consolidated Plan.

Anticipated Resources

Program	Source of Funds	Uses of Funds	Expected Amount Available Year 1				Expected Amount Available Remainder of ConPlan \$	Narrative Description
			Annual Allocation: \$	Program Income: \$	Prior Year Resources: \$	Total: \$		
CDBG	public - federal	Acquisition Admin and Planning Economic Development Housing Public Improvements Public Services	573,352	85,000	199,134	857,486	1,650,000	Prior year resources include reprogrammed funds from the following FY 2017 & FY 2019 activities: 1) Activity 206, Emergency Assistance for Displaced Residents- \$28,708.76; Activity 215, COL Street Lighting- \$164,257.17; 2) Activity 219, Administration- \$3,392.96; and 3) Activity 221, Section 108 Repayment- Interest- \$2,775.19. Anticipated program income of \$85,000 is composed of RLF activities for Major Home Repair and Down Payment Assistance. Section 108 income of \$60,419.70 is not included in this total.

Program	Source of Funds	Uses of Funds	Expected Amount Available Year 1				Expected Amount Available Remainder of ConPlan \$	Narrative Description
			Annual Allocation: \$	Program Income: \$	Prior Year Resources: \$	Total: \$		
Other	public - federal	Economic Development Housing Public Improvements Other	65,000	0	0	65,000	160,000	NSP1 funds were awarded to Lakewood in 2009 through the Washington State Department of Commerce to address issues of slums and blight through the demolition of homes that have been foreclosed, abandoned or have been left vacant. Funds may also be used to acquire and redevelop foreclosed and abandoned properties for the purpose of constructing safe, decent, affordable housing for low income individuals. Anticipated program income of \$65,000 for NSP1 Abatement Fund RLF activities.

Table 6 - Expected Resources – Priority Table

Explain how federal funds will leverage those additional resources (private, state and local funds), including a description of how matching requirements will be satisfied

Leverage ratios for 2021 CDBG projects are anticipated to remain very low as a majority of funding is being targeted to housing rehabilitation of homeowner occupied units and a small-scale infrastructure project. Homeowner rehabilitation projects usually see little to no leveraging of funds as the program typically assists clients who would otherwise be unable to pursue commercial loans to repair their home were it not for this type of program. Many clients have credit issues restricting their access to funding, limited incomes restricting their ability to deb service a conventional loan, or simply limited equity in their homes in which to access to make renovations or improvements. Infrastructure funding for the construction of sidewalks along Phillips Rd. SW is expected to generate matching funds in the amount of \$152,000.

Housing/foreclosure counseling and fair housing activities are anticipated to be funded solely with 2021 CDBG funding, including CDBG

Administration funds. HOME funds are likely to see larger leverage ratios as housing development projects conducted by non-profit housing developers like Tacoma Pierce County Habitat and the Homeownership Center of Tacoma typically see leverage ratios between 2.5:1 and 3.5:1. HOME funds match requirements and leverage is provided as part of the Tacoma-Lakewood HOME Consortium and is reported in Tacoma's portion of the Plan.

If appropriate, describe publically owned land or property located within the jurisdiction that may be used to address the needs identified in the plan

No publicly-owned land or property is scheduled to be included as a part of this plan.

Discussion

Annual Goals and Objectives

AP-20 Annual Goals and Objectives - 91.420, 91.220(c)(3)&(e)

Goals Summary Information

Sort Order	Goal Name	Start Year	End Year	Category	Geographic Area	Needs Addressed	Funding	Goal Outcome Indicator
1	Stabilize existing residents	2020	2024	Affordable Housing Homeless Non-Housing Community Development		Housing instability, including homelessness Need for safe, accessible homes and facilities	CDBG: \$349,107 NSP1: \$65,000	Homeowner Housing Rehabilitated: 10 Household Housing Unit Buildings Demolished: 3 Buildings
2	Increase rental and homeownership opportunities	2020	2024	Affordable Housing Public Housing		Affordable rental and homeowner opportunities	CDBG: \$15,000	Homeowner Housing Added: 2 Household Housing Unit
3	Prevent and reduce homelessness	2020	2024	Affordable Housing Homeless Non-Homeless Special Needs		Housing instability, including homelessness Need for accessible, culturally competent services	CDBG: \$28,709	Homelessness Prevention: 12 Persons Assisted
4	Need of accessible, culturally competent services	2020	2024	Homeless Non-Housing Community Development		Housing instability, including homelessness Need for accessible, culturally competent services	CDBG: \$50,000	Public service activities other than Low/Moderate Income Housing Benefit: 75 Persons Assisted

Table 7 – Goals Summary

Annual Action Plan
2021

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Goal Description

1	Goal Name	Stabilize existing residents
	Goal Description	Funding provides support of the stabilization of residents experiencing homelessness or displacement pressure. Activities include the creation of new or maintenance of existing affordable housing stock for low income homeowners or renters and the elimination of slums and blight through the demolition and redevelopment of blighted properties.
2	Goal Name	Increase rental and homeownership opportunities
	Goal Description	Projects in support of rental and homeownership opportunities include activities to create new homeownership opportunities for low income households through the construction of new, affordable single-family housing units.
3	Goal Name	Prevent and reduce homelessness
	Goal Description	Funding provides for services engaged in the reduction and prevention of homelessness through the provision of relocation assistance for households displaced through no fault of their own.
4	Goal Name	Need of accessible, culturally competent services
	Goal Description	Provides for increased access to culturally competent services for low income individuals with emphasis on extremely low income individuals, persons of color, immigrants, and the elderly. Activities include fair housing counseling, foreclosure counseling, and financial counseling aimed at those households most impacted by COVID 19. Some assistance is anticipated to be provided through CDBG administrative activities focusing on fair housing education and outreach, and landlord tenant education, and as such, will not be reported as clients served in the Consolidated Plan or subsequent Annual Action Plans due to CDBG administration reporting requirements.
5	Goal Name	Support of public infrastructure improvements
	Goal Description	Funding will provide for sidewalk construction along the west side of Phillips Rd. SW, from Agate Dr. SW to Onyx Dr. SW, serving low income households in census tract 072105 block group 2 and census tract 072106 block groups 3 & 4. The project will serve a total population of 5,345, with 2,895 being low income individuals (52.48% LMI).

AP-35 Projects - 91.420, 91.220(d)

Introduction

The City of Lakewood will aim to implement its federal funds in 2021 to accomplish the following goals and corresponding activities:

- **Stabilize existing residents** – Through funds for owner-occupied housing rehabilitation and renovation programs and sewer connections to maintain existing affordability, and through the removal of slums and blight by addressing and demolishing dangerous structures.
- **Increase diverse rental and homeownership opportunities** – Providing homeownership opportunities to low and moderate income families through the construction of new affordable housing units using the Affordable Housing Fund administered by TCRA.
- **Prevent and reduce homelessness** – Funding provided for emergency assistance for displaced residents for renters who have been displaced through no fault of the own.
- **Increase availability of accessible, culturally competent services** – Through funds for housing and foreclosure counseling and fair housing activities designed to assist low and moderate income households maintain housing access and affordability, and provide landlord tenant education and outreach.
- **Need for safe, accessible homes and facilities** – Funding will include provide for sidewalk construction along the west side of Phillips Rd. SW, from Agate Dr. SW to Onyx Dr. SW, serving low income households in census tract 072105 block group 2 and census tract 072106 block groups 3 & 4. The project will serve a total population of 5,345, with 2,895 being low income individuals (52.48% LMI).

The projects listed and described in this plan represent the proposed use of funds for the FY 2021 CDBG program.

#	Project Name
1	Administration
2	CDBG Administration of HOME Housing Services
3	Major Home Repair/Sewer Loan Program
4	Housing/Foreclosure Counseling & Fair Housing Counseling
5	Emergency Assistance for Displaced Residents
6	NSP1 Abatement Program
7	City of Lakewood Low Income Sidewalk Improvements

Table 8 – Project Information

Describe the reasons for allocation priorities and any obstacles to addressing underserved needs

Funding in FY 2021 will focus primarily upon the need to address housing instability, including homelessness; the need to increase the supply of diverse homeownership opportunities; the need to expand accessible, culturally competent services to low and moderate income households; and the need for safe, accessible homes and facilities. With housing prices and market rate rents continuing to rise, coupled with the impacts the coronavirus pandemic has unevenly leveled against minority and low income households, many low and moderate income and minority households have found themselves unemployed or with significant reductions in household income, pushed to the brink of housing instability, or simply find themselves homeless.

This is especially true of African American households whose average household wealth is already 10 times lower than white households (2016 Pew Research), and who are more likely to work for wages below the poverty level with 8% of black workers compared to 4% of white workers working for wages below the federal poverty level (2019 Bureau of Labor Statics). These factors, when coupled with historically low rates of homeownership, 41% for African American households compared to 71% of white households (2001-2016 American Community Survey), and double digit inflation of rental rates, have placed many African American households in an increasingly disadvantaged position.

Activities will include the funding of housing rehabilitation programs aimed at assisting low and moderate income households with housing repairs and renovations they may not otherwise be able to afford through conventional financing options; homeownership assistance programs directed toward expanding homebuyer assistance and the long-term wealth-building opportunities these programs afford low and moderate income households; providing housing stability and homelessness prevention through the provision of relocation assistance to families forced to relocate due to no fault of their own; through reduction of slums and blight in low income neighborhoods with the demolition and redevelopment of dangerous buildings; through financial and housing counseling programs and fair housing counseling services aimed at alleviating the economic impacts the coronavirus and rising skyrocketing housing costs have had on minority and low income households; and finally, through new access to infrastructure improvements seeking to improve community connectivity and access where such improvements are lacking or inadequate to ensure public safety and accessibility.

AP-38 Project Summary

Project Summary Information

1	Project Name	Administration
	Target Area	
	Goals Supported	Need of accessible, culturally competent services
	Needs Addressed	Need for accessible, culturally competent services
	Funding	CDBG: \$114,670
	Description	Administration to implement and manage the Consolidated Plan funds. Activities include providing fair housing and landlord tenant outreach and education.
	Target Date	6/30/2022
	Estimate the number and type of families that will benefit from the proposed activities	Administration funds are anticipated to be expended in support of general administration activities to administer the 2021 Annual Action Plan, including funding for fair housing activities in support of 25 low and moderate income households.
	Location Description	
2	Planned Activities	Administration funds are anticipated to be expended in support of general administration activities to administer the 2021 Annual Action Plan, including funding for fair housing activities in support of 25 low and moderate income persons.
	Project Name	CDBG Administration of HOME Housing Services
	Target Area	
	Goals Supported	Increase rental and homeownership opportunities
	Needs Addressed	Affordable rental and homeowner opportunities
	Funding	CDBG: \$15,000
	Description	Program administration and housing services in support of HOME Program.
	Target Date	6/30/2022
	Estimate the number and type of families that will benefit from the proposed activities	A total of 2 low and moderate income households will be assisted with HOME-funded activities.
	Location Description	City-wide

	Planned Activities	Program administration and housing services in support of HOME Program.
3	Project Name	Major Home Repair/Sewer Loan Program
	Target Area	
	Goals Supported	Stabilize existing residents
	Needs Addressed	Housing instability, including homelessness
	Funding	CDBG: \$349,107
	Description	Program provides home repair and/or sewer connection loans to eligible low income homeowners. Funding for program provided through FY 2021 entitlement funding and revolving loan funds.
	Target Date	6/30/2022
	Estimate the number and type of families that will benefit from the proposed activities	A total of 10 low income households will be assisted with housing rehabilitation activities or side sewer connections.
	Location Description	City-wide
	Planned Activities	The project will provide major home repair/sewer connection loans which include connection of side-sewer to sewer main, decommissioning of septic systems, roofing, removal of architectural barriers, plumbing, electrical, weatherization, major systems replacement/upgrade, and general home repairs for eligible low income homeowners. Program funding includes a total of \$85,000 in program income to be used for similar RLF activities
4	Project Name	Housing/Foreclosure Counseling & Fair Housing Counseling
	Target Area	
	Goals Supported	Stabilize existing residents Prevent and reduce homelessness
	Needs Addressed	Housing instability, including homelessness Need for accessible, culturally competent services
	Funding	CDBG: \$50,000
	Description	Program provides housing counseling, foreclosure counseling, and fair housing counseling for low and moderate income individuals facing eviction, foreclosure or other fair housing related issues.
	Target Date	6/30/2022

	Estimate the number and type of families that will benefit from the proposed activities	Activities including the provision of housing and foreclosure counseling and fair housing services are anticipated to benefit 75 low and moderate income individuals.
	Location Description	City-wide
	Planned Activities	Program provides housing counseling, foreclosure counseling, and fair housing counseling for low and moderate income individuals facing eviction, foreclosure or other fair housing related issue.
5	Project Name	Emergency Assistance for Displaced Residents
	Target Area	
	Goals Supported	Stabilize existing residents Prevent and reduce homelessness
	Needs Addressed	Housing instability, including homelessness
	Funding	CDBG: \$28,709
	Description	Program provides emergency relocation assistance to eligible low income households that are displaced through no fault of their own during building and code enforcement closures, fires, redevelopment, and other incidences resulting in homelessness.
	Target Date	
	Estimate the number and type of families that will benefit from the proposed activities	12 individuals will be assisted with emergency rental assistance for persons displaced due to no fault of their own.
	Location Description	City-wide
6	Planned Activities	Activities include emergency relocation assistance to low income individuals displaced due to no fault of their own. Assistance to include rental assistance, deposits, and other related relocation expenses. Funding for this activity will be provided through reprogrammed FY 2017 CDBG Emergency Assistance for Displaced Residents program funding.
	Project Name	NSP1 Abatement Program
	Target Area	
	Goals Supported	Stabilize existing residents
	Needs Addressed	Need for safe, accessible homes and facilities

	Funding	NSP1: \$65,000
	Description	Provides funding for the abatement of dangerous buildings that have been foreclosed, abandoned or are vacant. Activities funded with revolving loan fund.
	Target Date	6/30/2022
	Estimate the number and type of families that will benefit from the proposed activities	The City of Lakewood anticipates it will demolish a total of 3 dangerous buildings located in low income block groups in FY 2021 with the NSP1 Abatement Fund.
	Location Description	City-wide
	Planned Activities	Provides funding for the abatement of dangerous buildings that have been foreclosed, abandoned or are vacant. Funding for the program is provided through revolving loan funds generated from previous NSP1 abatement activities. Program funding includes a total of \$65,000 in program income to be used for similar RLF activities.
7	Project Name	City of Lakewood Low Income Sidewalk Improvements
	Target Area	
	Goals Supported	Support of public infrastructure improvements
	Needs Addressed	Need for safe, accessible homes and facilities
	Funding	CDBG: \$300,000
	Description	Funding will provide for sidewalk construction along the west side of Phillips Rd. SW, from Agate Dr. SW to Onyx Dr. SW, serving low income households in census tract 072105 block group 2 and census tract 072106 block groups 3 & 4. Construction will provide for sidewalks where none currently exist. The project will serve a total population of 5,345, with 2,895 being low income individuals (52.48% LMI).
	Target Date	6/30/2022
	Estimate the number and type of families that will benefit from the proposed activities	The project will serve low income households in census tract 072105 block group 2 and census tract 072106 block groups 3 & 4. Total population served of 5,345, with 2,895 being low income individuals (52.48% LMI).
	Location Description	Sidewalk construction along the west side of Phillips Rd. SW, from Agate Dr. SW to Onyx Dr. SW.

	Planned Activities	The project will provide for sidewalk construction along the west side of Phillips Rd. SW, from Agate Dr. SW to Onyx Dr. SW, serving low income households in census tract 072105 block group 2 and census tract 072106 block groups 3 & 4. Construction will provide for sidewalks where none currently exist. The project will serve a total population of 5,345, with 2,895 being low income individuals (52.48% LMI).
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AP-50 Geographic Distribution - 91.420, 91.220(f)

Description of the geographic areas of the entitlement (including areas of low-income and minority concentration) where assistance will be directed

In targeting CDBG and HOME funds, the City has typically looked to block groups with at least 51% low- and moderate-income populations as many of Lakewood's minority and ethnic populations continue to be concentrated in these areas. Many of these block groups tend to have large concentrations of older housing stock suffering from a lack of routine maintenance and infrastructure that is either inadequate or are outdated in accordance with current development requirements.

In 2021, Lakewood's second year of the 5-YR Consolidated Plan (2020-2024), funding prioritization will focus on increasing and preserving affordable housing stock through owner-occupied housing rehabilitation and the construction of new affordable homes, the prevention of homelessness through emergency rental assistance for displaced residents, improved access to housing and foreclosure counseling and fair housing activities directed at maintaining housing access and affordability for low and moderate income households, and the need for safe, accessible homes and facilities through the improved access to sidewalk improvements along the west side of Phillips Rd. SW, from Agate Dr. SW to Onyx Dr. SW. Additionally, the City plans to continue to target households living in Census Tracts 718.05, 718.06, 718.07 and 720.00 for its Major Home Repair/Sewer Loan Program, which assists low- and moderate-income homeowners make necessary improvements to their homes, including connecting to recently constructed sewers in the 720.00.

For all other funding, the City has not identified specific targeted areas; programs are open to eligible low and moderate income individuals city-wide.

Geographic Distribution

Target Area	Percentage of Funds

Table 9 - Geographic Distribution

Rationale for the priorities for allocating investments geographically

N/A

Discussion

AP-75 Barriers to affordable housing -91.420, 91.220(j)

Introduction

As identified in the 5-YR Consolidated Plan (2020-2024), low incomes, high housing costs, overcrowding, homelessness, and aging all present challenges to Lakewood residents which can have negative impacts on their quality of life. Most common among these housing problems for Lakewood residents is housing affordability, as measured through cost burdens. Renters, in particular, face more acute cost burdens than do homeowners.

For renters, of the 3,650 severely cost burdened households (paying more than half of their income on rent), 68% earn less than 30% AMI and 26% earn between 31% and 50% AMI. For the 6,824 renter households that are cost burdened (paying between 31% and 50% of their income on rent), 44% earn less than 30% AMI and 53% earn between 31% and 80% AMI. Of the 1,064 homeowners experiencing severe cost burdens, 28% earn 30% AMI or less and 31% earn between 31% and 50% AMI. For homeowners who are cost burdened, 21% earn between 31% and 50% AMI and 23% earn between 51% and 80% AMI.

Although household incomes have seen increases in recent years, Lakewood's median income of \$51,972 (2015-2019 American Community Survey) lags far below that of the county (\$72,113) and the United States (\$62,843). Of additional concern is Lakewood's apparent concentration of families living below the poverty rate- 16.6% compared to 9.4% for the county and 10.5% for the United States (2015-2019 American Community Survey). Even though median incomes have seen consistent improvements, so have housing related expenses, leaving little relief for low income households who are traditionally burdened with housing related expenses exceeding 30 percent of household income.

With the median home price in Lakewood at \$269,200 (2015-19 American Community Survey) and percentage increases continuing into the double digits, the dream of owning a home remains out of reach for many, if not for all low income households. Echoing this problem is the steady increase in the cost of rents year over year. With the median gross monthly rent now standing at \$1,034 (2015-2019 American Community Survey), housing cost is easily outstripping income growth, especially for low and moderate income households. These issues of rising housing costs relative to household incomes become strikingly apparent when looking at the percentages of Lakewood households where monthly housing cost exceeds 30 percent of household income- 36% for households with a mortgage and 55% for households who rent (2015-2019 American Community Survey).

With strong increases in property valuation and land costs, housing development costs have continued to push private investment into market rate housing and away from affordable housing. As this trend continues, the lack of investment in and the increased demand for affordable housing is forcing many communities to look for creative ways to develop affordable housing options for low and moderate income households. With conventional financing options diminishing and governmental subsidized funding packages become more competitive, the development horizon for affordable housing options

appears bleak.

Actions it planned to remove or ameliorate the negative effects of public policies that serve as barriers to affordable housing such as land use controls, tax policies affecting land, zoning ordinances, building codes, fees and charges, growth limitations, and policies affecting the return on residential investment

The City of Lakewood encourages the development of affordable housing through the City's Comprehensive Plan. The Plan addresses future housing needs for current and future residents of all incomes. Policies offers density bonuses to developers of affordable housing, encourage infill housing, cottage-style development, changes in zoning to permit higher densities, and incentivizes the construction of affordable housing.

Additional means used to encourage affordable development is though the City's Multifamily Tax Credit program which allows developers to defer taxes on valuation improvements for all housing repairs for up to 12 years withing certain urban use centers. This is especially important in areas where development is stymied by conditions of blight, the lack of infrastructure, or where rents are not conducive to sustaining redevelopment activities. With the tax deferral, the development of new housing or the modernization of existing substandard units can be incentivized as a means of neighborhood revitalization.

Discussion

With the COVID-related economic downturn, many low income families have struggled matching ongoing expenses with reduced household incomes, forcing many to choose between paying the rent and paying for food, utilities, and other essential services. This is especially true of Black, Other/Multiracial, and Latino households. With Black and Latino households suffering disproportionately high rates of job loss during the pandemic, it's not hard to see why these households are more likely to report they are not caught up on rent than are White, non-Latino households.

According to a recent Center for Budget and Policy Priority's analysis of Census Bureau Pulse Survey tables for February 17 – March 1, 2021, 33% of Black households, 22% of Other/Multiracial households, and 20% of Latino households report they are not caught up on rent. These numbers are higher than the percentage reported for all adults at 19%, and significantly higher than reported for White households at 13%. As communities struggle to shake off the effects of the coronavirus pandemic, housing stability and affordability will remain among the most important social issues moving forward.

AP-85 Other Actions - 91.420, 91.220(k)

Introduction

Actions planned to address obstacles to meeting underserved needs

In response to the economic impacts of the coronavirus pandemic, Lakewood is earmarking funding in support of housing and financial counseling services to assist those low and moderate income households who, due to job losses or other adverse economic impacts, have found themselves at risk of foreclosure or homelessness. Additional funding is being allocated in anticipation of strong demand for fair housing education and landlord tenant outreach activities as eviction moratoriums near expiration. With the economic impacts of the coronavirus epidemic unequally weighted against communities of color and those with extremely low incomes, the need for affordable housing and housing stability amongst these populations looms large.

Additional action to assist those economically impacted and at risk of homelessness is being provided through the City's emergency relocation assistance program for individuals at risk of homelessness through no fault of their own due to discriminatory housing practices, or as a result of building and code enforcement closures, fires, or redevelopment activities.

In FY 2021, the City will continue to offer an emergency housing repair program for households that do not qualify for the City's CDBG and HOME-funded programs. The program will utilize grant funds provided by the Nisqually Indian Tribe to make emergency repairs to low income, owner-occupied households who otherwise lack the means or the equity to make the necessary repairs.

Additional actions to address the needs of Lakewood's underserved population are addressed through the ongoing 1% allocation from the City's general fund; strategic priorities include emotional supports and youth programs, housing assistance and homelessness services, crisis stabilization and advocacy, access to health & behavioral health, and access to food. For 2021, the City has allocated \$380,000 in general fund dollars in support of these programs.

Actions planned to foster and maintain affordable housing

Lakewood recognizes the importance affordable housing and homeownership play in building vibrant communities and the direct connection they afford low income households in wealth-building opportunities. In 2021, the City continues its partnership with Tacoma-Pierce County Habitat for Humanity and the Homeownership Center of Tacoma in redeveloping vacant, abandoned and blighted properties into new homeownership opportunities for low income families. Both partners are actively acquiring troubled properties in Lakewood and have multiple active construction sites.

Support for existing affordable housing will be provided through the Major Home Repair/Sewer Loan

Annual Action Plan

2021

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Program and HOME Housing Rehabilitation, while multifamily housing is supported and encouraged with Lakewood's HOME Affordable Housing Fund as well as through zoning density bonuses offered to developers of affordable housing. With homeownership opportunities increasingly difficult as housing development costs continue to escalate, the City will continue to offer down payment assistance to low income homebuyers to help offset the met the minimum cash investment necessary for a down payment.

Efforts to maintain existing owner occupied housing are supported through the Major Home Repair/Sewer Loan Program, HOME Housing Rehabilitation, and Nisqually Repair Funds, while multifamily housing is supported and encouraged with Lakewood's HOME Affordable Housing Fund as well as through zoning density bonuses offered to developers of affordable housing. With homeownership opportunities becoming more difficult as housing process creep higher, the City offers developer subsidies to encourage low income housing development and offers a revolving loan fund to assist low income homebuyers with down payment assistance designed to help met the minimum cash investment necessary to qualify for a conventional home loan.

Additional efforts in support of housing stability have come in the form of approval of an annual 0.0073% sales and use tax (SHB-1406) to be used to provide additional financing for the development and maintenance of affordable housing. As a result, an estimated \$1.95 million dollars is expected to be raised in support of affordable and supportive housing development over the next 20 years.

Rental habitability and affordability remain a priority for the City. With approximately 53 percent of Lakewood's housing stock being rental, and more than 65 percent of Lakewood's housing stock constructed prior to 1960, the importance of maintain this housing stock remains of paramount importance. In 2017, the City began its Rental Housing Safety Program (RHSP) requiring all residential rental properties (apartments, single family homes, duplexes, etc.) within the Lakewood city limit be registered on an annual basis and maintain specific life and safety standards for those properties. Since substandard housing disproportionately affects the poor, working class families, seniors, the disabled, and persons who suffer from chronic illness, it is the aim of the RHSP to eliminate all substandard rental housing in Lakewood and improve the quality of life for all Lakewood residents. Since the program's inception, the City has seen substantial improvements in both the quality and condition of many of the City's substandard rental properties. Inspections of rental properties in the City of Lakewood will be ongoing throughout the term of the Consolidated Plan.

Actions planned to reduce lead-based paint hazards

Consistent with Title X of the Housing and Community Development Act of 1992, Lakewood provides information on lead-safe practices to owners of all properties receiving up to \$5,000 of federal assistance. If work on painted surfaces is involved in properties constructed prior to 1978, the presence of lead is assumed, and safe work practices are followed if testing is not conducted.

In addition to the above, homes with repairs in excess of \$5,000 in federally funded rehabilitation assistance are assessed for risk (completed by a certified Lead Based Paint firm) or are presumed to have lead. If surfaces to be disturbed are determined to contain lead, interim controls are exercised, occupants notified, and clearance testing performed by an EPA-certified firm. Properties constructed prior to 1978 and acquired with federal funds are inspected for hazards and acquired rental properties are inspected periodically.

With approximately 65% of Lakewood's 26,453 housing units being built prior to 1978, there exists the potential for some 17,000 housing units to contain lead-based paint hazards. To inform the community of the hazards of lead-based paint, the City offers copies of the EPA's "Protect Your Family from Lead in Your Home" and HUD's "Renovate Right" pamphlets at City Hall and provides copies of these pamphlets to all housing repair program applicants. As part of the City's single and multifamily housing programs, XRF paint inspections and Risk Assessments are conducted, lead-safe work is conducted by Washington State certified RRP renovation contractors, abatement work is conducted by certified abatement contractors, and Clearance testing of all disturbed surfaces is performed by certified Risk Assessors.

The City conducts lead paint inspections on all pre-1978 properties where persons are relocated to with the Emergency Assistance for Displaced Residents and where homeownership assistance is provided for existing housing. Risk assessments are to be conducted on all pre-1978 homes served by housing repair programs where painted surfaces are to be disturbed as part of the scope of repairs. When completed, all homes will be free of lead-based paint hazards.

Actions planned to reduce the number of poverty-level families

The goals in the Strategic Plan have the capacity to reduce the number of households living in poverty. The goals emphasize stable and affordable housing and services as a means to address poverty and high-quality infrastructure as a way to revitalize communities.

For instance, the goal to address housing instability among residents, including homelessness includes projects which address long-term housing stability through housing rehabilitation programs designed to address housing repairs and renovations vital to maintaining existing affordable housing stock. Housing improvements also provide for accessibility and energy efficiency improvements providing for safer and more economical housing. Funding used to acquire blighted properties and replace them with new homeownership opportunities revitalizes neighborhoods, increases the value of neighboring properties, and creates ownership avenues designed to build household wealth and move families from poverty.

Finally, the importance that stable housing plays in a family's life cannot be understated. Families who have been displaced through code enforcement, fires, and redevelopment activities are often pushed into varying situations of housing instability, including homelessness; especially susceptible are those living on the edge of poverty, those with limited English, the elderly, and persons with disabilities. By

catching these individuals before they become homeless, the Emergency Assistance for Displaced Residents program aims at breaking the cycle of housing instability experienced by those living on the edge of homelessness.

The goal to address the limited supply of diverse rental and homeownership opportunities includes funding for the construction of new, safe and affordable housing units for low income homebuyers. By subsidizing project development costs, the City is able to effectively decrease the share of total income a family commits for housing cost, allowing the household to use those extra funds to pay down short-term debt and pay for other necessities, such as transportation, healthcare, and food, or save for the future.

To address the need for accessible, culturally competent services, programs will focus on the financial impacts the coronavirus pandemic has had on low and moderate income households. Programs will provide housing counseling, foreclosure counseling, and fair housing counseling for those households most impacted by COVID 19. Assistance will provide financial and debt counseling, foreclosure counseling, and fair housing counseling aimed at assisting low income households access financial assistance, restructure debt, to develop payment plans for unpaid mortgage and rent balances, better understand landlord-tenant laws, and to assist households move into homeownership opportunities resulting in long-term wealth building.

Additional effort to reduce the number of poverty-level families include funding through the 1% human services allocation is targeted to programs that provide low income housing and homelessness assistance, crisis stabilization and advocacy, emotional supports & youth programs, access to health and behavioral services, and access to food. For 2021, the City has allocated \$380,000 in General Fund dollars in support of these programs.

Actions planned to develop institutional structure

Lakewood, Tacoma, and Pierce County have a long history of working closely together to coordinate funding activities and ensure funding strategies don't overlap or conflict. This is especially true today, as all three jurisdictions have been grappling with the economic and social impacts of the coronavirus pandemic. Regional coordination on anti-poverty strategies, affordable housing strategies, and homelessness initiatives have been pushed to the mainstream as jurisdictions seek to affect change on a region-wide basis. These initiatives aim to increase the supply of affordable housing (both rental and homeownership), lower the overall cost of housing for residents, create access to financial assistance and services, increase the earning potential of low income households, reduce or eliminate the time a family is homeless, and creates pathways to build wealth and assets.

Both Tacoma and Lakewood are represented on the Tacoma/Pierce County Affordable Housing Consortium to work on issues of affordable housing, including state-level policies and programs to

increase resources and opportunities to address local housing needs. Tacoma and Lakewood participate in a multicounty planning system (Puget Sound Regional Council) that looks at regional growth and economic development, as well as equal access to opportunities.

On a local level, Lakewood continues to coordinate federal funding opportunities and general fund expenditures through the Community Services Advisory Board (CSAB). The CSAB is tasked with policymaking and general funding recommendation authority as they relate to community development and human services activities city-wide. Board responsibilities include facilitating the cooperation and coordination of human services and Consolidated Plan activities, holding public hearings to receive input on community development and human service's needs, developing policy guidance and program evaluation criteria, and making funding recommendations.

With regard to the Tacoma-Lakewood HOME consortium and the allocation of HOME funds, the City of Tacoma has established the Tacoma Community Redevelopment Authority (TCRA), a public corporation organized and dedicated to the administration of federal grants and programs such as the Community Development Block Grant and HOME Investment Partnerships Program. The TCRA is responsible for administering the HOME Program, including the review of all housing programs for both Tacoma and Lakewood. The Board is staffed by ten Board members and is supported by City of Tacoma staff.

Actions planned to enhance coordination between public and private housing and social service agencies

The City is the convener of monthly human services Collaboration meetings. Collaboration partners include for-profit and nonprofit providers of housing, services, homeless programs, dv and family services, youth programs, food banks, and healthcare services.

City of Lakewood staff routinely coordinate with City of Tacoma, as part of the HOME Consortium, and participate in regional efforts coordinating planning efforts and service delivery. The Cities participate in the Tacoma/Lakewood/Pierce County Continuum of Care and are active in the Tacoma Pierce County Affordable Housing Consortium, the Economic Development Board for Tacoma-Pierce County, the Pierce County Human Services Coalition and other public entities and associations that set priorities for use of resources in the region, set goals, and measure progress in meeting those goals. Lakewood staff participate in monthly meetings with service providers and coordinate on the development of plans and strategies. Coordination with public and assisted housing providers along with governmental agencies for health, mental health, and other services focus on economic development, transportation, public services, special needs, homelessness, and housing. As the need for affordable housing and services continues to increase, the Cities of Tacoma and Lakewood, Pierce County, and the Puget Sound Regional Council continue to collaborate on long-term priorities to leverage limited funding to meet the needs of the community.

Through the City's annual 1% human services allocation, relationships have been maintained with providers of services such as, Catholic Community Services, Tacoma Rescue Mission, Emergency Food Network, Community Health Care, Greater Lakes Mental Healthcare, YWCA & YMCA, Boys & Girls Club of South Puget Sound, Rebuilding Together South Sound, Asian Pacific Cultural Center, and other services providers offering programs to low and moderate income households throughout the County.

Collaboration continues with both the Pierce County Housing Authority and the Tacoma Housing Authority on the expansion of safe, decent, affordable housing options for low income citizens.

Additional effort to reduce the number of poverty-level families include funding through the 1% human services allocation is targeted to programs that provide low income housing and homelessness assistance, crisis stabilization and advocacy, emotional supports & youth programs, access to health and behavioral services, and access to food. For 2021, the City has allocated \$380,000 in General Fund dollars in support of these programs.

Discussion

Program Specific Requirements

AP-90 Program Specific Requirements - 91.420, 91.220(l)(1,2,4)

Introduction

Community Development Block Grant Program (CDBG)

Reference 24 CFR 91.220(l)(1)

Projects planned with all CDBG funds expected to be available during the year are identified in the Projects Table. The following identifies program income that is available for use that is included in projects to be carried out.

1. The total amount of program income that will have been received before the start of the next program year and that has not yet been reprogrammed	0
2. The amount of proceeds from section 108 loan guarantees that will be used during the year to address the priority needs and specific objectives identified in the grantee's strategic plan.	0
3. The amount of surplus funds from urban renewal settlements	0
4. The amount of any grant funds returned to the line of credit for which the planned use has not been included in a prior statement or plan	0
5. The amount of income from float-funded activities	0
Total Program Income:	0

Other CDBG Requirements


1. The amount of urgent need activities	0
2. The estimated percentage of CDBG funds that will be used for activities that benefit persons of low and moderate income. Overall Benefit - A consecutive period of one, two or three years may be used to determine that a minimum overall benefit of 70% of CDBG funds is used to benefit persons of low and moderate income. Specify the years covered that include this Annual Action Plan.	100.00%

Discussion



TO: Mayor and City Council

FROM: Mary Dodsworth, Parks, Recreation and Community Services Director

THROUGH: John Caulfield, City Manager 

DATE: May 3, 2021

SUBJECT: Chambers Creek Canyon Trail Update

ATTACHMENTS: Chambers Creek Canyon Trail Master Plan
Pierce County Bid Tab – phase one construction
[Link to Master Plan document](#)
[Link to Pierce County, UP, City ILA](#)

BACKGROUND: The development of trails in and around the Pierce County owned Chambers Creek Canyon properties was included in the 1997 Chambers Creek Properties Master Site Plan. In 2012 Pierce County and the cities of Lakewood and University Place began meeting to discuss how we could work together to improve access in this area. In 2015 the City signed an interlocal agreement (ILA) for cooperative planning of the Chambers Creek Canyon Trail. A design and phased development plan was completed and approved in 2017. The goal of this plan was to create a 2.5 mile trail from one end of the canyon to the Chambers Creek Properties and ultimately the Puget Sound. The ILA was updated in 2019 to continue the necessary planning work to design, permit, construct and maintain the canyon trail. Throughout this period we have cohosted open houses, planning sessions, community presentations and work parties and supported a variety of grant applications. The Chambers Creek Canyon trail project is included in the City's Legacy Plan and the six year parks Capital Improvement Plan. The City has appropriated \$300,000 in support of Phase 1 and Phase 2 of this project in the 2021-22 parks CIP for this project.

CURRENT STATUS: Pierce County and the cities of Lakewood and University Place continue to work on development of this trail project. Several grants have been received to offset costs. Each agency is supporting the project and involved in all decision making. UP is taking the lead on design and permitting, Lakewood is supporting the planning and community events and Pierce County is leading grant management, project coordination and construction. All agencies have agreed to contribute to the development and future maintenance of the trail.

The funding agencies have allowed Pierce County to combine grants and trail phases for planning, permitting and construction efficiencies. Phase one is designed and permitted.

This phase will connect Lakewood at Phillips Road to University Place at Kobayashi Park through the east end of the canyon. Below is a map of the proposed trial project with phase one circled.



Pierce County managed the bid process for Phase One and closed the bids on April 14, 2021. Seven bids were received ranging from \$437,028 to \$687,205. The lowest responsive bid (\$437,028) was accepted and the County is moving forward with contracting for the project. The element that will drive the construction schedule is the pre-fabricated span bridge. Construction is anticipated to begin in late July and should be done by the end of the year.

PHASE ONE PROJECT COSTS: The chart below shows the phase one project costs and the anticipated revenue including the City's contribution to this portion of the project. This project is supported by a State RCO grant of \$150,000 which will offset the total project cost. The updated costs include design, permitting, updating the trails, construction and installation of bridge #1, a 10% contingency, sales tax, and construction support. The City's portion is anticipated to be \$179,541 (one third of total project cost). Each jurisdiction has spent \$41,483 to date leaving \$137,968 remaining responsibility for PC, Lakewood, and UP.

Phase One

Revenue Sources	Amount
City of Lakewood	\$179,451
University Place	\$179,451
Pierce County	\$179,451
RCO Grant	\$150,000
Total	\$688,353

Task	Expenditures
A/E and permitting	\$142,530
Construction	\$437,028
10% contingency	\$ 43,703
Construction mgt	\$ 17,500
Sales Tax	\$ 47,592
Total	\$688,353

PHASE TWO UPDATE: The Cities and County are working on Phase One and Two concurrently. Due to grant funds, the costs for each phase is being tracked and accounted for separately. The Phase Two portion of the trail has been difficult to design. Staff and consultants have hiked the canyon on the north and south sides during various seasons to determine the best route. The original master plan has the trail on both the north and south side of the creek. When more detailed design was completed and an engineer's cost estimate was created, the project cost was \$1.5+ million over initial Phase Two cost estimates. The increased costs are associated with construction specifications necessary to create board walks, additional bridges and sustainable infrastructure in areas with significant wetlands, environmental impacts near creek and canyon topography (steep slopes, wet hillsides, unstable areas and slide areas).

Due to these challenges, the project team pursued a phase 2 option that focused on the south (Lakewood) side of the canyon. The final portion of the phase two trail is on the southern side and most of the trail is already passable. The hope was to find more stable land to create a trail from one end of the canyon to the other on the south side and reduce costs by eliminating one bridge. Several attempts were made to cross on this side of the canyon. When it was determined we were entering private property, we reached out to various property owners to receive right of access to continue the investigation. This was granted. During this time several homeless encampments were found deep in the canyon and there was no way to get to them to clean them up. Most of the camps were bordering on private and public (County) property and campers were using fuel for fires and that created fire and safety concerns. The idea of creating a trail that would allow access for positive use of the area and provide county staff the ability to monitor and clean up the areas was an incentive for property owners. Unfortunately, after attempting to find a southern route, we discovered the same challenging issues (unstable and steep slopes and active slides) occurred on this side and the only land really accessible for a trail was high up on the southern hillside, within view of the private property owners. It was determined that we would need to have access and easements from too many property owners to enable a trail to cross through this area and anything up close to the homes was not agreeable to most of the residents.

So we are back to finalizing a northern route for the beginning of phase two. The idea is to create the trail design and then determine if funds can be available now or in the future. If matching funds are not available, the County may have to withdraw the grant applications. They are already on grant extensions to try to solve the problems and find solutions.

Below is a summary of estimated phase two project costs and projected revenue sources

Phase Two Estimates

Revenue Sources	Amount
City of Lakewood	\$ 657,656
University Place	\$ 657,656
Pierce County	\$ 657,656
RCO Grant	\$ 709,000
Total	\$2,681,969

Task	Cost Estimate
A/E and permitting	\$ 236,969
Construction	\$2,445,000
Total	\$2,681,969

*to date we've spent \$ \$24,819 in phase 2 design fees.

RECOMMENDATION: Staff is requesting to put the following recommendation on the May 17 Council consent agenda. Council will be asked to authorize the City Manager to approve funded expenditures related to Phase One development and Phase Two planning in an amount not to exceed \$300,000.

I hereby certify this to be a true copy of the
tabulation of the bids as received and opened.

BID OPENING:
WEDNESDAY, April 14, 2021


Brian D. Stacy, P.E.
County Engineer

				(1)		(2)		(3)		(4)		(5)		(6)		(7)			
				Engineer's Estimate		Boettcher & Sons Inc PO Box 992 Eatonville, WA 98328		CCT Construction Inc 13018 Canyon Road East Puyallup, WA 98373		Nordvind Company LLC 43112 248th Avenue East Enumclaw, WA 98022		Combined Construction Inc 3701 South Road Mukilteo, WA 98275		Quigg Brother's Inc 819 West State Street Aberdeen, WA 98520		Highmark Concrete Contractors LLC 2003 136th Avenue East Sumner, WA 98390		Razz Construction Inc 4055 Hammer Drive Bellingham, WA 98226	
ITEM NO.	QUANTITY	UNIT	ITEM	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT
1	LUMP SUM		Mobilization	29,300.00	29,300.00	40,000.00	40,000.00	45,000.00	45,000.00	50,000.00	50,000.00	60,000.00	60,000.00	60,000.00	60,000.00	68,000.00	68,000.00	68,000.00	68,000.00
2	LUMP SUM		Clearing and Grubbing	5,000.00	5,000.00	6,500.00	6,500.00	12,000.00	12,000.00	25,000.00	25,000.00	10,000.00	10,000.00	20,000.00	20,000.00	11,750.00	11,750.00	40,000.00	40,000.00
3	505 CY		Excavation, Embankment and Grading Incl Haul	60.00	30,300.00	28.00	14,140.00	50.00	25,250.00	75.00	37,875.00	130.00	65,650.00	50.00	25,250.00	82.25	41,536.25	80.00	40,400.00
4	195 TON		Gravel Borrow Incl Haul	55.00	10,725.00	38.00	7,410.00	60.00	11,700.00	75.00	14,625.00	130.00	25,350.00	70.00	13,650.00	51.00	9,945.00	90.00	17,550.00
5	LUMP SUM		Prefabricated Steel Truss Bridge No PRT27202-A	220,000.00	220,000.00	275,140.00	275,140.00	270,000.00	270,000.00	191,500.00	191,500.00	310,870.00	310,870.00	380,000.00	380,000.00	315,750.00	315,750.00	326,000.00	326,000.00
6	10 CY		Unsuitable Foundation Excavation Incl Haul	50.00	500.00	40.00	400.00	10.00	100.00	100.00	1,000.00	150.00	1,500.00	70.00	700.00	274.00	2,740.00	90.00	900.00
7	130 SF		Modular Block Wall	50.00	6,500.00	42.00	5,460.00	45.00	5,850.00	100.00	13,000.00	80.00	10,400.00	70.00	9,100.00	170.00	22,100.00	80.00	10,400.00
8	9 TON		Gravel Backfill for Walls	45.00	405.00	130.00	1,170.00	80.00	720.00	100.00	900.00	150.00	1,350.00	100.00	900.00	77.50	697.50	80.00	720.00
9	45 TON		Crushed Surfacing Top Course	50.00	2,250.00	42.00	1,890.00	90.00	4,050.00	75.00	3,375.00	100.00	4,500.00	100.00	4,500.00	89.00	4,005.00	70.00	3,150.00
10	418 TON		Crushed Surfacing Base Course	50.00	20,900.00	42.00	17,556.00	75.00	31,350.00	75.00	31,350.00	80.00	33,440.00	70.00	29,260.00	100.00	41,800.00	70.00	29,260.00
11	12 TON		Commercial HMA	300.00	3,600.00	190.00	2,280.00	800.00	9,600.00	350.00	4,200.00	600.00	7,200.00	500.00	6,000.00	450.00	5,400.00	600.00	7,200.00
12	LUMP SUM		Erosion Control and Water Pollution Prevention	12,500.00	12,500.00	4,300.00	4,300.00	2,800.00	2,800.00	25,000.00	25,000.00	5,000.00	5,000.00	10,000.00	10,000.00	26,500.00	26,500.00	20,000.00	20,000.00
13	230 TON		Rock for Erosion and Scour Protection	55.00	12,650.00	72.50	16,675.00	120.00	27,600.00	150.00	34,500.00	90.00	20,700.00	80.00	18,400.00	82.50	18,975.00	150.00	34,500.00
14	85 CY		Topsoil Type A	45.00	3,825.00	40.00	3,400.00	90.00	7,650.00	75.00	6,375.00	70.00	5,950.00	120.00	10,200.00	51.50	4,377.50	55.00	4,675.00
15	110 CY		Wood Chip Mulch	35.00	3,850.00	50.00	5,500.00	110.00	12,100.00	75.00	8,250.00	70.00	7,700.00	125.00	13,750.00	58.00	6,380.00	35.00	3,850.00
16	LUMP SUM		Project Temporary Traffic Control	4,000.00	4,000.00	4,200.00	4,200.00	4,000.00	4,000.00	25,000.00	25,000.00	5,000.00	5,000.00	20,000.00	20,000.00	11,000.00	11,000.00	35,000.00	35,000.00
17	LUMP SUM		Locate Existing Utilities	500.00	500.00	800.00	800.00	200.00	200.00	15,000.00	15,000.00	2,000.00	2,000.00	2,000.00	2,000.00	4,250.00	4,250.00	1,500.00	1,500.00
18	LUMP SUM		Structure Surveying	1,000.00	1,000.00	4,575.00	4,575.00	4,000.00	4,000.00	15,000.00	15,000.00	5,000.00	5,000.00	5,000.00	5,000.00	2,600.00	2,600.00	2,200.00	2,200.00
19	LUMP SUM		Roadway Surveying	1,000.00	1,000.00	5,250.00	5,250.00	1,500.00	1,500.00	15,000.00	15,000.00	5,000.00	5,000.00	5,000.00	5,000.00	7,750.00	7,750.00	7,000.00	7,000.00
20	LUMP SUM		Trimming and Cleanup	2,500.00	2,500.00	2,800.00	2,800.00	3,500.00	3,500.00	25,000.00	25,000.00	3,500.00	3,500.00	3,000.00	3,000.00	30,000.00	30,000.00	4,500.00	4,500.00
21	144 LF		Pedestrian Railing	80.00	11,520.00	70.00	10,080.00	90.00	12,960.00	10.00	1,440.00	150.00	21,600.00	200.00	28,800.00	209.50	30,168.00	150.00	21,600.00
22	14 LF		Pedestrian Guardrail	120.00	1,680.00	93.00	1,302.00	150.00	2,100.00	10.00	140.00	300.00	4,200.00	300.00	4,200.00	403.50	5,649.00	250.00	3,500.00
23	LUMP SUM		Project Documentation	1,500.00	1,500.00	1,200.00	1,200.00	800.00	800.00	2,500.00	2,500.00	1,000.00	1,000.00	3,000.00	3,000.00	4,650.00	4,650.00	300.00	300.00
24	5,000 DOL		Minor Change	1.00	5,000.00	1.00	5,000.00	1.00	5,000.00	1.00	5,000.00	1.00	5,000.00	1.00	5,000.00	1.00	5,000.00	1.00	5,000.00
CALC																			
					391,005.00		437,028.00		499,830.00		551,030.00		621,910.00		677,710.00		681,023.25		687,205.00



TO: Mayor and City Council

FROM: Mary Dodsworth, parks, Recreation & Community Services Director

THROUGH: John Caulfield, City Manager 

DATE: May 3, 2021

SUBJECT: Park Sign Update

ATTACHMENTS: Park Sign Update PowerPoint

Background: Soon after incorporation the City began building new parks and renovating existing park sites that were previously owned and operated by Pierce County. A park sign program was created in 2002 so park guests would know the name of the site and that it was a city managed public space available for use. The original park signs were a monument style; made of concrete and included the original city logo. Concrete was selected to be vandal resistant as well as to enable staff to easily paint or pressure wash off any graffiti. The signs are almost 20 years old and showing their age. Also, the City logo on the sign is no longer current.

Council approved a new sign program in the 2021 Parks CIP. The Park and Recreation Advisory Board reviewed a variety of sign options (colors, lettering, styles) at their February 23, 2021 meeting and is recommending approval of a standardized park sign to be replicated at all park sites. The new signs have City logo features (color, mountain, water) but not a city logo. The signs may vary in size based on the park size and location of the sign area. The park sign areas will be enhanced with landscaping and other beautification elements. We do not anticipate lighting the signs as parks are closed at night. All City park signs will be updated over the next three years as funding is available and park projects are completed.

Current City Sign



Proposed Sign



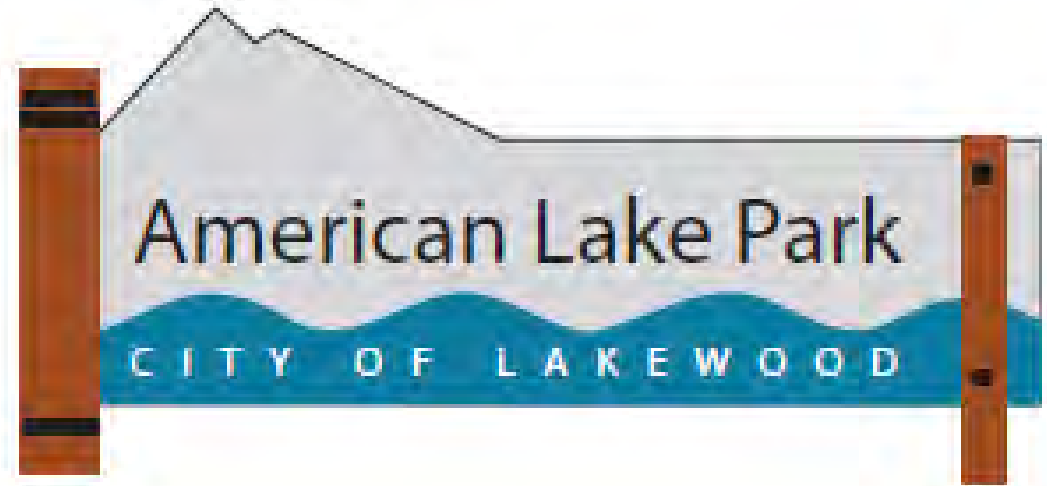
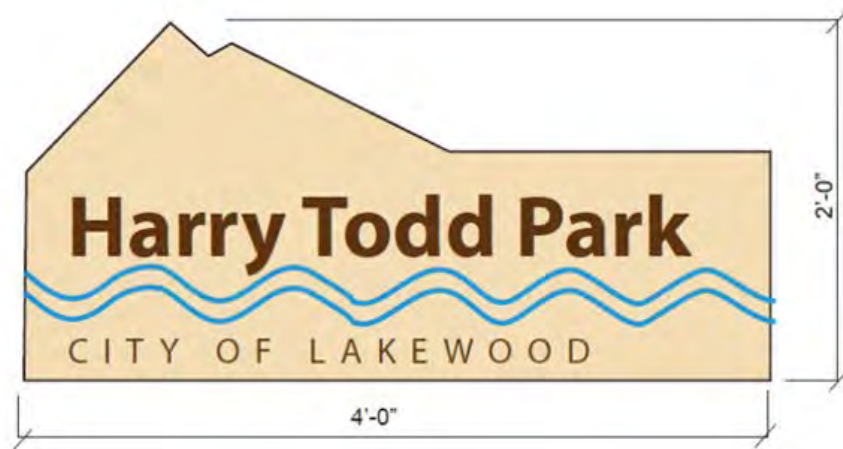
City Park Signs Installed 2002

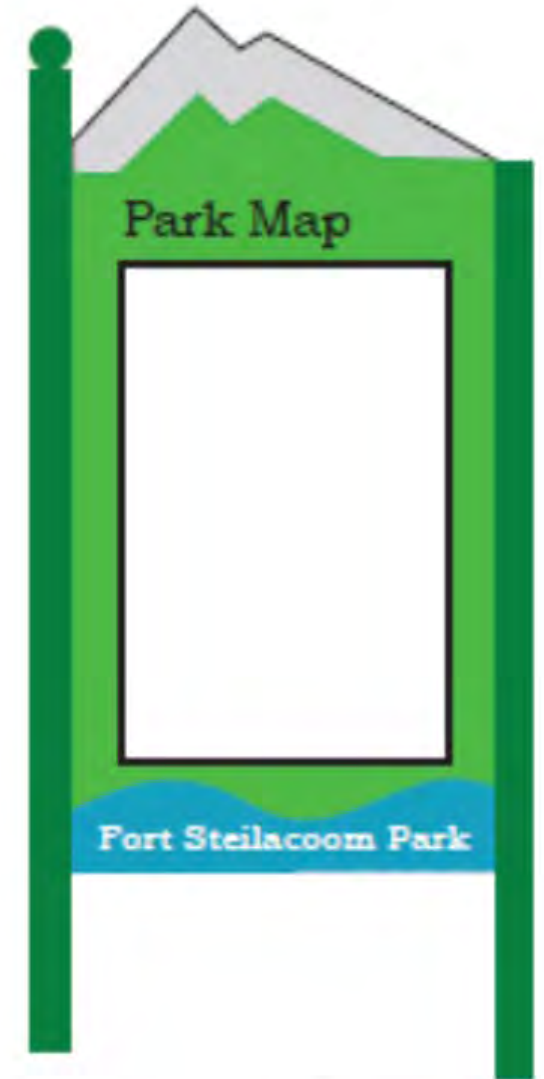
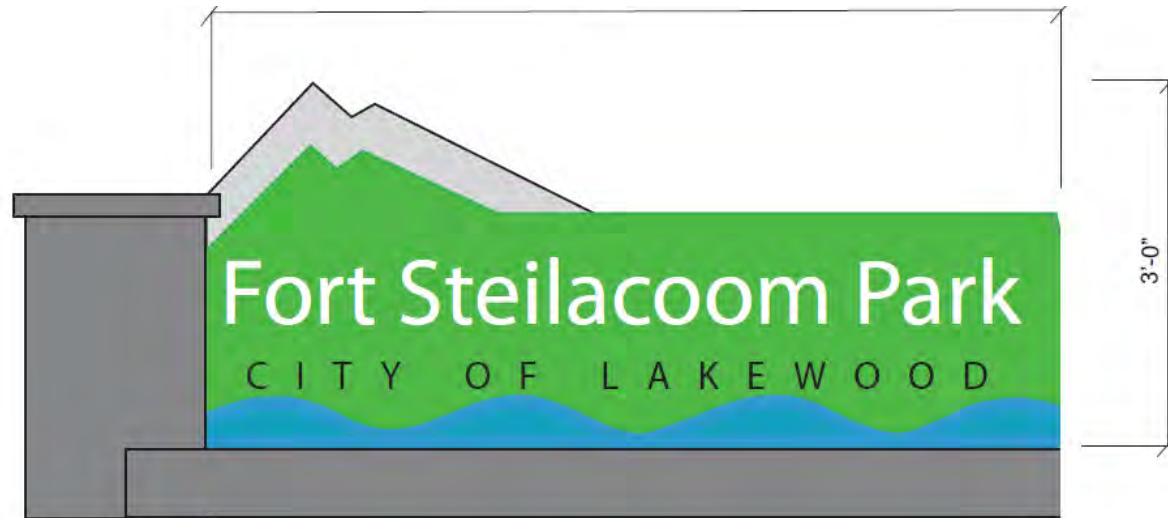
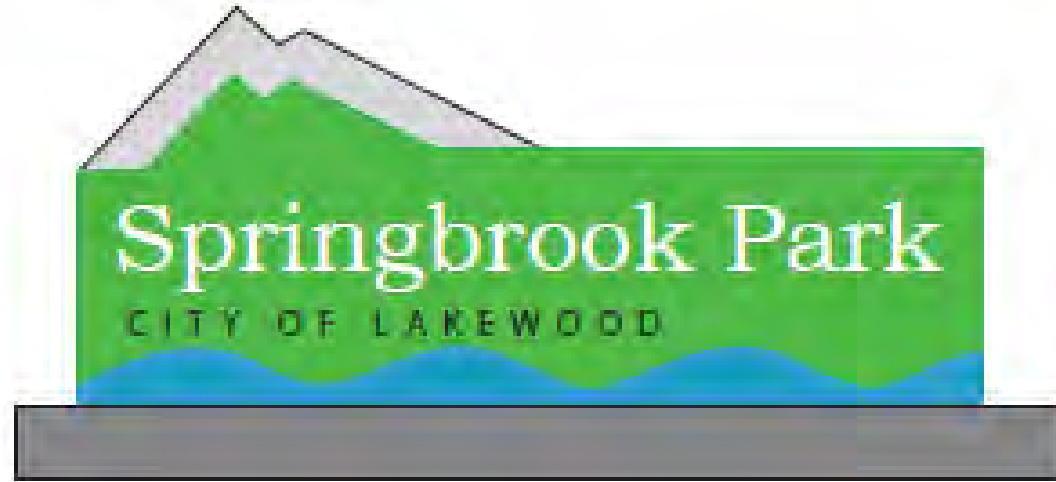


Fort Steilacoom Park Gateway Entry

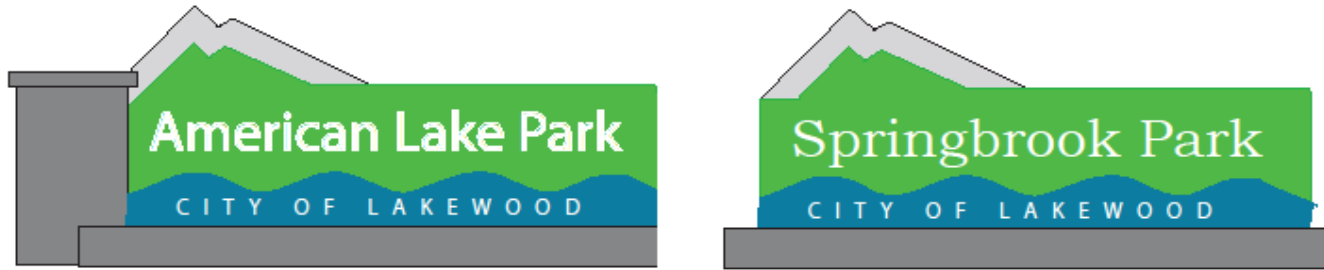


Neighborhood Park Signs Options to Consider

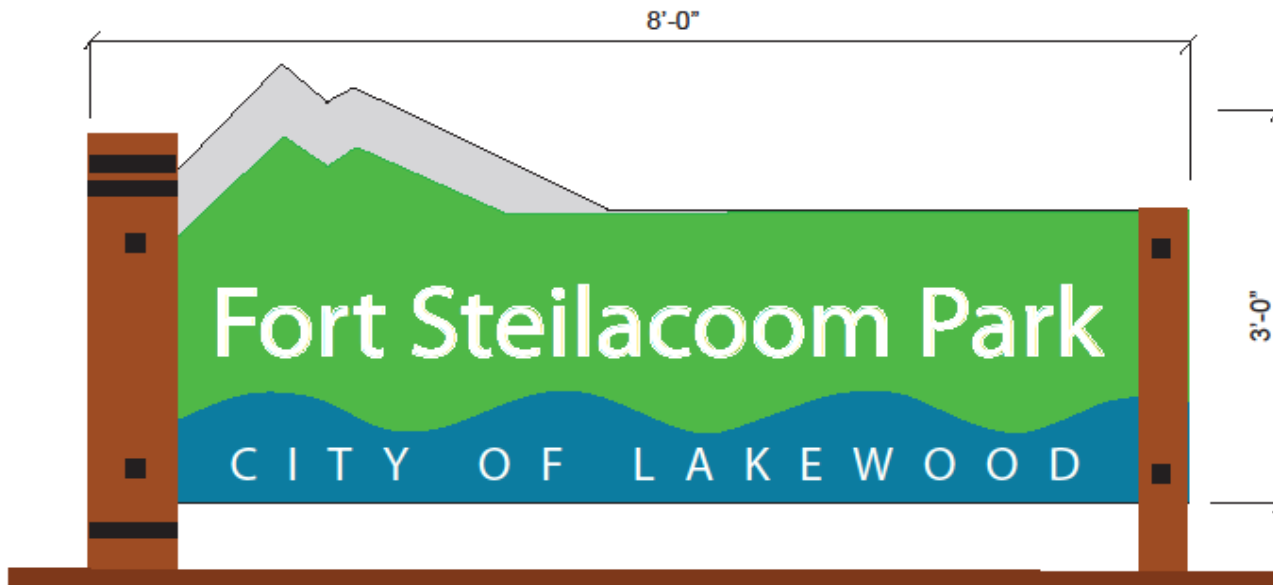




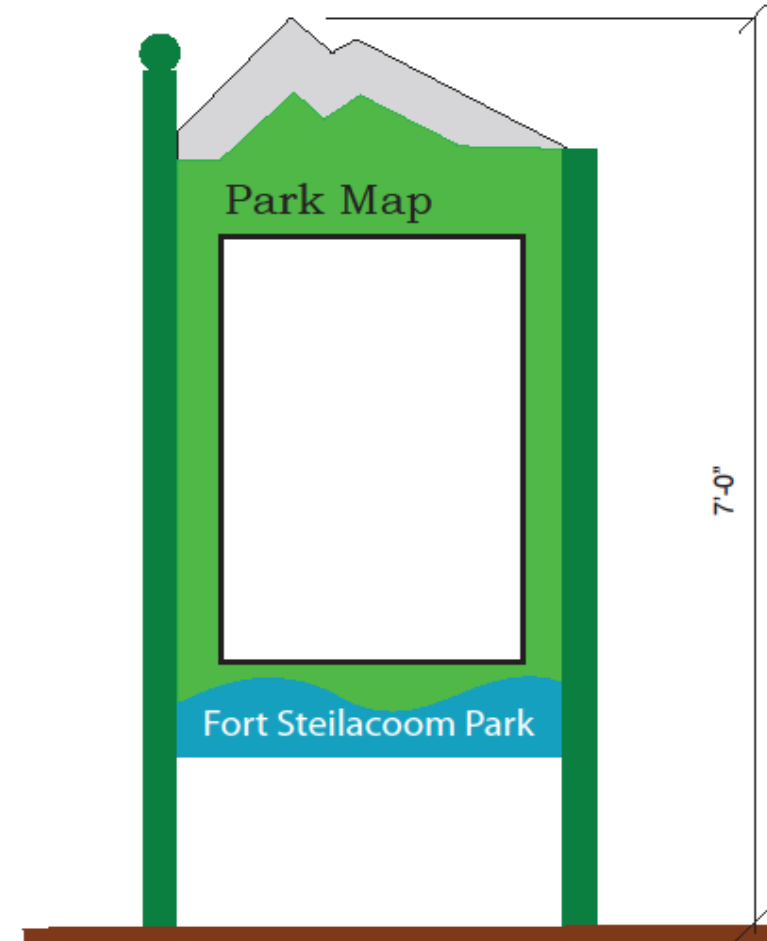
Kiosk Application



Alternate Mounting Options



Park Entry Sign



Kiosk / Rules Sign